

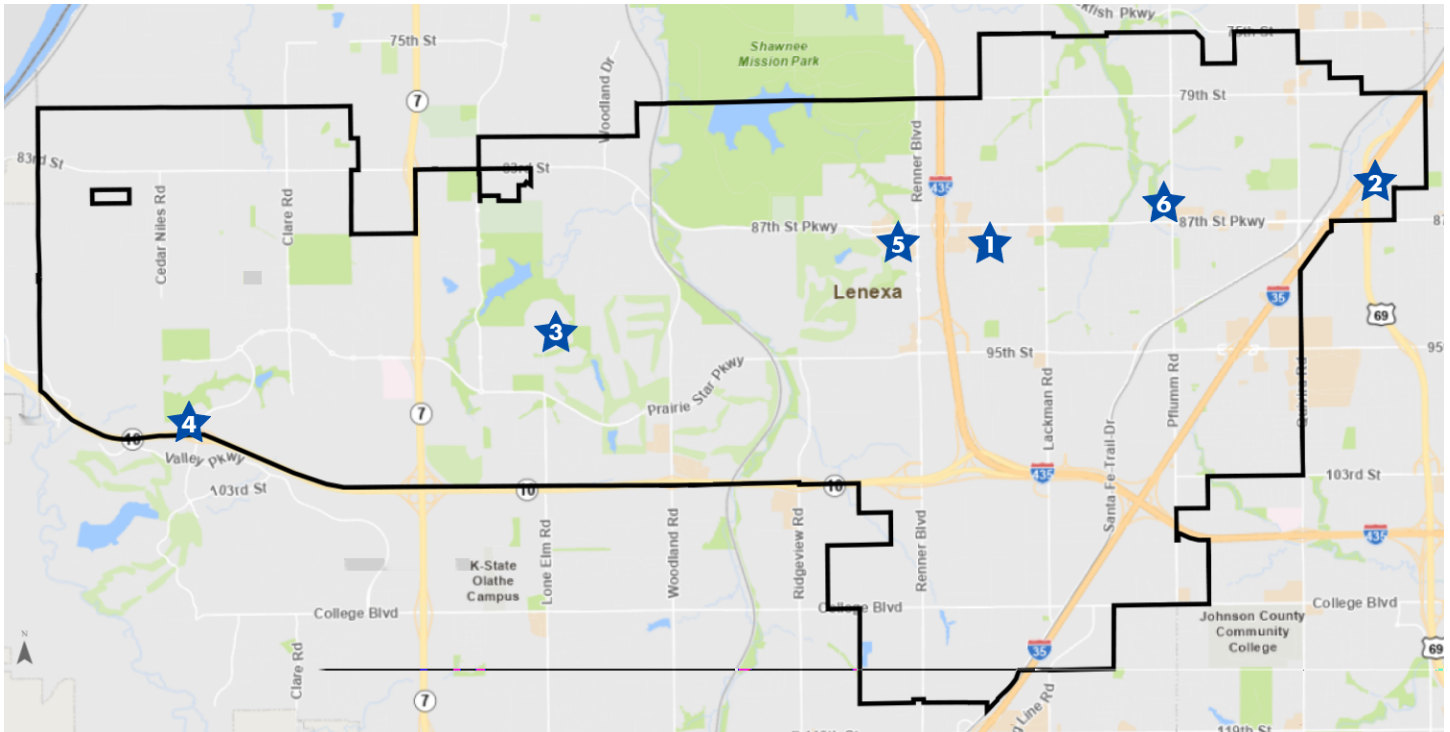
PLANNING COMMISSION AGENDA

FEBRUARY 5, 2024 at 7:00 PM

Community Forum at City Hall
17101 W. 87th Street Parkway
Lenexa, KS 66219



AGENDA MAP



CALL TO ORDER

ROLL CALL

APPROVE MINUTES

CONSENT AGENDA

All matters listed within the consent agenda have been distributed to each member of the Planning Commission for review, are considered to be routine, and will be enacted by one motion with no separate discussion. If a member of the Planning Commission or audience desires separate discussion on an item, that item may be removed from the consent agenda and placed on the regular agenda.

- 1. Burg & Barrel - Consideration of a revised final plan for a new outdoor patio and a sign deviation for a monument sign for property located at 8725 Bourgade Avenue within the CP-1, Planned Neighborhood Commercial District. PL24-02FR**
- 2. Sunflower MOB - Consideration of a revised final plan and a parking deviation on property located at 10950 West 86th Street within the CP-2, Planned Community Commercial District. PL24-01FR**

3. **Timber Rock, Fifth Plat - Consideration of a final plat to replat two single-family residential lots for properties located at 9374 Deer Run Street & 9392 Deer Run Street within the RP-1, Planned Residential (Low Density) District. PT24-03F**

REGULAR AGENDA

4. **Canyon Ridge Apartment Homes - Consideration of a rezoning and preliminary plan/plat for a mixed-use development including multifamily residential, nursing home, and convenience store/gasoline sales uses on property located near the northwest corner of K-10 Highway & Canyon Creek Boulevard. RZ23-07, PL23-12P**
 - a. **Consideration of a rezoning from the AG, Agricultural, CP-O, Planned General Office, and CP-2 Planned Community Commercial Districts to the PUD, Planned Unit Development District. RZ23-07**
 - b. **Consideration of a preliminary plan/plat for a mixed-use development. PL23-12P**

CONTINUED APPLICATIONS (NO DISCUSSION)

5. **City Center Area C - Consideration of a preliminary plan for a mixed-use development on property located approximately at the southwest corner of 87th Street Parkway & Scarborough Street within the CC, Planned City Center District. PL24-02P**
6. **Express Oil & Tire Engineers - Consideration of a final plan for an automotive service center on property located at 8610 Pflumm Road within the CP-2, Planned Community Commercial District. PL24-02F**

STAFF REPORTS

ADJOURN

APPENDIX

7. **Draft Minutes - January 8, 2024 Meeting**

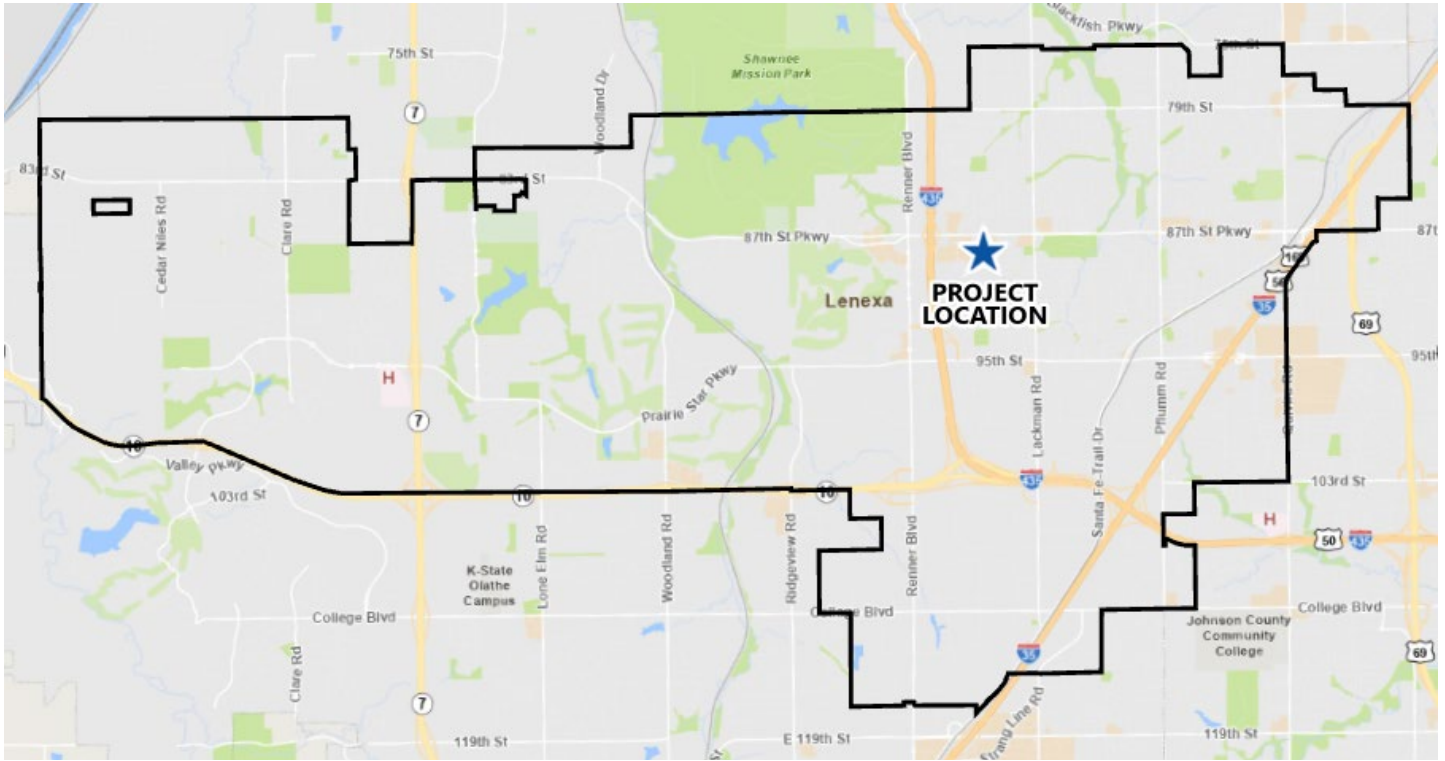
If you have any questions about this agenda, please contact Stephanie Kisler, Planning Manager, at skisler@lenexa.com.

*If you need any accommodations for the meeting, please contact the City ADA Coordinator at 913-477-7550 at least 48 hours prior to the meeting.
Kansas Relay Service: 800-766-3777*

Assistive Listening Devices are available for use in the Community Forum by request.

BURG & BARREL PATIO & SIGN

Project #:	PL24-02FR	Location:	8725 Bourgade Avenue
Applicant:	Mark Murdick	Project Type:	Final Plan
Staff Planner:	Kim Portillo	Proposed Use:	Restaurant



PROJECT SUMMARY

The applicant requests approval of a revised final plan for the addition of a 541 SF exterior patio area for an existing restaurant known as Burg & Barrel. The final plan also includes a request for a sign deviation related to a monument sign. Staff supports the proposed deviation. The proposed final plan, except for the new patio area, is consistent with the previously approved final plan (PL95-18F), which was approved by the Planning Commission on January 2, 1996; however, the applicant must replace some landscaping that was previously removed. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

- The site has housed various restaurants since initial approval of the site development for a restaurant.
- The site was platted under final plat P9-93 for Loiret Office Park, a replat of Estate Offices of Loiret, a condominium subdivision. The final plat was approved by the Planning Commission on November 4, 1993.
- The site was rezoned from CP-O, Planned General Office Zoning District, to CP-1, Planned Neighborhood Commercial Zoning District under RZ95-09, for Pizza Maker, a restaurant use. The rezoning was approved by the Governing Body on December 21, 1995.
- A concurrent preliminary plan, PL95-18P, related to PL95-18F (Pizza Maker) was approved by the Governing Body on December 21, 1995. A 36’ strip of land that was purchased from Country Kids Daycare was included in the rezoning and preliminary plan request for Pizza Maker at 8725 Bourgade Avenue.
- A final plan for Pizza Maker, PL95-18F, was approved by the Planning Commission on January 2, 1996.
- A building permit for Burg & Barrel, B23-0956, for interior renovations, was approved in July 2023. The plans included site work and fire sprinklers for the patio space. The owner was permitted to proceed with the work since the contractor was mobilized onsite; however, Staff informed the applicant that the patio space could not be utilized until approval of a final plan. The patio space must be returned to green space if this final plan is not approved. A temporary certificate of occupancy has been issued for the building while a final certificate of occupancy is contingent on inspections and approval of the final plan.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
0.96	3,575 541 (Patio)	CP-1	Neighborhood Commercial Center



Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The business is Burg & Barrel, a dine-in restaurant that now proposes use of an outdoor patio. The site has hosted a variety of different restaurants since the initial final development plan for a pizza restaurant. Burg & Barrel’s hours are 11 AM to 10 PM, seven days a week.

The use of a restaurant is allowed by-right within the CP-1, Planned Neighborhood Commercial District and is aligned with the future land use classification of Neighborhood Commercial Center. Surrounding uses include office and retail. Restaurant uses are common along nearby W. 87th Street Parkway.

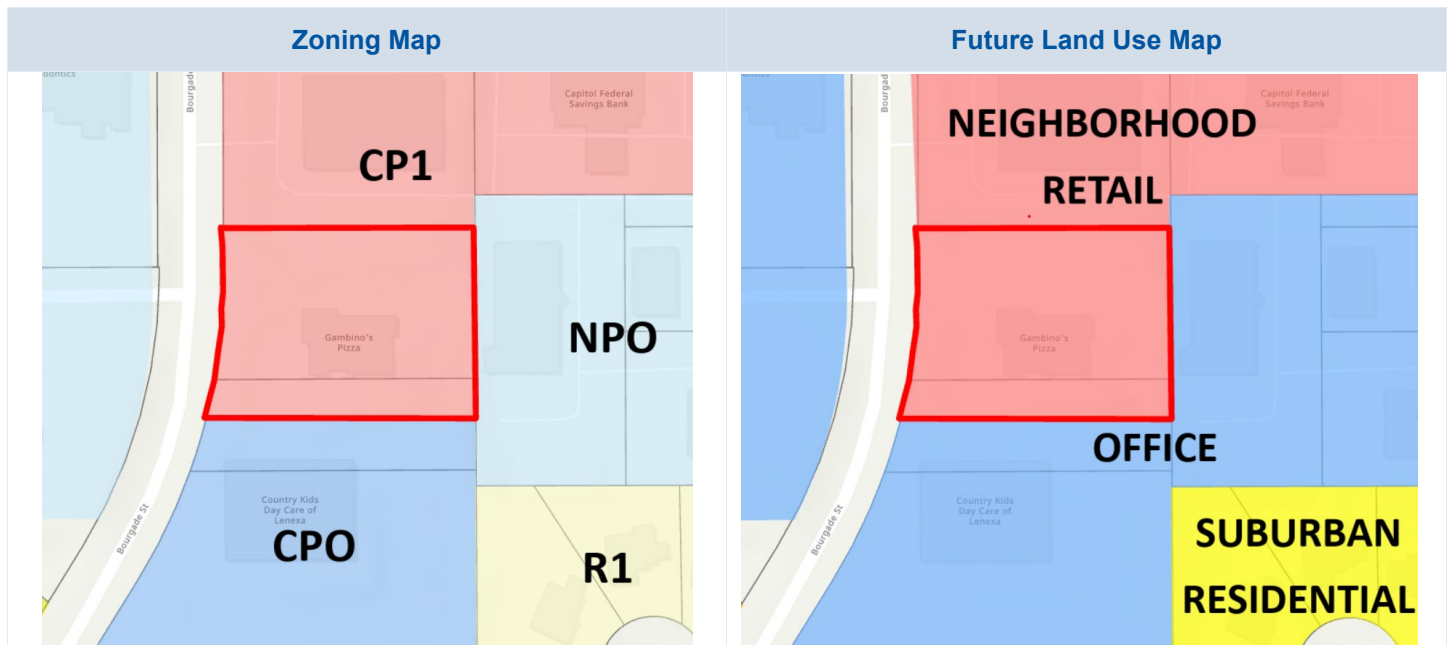


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Neighborhood Commercial Center	CP-1, Planned Neighborhood Commercial District	Restaurant
North	Neighborhood Commercial Center	CP-1, Planned Neighborhood Commercial District	Retail
South	Office/ Employment Center	CP-O, Planned General Office District	Daycare
East	Office/ Employment Center	NP-O, Planned Neighborhood Office District	Retail
West	Office/ Employment Center	NP-O, Planned Neighborhood Office District	Offices

FINAL PLAN REVIEW

This final plan is for the addition of a 541 SF outdoor patio seating area in what was previously a green space area at the base of the building with a sidewalk. A five-foot wide sidewalk will remain around the patio for accessibility purposes. The overall site layout is not proposed to change and has one drive entrance at the northwest corner of the site onto Bourgade Avenue, a centrally located building with parking on the north, east, and west sides of the building, a trash enclosure at the southeast corner, and green space to the south. The final plan also includes a request for one deviation, which is related to a monument sign. The deviation request is discussed in further detail in the *Deviations* section of the staff report.

As previously noted, the building permit issued in July 2023 allowed the patio area to be constructed but was to remain unoccupied until a final plan was approved. The applicant now seeks approval of the final plan that would allow the restaurant to fully utilize the patio space for outdoor dining. If the final plan is not approved, the area must be converted back into green space.



Exhibit 2: Site Layout

DIMENSIONAL STANDARDS

The patio area is located along the north side of the building and meets all setback requirements for the zoning district.

PUBLIC IMPROVEMENTS

No public improvements are proposed with this project.

ACCESS, TRAFFIC, AND PARKING

The site has one access point on to Bourgade Avenue, approximately 270 feet south of W. 87th Street Parkway. Bourgade Avenue is a local street that serves surrounding commercial, and office uses as well as residential neighborhoods to the south. There are sidewalks on both sides of Bourgade Avenue.

Parking for the restaurant is provided within a private parking lot. The existing number of parking spaces meets the code requirement for the indoor restaurant area plus the new patio area.

TABLE 2: PARKING ANALYSIS				
Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Restaurant (Existing Area) 3575 SF	1 per 75 SF	48	-	-
Restaurant (New Patio) 541 SF	1 per 75 SF	7	-	-
Total	1 per 75 SF	55	58	+3

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

LIGHTING

The applicant does not propose changes to site lighting at this time.

LANDSCAPING

A monument sign requires a landscape area equal to three times the area of the face of the sign and monument base combined. The landscape plan provides a landscape area for the sign that meets code requirements.

Staff also noted that sometime between 2022 and 2023 aerial images show perimeter landscaping along Bourgade Avenue was removed. The landscaped area included required perimeter plantings including a mix of young and mature shrubs. Perimeter plantings are required per [Section 4-1-D-2-L](#), Table 1 of the Unified Development Code and are also shown on the originally approved site plan for a restaurant use. The missing landscaping is shown on the landscape plan to be replaced as part of this approval, as seen in Exhibit 2.

The area where the patio is located was formerly a green space and landscape area at the base of the building. To accommodate the removal of this landscape area, the applicant has agreed to provide planters on the patio.

ARCHITECTURE

The patio area is open-air on a concrete slab with a three-foot-tall black metal fence. The patio opens into the restaurant through an existing door.

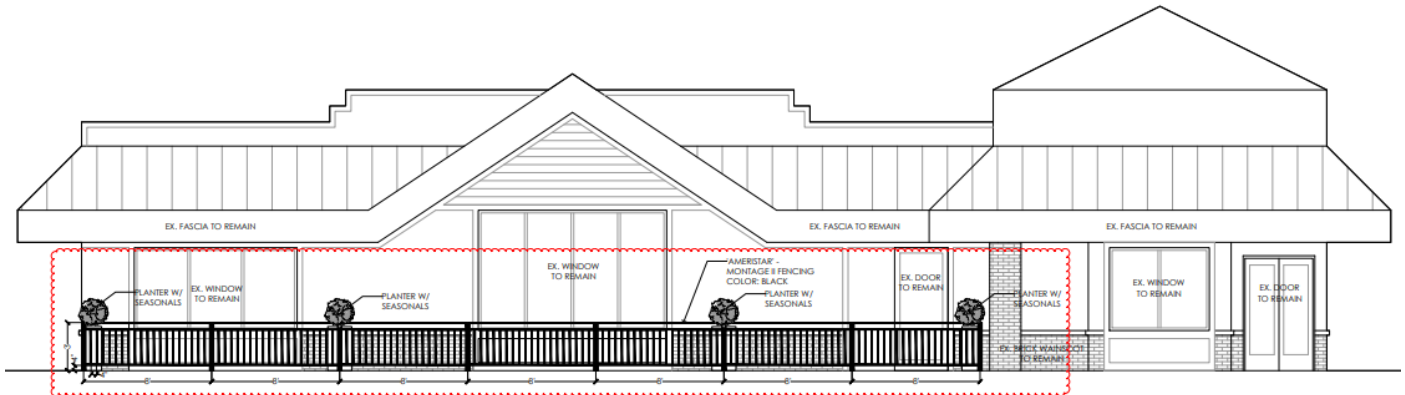


Exhibit 4: Elevation Plan



Exhibit 5: Outdoor Patio

SIGNAGE

The proposed monument sign, located 10 feet from the right-of-way of Bourgade Avenue, meets the required setback for a monument sign. The location is also outside of a utility easement that runs along Bourgade Avenue.

The sign dimensions, which are outlined in Table 2, meet the dimensional standards for signs in the Unified Development Code.

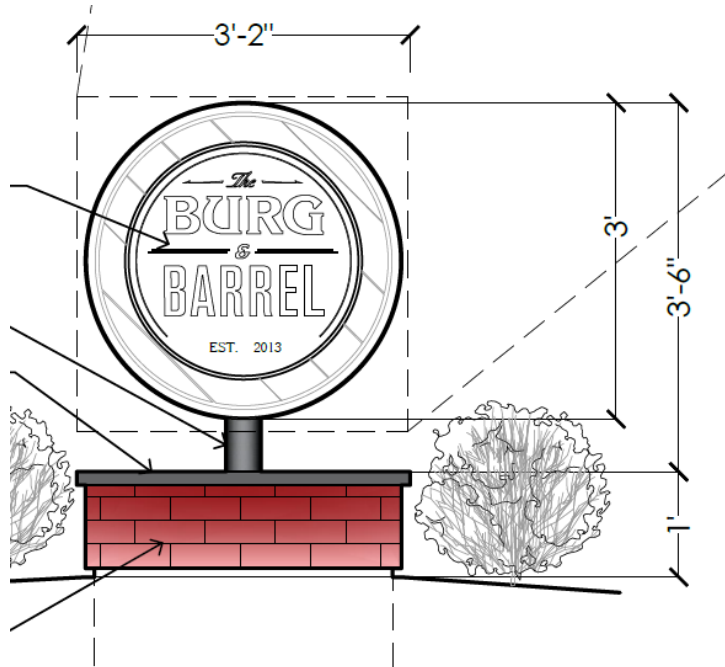


Exhibit 3: Sign Dimensions

TABLE 3: SIGN DIMENSIONS	
Aspect	Measurement
Sign Height	4 ft 6 in
Side Width	3 ft 2 in
Area (sign face)	9.5 SF
Area (including base)	14.25 SF

The proposed monument sign will be internally illuminated and features an acrylic back-lit sign face on a thin-brick CMU block base. The color of the monument base will match the building (see Exhibit 6).



Exhibit 6: Sign Face Color Detail

DEVIATIONS

The applicant requests a deviation to allow a monument sign. A monument sign is not allowed per [Section 4-1-E-D-2](#) of the Unified Development Code, which states that a minimum of 200 feet of street frontage shall be required for the use of a monument sign by individual commercial buildings.

TABLE 4: DEVIATION ANALYSIS		
UDC Minimum	Existing Condition	Difference
200 ft of street frontage	182 ft	18 ft

The following criteria are considered when reviewing a request for a sign deviation:

1. Purpose and intent of the code.

The purposes of the sign code are outlined in [Section 4-1-E-2](#) of the Unified Development Code and can be summarized as aiming to prevent adverse community appearance, ensure public safety, regulate the size and location of signs, and protect property values by enhancing harmony between different zoning districts while also recognizing the business community’s needs for identification and advertisement and acknowledging the differing design concerns and needs for signage in certain specialized areas. Many of the concerns, such as safety, visual clutter, and impact on adjacent properties are discussed in greater detail below. Overall, the size, materials and design of the proposed sign would be harmonious with the developed surroundings. Allowing the sign aligns with the purpose of the code to recognize unique situations in specialized areas where additional signage may be beneficial without causing a negative impact on the community.

2. Impacts on adjacent properties.

Burg & Barrel is located on a street with commercial properties to the north, south, and across the street to the west. All surrounding properties have monument signs. While the surrounding properties are able to meet the 200 ft requirement, it would not be out of character for the streetscape to allow a monument sign at this location. Additionally, the requested sign area (9.5 SF) is less than half of the maximum size that would be allowed, which is 24 SF and would not have a negative impact on adjacent properties.

3. Safety.

The design team for the applicant confirmed the sign will be set back so as not to interfere with the sight distance triangle when turning out of the parking lot onto Bourgade Avenue. The sign does not have any flashing lights or moving features that would create a distraction for drivers on the street. There are no safety concerns with allowing the sign.

4. Visual clutter.

The consideration of visual clutter is one of the basic tenets for the code requirement from which a deviation is now requested. Requiring a minimum of 200 feet of street frontage for a monument sign reduces the feel of cluttering within the neighborhood. To evaluate this point, Staff looked at the surrounding conditions of the site to see if there were other items of clutter. The site is not far off from the required minimum at 182 feet and overall does not have any other items of site design that detract from the open space feel of the street-adjacent area. The sign is of appropriate size and proportionate dimensions and is smaller than the maximum size allowed for other properties with 200 feet of



Exhibit 7: Surrounding Monument Signs

of frontage. The building and parking areas meet setbacks with green space provided along the right-of-way. In addition to the green space area on private property there is also an area of green space and sidewalk approximately 15 feet wide between the property line and outside lane of traffic, further enhancing the open space feel. There are utility boxes and a USPS drop-box in the right-of-way adjacent to the property, which do generally contribute to visual clutter; however, those are not held against the property owner as they have no control over such items. The site is also located along a curve, which limits visibility of the full length of the street and reduces the line-of-sight for street-side signs and other clutter. For these reasons, it is determined the sign will not create visual clutter for the neighborhood.

5. Site constraints.

The site, located on a side street, does not have any frontage or visibility onto a major roadway. The applicant requests the sign to increase visibility for the restaurant. Staff recognizes the applicant’s concern is that the site is tucked back in an area that is primarily office uses, thus potentially at a disadvantage compared to other restaurant uses with direct frontage or visibility from W. 87th Street Parkway.

6. Lighting.

The sign will have backlit internal illumination. The illumination will not flash, move, rotate, scintillate, blink, or flicker to cause any hazard or undue attention attraction beyond a standard sign.

7. Promotion of high-quality, unique design.

The design of the sign would be unique and visually interesting. The sign draws inspiration from the restaurant’s branding and the materials of the primary structure. The sign shall be constructed of high-quality materials.

For these reasons, Staff supports the requested deviation to allow a monument sign for a property with 182 feet of frontage along a street when 200 feet is required.

REVIEW PROCESS

- *The Planning Commission is the final authority for approval of this project.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

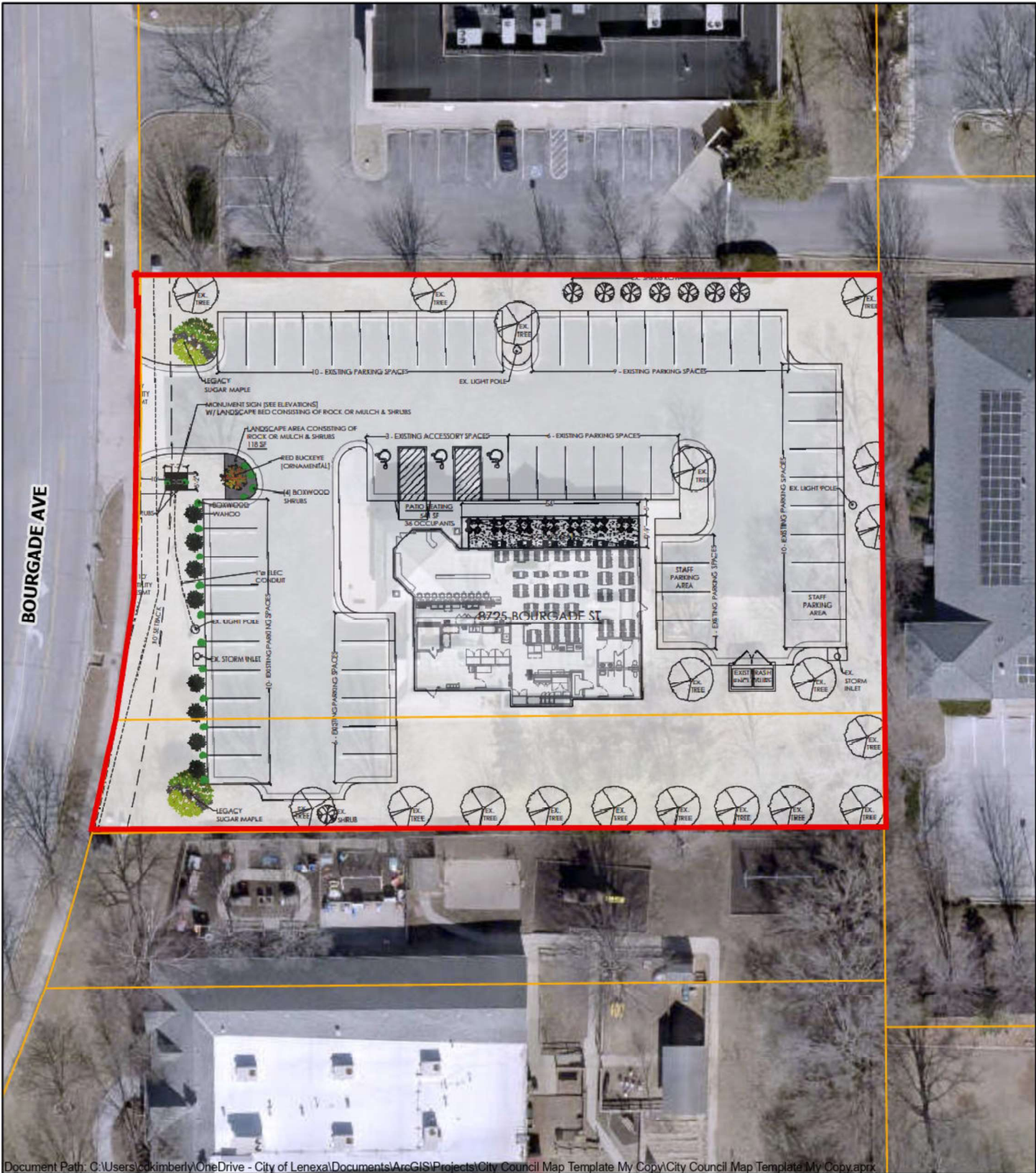
RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the proposed Final Plan for Burg & Barrel Patio & Sign.**

- This is a final plan request for the addition of an outdoor seating area for a restaurant use. The plan includes one deviation request to allow a monument sign where the minimum street frontage requirement to allow a monument sign is not met.
- The project is consistent with Lenexa’s goals through ***Responsible Economic Development*** to create ***Thriving Economy***.

FINAL PLAN

Staff recommends **approval** of the final plan for PL24-02FR – **Burg & Barrel Patio & Sign** at 8725 Bourgade Avenue, for a restaurant with outdoor patio, with one deviation to allow a monument sign for a property with a street frontage of 182 feet where the code requires 200 feet.

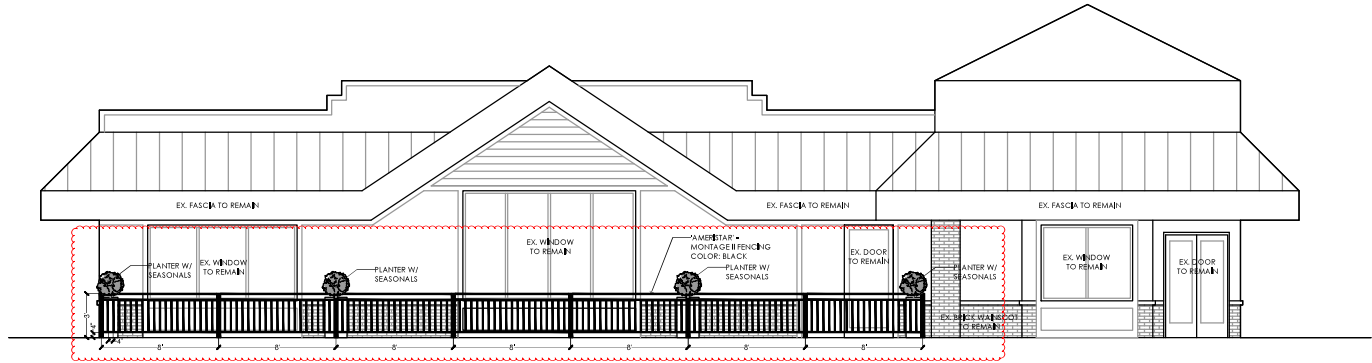


Document Path: C:\Users\ckimberly\OneDrive - City of Lenexa\Documents\ArcGIS\Projects\City Council Map Template My Copy\City Council Map Template My Copy.aprx
 Data Source: City of Lenexa and Johnson County Kanzas
 For further information, please call 913-477-7500

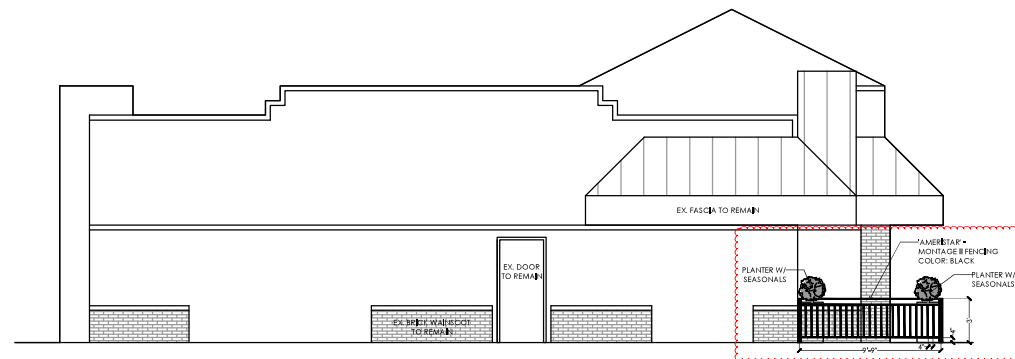
Burg & Barrel Patio & Sign PL24-02FR



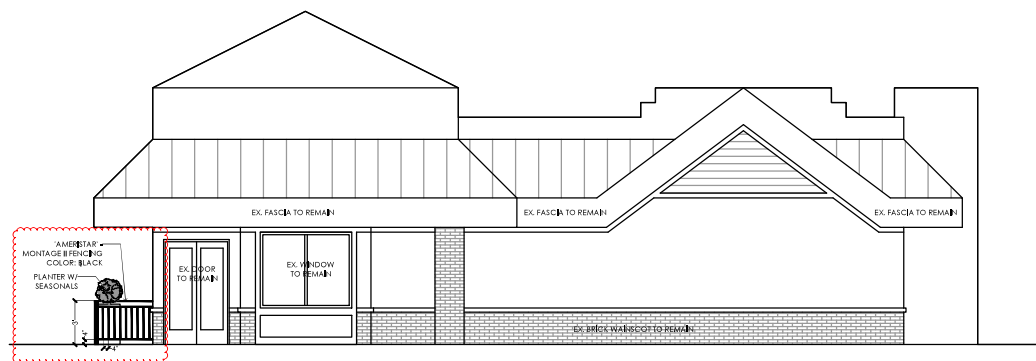
CODE ANALYSIS:	
1. TENANT NAME	NAME: THE BURG & BARREL
	ADDRESS: 8725 BOURGADE AVE, LENEXA
1. BUILDING OWNER NAME	OWNER: BOURGADE LLC
	ADDRESS: 8725 BOURGADE AVE, LENEXA, KS 66219
2. ARCHITECT INFORMATION	SULLIVAN PALMER ARCHITECTS
	ADDRESS: 8421 JENNIFER DR, MERIAM, KS 66202
	CONTACT: M SULLIVAN / P: 913-886-8540
3. CODE ITEMS	2018 INTERNATIONAL BUILDING CODE
	2017 NATIONAL ELECTRIC CODE
	2018 INTERNATIONAL FIRE CODE
	2018 INTERNATIONAL MECHANICAL CODE
	2018 INTERNATIONAL PLUMBING CODE
	2017 INTERNATIONAL ENERGY CONSERVATION CODE
	2009 IBC CANADIAN ACCESSIBILITY
	ALL CODE ITEMS AS REVISED BY THE CITY OF LENEXA, KS
4. EXISTING BUILDING DATA	CONSTRUCTION TYPE: 4-1 UNFINISHED SINGLE STORY
	FLOOR FINISH: ONE FINISHED (B) BOURGEOIS COMMERCIAL
	FLOORING: WOOD FLOORING: 3,275 SF
	USE: RESTAURANT
5. NEW TENANT FINISH DATA	BUILDING INTERIOR FLOOR FINISH: 3,427 SF
	USE: RESTAURANT
	CONSTRUCTION TYPE: 4-1 UNFINISHED
	SMALL MEET ADA GUIDELINES
6. OCCUPANCY TYPE: 4-2	SEATING: (SF/15 OCC) 1,147 / 15 = 76.2 (27) OCC
	BAR: (SF/200 OCC) 127 / 200 = 0.63 (1) OCC SEATS: 22
	ENTR: (SF/15 OCC) 225 / 15 = 15 OCC
	KITCHEN: (SF/200 OCC) 185 / 200 = 0.93 (1) OCC
	TOILET: (SF/50 OCC) 29 / 50 = 0.58 (1) OCC
	PANTRY: (SF/100 OCC) 147 / 100 = 1.47 OCC
	TOTAL OCC LOAN: 122
	ALL OCCUPANCY SIGNAGE SHALL BE POSTED PER IBC 2018
	BUILDING CODES: 3, 12, 100, 107, 108 STATE ONLY
	MINIMUM BARREL SEATING: 22
7. PARKING REQUIREMENTS PROPOSED PARKING	PER LENEXA ORDINANCE 11 (SPACE / 75 SF)
	PARKING: 100 SF/75 SPACES (INCLUDES ADA SPACES)
	PARKING REQUIRED FOR BUILDING: 47.6 (48)
	PARKING REQUIRED FOR PARKING: 7.21 (8)
8. PLUMBING REQUIREMENTS	PER TABLE 403.1-A
	REQUIRED WATER CLOSET: MALE = 1 PER 75 FEMALE = 1 PER 25
	PROVIDE WATER CLOSET: MALE = 1 FEMALE = 2
	REQUIRED LAVATORIS: MALE = 1 PER 200
	PROVIDE LAVATORIS: MALE = 1 FEMALE = 2
	SERVICE: NOT PROVIDED
9. LEGAL DISCUSSION	USE: RESTAURANT, PARKING: 11 K.S.B.S. SEC. 836.07 (1)



1 NORTH ELEVATION
1/4" = 1'-0"



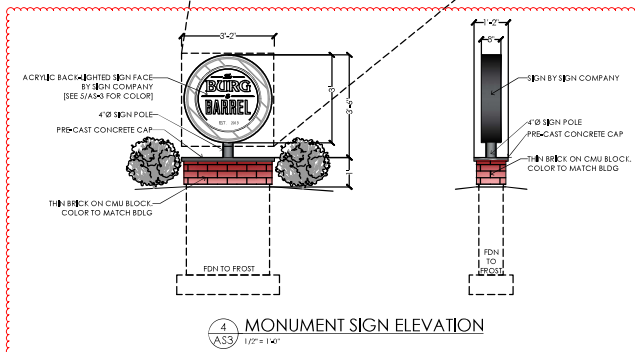
2 EAST ELEVATION
1/4" = 1'-0"



3 WEST ELEVATION
1/4" = 1'-0"



5 MONUMENT SIGN COLOR DETAIL
1/8\"/>



4 MONUMENT SIGN ELEVATION
1/2\"/>



PROJECTS CONSULTANTS
BURG & BARREL RESTAURANT
FINAL PLAN W/ DEVIATION
8725 BOURGADE ST, LENEXA, KANSAS

SULLIVAN PALMER ARCHITECTS
1001 W. 11TH AVENUE, SUITE 100
LENEXA, KANSAS 66219
TEL: 913-886-8540
WWW.SULLIVANPALMERARCHITECTS.COM

aia
SULLIVAN PALMER ARCHITECTS

Date: 26 December 2023

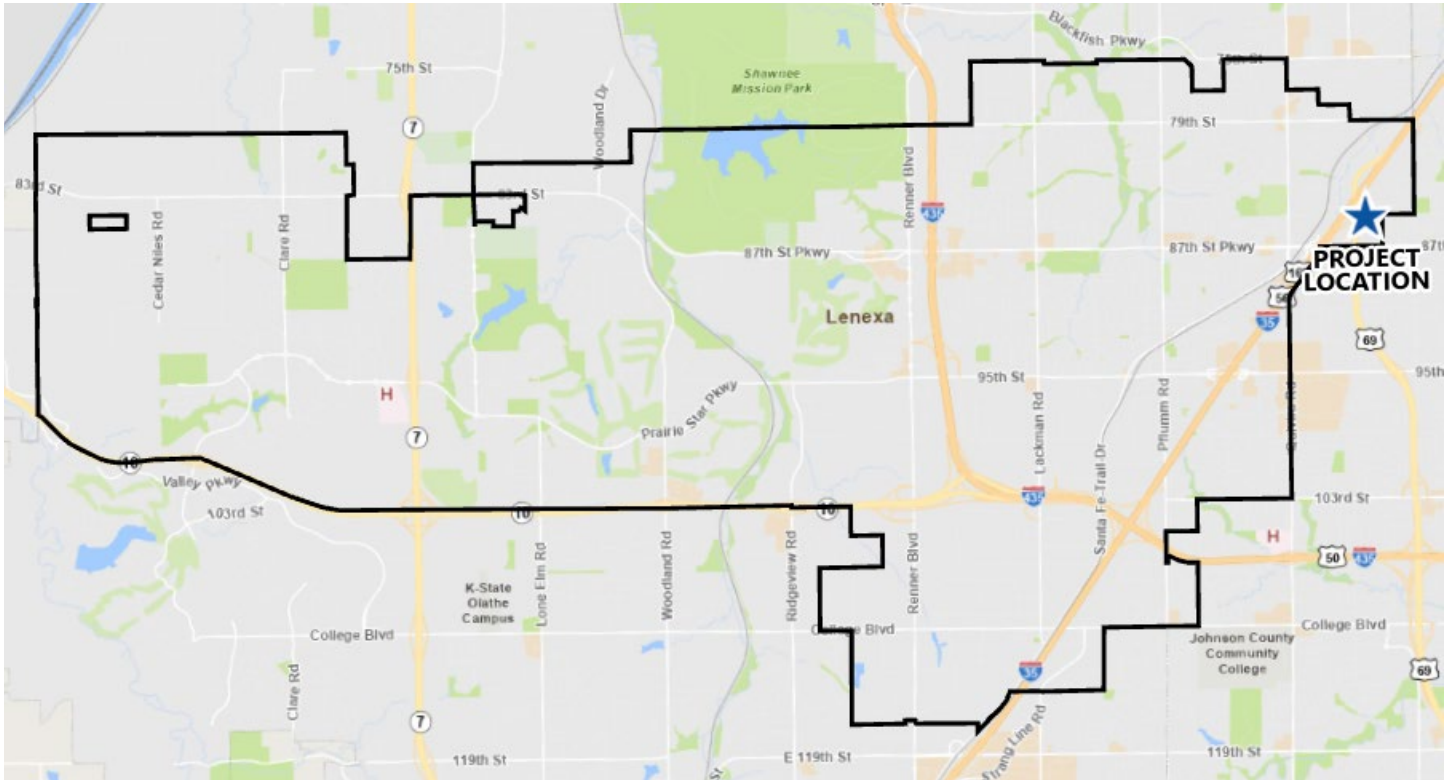
Revision:	
01	22 January 2024

Drawn by: MWM
Sheet:

AS-3
elevations

SUNFLOWER MOB

Project #:	PL24-01FR	Location:	10900 W. 86 th Street
Applicant:	Dustin Burton	Project Type:	Revised Final Plan
Staff Planner:	Kim Portillo	Proposed Use:	Medical Office



PROJECT SUMMARY

The applicant requests approval of a revised final plan only to modify the conditions of the previously approved final plan for a medical office building (PL22-14F) known as Sunflower Medical Office Building (MOB). The proposed modification would remove one condition that limits use of the third tenant space until a shared parking agreement can be achieved with a neighboring property. There are no planned improvements to the site or building with this final plan. The only component being reviewed as part of this application is the parking study and the parking deviation request. The applicant now requests a parking deviation as part of the revised final plan based on a parking evaluation of the demonstrated use of the building while in operation. Approval of the parking deviation would allow the entire building to be occupied. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The original final plan (PL22-14F) was approved by the Planning Commission for a medical office use on June 27, 2022 with a condition requiring the execution of a shared parking agreement with the neighboring property, Drexel. The shared parking agreement would have allowed the sharing of 31 parking spaces. The original plans also included exterior renovations that have since been completed.

The applicant was unable to secure the shared parking agreement with the neighboring property, which led to submission of a revised final plan (PL22-05FR) to modify the conditions of approval of PL22-14F and request a parking deviation. The two tenant spaces to be used totaled 23,800 SF, requiring 119 parking spaces, a deficit of eight spaces. The revised final plan was approved by the Planning Commission on October 3, 2022, with a condition that the third tenant space shall not be used until an executed shared parking agreement be recorded so the use can meet the parking requirement.

The building received a building permit and certificates of occupancy for two of the tenant spaces in 2022. The third tenant space has remained vacant.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
2.32	28,080	CP-2	Community Commercial Center



Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The land use is *medical office*, which is a permitted use in the CP-2, Planned Community Commercial District. There are three tenant spaces with individual entrances. There are two current tenants and one proposed tenant that operate under the Sunflower Medical Group name. Existing and proposed tenant details are outlined in Table 1 below.

TABLE 1: SUNFLOWER MEDICAL GROUP TENANTS

	Sunflower Primary Care (Existing)	Orthopedic Health Practice (Existing)	Orthopedic Health Physical Therapy (Proposed)
Business Hours	M-F 8am-7pm Sat 9am-2pm Sun 10am-2pm	M-F 8am-8pm Sat 8am-12pm	M-F 8am-5pm
# of Employees	29	14	3
Tenant Space Area (SF)	12,280	11,520	4,280

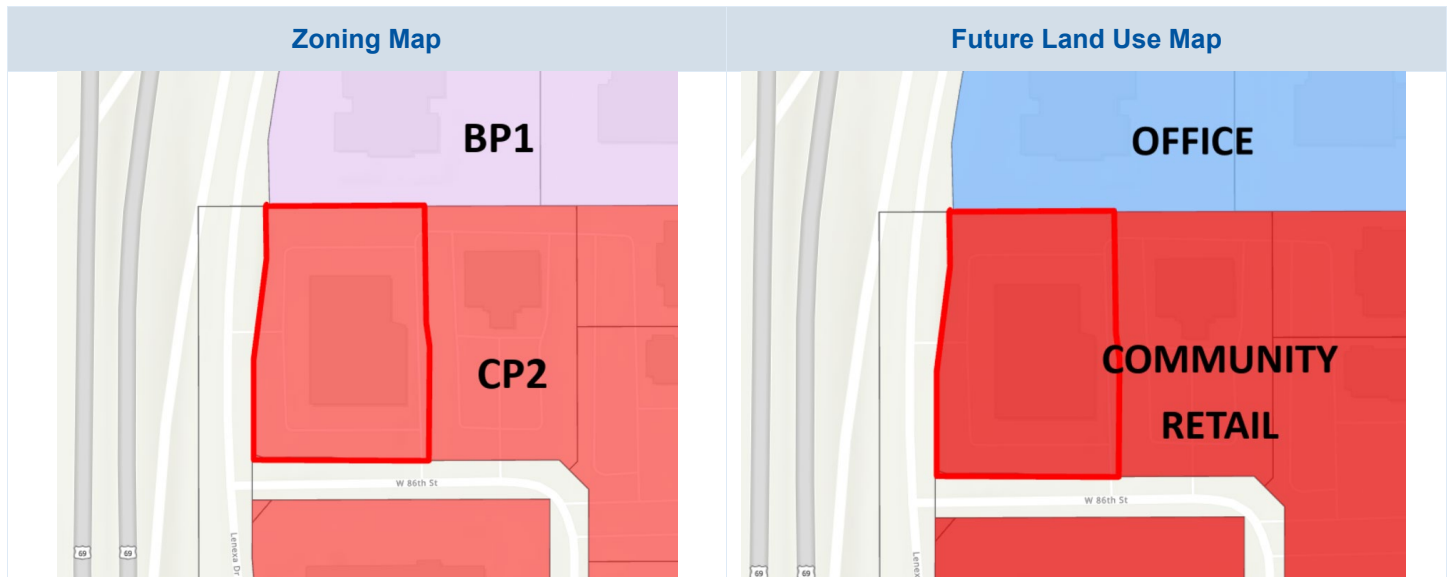


TABLE 2: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Community Commercial Center	CP-2 Planned Community Commercial	Medical Office
North	Office/Employment Center, Business Park	BP-1, Planned Business Park	Office
South	Community Commercial Center; Overland Park	CP-2, Planned Community Commercial	Office
East	Community Commercial Center; Business Park; Overland Park	CP-2, Planned Community Commercial	Office
West	Regional Commercial Center; Office; Business Park; Public/Open Space	Highway Rights-of-way	Highway Rights-of-way

FINAL PLAN REVIEW

This is a revised final plan request to modify original conditions of a final plan approval to allow a parking deviation for a 28,080 SF medical office building with three tenant spaces. The site includes a 28,080 SF multitenant building located at the center of the site with parking to the north, south, east, and west of the building. The original final plan included removing a delivery entrance with an overhead bay door on the east side of the building to regrade and create additional parking. No new site improvements are proposed as part of this revised final plan. The only component being reviewed as part of this application is the parking study and the parking deviation request. The proposed final plan layout is consistent with the approved final plan (PL22-14F), which was approved by the Planning Commission on June 27, 2022.

DIMENSIONAL STANDARDS

The site meets dimensional standards under the original final plan. No dimensional changes to the building or site are proposed with this revised final plan application.

PUBLIC IMPROVEMENTS

This project does not include any public improvements.

ACCESS, TRAFFIC, AND PARKING

The site has one entrance from Lenexa Drive, a collector road, and one entrance from W. 86th Street, a local road. There are public sidewalks along both streets, and an internal sidewalk connection to Lenexa Drive.

TABLE 3: PARKING ANALYSIS

Land Use Medical Office	Parking Formula	Required Parking	Proposed Parking	Difference
Sunflower Primary Care 12,280 SF	1 space per 200 SF	61	111	-29
Orthopedic Health Practice 11,520 SF	1 space per 200 SF	58		
Orthopedic Physical Therapy 4,280 SF	1 space per 200 SF	21		
Total Medical Office 28,080 SF	1 space per 200 SF	140		

The property was initially developed for office use with fewer parking requirements compared to a medical office use. Although additional spaces were added in the original final plan to accommodate the increased parking requirement, a deviation of eight spaces was granted for two out of three tenant spaces. The applicant now seeks an additional parking deviation of 21 spaces, bringing the total parking space deficit to 29 spaces. This is discussed in greater detail within the *Deviations* section of this Staff Report.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

LIGHTING

There are no changes proposed to site lighting, which was reviewed and approved with the original final plan application for a medical office use.

LANDSCAPING

There are no proposed changes to landscaping and existing landscaping will remain. Such landscaping includes shrubs and trees along Lenexa Drive, W. 86th Street, and in parking lot islands.

ARCHITECTURE

No changes are proposed to the previously approved architecture of the building. The exterior of the building will remain as split-faced block painted “gray screen” with a horizontal accent band in the color “software” around the base of the building and a horizontal accent band in “network grey” above windows and at the roofline. Existing glass panels will remain.

DEVIATIONS

The applicant requests a 29-space deviation from the parking requirement to allow 111 parking spaces where 140 spaces are required for a 28,080 SF medical office building.

The applicant conducted an evaluation of parking usage with current operations of the two tenants at full staffing and full scheduling. The parking evaluation tracked parking spaces hourly over the course of one month (September 6, 2023 to October 6, 2023). This evaluation was submitted for Staff review by Dusin Burton, P.E., of Renaissance Infrastructure Consulting. The site is deficient by 29 parking spaces according to the minimum parking required by the Unified Development Code (UDC). The parking evaluation determined that there are sufficient vacant parking spaces available at any given time to merit occupancy of the third tenant space.

TABLE 4: PARKING EVALUATION

Metric	Occupied Spaces	Available Spaces	Occupancy %
Max. Space Occupancy During 31 Day Study Period	83	28	74.7%
Min. Space Occupancy During 31 Day Study Period	15	96	13.5%
Average Daily Space Use	56	55	50.4%
High Average Occupancy (10 AM)	69	42	62.1%
Low Average Occupancy (4 PM)	39	72	35.1%
<i>Data was collected at the following times: 8AM, 10AM, 12PM, 2PM, and 4PM daily</i>			

Staff has not conducted ongoing formal inspections of the parking occupancy at the site; however, there have been no complaints related to the site or parking issues brought to the attention of Staff. Furthermore, during site visits for applications or driving by for other purposes, Staff has not observed any concerns with overcrowding of parking.

The most recent final plan approval included a deviation to allow the two larger tenant spaces, with a combined total of 23,800 SF, to operate with a deficit of eight parking spaces. The parking evaluation indicates that the current tenant spaces are operating below what is mandated by the code, with an average availability of 38% of parking spaces during the busiest time of day, and an availability of 25% of the parking spaces at the single highest point of utilization of parking during the study period.

The third tenant space is 4,280 SF and is currently vacant. Filling this tenant space with medical office use would require an additional 21 parking spaces based on the code requirement of one space per 200 SF; therefore increasing the total site parking deficit from eight spaces to 29 spaces. Based on the current utilization of parking spaces, there would be parking spaces available for use by the new tenant.

Staff notes that on-street parking is not allowed in this area. Lenexa Drive has posted no parking signs and no parking signs may be added on W. 86th Street at the City's discretion. Based on the evaluation provided it is not anticipated that there will be an excess amount of parking that necessitates parking off-site.

Staff supports the requested parking deviation, which would result in allowing use of the 4,280 SF third tenant space as a medical office with the existing 111 on-site parking spaces.

REVIEW PROCESS

- *The Planning Commission is the final authority for approval of this project.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

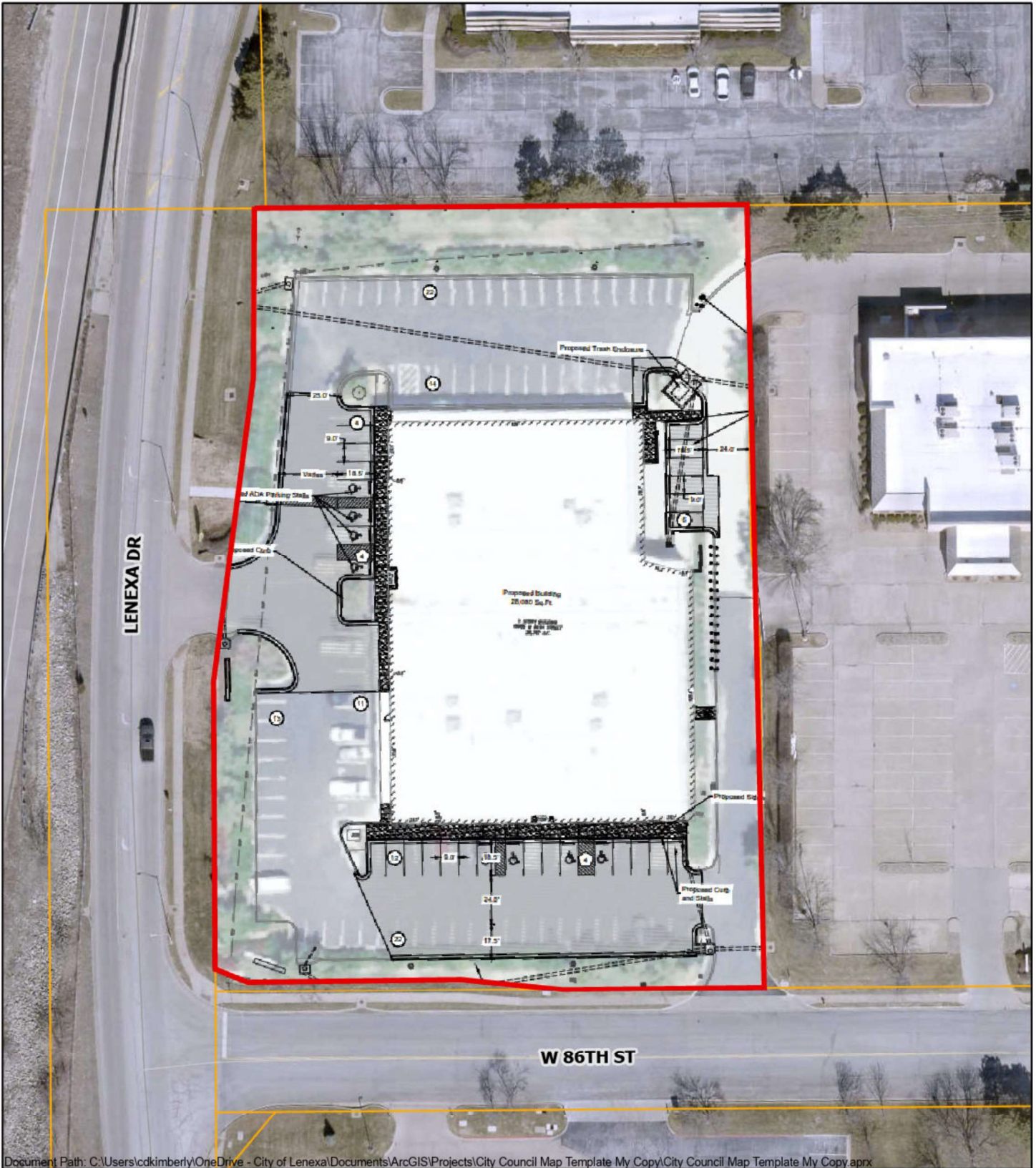
RECOMMENDATION FROM PROFESSIONAL STAFF

★ Staff recommends approval of the proposed Revised Final Plan for Sunflower MOB.

- This final plan is requested to modify the conditions of the previous approved final plan PL22-14F, for a medical office building, to remove one condition limiting use of the third tenant space until a parking agreement can be achieved with a neighboring property. The applicant requests a parking deviation based on demonstrated use of the building.
- The project is consistent with Lenexa's goals through **Responsible Economic Development** to create **Healthy People**.

REVISED FINAL PLAN

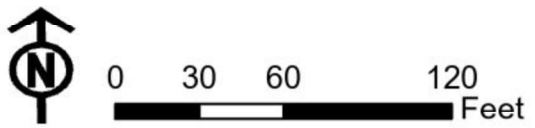
Staff recommends **approval** of the final plan for PL24-01FR – **Sunflower MOB** at 11900 W. 86th Street, for a medical office building, with one deviation to allow 111 parking spaces where 140 are required, thus allowing the remaining 4,280 SF tenant space to be occupied by a medical office use.



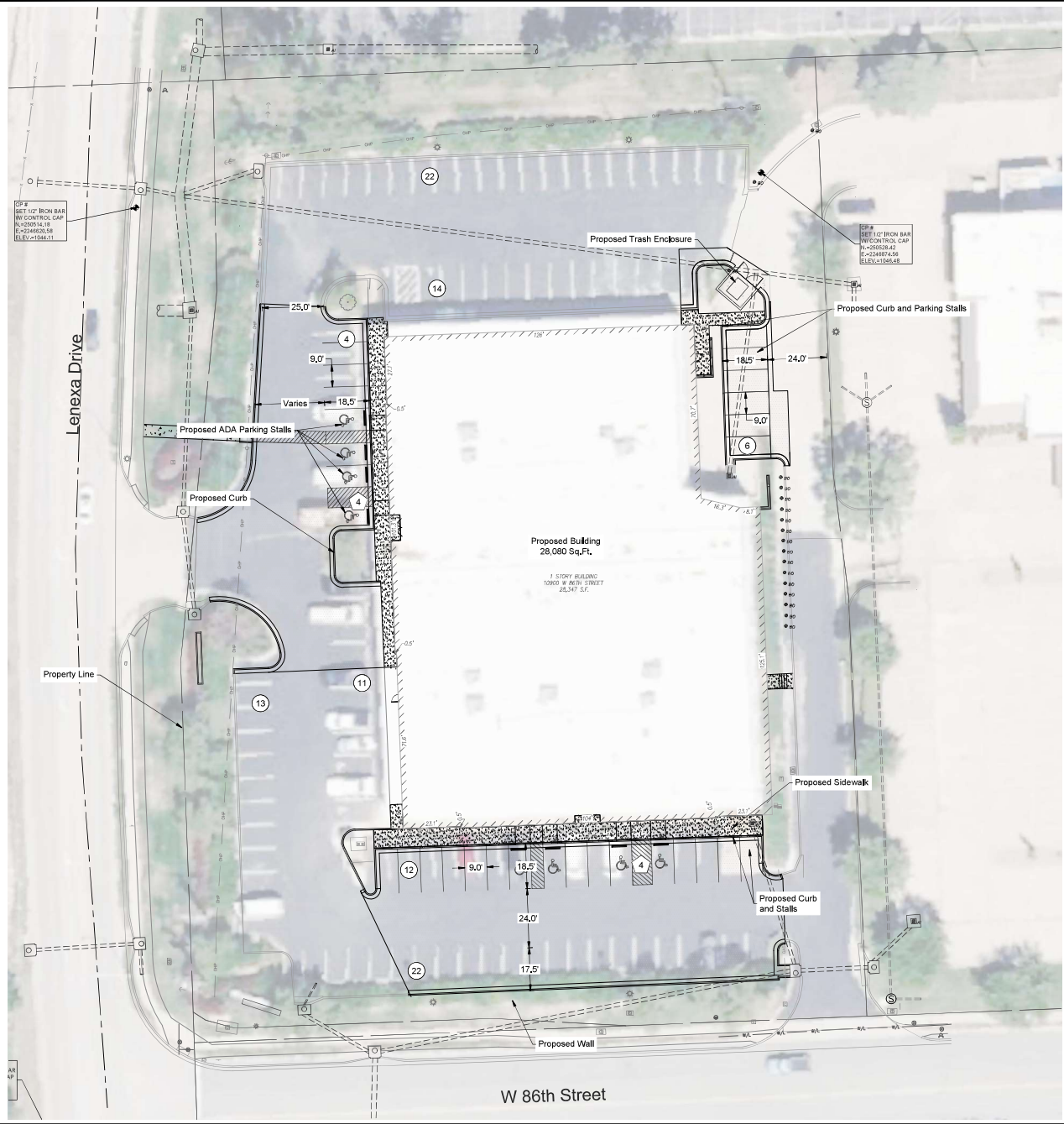
Document Path: C:\Users\cdkimberly\OneDrive - City of Lenexa\Documents\ArcGIS\Projects\City Council Map Template My Copy\City Council Map Template My Copy.aprx

Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Sunflower MOB Lenexa PL24-01FR



Revised
Jan 02, 2024-8:10am
304 E. Campbell Rd, Suite 100, Lenexa, KS 66151
www.rc-consult.com



- Site Data Table :**
- Total Lot Area: 2.34 Acres
 - Existing Zoning: CP-2
Proposed Zoning: CP-2
 - Proposed Use: Medical Office
 - Building Area Breakout:
-Gross Area: 28,080 Sq. Ft.
-Storage/Restroom Used Space: 3,699 Sq. Ft.
-Interior Area Used: 24,381 Sq. Ft. (for Parking Calculation)
 - Floor Area Ratio: .20
 - Parking:
-Parking Required by Gross Area: 142 Stalls (1/200 Sq. Ft.)
-Parking Required by Interior Area Used: 122 Stalls (1/200 Sq.Ft. of Usable Area)
-ADA Parking Required: 5 stalls
-ADA Parking Provided: 8 stalls
-Parking Provided: 112 Stalls
*Parking Deviation has been requested to allow provided spaces based upon Parking Study.
-Bicycle Parking Required: 3 Stalls (1/10,000 Sq.Ft)
-Bicycle Parking Provided: 3 Stalls
% Impervious: 74.88%
-75,221 sq ft Impervious
-25,570 sq ft Open Space

- Parking Count Legend**
- ADA Stall Count
- Standard Stall Count

- Hatch Legend**
- New Pavement / Pavement Replacement
- New Sidewalk / Sidewalk Replacement



Final Development Plans
22-0131
Lenexa Medical Office Building
Lenexa, Johnson County, Kansas

General Layout

NO.	DATE	REVISION

DRAWN BY: BOB
CHECKED BY: CB

Renaissance Infrastructure Consulting
400 E 17th Street
Lenexa, KS 66151
www.rc-consult.com
816.800.0952



Sheet
C03

KEYNOTES - FLOOR PLAN	
NUMBER	COMMENTS
1	DATE: 1/14/22
2	JOB NUMBER: 3-21204
3	DRAWN BY: AA
4	CHECKED BY: JTS
5	REVISION
6	DATE: 01/26/22
7	DESCRIPTION: Addendum 1
8	DATE: 02/11/22
9	DESCRIPTION: Addendum 2
10	DATE: 02/23/22
11	DESCRIPTION: City Comments
12	DATE: 02/23/22
13	DESCRIPTION: Addendum 3
14	DATE: 04/01/22
15	DESCRIPTION: RFI #6
16	DATE: 04/02/24
17	DESCRIPTION: Planning Exhibit

Samuel K. Beckman - Architect
License # Kansas 96937

ACI BOLAND ARCHITECTS

ACI/Boland, Inc.
Kansas City | St. Louis
1710 W. Broadway
Kansas City, MO 64108
T: 816.763.9600

Licensee's Certificate of Authority Number:
Kansas: 84-008

MEP CONSULTANT

SBS EXISTING, LLC
1100 MARK STREET, FLOOR 4
KANSAS CITY, MO 64105
913.689.9449

FLOOR PLAN LEGEND			
	NOTH SCOPE		NOTH ARCHITECTURAL SCOPE
	CONCRETE SLAB		EXISTING WALL
	NEW WALL		EXISTING EXPANSION JOINT
	EXPANSION JOINT		EXISTING DOOR
	NEW DOOR		

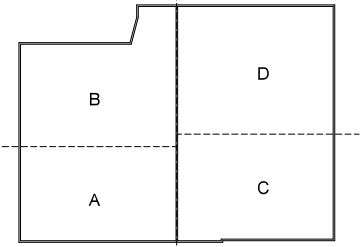
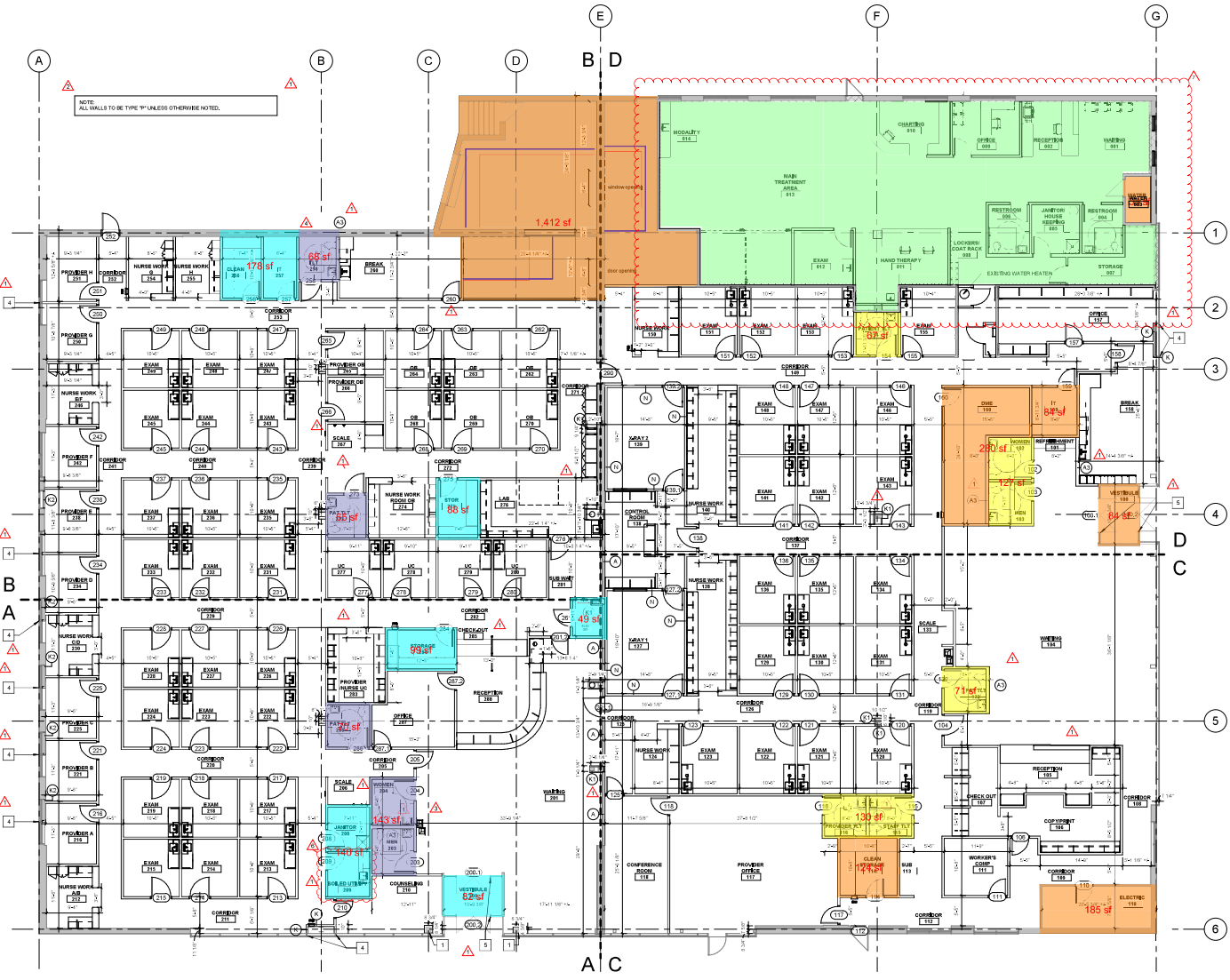
AREA EXHIBIT:

PROPOSED TENANT SPACE - 2,950 SF

ORTHO STORAGE - 2,322 SF
ORTHO RESTROOM - 395 SF
ORTHO TOTAL - 2,717

SUNFLOWER STORAGE - 636 SF
SUNFLOWER RESTROOM - 346 SF
SUNFLOWER TOTAL - 982 SF

TOTAL ALL AREAS - 3,699 SF



A2 FIRST FLOOR PLAN
1/8" = 1'-0"

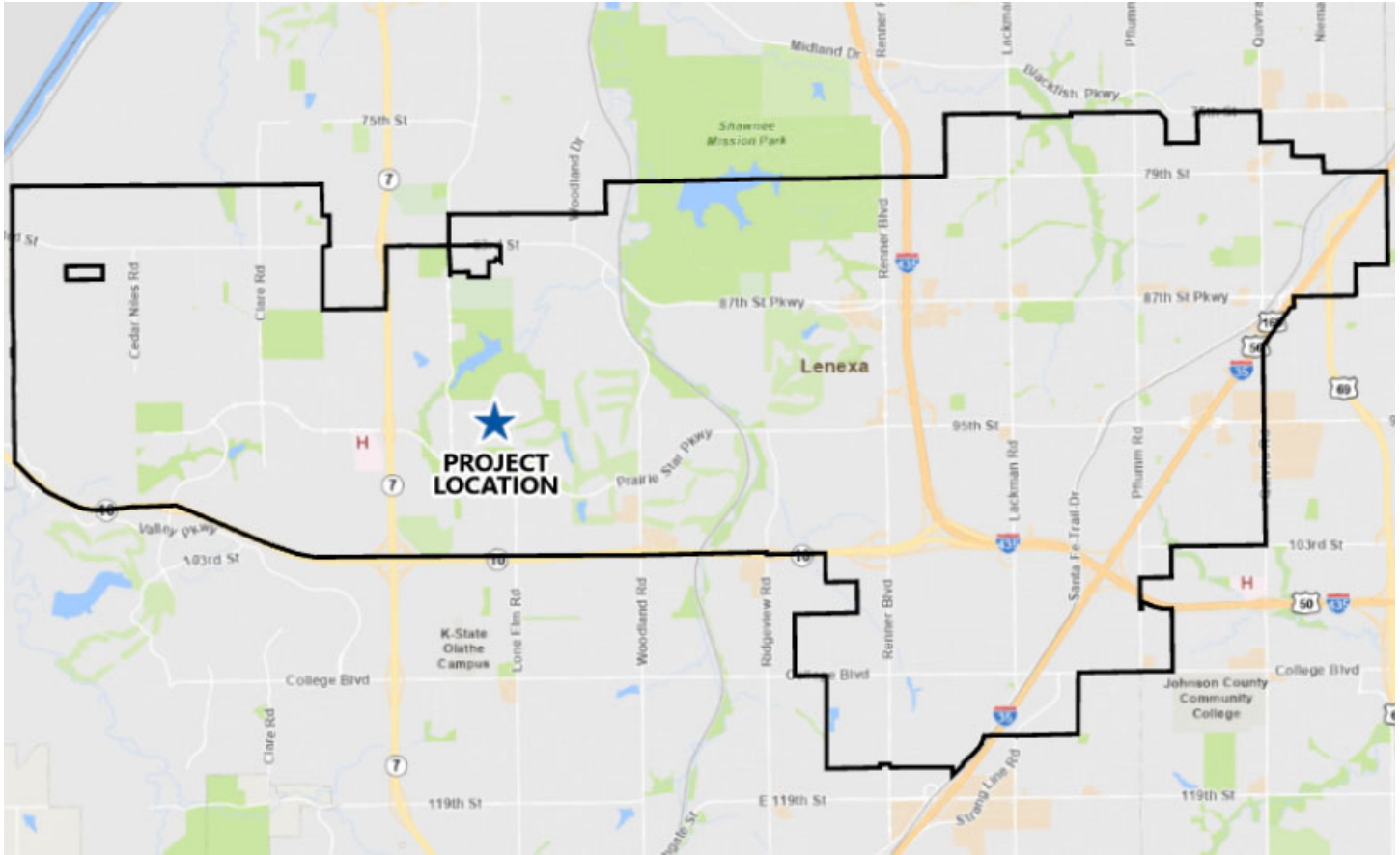
KEY PLAN - FIRST FLOOR
1/32" = 1'-0"

Lenexa Medical Office Building
Orthopedic Health & Sunflower Tenant Finish
10900 W. 86th Street, Lenexa, KS 66214

Number	Date	Description
1	01/26/22	Addendum 1
2	02/11/22	Addendum 2
3	02/23/22	City Comments
4	02/23/22	Addendum 3
6	04/01/22	RFI #6
7	04/02/24	Planning Exhibit

TIMBER ROCK, FIFTH PLAT

Project #:	PT24-03F	Location:	9392 & 9374 Deer Run Street
Applicant:	David Gambler, Phelps Engineering	Project Type:	Final Plat
Staff Planner:	Logan Strasburger	Proposed Use:	Single-Family Residential



PROJECT SUMMARY

The applicant requests approval of a final plat to replat two undeveloped adjacent lots located in the Timber Rock subdivision to create a single 0.87-acre lot for construction of a single-family residential dwelling. The new lot will be known as Lot 132 and will be addressed as 9374 Deer Run Street. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The site is zoned RP-1, Planned Single-Family Residential District. The subject lots are part of two separate plats. The northern property is Lot 116 in Timber Rock, Second Plat and was approved in 2018. The southern property is Lot 24 in Timber Rock, First Plat and was approved in 2017. The lots are currently undeveloped.

<p>LAND AREA (AC) 0.87</p>	<p>CURRENT ZONING RP-1</p>	<p>COMP. PLAN Suburban Residential</p>
---------------------------------------	---------------------------------------	---



Exhibit 1: Aerial Image of Subject Site



Exhibit 2: Street view of boundary between Lot 25 and Lot 24, facing Northwest.



Exhibit 3: Street view of boundary between Lot 24 and Lot 116, facing West.

LAND USE REVIEW

The two subject lots, Lot 24 (9392 Deer Run Street) of Timber Rock, First Plat and Lot 116 (9374 Deer Run Street) of Timber Rock, Second Plat, are undeveloped but will be combined to facilitate construction of one single-family residence. There is no proposed change to the use of the lots as part of this application.

In 2021, Timber Rock, Fourth Plat, was approved to combine two lots directly east from the subject site, so the proposed Fifth Plat to combine two lots is consistent with other development activities in the vicinity.

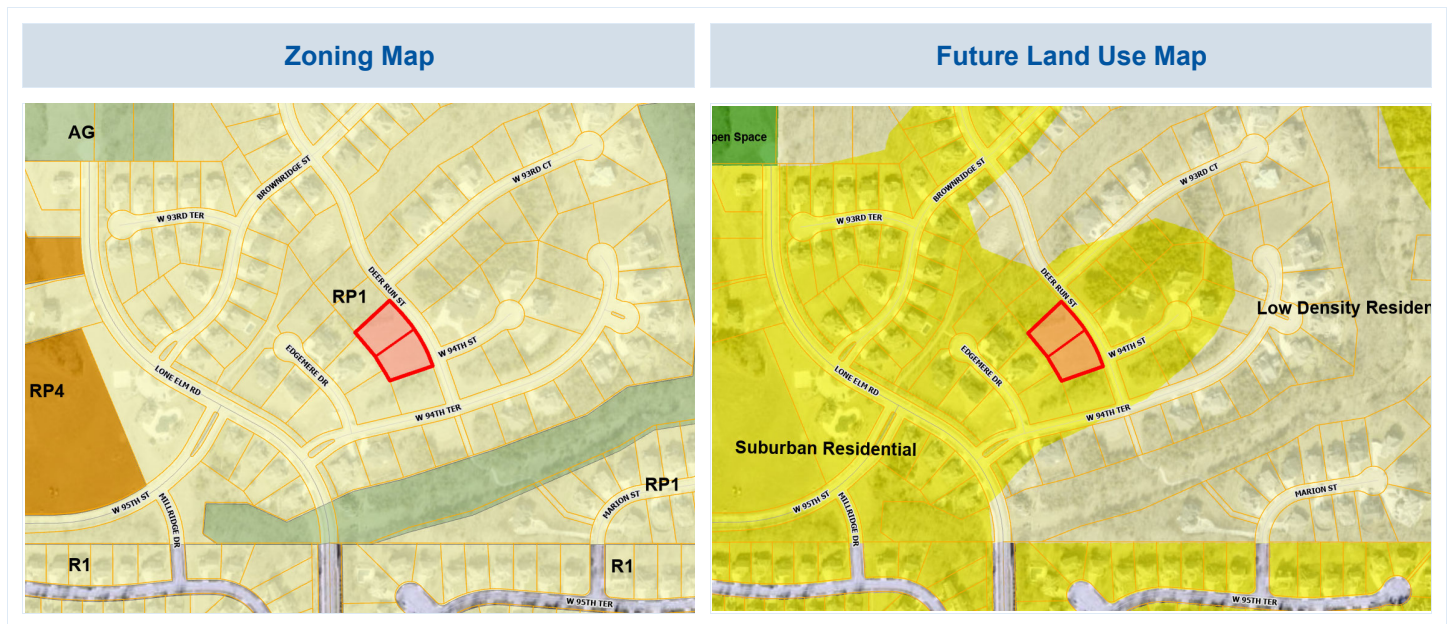


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Suburban Residential	RP-1, Single-Family Residential District	Undeveloped
North	Suburban Residential; Low Density Residential	RP-1, Single-Family Residential District; AG, Agricultural District	Single-Family
South	Suburban Residential; Low Density Residential	RP-1, Single-Family Residential District; AG, Agricultural District	Single-Family
East	Suburban Residential; Low Density Residential	RP-1, Single-Family Residential District; AG, Agricultural District	Single-Family
West	Suburban Residential	RP-1, Single-Family Residential District; RP-4, Planned Residential (High Density) District	Single-Family; Multifamily

FINAL PLAT REVIEW

Timber Rock, First Plat, was recorded November 27, 2017. Timber Rock, Second Plat, was recorded May 9, 2019. This replat will remove the common property line between Lot 116 of Timber Rock, Second Plat, and Lot 24 of Timber Rock, First Plat. The proposed final plat will create Lot 132 of Timber Rock, Fifth Plat and will be addressed as 9374 Deer Run Street. There are no easements or existing utilities located along the common property line between the two lots. Existing lots conform with current code and proposed Lot 132 will conform to current code. As part of this lot consolidation there is no need to vacate any of the easements and no new easements are necessary with this final plat. Each lot is currently roughly 0.43 acres. The replat will create a lot that is approximately 0.87-acres in size.

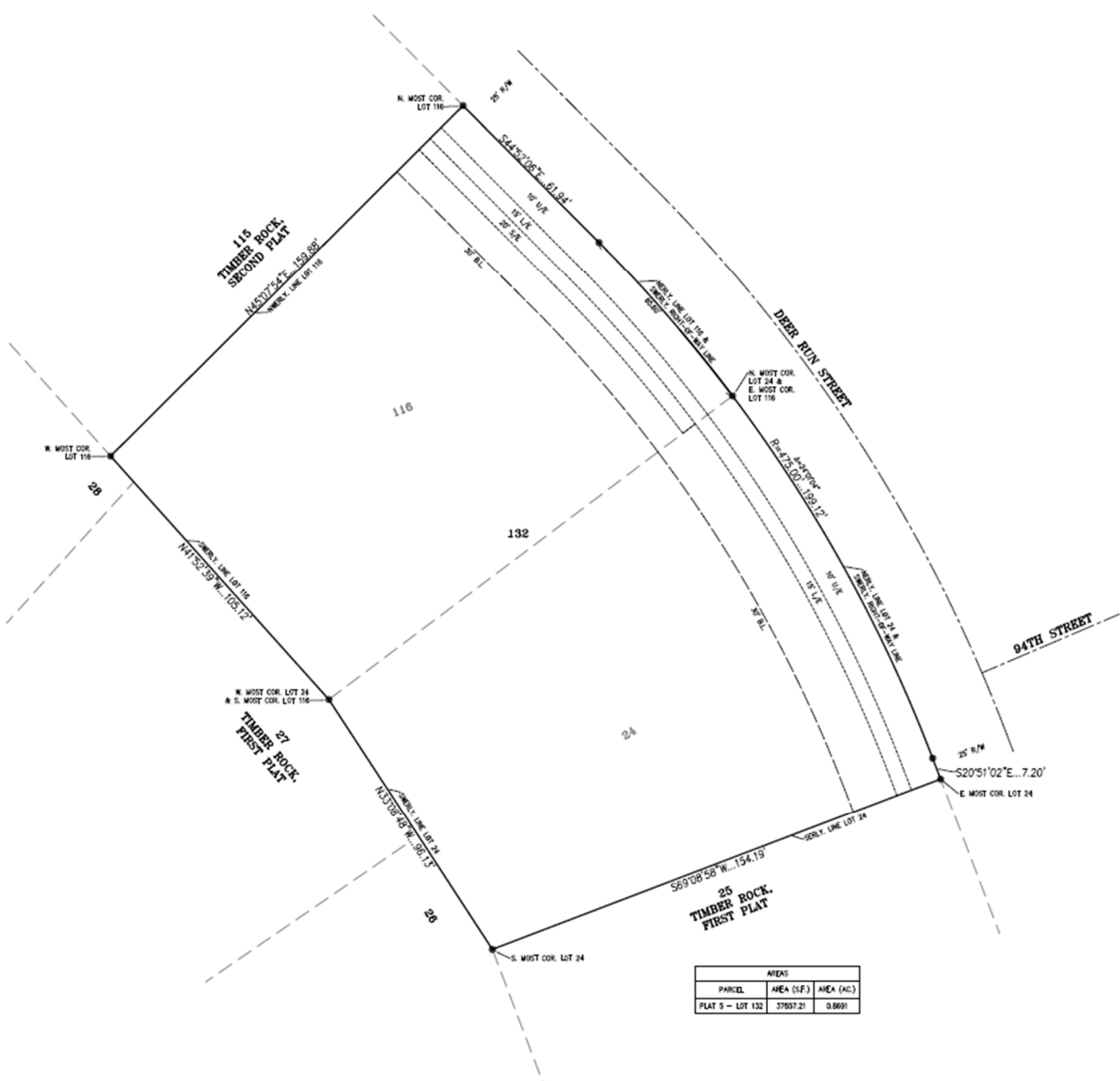


Exhibit 4: Excerpt of the proposed replat with lot details.

DEVIATIONS

The applicant is not requesting any deviations from the Unified Development Code.

REVIEW PROCESS

- *The Planning Commission is the final authority for approval of this project.*

RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the proposed Final Plat for Timber Rock, Fifth Plat.**

- This replat will remove the common property line between subject lots to create a single lot for a single-family residence.
- The project is consistent with Lenexa's goals through ***Sustainable Policies and Practices*** to create ***Vibrant Neighborhoods***.

FINAL PLAT

Staff recommends **APPROVAL** of the final plat for PT24-03F – **Timber Rock, Fifth Plat**, located at 9374 & 9392 Deer Run Street, for a single-family residential use.

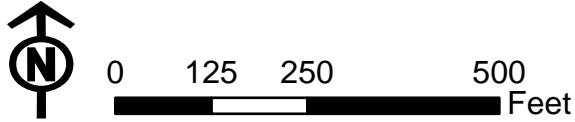


Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Timber Rock, Fifth Plat

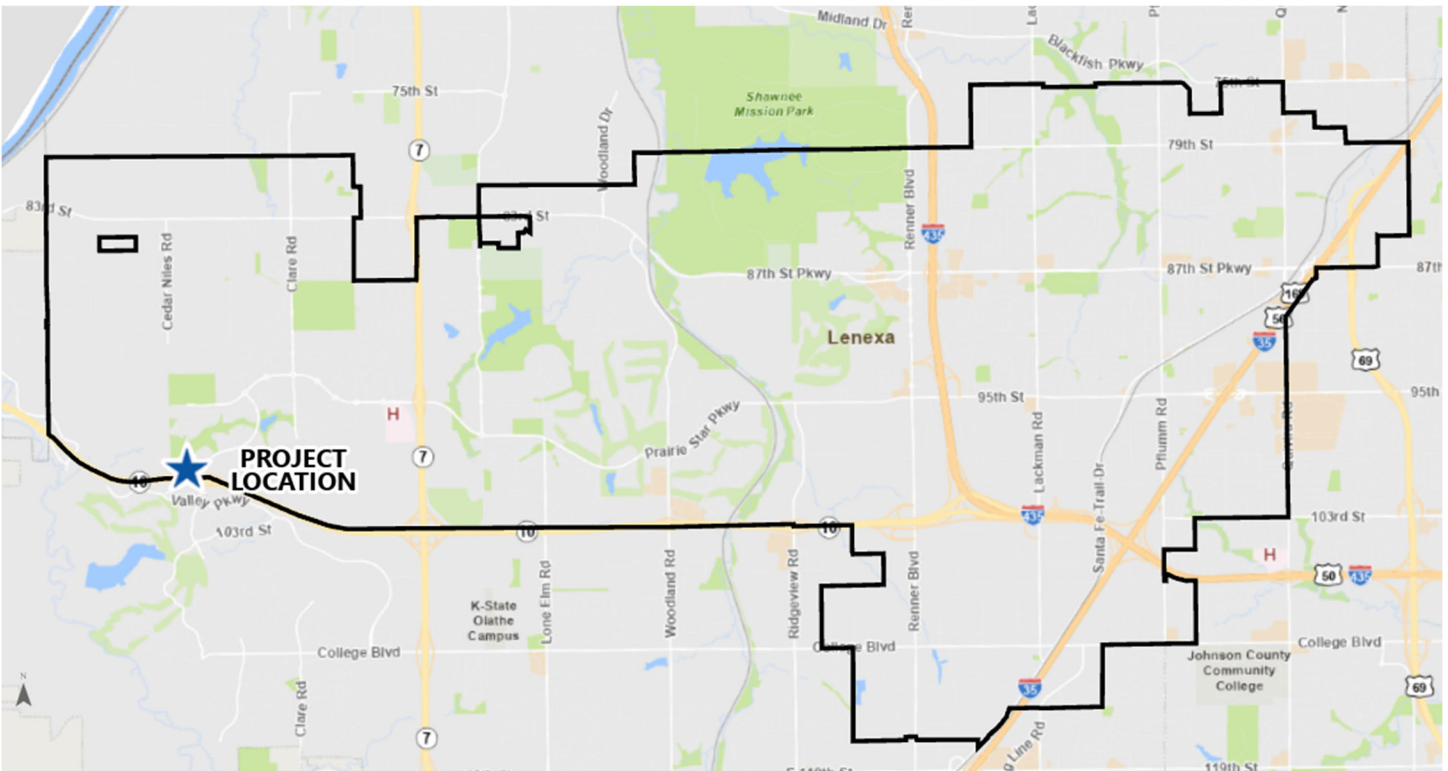
Final Plat

PT24-03F



CANYON RIDGE APARTMENT HOMES

Project #:	RZ23-07 & PL23-12P	Location:	Northwest corner of K-10 Highway and Canyon Creek Boulevard
Applicant:	Henry Klover, Klover Architects	Project Type:	Rezoning & Preliminary Plan/Plat
Staff Planner:	Dave Dalecky	Proposed Use:	Multifamily Residential, Nursing Home, and Convenience Store with gasoline sales



PROJECT SUMMARY

The applicant requests approval to rezone the property at the northwest corner of K-10 Highway and Canyon Creek Boulevard from the AG, Agricultural, CP-O, Planned General Office, and CP-2, Planned Community Commercial Districts to the PUD, Planned Unit Development District to allow a mixed-use development comprised of multifamily residential, nursing home, and convenience store with gasoline sales uses. The companion preliminary plan shows apartments on the north and west part of the site, a nursing home on the southeasterly part of the site, and convenience store with gasoline sales on the east part of the site along Canyon Creek Boulevard. A new public street will extend into the site to provide access to the different components of the development and to an adjacent undeveloped parcel. The applicant requests four deviations from the Unified Development Code (UDC) related to parking, convenience store size, freeway setback, and gasoline pump island setback. A Public Hearing is required for the rezoning request.

The project has been revised from the original submittal reflected in the January 8, 2024 Staff Report. The changes are related to the nursing home building and parking for the multifamily development.

- The nursing home building is increased in height from two stories to three stories tall and is now proposed to be 68,800 square feet in area, which is an increase of 34,400 square feet from the original 34,400 square feet. The revised plan shows the nursing home to have 80 units (beds), which is an increase of 40 units from the original 40-unit proposal.
- The revised plan now shows 41 new deferred parking spaces for the multifamily component of the project. The additional parking spaces result in 607 total parking spaces, which is an increase from the 566 parking spaces originally proposed.

The Staff Report has been revised to note the changes to the development.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

This site is a 45.57-acre undeveloped tract of land located at the northwest corner of the intersection of K-10 Highway and Canyon Creek Boulevard. The site is an irregular shaped tract of land with limited access onto the public street network and extensively sloped terrain varying in approximately 60 feet of grade change. The site has approximately 570 feet of frontage along Canyon Creek Boulevard. The plan includes a new public street for access to the westernmost part of the site and to provide right-of-way to an adjacent undeveloped parcel. A 74-acre tract of City-owned property is to the north and east of the site. This land is the site of Cedar Station Park. The park includes a valley with a stream channel that abuts multiple residential subdivisions, crosses Canyon Creek Boulevard, and continues easterly.

LAND AREA (AC) 45.57	CURRENT ZONING AG CP-O CP-2	COMP. PLAN Office/Employment Center, Community Retail, and Suburban Density Residential
DWELLING UNITS (UPA) 9.62 (346 U/35.95 AC)	NURSING HOME 68,800 square feet & 80 beds	RETAIL SQUARE FEET (CONVENIENCE STORE WITH GASOLINE SALES) 6,100

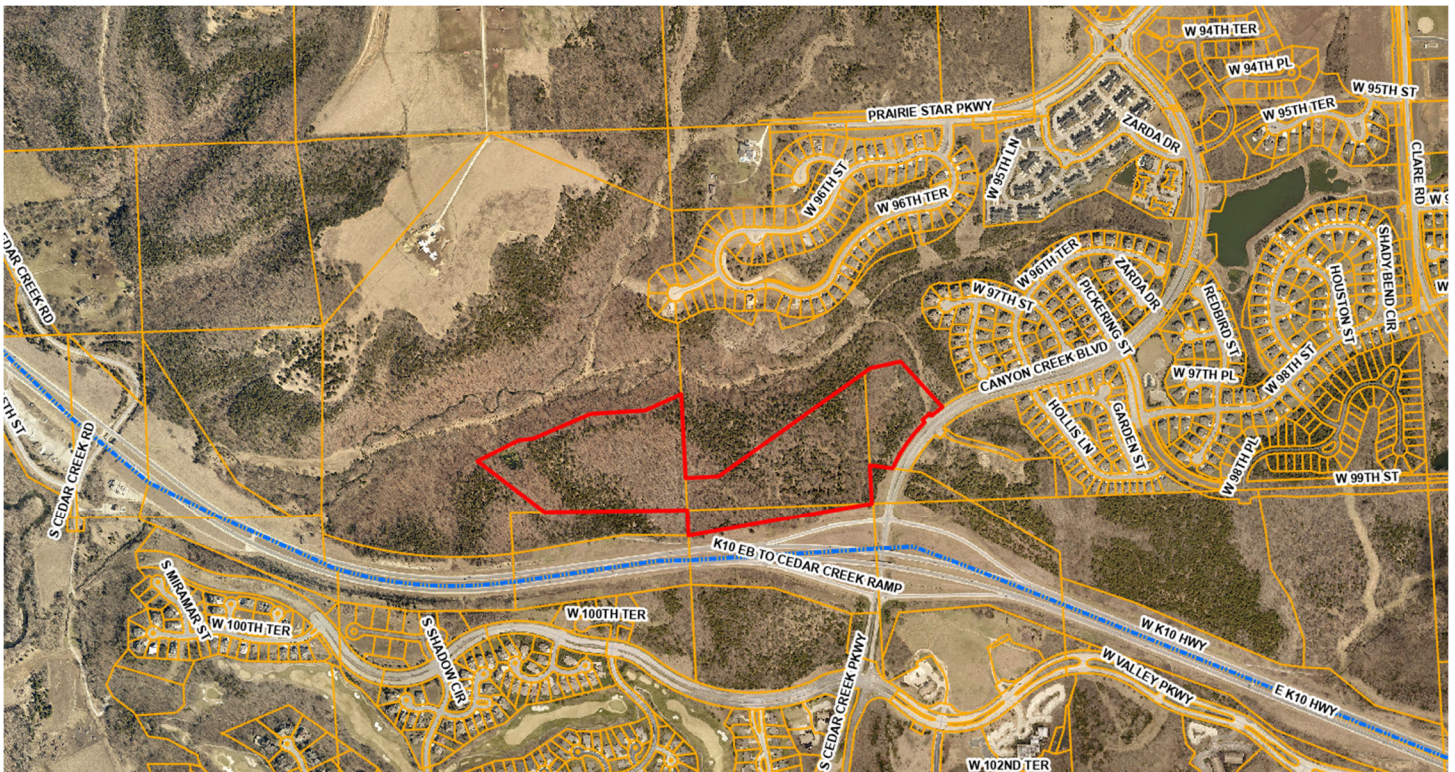


Exhibit 1: Aerial Image of Subject Site.

SITE HISTORY

The property was partially annexed into the incorporated limits of Lenexa in 1988 and was zoned AG by default. The remaining portion was annexed in 1999. In 2001, part of the site was rezoned to CP-O and CP-2 and a companion concept plan was approved (RZ01-07 & PL01-01CP). The site was originally part of a 490-acre development called Canyon Creek. This development included retail, office, apartment, duplex, and single-family residential development. The concept plan for this site did not show any buildings or parking areas but did note the total square footage for these uses. A total of 27,000 square feet of retail and 43,200 square feet of office uses were approved.

A rezoning, concept, and preliminary plan application was submitted for this site in the summer of 2018 (RZ18-05, PL18-04CP & PL18-11P). The proposed plan is shown in Exhibit 2. The concept plan was for a convenience store building in the same location as the proposed convenience store for the currently proposed plan. The preliminary plan was for a 12-building apartment development containing 294 units. The application was for 25 acres of land and did not include the westerly portion of site now included in the current application. Table 1 shows a comparison of the 2018 Preliminary Plan in relation to the current application.



Exhibit 2. 2018 Rezoning site plan.

TABLE 1: 2018 PLAN COMPARISON TO PROPOSED PLAN

	Zoning	Land Area	No. of Apartment Buildings	Total Units Per Acre (UPA)	Building Stories	C-Store Area	Nursing Home
2018 Plan	CP-2 & RP-4	25 acres	12	294 Units 13.36 UPA	Three-four-story	4,773 SF	NA
Proposed Plan	PUD	45.57 acres	22	346 Units 9.62 UPA	Two-three-story	6,100 SF	68,800 SF (Three stories)

Action taken on the 2018 applications is described in Table 2.

TABLE 2: 2018 REZONING ACTION

Date	Action
May 30, 2018	Application submitted
July 2, 2018	Planning Commission recommends approval
July 17, 2018	City Council remands back to Planning Commission
August 27, 2018	Planning Commission recommends approval of revised plans
September 18, 2018	City Council denies Rezoning, Concept Plan and Preliminary Plan

LAND USE REVIEW

The proposed use is a PUD, Planned Unit Development, containing apartment buildings, a nursing home, and a convenience store with gasoline sales. The proposed PUD is to establish a particular site layout, building design and coordinated concept for the overall project. Two examples of PUD projects in Lenexa are Vista Village (RZ15-06) at the southeast corner of Prairie Star Parkway and Ridgeview Road and Sonoma Plaza (RZ16-07) at the southeast corner of 87th Street Parkway and I-435 Interstate Highway.

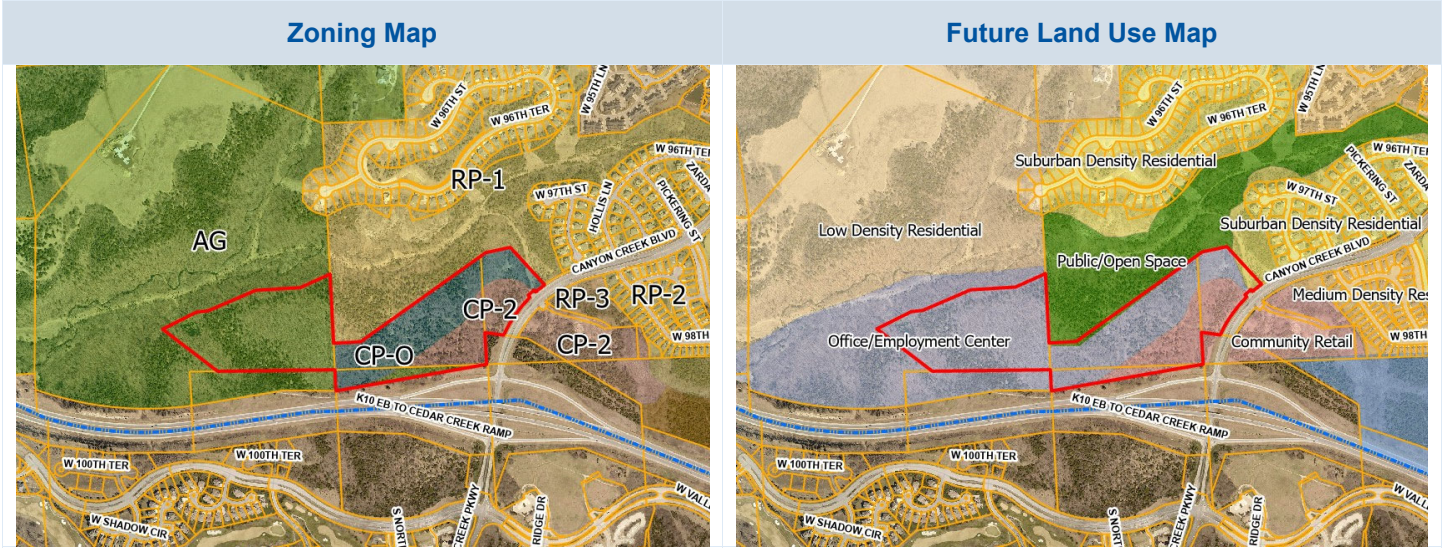


TABLE 3: COMPARISON OF SURROUNDING PROPERTIES

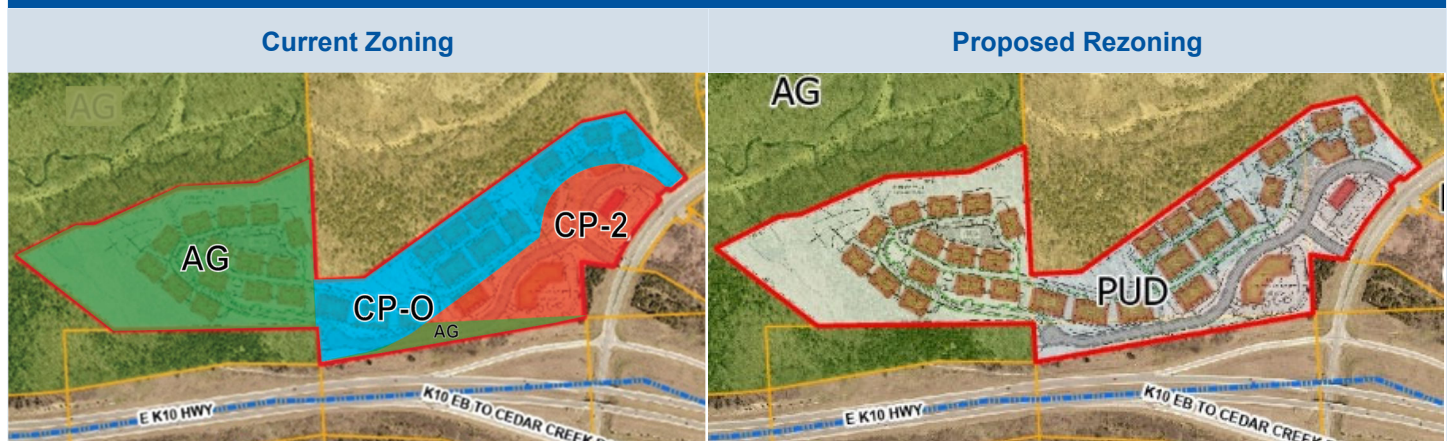
Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Office/Employment Center, Community Retail, and Suburban Density Residential	AG, Agricultural District, CP-O, Planned General Office District, and CP-2, Planned Community Commercial District	Undeveloped land
North	Office/Employment Center, Low Density Residential, and Public/Open Space	RP-1, Planned Residential Single-Family (Low-Density) District	Undeveloped land and Public park
South	Office/Employment Center	AG, Agricultural District and <i>City of Olathe (across K-10 Highway)</i>	Undeveloped land and <i>single-family residential in City of Olathe (across K-10 Highway)</i>
East	Community Retail, Medium Density Residential, and Suburban Density Residential	RP-1, Planned Residential Single-Family (Low-Density) District, RP-2, Residential Planned (Intermediate Density), and CP-2, Planned Community Commercial District	Undeveloped land and single-family residential
West	Office/Employment Center and Low Density Residential	AG, Agricultural District	Undeveloped land

REZONING REVIEW

The applicant proposes to rezone the subject site from the AG (Agricultural), CP-O, Planned General Office, and CP-2, Planned Community Commercial Zoning Districts to the PUD, Planned Unit Development District. The purpose statement of the PUD Zoning District states:

“It is the intent of the PUD District to provide flexibility from use and site development regulations in order to encourage innovative, well-designed projects that achieve a high level of environmental sensitivity, energy efficiency, safety, aesthetics and other community goals.”

TABLE 4: REZONING ANALYSIS



Staff provides the following analysis for the review criteria within [Section 4-1-G-5](#) of the Unified Development Code (UDC).

1. The character of the neighborhood.

The character of the neighborhood is a mix of existing residential development and undeveloped land. The site is at the intersection of a state highway (K-10 Highway) and an arterial street (Canyon Creek Boulevard). A substantial amount of the undeveloped land is adjacent to the K-10 Highway right-of-way. The sites that have developed, or are currently developing, are a mix of multifamily (apartment development, duplex residential) and single-family residential. The site to the east, across Canyon Creek Boulevard, was recently rezoned for both residential and nonresidential uses. The recently approved zoning changes include multifamily residential, commercial, office, and industrial zoning districts. This site is referred to as Cedar Canyon West (RZ22-09). A preliminary plan was approved for a six-building commercial development (PL23-08P) and final plans were recently approved for an apartment development (PL23-20F).

A large tract of City-owned land is immediately to the north of the subject site. This tract is Cedar Station Park/Mize Lake. The site contains a stream channel within a valley and includes an extensive wooded area. Cedar Station Park is to the west, north and east of the site. New site amenities and play equipment for the public park are nearing completion at Cedar Station Park.

2. The zoning and use of properties nearby.

The zoning and uses of the adjacent properties vary. The zoning and land use of adjacent properties is noted in Table 3.

3. The suitability of the subject property for the uses to which it has been restricted.

The property is currently restricted to commercial, office, and agricultural uses based on its current zoning. The property is suited for various types of multifamily and nonresidential development. The site is adjacent to the highway, therefore is not suitable for single-family development due to noise and other potential negative impacts from the nearby highway.

The Comprehensive Plan designates the site for office and commercial development. Nonresidential uses such as office and retail will have similar impact on vehicle trip generation within the area. As Staff noted, the site is at the intersection of an arterial street and K-10 Highway. The street network is sufficient to support land uses of greater intensity than suburban density development.

The site transitions in grade, descending from the southeast to the northwest. The grade descends approximately 50 feet from the highest elevation to the lowest elevation, from east to west. The site is also an irregular shaped parcel narrowing to approximately 230 feet wide at the middle of the site. The significant grade transition and narrow shape make the site difficult to develop with large-footprint building and surface parking areas commonly constructed for nonresidential buildings.

4. The extent to which the proposed use will detrimentally affect nearby property.

The proposed uses will not detrimentally affect nearby property of any greater impact than if the site were to be developed with office and retail uses as the site is now zoned. It is Staff's opinion the proposed uses are compatible with the existing and planned uses in the vicinity.

Traffic will not have to cut through any nearby subdivisions for access to the proposed development.

Similar uses and intensities were recently approved east of Canyon Creek Boulevard adjacent to K-10 Highway. The larger region north of K-10 Highway, toward Prairie Star Parkway, has several different land uses of various intensities. The subject site is anticipated for a use that is of a greater development intensity than suburban residential use.

There is adequate buffering between the proposed development and the next closest development. The subject site is separated from the existing single-family residential subdivisions to the north by City-owned parkland. The proposed apartment buildings are a minimum of 340 feet to the closest single-family residential lot line to the east, a minimum of 530 feet to the closest single-family residential lot to the north, and a minimum of 750 feet from single-family residential lots to the northwest (see Exhibit 3).

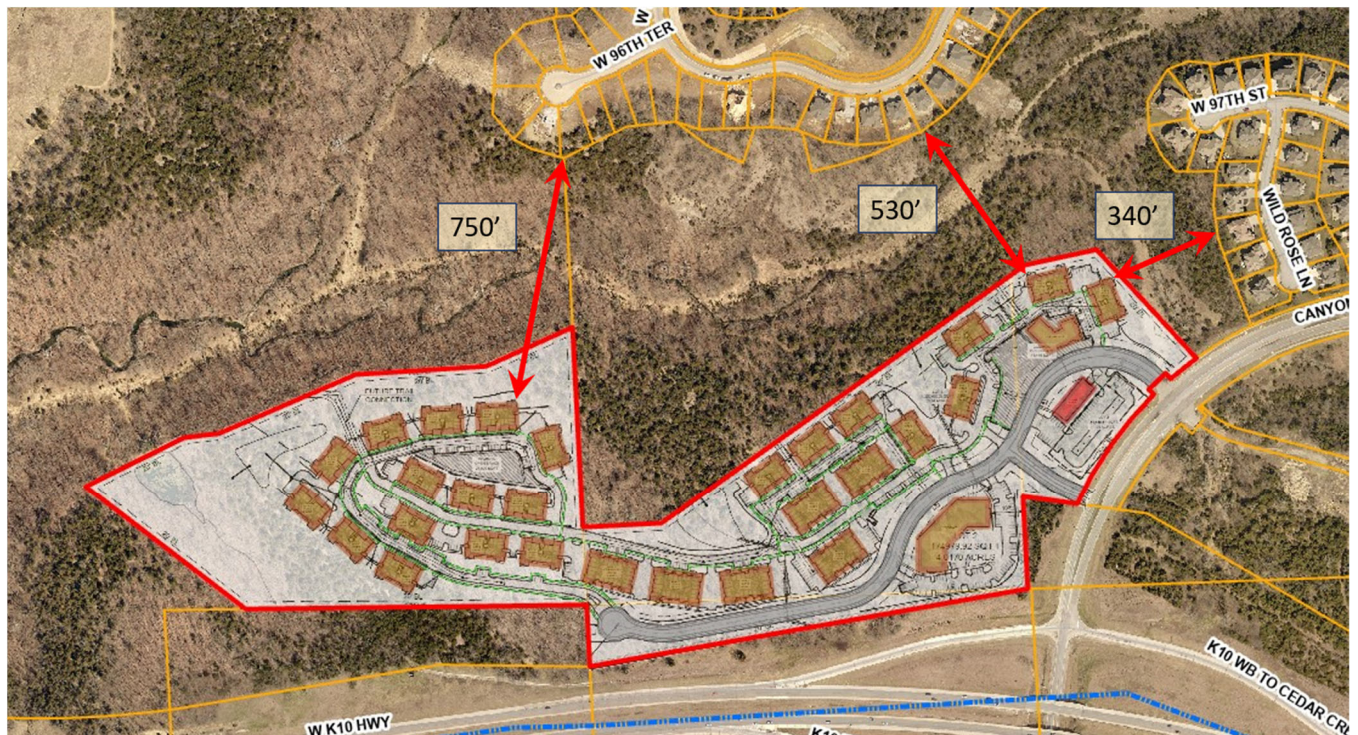


Exhibit 3: Proximity to adjacent development.

5. The length of time the subject property has remained vacant as zoned.

The property is undeveloped land and has been zoned AG since being annexed into the City. The property was partially annexed into the incorporated limits of Lenexa in 1988. The remaining portion of the site was annexed in 1999. A portion of the site was rezoned to CP-O and CP-2 in 2001 but the development associated with the rezoning never came to fruition.

6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.

It is Staff’s opinion that denial of this rezoning would have no gain to public health, safety, or welfare since the proposed development’s density and uses are compatible with surrounding development and appropriate infrastructure is available to serve the site. Denial of the application would restrict the property to the existing zoning of CP-2, CP-O, and AG in a market where office uses appear to be less viable for development for the foreseeable future.

7. Recommendation of City's permanent professional staff.

See Staff's recommendation and the end of this report.

8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.

The Future Land Use (FLU) Map designation for the site is Office/Employment Center and Community Retail and includes a sliver of Suburban Density Residential. The City is nearing completion of a major Comprehensive Plan update. The update includes studying several undeveloped areas of the City which are currently designated as Office/Employment Center. The market for office-park type of land uses has changed over time and office parks, such as Corporate Woods in Overland Park, are not common in the current market. Office development is not typically built speculatively in today's market. Most office uses are purpose-built development for a specific building tenant. The subject site and the site on the east side of the K-10 Highway and Canyon Creek Boulevard intersection were both studied to consider what may be reasonable alternatives to the Office/Employment Center FLU Map designation. The probable alternative to an office building development is multifamily residential development.

Multifamily, or apartment, development is often relegated to areas designated for Medium Density Residential development on the FLU Map. The proposed apartments are not consistent with the designations shown on the FLU Map within the Comprehensive Plan. However, multifamily uses are contextually appropriate for the subject site because the site provides proximity to the major street network and a multifamily use provides a buffer between K-10 Highway and existing single-family residential subdivisions.

The applicant requests that a portion of the site be developed as a convenience store with gasoline sales, which is a use that is common in regions of the City that are designated for Community Retail uses. The nursing home component of the development is consistent with medical office type uses. Nursing homes and convalescent homes are often associated with or are immersed within office building developments and office or commercial uses.

9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.

The site is in a developing area of the City. Several nearby properties are either developed, developing, or have a plan approved for future development. Adequate utilities and services are available to the subject property. The site is subject to the City's stormwater management requirements which are applicable to all development in the City. The site is within the Olathe School District.

10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.

It is Staff's opinion the proposed use will not adversely impact the capacity or the safety of the street network or present a parking problem in the vicinity of the site. The site abuts Canyon Creek Boulevard, an arterial street, to the east, and is in close proximity to K-10 Highway.

Off-street parking requirements are shown on Table 5.

TABLE 5: PARKING ANALYSIS			
Use	Requirement	Required	Provided
Multifamily	1 space per efficiency unit, 1.5 spaces per 1-bedroom unit, 1.75 spaces per 2-bedroom unit, 2 spaces per 3+-bedroom units and 0.25 spaces per unit for visitor parking if parking spaces are located in common parking area	635	607 (41 deferred)
Nursing Home	1 space per 3 beds, and 1 space per employee	47	89
Convenience Store with Gasoline Sales	1 space per 250 square feet, minimum of 5 spaces	24	39
TOTAL		706	735

The plan includes 41 deferred parking spaces. Per [Section 4-1-D-1-J](#) of the UDC, deferred parking is permitted provided the location of the deferred parking spaces is shown on a plan and the plan is approved by the Planning Commission. The applicant requests a deviation to allow 28 fewer parking spaces than what is required for multifamily residential uses per [Section 4-1-D-1-C](#) of the UDC.

More parking is provided for both the nursing home and convenience store components of the development than what is required. The result is a net of 29 more parking spaces than what is required for the combined uses. The additional parking spaces are not conducive for the residents and visitors of the apartments to use; therefore, Staff concludes that a deviation is still required for the apartment component of the PUD. This deviation request is discussed in more detail in the Deviations section of this report.

11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.

The proposed PUD is not anticipated to generate any environmental impacts exceeding the requirements of the UDC.

12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.

The site is subject to the UDC requirements for stormwater management and is required to meet the same standards as any new development.

13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.

The preliminary plan is in compliance with the UDC requirements for the PUD Zoning District. The applicant requests four deviations, which are summarized below and are discussed in more detail within the Deviations section of this report.

1. [Section 4-1-D-1-C](#) of the UDC lists the parking requirement for multifamily development. The applicant is requesting to reduce the total number of parking spaces provided with the apartment component of the development.
2. [Section 4-3-C-3](#) of the UDC states that a convenience store is not to exceed 5,000 square feet in floor area. The applicant is requesting the convenience store be 6,100 square feet in floor area.
3. [Section 4-1-B-26-C-1](#) of the UDC states the minimum setback from freeway right-of-way is 100 feet. [Section 4-1-D-2-L](#) of the UDC requires a 100-foot landscape buffer along freeway right-of-way. The applicant is requesting a reduction of the setback therefore a reduction to the landscaping requirement along the freeway.
4. [Section 4-1-D-1-N-1](#) of the UDC states that 50-foot queue space is to be provided from each end of a gasoline pump island. The applicant is requesting deviations to reduce the setback of the parking area for the nursing home from K-10 Highway and to reduce the queue space from around the gas pump island of the convenience store.

A deviation request may be considered using the criteria listed in [Section 4-1-B-27-G-4](#) of the UDC.

PRELIMINARY PLAN REVIEW

The subject site is located at the northwest corner of K-10 Highway and Canyon Creek Boulevard. The site has frontage onto Canyon Creek Boulevard for access to a public street and abuts City-owned parkland northerly along the east half of the site. A six-acre undeveloped parcel exists south of the west half of the site. A new public street is proposed to connect to Canyon Creek Boulevard and extend westerly through the site and terminate in a cul-de-sac. The new public street will provide access to the apartments, nursing home, convenience store, and to the six-acre undeveloped parcel to the south of this development.

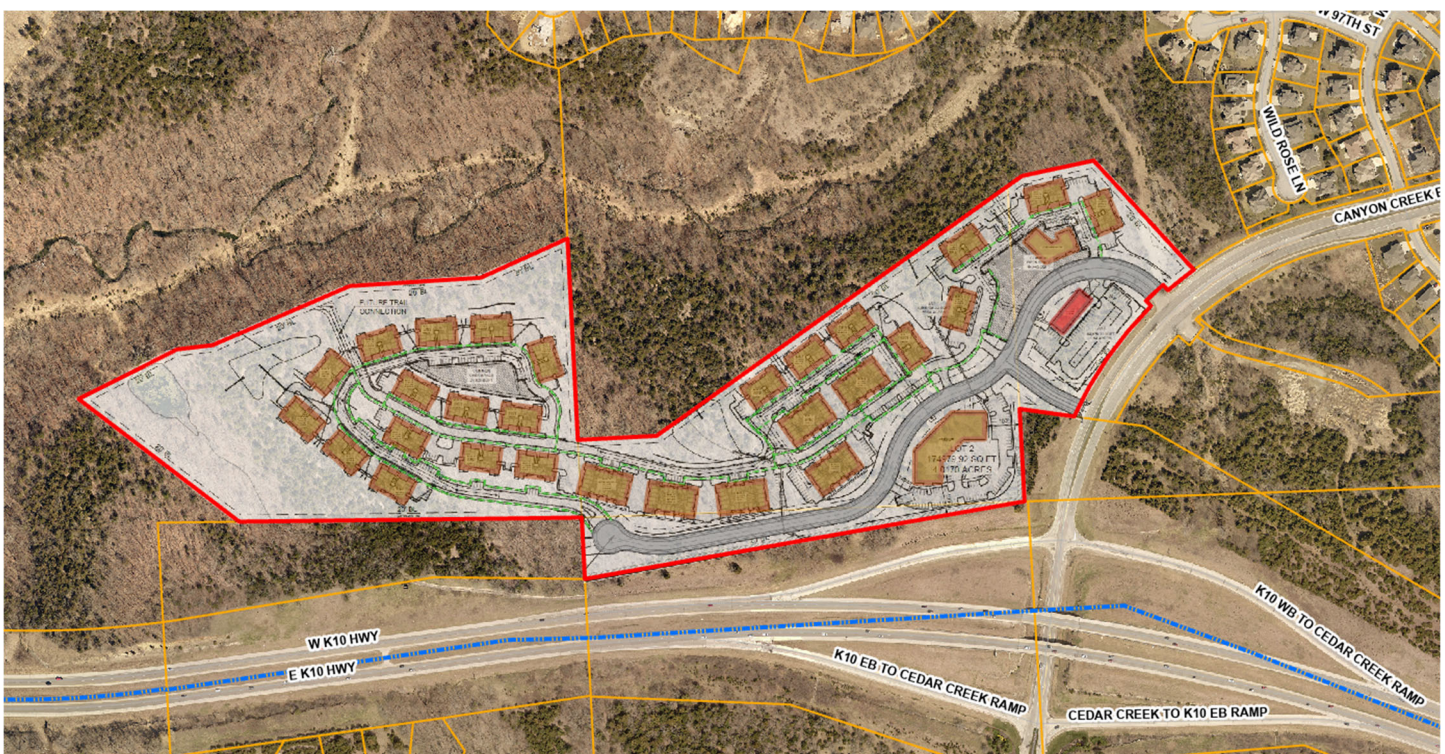


Exhibit 4: Site Plan.

The PUD consists of three components, the largest of which is multifamily residential. This component contains 346 dwelling units distributed among 22 buildings on 35.95 acres. The density of the apartments is 9.62 units per acre. Per [Section 4-1-B-20-C-5](#) of the UDC the maximum density of the residential portion of a mixed-use PUD is 16 units per acre. The apartment component incorporates two different building types, a 12-unit building and 14-unit building. Most of the buildings are two-story, some are a two-three-story split building where the grades transition and result in the lower level of the buildings being exposed.

The other components are a nursing home building on four acres and a convenience store with gasoline sales on two acres. The nursing home component is a three-story, 68,800 square-foot building containing 80 residential dwelling units. The convenience store is a one-story, 6,100 square foot building. The nursing home and convenience store are at the southeasterly part of the site. The convenience store is on a part of the site that is currently zoned to allow this use. The nursing home site is currently zoned CP-O. Per [Section 4-3-C-2](#) of the UDC a *nursing home* is a use distinct from multifamily residential. The use is categorized a *public or civic use*. A nursing home is allowed in the CP-O Zoning District with a special use permit.

Access for the site is provided by a new public street. The street intersects with Canyon Creek Boulevard, across from 100th Street, which is a new street that will be constructed for the development on the east side of Canyon Creek Boulevard. A short street segment is on the southerly side of the convenience store to provide additional circulation for the site. This shorter section of street will have a right-in-right-out only movement onto Canyon Creek Boulevard.

The new street will provide access to the remaining six-acre private parcel on the north side of K-10 Highway. The plan shows the street ends in a cul-de-sac bulb. The edge of the cul-de-sac bulb is approximately 30 feet from the property line. The 30-foot gap from the cul-de-sac bulb to the adjacent property is dedicated as right-of-way for the continuation of the public street.

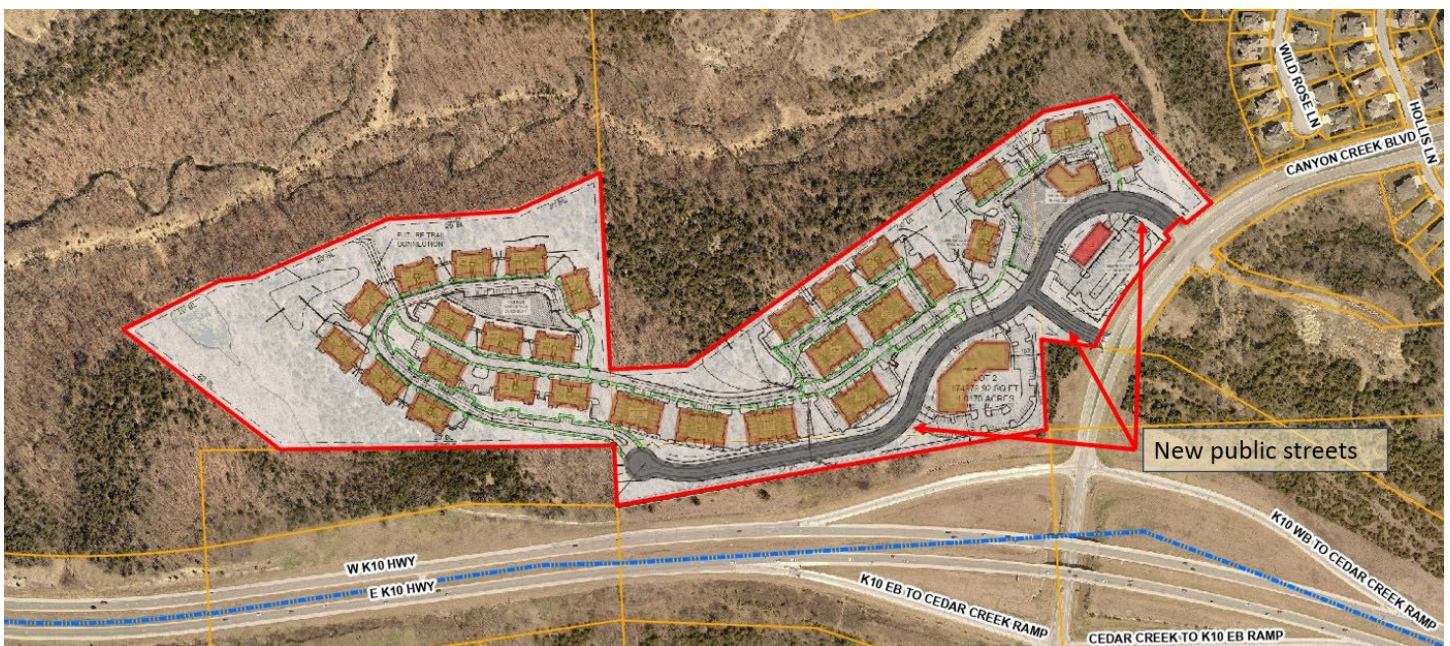


Exhibit 5: New public streets.

The applicant submitted a preliminary stormwater management study identifying the stormwater measures proposed to meet the City’s requirements. These measures include three dry detention basins, a hydro-dynamic separator, as well as preserved native vegetation.

The multifamily residential component of the development is comprised of clusters of apartment buildings arranged along a main drive through the development. The irregular shape of the site and the extent of the grade change from southeasterly to northwesterly dictates where building sites may be located. This results in three main clusters of buildings. The community clubhouse is located within the easternmost cluster of buildings.

The apartment buildings are configured to have an attached garage space for each apartment unit. The apartment buildings have “driveway courts” between buildings for maneuvering into and out of garages. Additional parking is provided in small lots of four to twelve parking spaces throughout the development.

The nursing home component is located on the southerly side of the new public street, a double-loaded row of parking is between the building and K-10 Highway right-of-way. A drop-off court is provided in a central location of the building. The convenience store is oriented toward Canyon Creek Boulevard. The gas pump canopy is between the building and the street. Parking areas are provided close to the building. Both the nursing home and the convenience store require further detail at the final plan stage. These details include drive entrances, placement of the trash enclosures, sidewalks, pedestrian-oriented features, and landscaping.

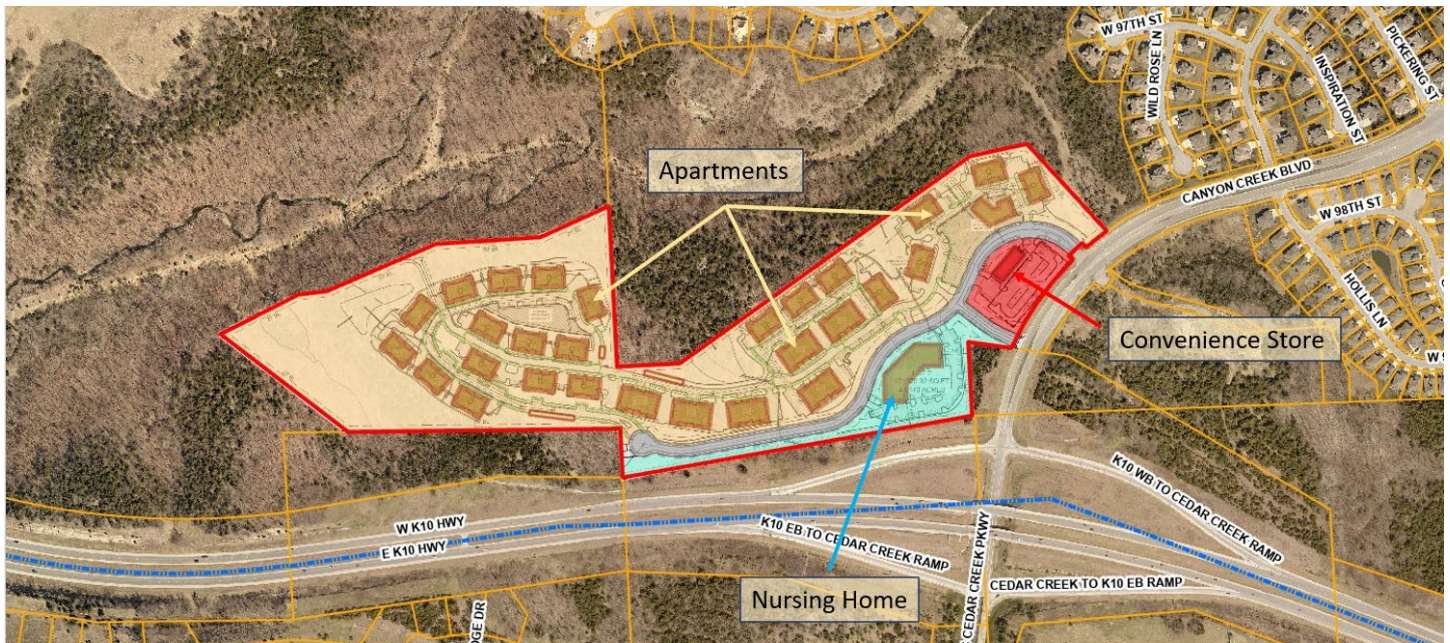


Exhibit 6: PUD Components.

As part of the PUD, a managing document is to be provided to ensure development consistency among all the different components. The applicant provided a Design Guidelines document that lists several controlling features of the development including, but not limited to, building design, landscaping, signs, and lighting. The part of the Design Guidelines document regarding signs for the development will be reviewed in greater detail prior to the issuance of any sign permits for the project. Per [Section 4-1-E](#) of the UDC, the number, size, and placement of signs are subject to the sign regulations. This document will be used to review any subsequent plans submitted as part of the overall development.

The PUD is expected to incorporate interconnecting features throughout the development to establish continuity among the various components. These types of features may include site furniture, wayfinding features like street signs and numbers, and a site-wide pedestrian network. A 10-foot-wide sidewalk is provided along the main internal drive of the development. This pedestrian link starts at the clubhouse and extends to the farthest west apartment building. Sidewalks are provided from the main walk to the doors of every apartment building.

Sidewalks also connect to the nursing home and convenience store buildings. The specific design and placement of sidewalks will be reviewed in greater detail for each component with a final plan.

The plan shows a trail connection from the apartments to the trail in Cedar Station Park. Staff requested a second trail connection be provided on the easterly side of the development, closer to the clubhouse. Specific trail locations and connections are a detail that can be reviewed and coordinated with the applicant at final plan stage.

DEVIATIONS

The applicant requests four deviations from the requirements within the UDC. The deviations are noted in Table 6 and summarized below. The Planning Commission has the authority to approve deviations if the criteria from [Section 4-1-B-27-G-4](#) of the UDC are met.

1. [Section 4-1-D-1-C](#) of the UDC lists the parking requirement for multifamily development. The applicant is requesting to reduce the total number of parking spaces provided with the apartment component of the development.
2. [Section 4-3-C-3](#) of the UDC states that a convenience store is not to exceed 5,000 square feet in floor area. The applicant is requesting the convenience store be 6,100 square feet in floor area.
3. [Section 4-1-B-26-C-1](#) of the UDC states the minimum setback from freeway right-of-way is 100 feet. In addition to the special setback, [Section 4-1-D-2-L](#) of the UDC requires a 100-foot landscape buffer along freeway right-of-way. The applicant is requesting a reduction of the setback therefore a reduction to the landscaping requirement along the freeway.
4. [Section 4-1-D-1-N-1](#) of the UDC states that 50-foot queue space is to be provided from each end of a gasoline pump island. The applicant is requesting deviations to reduce the setback of the parking area for the nursing home from K-10 Highway and to reduce the queue space from around the gas pump island of the convenience store. A deviation request may be considered using the criteria listed in [Section 4-1-B-27-G-4](#) of the UDC.

TABLE 6: REQUESTED DEVIATIONS

Deviation	Requirement	Proposed	Difference
Multifamily Parking	635 spaces for multifamily residential	607 spaces	-28 spaces
Convenience Store Floor Area	Not to exceed 5,000 SF	6,100 SF	1,100 SF
Freeway Special Setback	100 feet	28 feet	72 feet
Gasoline Pump Island Queue Space	50 feet from each end of pump island	25 feet	25 feet

MULTIFAMILY PARKING

The applicant justifies this reduction by stating the parking demand for this type of apartment product is closer to 1.5 stalls per dwelling unit, which results in a parking demand of 519 spaces for 346 units leaving 88 spaces available for guest parking. A total of 87 guest parking spaces are required per [Section 4-1-D-1-C](#) of the UDC. The deviation request results in a parking ratio of 1.64 spaces per unit based on 566 stalls provided and a ratio of 1.75 spaces per unit based on 607 parking spaces, when including the 41 deferred parking spaces. The parking ratio for the required 635 parking spaces is 1.84 spaces per unit.

It is Staff’s opinion a reduction from the parking requirement for multifamily development may be warranted based on certain factors such as the mix of unit types, distribution of surface parking spaces throughout the site, and the provision of garages for each unit. The applicant has provided an analysis of a similar development which has the same unit mix, garage spaces per unit, and distribution of surface parking spaces. The applicant’s analysis shows that similar apartment developments have a surplus of parking even though the developments have a lesser parking ratio and concludes this development will satisfy the peak parking demand with the requested reduction of 28 spaces and the 41 deferred parking spaces.

Per [Section 4-1-D-1-J](#) of the UDC, deferred parking may be shown on the plan and approved by the Planning Commission. The deferred parking is shown in a central area of the site. The surface parking is evenly distributed throughout the site providing spaces for each building. The developer will be required to enter into a deferred parking agreement that will require the deferred parking to be constructed when determined by the City the parking is necessary to address parking issues. A condition for the applicant to submit a statement to enter into a deferred parking agreement with the City will be included with a Final Plan for any portion of the apartment buildings. Staff supports the deviation request for 607 parking spaces where 635 parking spaces are required.

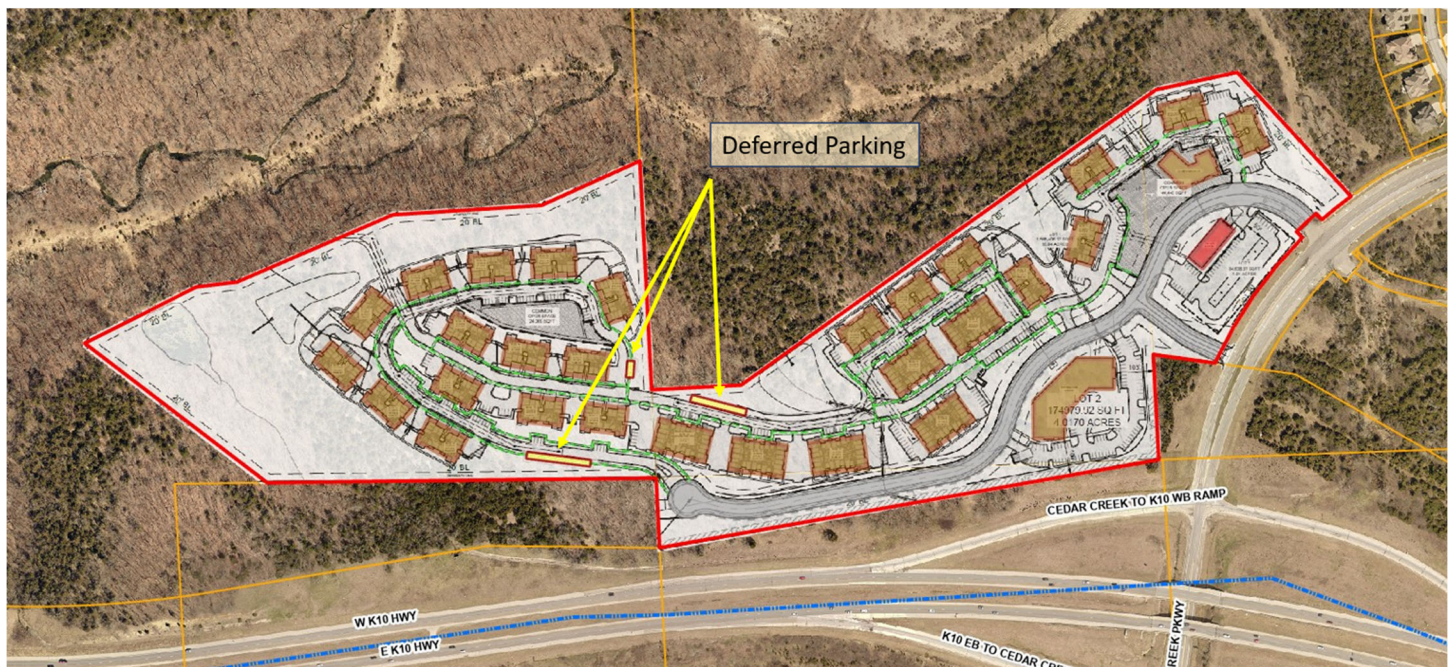


Exhibit 7: Deferred Parking.

CONVENIENCE STORE FLOOR AREA

The request for a convenience store with gasoline sales that exceeds the 5,000 square feet of gross floor area limitation is reasonable. A modern convenience store provides multiple goods and often includes prepared foods unlike the operation of convenience stores of previous decades. A modern convenience store is more like a retail and service type of use, not exclusively for sales of snack items. Staff supports the deviation request for a 6,100 square-foot convenience store.

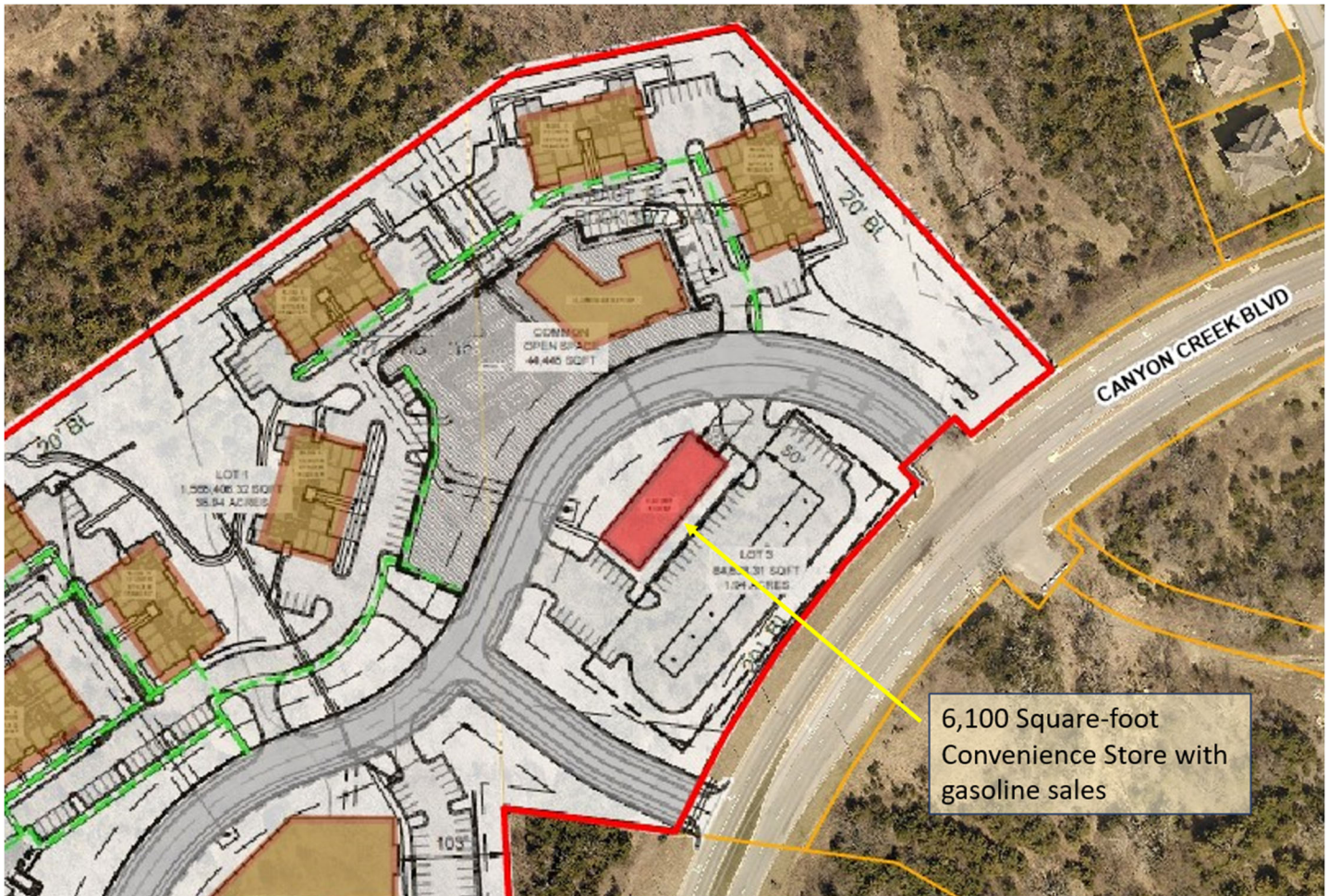


Exhibit 8: Convenience Store Component.

FREEWAY SETBACK

The deviation request to reduce the 100-foot special setback along K-10 Highway is reasonable. The plan shows the parking area for the nursing home to encroach into this setback. The purpose of the setback is to reduce the adverse impact from road noise on development adjacent to the highway.

This site is at the intersection of Canyon Creek Boulevard and K-10 Highway. All buildings within this development will be outside of the 100-foot setback. The parking lot for the nursing home component is shown as close as 28 feet from the property line. The development is next to an on-ramp where the highway right-of-way is particularly wide and the through lanes of the highway range from 280 feet to 330 feet from the property line. In addition to the freeway special setback, the UDC states landscaping is to be installed within the 100-foot space. By virtue of the reduced setback, the landscape buffer would be reduced as well. Staff supports the deviation request to reduce the 100-foot freeway special setback and the 100-foot landscape buffer to 28 feet at the closest point and allow the parking lot and drive aisles to encroach.

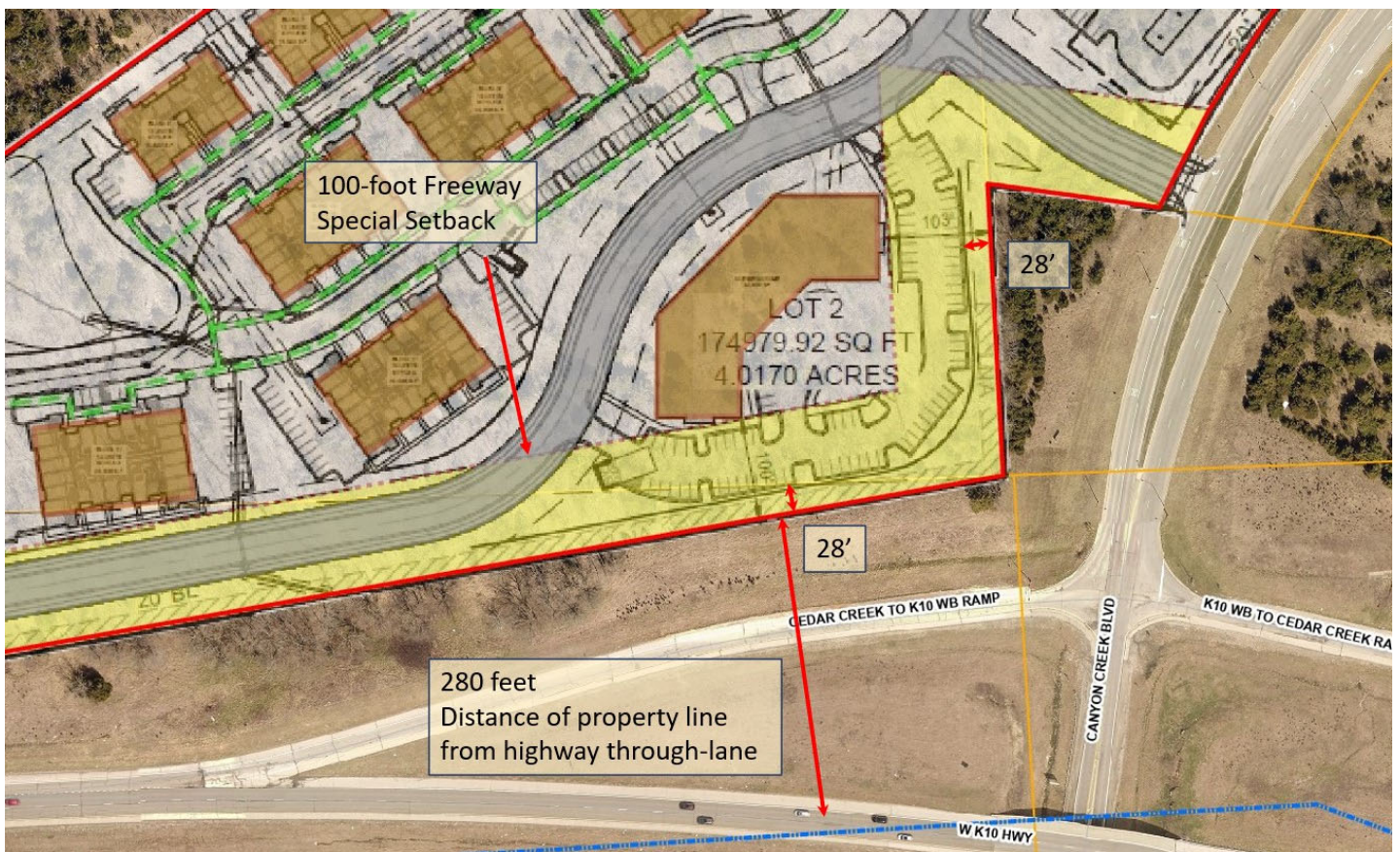


Exhibit 9: Special setback encroachment.

GASOLINE PUMP ISLAND QUEUE SPACE

The third deviation is to reduce the queue width requirement of 50 feet from the end of the gasoline pump island. This deviation is a common request for convenience stores with gasoline sales. Nearly all convenience stores with gasoline sales in Lenexa have requested a similar reduction to this requirement. The additional space is intended to provide adequate room for vehicles to line up for a gas pump and still allow traffic to circulate around the pump island within the parking lot. The gasoline fueling island or canopy area which covers the individual gas pumps is typically a larger structure than previous generations of gas stations. These sites provide adequate space for vehicles to line queue behind a vehicle while still allowing other vehicles to enter and exit the site.

The requirement of 50 feet is not necessary for a modern convenience store with gasoline sales where sufficient space is provided and the number of gas pumps available reduce the need to queue behind a vehicle while fueling. Staff supports the deviation request to reduce the 50-foot queue space for the gas pump island to a minimum of 25 feet.

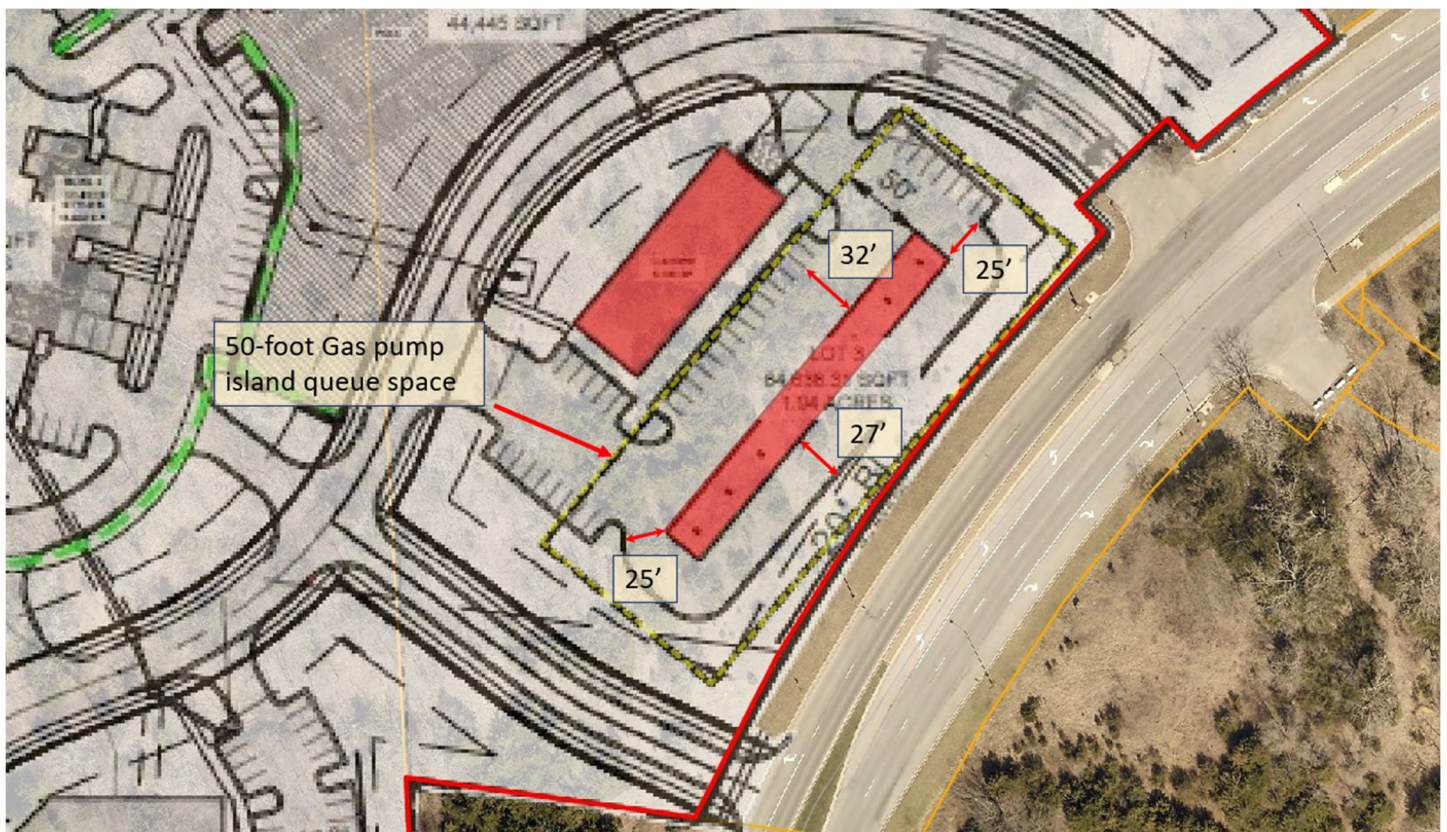


Exhibit 10: Gas pump island queue space encroachment.

ARCHITECTURE

The development is of a traditional architectural character. The apartment buildings use stone, horizontal lap siding, and stucco materials, which are all common materials for residential buildings. The nursing home and the convenience store use the same material palette and introduce brick. The apartment buildings use pitched roof forms and composite asphalt shingles. The apartment buildings have several articulations to the wall plane which create multiple shadow lines and offsets. The buildings have a complex roof line with several gable elements of varying sizes. The materials are used to strategically mass certain elements and create visual breaks along the facades. The apartment buildings are predominantly two-story buildings. Some buildings will have an exposed basement level and will appear three-stories from one side. It is common for both single-family and apartment buildings to appear two-story from one side and three stories from another. Building height is measured by calculating the average height of all sides of the building. The two-story facing sides of the apartment buildings are 30 feet tall.



Exhibit 11: Apartment building elevations.

The PUD Zoning District does not state a maximum building height for buildings. [Section 4-1-B-20-C-6](#) states the following:

“A PUD shall be harmonious and not conflict with surrounding neighborhoods and existing natural features. It shall be planned, designed and constructed so as to avoid undue traffic congestion in the surrounding area and provide a compatible land-use relationship with the surrounding area, making use of landscaping, screening, natural streamways and storm water management, open space and the placement of buildings where appropriate in accordance with land-use planning and design principles.”

The apartment buildings are the same height as a typical single-family home with the basement exposed. The following section shows the comparison of the apartment buildings to single-family homes across the City-owned parkland.

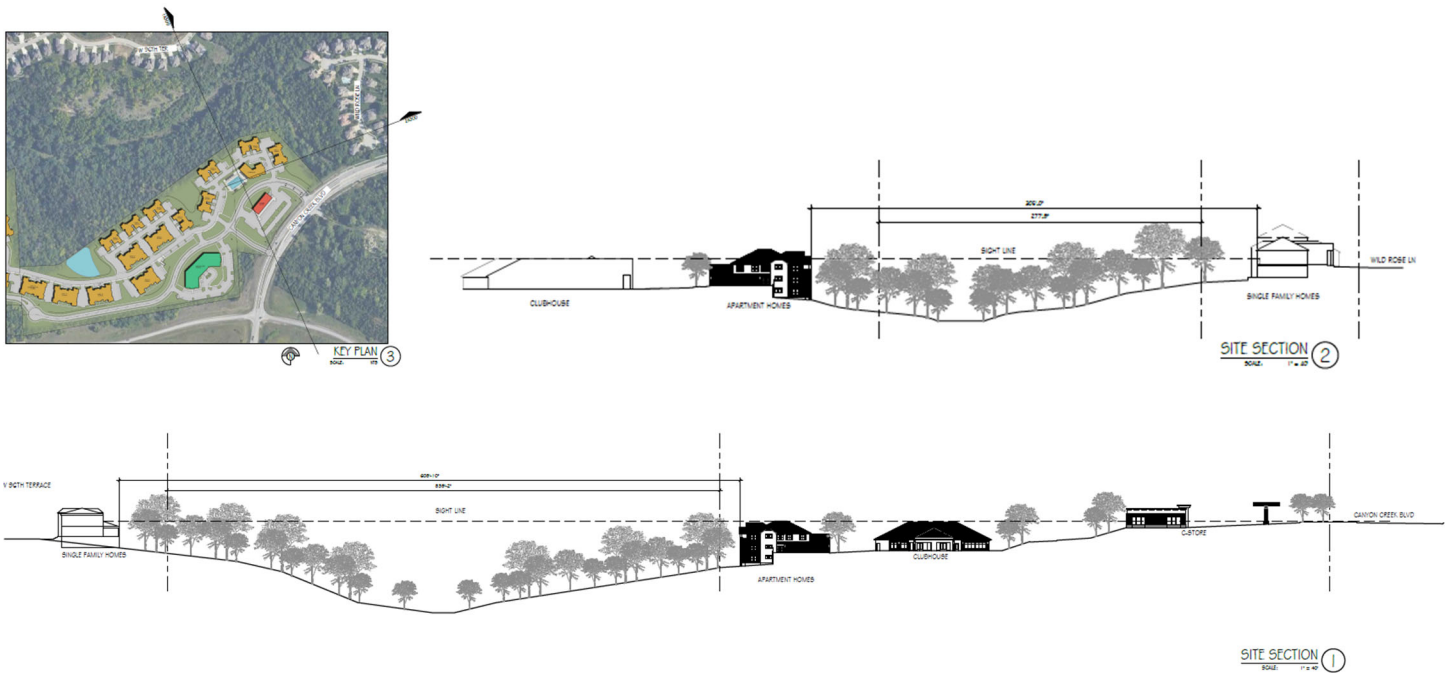


Exhibit 12: Site sections.

The nursing home building and the convenience store use a significant amount of brick and stone material and reflect a commercial character. Both buildings use a flat roof. The nursing home is a larger building and incorporates several more articulations to the wall plane. The nursing home is three stories tall which is 37 feet 9 inches at the tallest part of the building. The nursing home building is closest to K-10 Highway than the apartments or the convenience store.



Exhibit 13: Nursing home and convenience store elevations.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of final plan and building permit documentation submittal.

LANDSCAPING

The landscape plan shows trees and shrubs installed around the site perimeter boundaries, street frontages, and the parking lot. The landscape plan includes a request for a deviation from the freeway buffer landscape requirement. The west, north and northeasterly property line of the development is adjacent to City parkland. The south property line abuts an undeveloped six-acre tract of land which is designated for Office/Employment Center. The land uses of the adjacent property results in the site not requiring a Land Use Intensity (LUI) landscape buffer requirement per [Section 4-1-D-2-N](#) of the UDC.

Additional perimeter screening of a PUD is required per [Section 4-1-B-20-C-12](#) of the UDC. Staff recommends an enhancement of the landscape screening along the part of the site closest to the neighboring single-family development be provided. These details will be reviewed at final plan stage.

Detailed landscape designs are typically not shown with the preliminary plan. The preliminary plan will show the applicant's intent to meet the numerical quantity and placement requirements of the UDC. The landscape plan does not show details of the landscaping around the apartment buildings, nursing home, or the convenience store. Landscaping close to the buildings and in clustered planting areas will be shown with the final plans for any part of the development.

The site is currently extensively wooded. The landscape plan shows preservation of the areas of the site that do not contain buildings, parking, or grading. The applicant intends to apply this existing landscaping to the required perimeter planting requirements. Per [Section 4-1-D-2-I](#) of the UDC allows credit for existing trees that are to be preserved.



Exhibit 14: Preserved trees.

PRELIMINARY PLAT

A preliminary plat was provided with the preliminary plan. The plat shows three lots, right-of-way dedication for public streets, and utility dedications for sanitary and storm sewers. Each of the three components of the development, the multifamily residential, nursing home, and convenience store will be contained within their respective lot. The preliminary plat complies with the subdivision requirements of [Section 4-2](#) of the UDC.

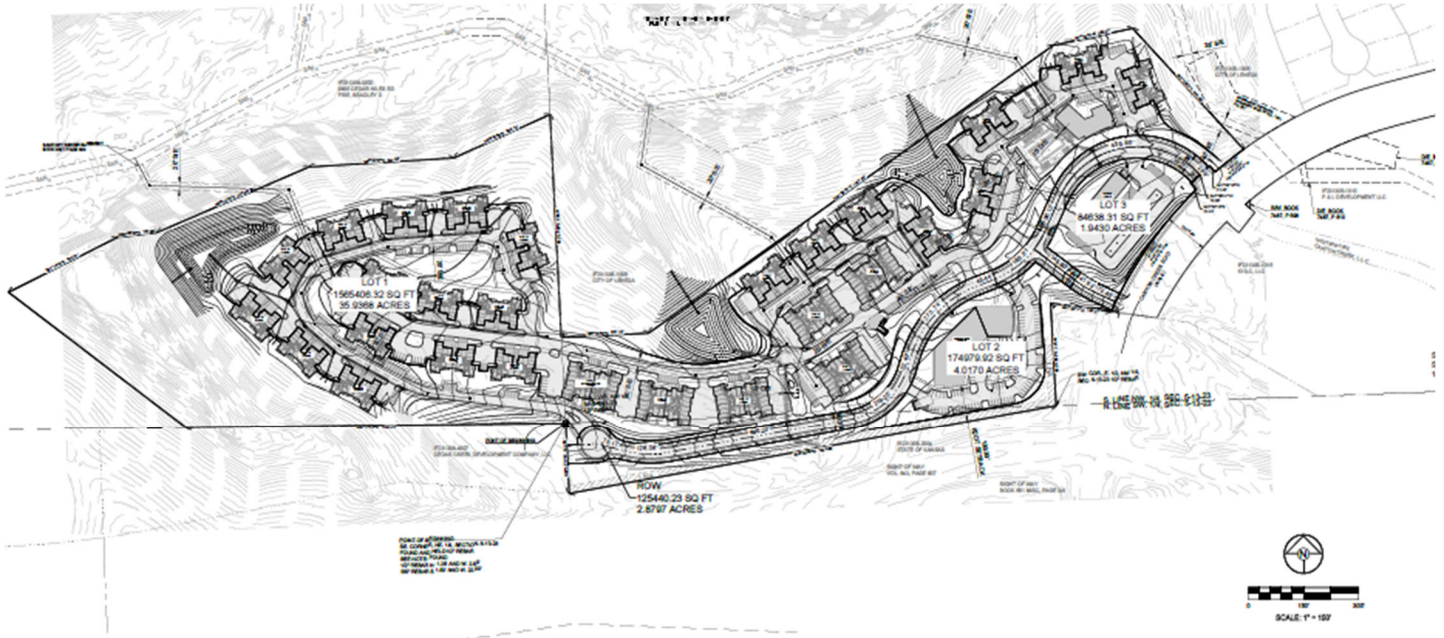


Exhibit 15: Preliminary Plat.

REVIEW PROCESS

- This project requires a recommendation from the Planning Commission and approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration by the City Council on February 20, 2024.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

- ★ **Conduct a Public Hearing for the rezoning request.**
- ★ **Staff recommends approval of the proposed rezoning and preliminary plan for Canyon Ridge Apartment Homes.**
 - The project is consistent with Lenexa’s goals through *Responsible Economic Development* to create *Vibrant Neighborhoods* and a *Thriving Economy*.

REZONING

Staff recommends **approval** for rezoning property from AG, CP-O, and CP-2 to PUD for **Canyon Ridge Apartment Homes** located at the northwest corner of K-10 Highway and Canyon Creek Boulevard.

PRELIMINARY PLAN/PLAT

Staff recommends **approval** of the preliminary plan/plat for **Canyon Ridge Apartment Homes** located at the northwest corner of K-10 Highway and Canyon Creek Boulevard for a mixed-use PUD including multifamily residential, nursing home, and convenience store with gasoline sales uses, with the following deviations:

1. A deviation from Section 4-1-D-1-C of the UDC to allow a reduction in the total number of parking spaces of 28 parking spaces from the minimum required 635 spaces for the apartment component of the PUD.
2. A deviation to from Section 4-3-C-3 of the UDC to allow a convenience store of 6,100 square feet in area, exceeding the maximum allowed 5,000 square-foot floor area.
3. A deviation from Sections 4-1-B-26-C-1 and 4-1-D-2-L of the UDC to allow a setback of 28 feet, a reduction of 72 feet from the 100-foot freeway special setback and the 100-foot landscape buffer along K-10 Highway.
4. A deviation from Section 4-1-D-1-N-1 of the UDC to allow a vehicle queue area of 25 feet, a reduction of 25 feet from the 50-foot queue area from the ends of a gas pump island.

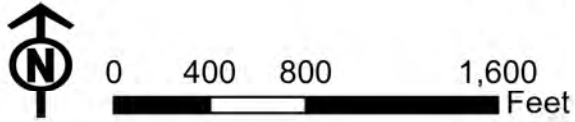


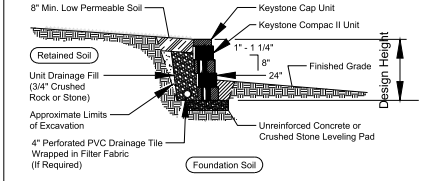
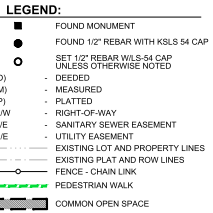
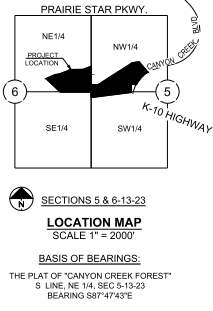
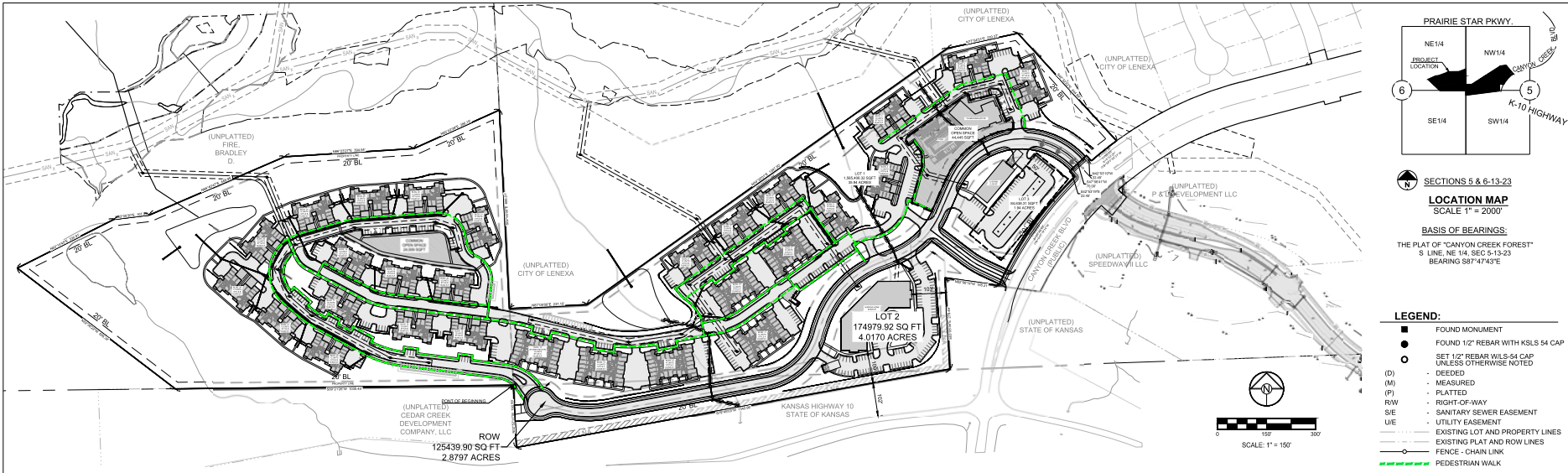
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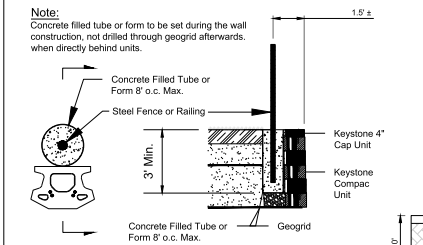
Canyon Ridge Apartment Homes

RZ23-07 & PL23-12P

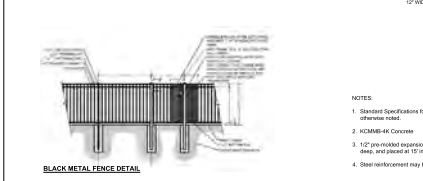




Typical Gravity Wall Section
Compact Unit - 1" Setback



Fence Plan Detail
Fence Section & Plan Detail
Compact Unit - Near Vertical Setback



BLACK METAL FENCE DETAIL

PRIVATE CONCRETE CROSSWALK DETAIL

- NOTES
- Standard Specifications for State Road and Bridge Construction, Kansas Department of Transportation, latest edition, are incorporated except as otherwise noted.
 - KCM&K Concrete
 - 1/2" pre-encased expansion joints shall be placed at points of curvature, curb returns, curb ends, and at 250' centers. Construction joints shall be 2" deep and placed at 10' intervals evenly spaced between expansion joints. KCM&K concrete shall be used throughout unless otherwise noted.
 - Steel reinforcement may be omitted from curb & gutter placed on a minimum of 4" of granular concrete.

LOT 1 SITE DATA (MULTIFAMILY)

LOT AREA	38.96 AC (1,668,406.32 S.F.)
NUMBER OF UNITS PROPOSED	346 UNITS
PROPOSED DENSITY	9.62 DU PER ACRE
SITE AREA PER DWELLING UNIT PROPOSED	4,524.28 S.F. (1,955,406.37 SQ. FT.)
BUILDING AREA PROPOSED	203,897 S.F.
PARKING LOT AND DRIVE AREA PROPOSED	292,090 S.F.
TOTAL COVERAGE AREA PROPOSED	495,987 S.F.
OPEN SPACE REQUIRED BY CODE(80% OF NET AREA)	21.56 AC (939,243.79 S.F.)
OPEN SPACE PROVIDED	1,026,419.93 S.F. (36.31 AC)
PARKING LOT AREA	33,755 S.F.
PARKING LOT INTERNAL LANDSCAPE AREA REQ. BY CODE 4-1-D-3-M-1	1,851.87 S.F. (10%)
MINIMUM HEIGHT PROPOSED	35 FT.
MINIMUM SETBACKS FROM STREETS PROPOSED	BUILDING 20' PARKING 10'
MINIMUM SETBACKS REAR PROPOSED	BUILDING 20' PARKING 10'
MINIMUM SETBACKS OTHER PROPOSED	BUILDING 7' PARKING 10'

PARKING DATA

PARKING REQUIRED FOR 1 BEDROOM UNITS (20 UNITS X 1.5 SPACES PER UNIT)	345 SPACES
PARKING REQUIRED FOR 2 BEDROOM UNITS (116 UNITS X 1.75 SPACES PER UNIT)	203 SPACES
PARKING REQUIRED FOR 3 BEDROOM UNITS (20 UNITS X 2 SPACES PER UNIT)	400 SPACES
PARKING REQUIRED FOR GUESTS (248 X 0.25)	62 SPACES
TOTAL APARTMENT PARKING REQUIRED PER CODE	635 SPACES (13 ADA SPACES)

REQUIRED BY DEVIATION REQUESTED

PARKING (248 UNITS X 1.5 SPACES PER UNIT)	372 SPACES
PARKING FOR GUESTS (248 X 0.125)	31 SPACES
TOTAL APARTMENT PARKING REQUIRED PER DEVIATION	503 SPACES (13 ADA SPACES)

DEVIATION

SURFACE PARKING PROVIDED	220 SPACES
GAUGE PARKING PROVIDED	346 SPACES
TOTAL APARTMENT PARKING PROVIDED	566 SPACES (INCL. 13 ADA SPACES)
BIKE PARKING REQUIRED BY CODE (1 SPACE PER UNIT)(3545)	71 SPACES
BIKE PARKING PROVIDED	71 SPACES

DECKED PARKING

SURFACE PARKING PROVIDED	41 SPACES
TOTAL APARTMENT PARKING PROVIDED	507 SPACES (INCL. 13 ADA SPACES)
BIKE PARKING REQUIRED BY CODE (1 SPACE PER UNIT)(3545)	71 SPACES
BIKE PARKING PROVIDED	71 SPACES

COMMON OPEN SPACE DATA

LOT AREA	35.95 AC (1,566,406.32 S.F.)
OPEN SPACE REQUIRED BY CODE(80% OF NET AREA)	21.56 AC (939,243.79 S.F.)
OPEN SPACE PROVIDED	1,026,419.93 S.F. (36.31 AC)
COMMON OPEN SPACE PROVIDED BY CODE(5% OF GROSS AREA)	1,068 AC (46,262.19 S.F.)
COMMON OPEN SPACE PROPOSED	68,455 S.F. (1.73 AC)

PROJECT INFORMATION

EXISTING ZONING	AG, CP-0 & CP-2
PROPOSED ZONING	PUD
GROSS SITE AREA	45,574 AC (INCLUDES 1/2 ROW OF CANYON CREEK BLVD)
OVERALL DENSITY	8.47 DU PER ACRE
ROW AREA	343 AC (15,009.94 S.F.)
NET SITE AREA	41,942 AC (1,827,938.58 S.F.)
NUMBER OF LOTS	3 LOTS

LOT 2 SITE DATA (RESIDENTIAL)

LOT AREA	4.06 AC (176,904.57 S.F.)
NUMBER OF UNITS PROPOSED	10 UNITS
PROPOSED DENSITY	19.70 DU PER ACRE
GROSS SITE AREA	68,800 S.F.
OVERALL DENSITY	37.74 DU PER ACRE
ROW AREA	71,462.88 S.F.
NET SITE AREA	2,444 AC (106,142.74 S.F.)
NUMBER OF LOTS	194 UNITS (8.9 S.F. PER UNIT)
MINIMUM HEIGHT PROPOSED	11.50 S.F. (10%)
MINIMUM SETBACKS FROM STREETS PROPOSED	11.50 S.F. (10%)
MINIMUM SETBACKS REAR PROPOSED	11.50 S.F. (10%)
MINIMUM SETBACKS INTERIOR SIDE PROPOSED	11.50 S.F. (10%)

PARKING DATA

PARKING REQUIRED (1 STALL PER 1 BEDROOM)(10*2)	27 SPACES
PARKING REQUIRED (1 STALL PER STAFF)	20 SPACES
TOTAL PARKING REQUIRED PER CODE	47 SPACES (2 ADA SPACES)
PARKING PROVIDED	89 SPACES (INCL. 2 ADA SPACES)
BIKE PARKING REQUIRED BY CODE (1 SPACE PER UNIT)(3545)	16 SPACES
BIKE PARKING PROVIDED	16 SPACES

DEVIATIONS REQUESTED FOR LOT 2

- REDUCE SETBACK ALONG K-10 TO 20'
- REDUCE MINIMUM SETBACK FROM STREET TO 20'

LOT 3 SITE DATA (RESIDENTIAL)

LOT AREA	1.94 AC (84,638.31 S.F.)
BUILDING AREA PROPOSED	5,100 S.F.
PARKING LOT AND DRIVE AREA PROPOSED	38,629.80 S.F.
TOTAL COVERAGE AREA PROPOSED	43,729.80 S.F.
OPEN SPACE REQUIRED BY CODE(80% OF NET AREA)	2,444 AC (106,142.74 S.F.)
OPEN SPACE PROVIDED	109,362.87 S.F. (11.83 AC)
PARKING LOT AREA	18,927.73 S.F.
PARKING LOT INTERNAL LANDSCAPE AREA REQ. BY CODE 4-1-D-3-M-1	1,801.87 S.F. (10%)
MINIMUM HEIGHT PROPOSED	11.50 S.F.
MINIMUM SETBACKS FROM STREETS PROPOSED	11.50 S.F.
MINIMUM SETBACKS REAR PROPOSED	11.50 S.F.
MINIMUM SETBACKS INTERIOR SIDE PROPOSED	11.50 S.F.

PARKING DATA

PARKING REQUIRED (1 STALL PER 250 S.F.)	24 SPACES (2 ADA SPACES)
PARKING PROVIDED	39 SPACES (INCL. 2 ADA SPACES)
BIKE PARKING REQUIRED BY CODE (1 SPACE PER 5,000 S.F.)	1 SPACES
BIKE PARKING PROVIDED	1 SPACES

DEVIATIONS REQUESTED FOR LOT 3

- REDUCE PARKING AND BUILDING SETBACK TO 20'

EXISTING ZONING

AG, CP-0 & CP-2
PUD
45,574 AC (INCLUDES 1/2 ROW OF CANYON CREEK BLVD)
8.47 DU PER ACRE
343 AC (15,009.94 S.F.)
41,942 AC (1,827,938.58 S.F.)
3 LOTS

LOT 2 SITE DATA (RESIDENTIAL)

LOT AREA	4.06 AC (176,904.57 S.F.)
NUMBER OF UNITS PROPOSED	10 UNITS
PROPOSED DENSITY	19.70 DU PER ACRE
GROSS SITE AREA	68,800 S.F.
OVERALL DENSITY	37.74 DU PER ACRE
ROW AREA	71,462.88 S.F.
NET SITE AREA	2,444 AC (106,142.74 S.F.)
NUMBER OF LOTS	194 UNITS (8.9 S.F. PER UNIT)
MINIMUM HEIGHT PROPOSED	11.50 S.F. (10%)
MINIMUM SETBACKS FROM STREETS PROPOSED	11.50 S.F. (10%)
MINIMUM SETBACKS REAR PROPOSED	11.50 S.F. (10%)
MINIMUM SETBACKS INTERIOR SIDE PROPOSED	11.50 S.F. (10%)

PARKING DATA

PARKING REQUIRED (1 STALL PER 1 BEDROOM)(10*2)	27 SPACES
PARKING REQUIRED (1 STALL PER STAFF)	20 SPACES
TOTAL PARKING REQUIRED PER CODE	47 SPACES (2 ADA SPACES)
PARKING PROVIDED	89 SPACES (INCL. 2 ADA SPACES)
BIKE PARKING REQUIRED BY CODE (1 SPACE PER UNIT)(3545)	16 SPACES
BIKE PARKING PROVIDED	16 SPACES

DEVIATIONS REQUESTED FOR LOT 2

- REDUCE SETBACK ALONG K-10 TO 20'
- REDUCE MINIMUM SETBACK FROM STREET TO 20'

LOT 3 SITE DATA (RESIDENTIAL)

LOT AREA	1.94 AC (84,638.31 S.F.)
BUILDING AREA PROPOSED	5,100 S.F.
PARKING LOT AND DRIVE AREA PROPOSED	38,629.80 S.F.
TOTAL COVERAGE AREA PROPOSED	43,729.80 S.F.
OPEN SPACE REQUIRED BY CODE(80% OF NET AREA)	2,444 AC (106,142.74 S.F.)
OPEN SPACE PROVIDED	109,362.87 S.F. (11.83 AC)
PARKING LOT AREA	18,927.73 S.F.
PARKING LOT INTERNAL LANDSCAPE AREA REQ. BY CODE 4-1-D-3-M-1	1,801.87 S.F. (10%)
MINIMUM HEIGHT PROPOSED	11.50 S.F.
MINIMUM SETBACKS FROM STREETS PROPOSED	11.50 S.F.
MINIMUM SETBACKS REAR PROPOSED	11.50 S.F.
MINIMUM SETBACKS INTERIOR SIDE PROPOSED	11.50 S.F.

PARKING DATA

PARKING REQUIRED (1 STALL PER 250 S.F.)	24 SPACES (2 ADA SPACES)
PARKING PROVIDED	39 SPACES (INCL. 2 ADA SPACES)
BIKE PARKING REQUIRED BY CODE (1 SPACE PER 5,000 S.F.)	1 SPACES
BIKE PARKING PROVIDED	1 SPACES

DEVIATIONS REQUESTED FOR LOT 3

- REDUCE PARKING AND BUILDING SETBACK TO 20'

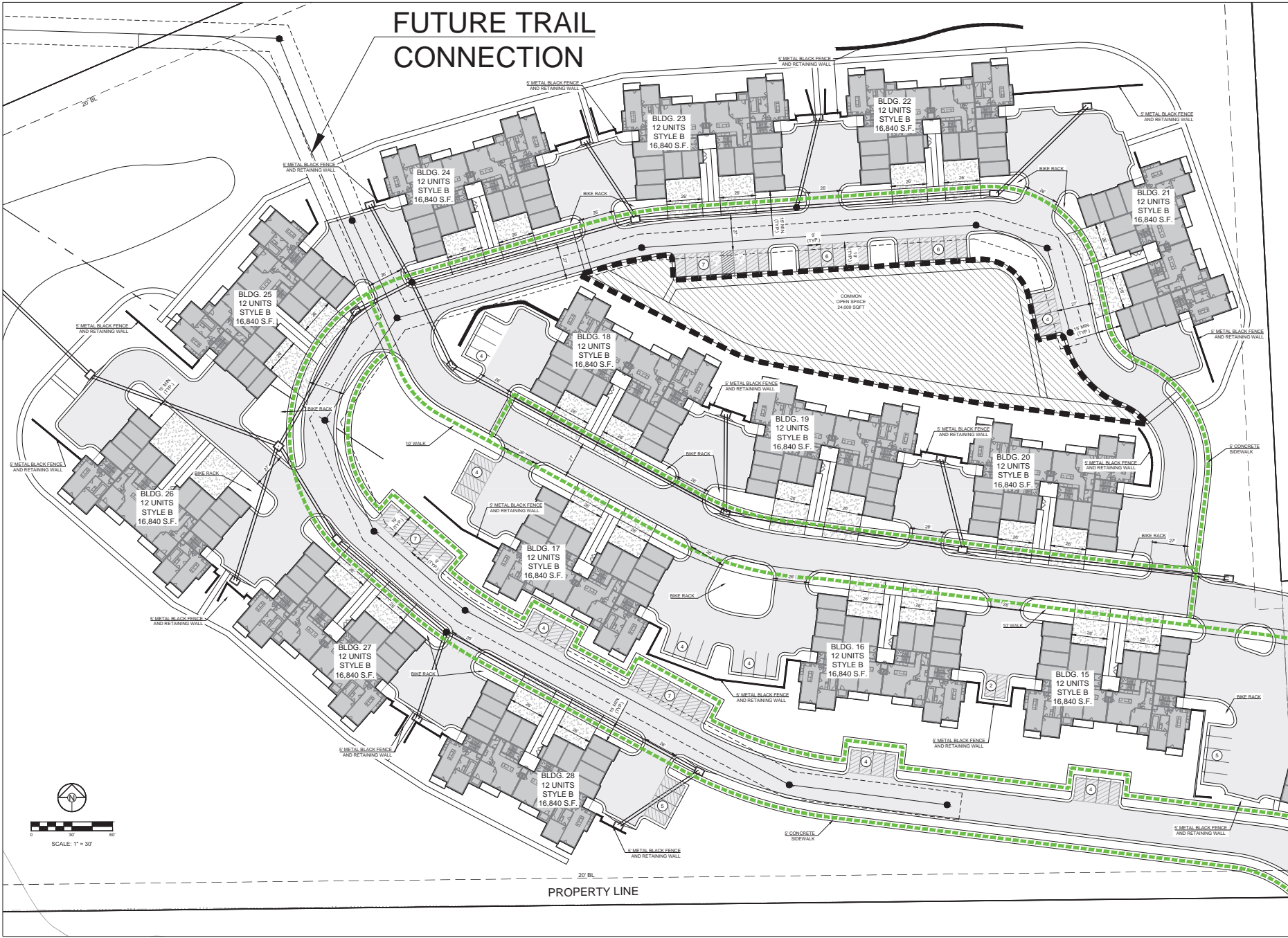
OVERALL LEGAL
Part of the West One-Half of Section 5 and part of the Northeast One-Quarter of said Section 6, thence along the South line of said Northeast One-Quarter, South 85 degrees 21 minutes 28 seconds West, a distance of 1030.45 feet; thence North 52 degrees 38 minutes 22 seconds West, a distance of 606.34 feet; thence North 03 degrees 14 minutes 45 seconds East, a distance of 1200.37 feet; thence North 83 degrees 05 minutes 30 seconds East, a distance of 107.99 feet; thence North 86 degrees 40 minutes 47 seconds East, a distance of 448.95 feet; thence North 86 degrees 23 minutes 27 seconds East, a distance of 504.54 feet; thence North 65 degrees 42 minutes 39 seconds East, a distance of 286.19 feet to a point on the East line of the said Northeast One-Quarter, thence along said East line, South 02 degrees 55 minutes 00 seconds East, a distance of 606.01 feet; thence North 87 degrees 05 minutes 00 seconds East, a distance of 237.15 feet; thence North 54 degrees 35 minutes 01 seconds East, a distance of 1347.60 feet; thence North 17 degrees 54 minutes 01 seconds East, a distance of 220.37 feet; thence South 42 degrees 32 minutes 22 seconds East, a distance of 444.99 feet to a point of closure on the West right-of-way line of Canyon Creek Boulevard as established in Book 7467 at Page 594, thence along said West right-of-way line for the following three courses, along a curve to the right having an initial tangent bearing of South 53 degrees 56 minutes 27 seconds West, a radius of 1120.00 feet, a central angle of 05 degrees 02 minutes 20 seconds and an arc length of 98.50 feet; thence North 42 degrees 53 minutes 19 seconds West, a distance of 22.45 feet; thence South 47 degrees 56 minutes 41 seconds West, a distance of 70.00 feet; thence South 42 degrees 53 minutes 19 seconds East, a distance of 132.45 feet to a point of curvature; thence along a curve to the left having an initial tangent bearing of South 45 degrees 19 minutes 14 seconds West, a radius of 1120.00 feet, a central angle of 23 degrees 40 minutes 05 seconds and an arc length of 406.64 feet; thence North 02 degrees 05 minutes 14 seconds West, a distance of 165.21 feet; thence South 03 degrees 05 minutes 36 East, a distance of 276.49 feet to a point on the South line of the Northeast One-Quarter of said Section 5, thence South 79 degrees 45 minutes 03 seconds West, a distance of 1342.86 feet to a point on the West line of the Southwest One-Quarter of said Section 5, thence along said West line, North 02 degrees 52 minutes 11 seconds West, a distance of 190.58 feet to the Point of Beginning, and containing 44,777 sq. ft. more or less.

- GENERAL NOTES:**
- FLOOD NOTE: The floodplain indicated on this map is from Panel F of the FEMA Map revised.
 - Boundary and easement data is from an ALTA survey prepared by Schlagel & Associates, P.A.
 - Topography and adjacent plat and improvements from JCOO AIMS.
 - Existing utilities have been shown to the greatest extent possible based upon field locates by utility companies and information provided to the engineer.
 - Adjacent parcel information is based upon JCOO AIMS and information provided by consultants for the adjacent projects.
 - All parking lot lighting shall comply with the city codes and ordinances.
 - All new on-site wiring and cables shall be placed underground within the city codes and ordinances.
 - All above ground electrical and/or telephone cabinets shall be placed within the interior side or rear building setback yards and the planning official approves the placement of the cabinets in the front or corner yard as outlined in the city code.
 - All wall and monument signs will require a sign permit through the codes division.
 - All parking spaces shall meet the requirements in the UDDO.
 - All improvements must meet the City of Lenexa design criteria, standards and codes.
 - Approval of this plan does not result in generating a building permit. A separate building permit approval process must be completed.
 - Approval of this plan does not constitute a complete review of the project for compliance with the City building codes. A full code analysis shall be submitted with the building permit. Revisions may be required to make the project building code compliant.
 - Approval of this plan does not constitute a complete review of the project for compliance with the ADA regulations. The project shall comply with all applicable regulations of the ADA.
 - Approval of this plan does not constitute compliance with the Fire Code. Complete fire sprinkler and fire alarm plans are required to obtain a building permit.
 - A sewer permit from Johnson County Wastewater is required prior to obtaining a building permit.
 - All parking stall lines shall be painted white in color.

SCHLAGEL
PREPARED BY:
SCHLAGEL & ASSOCIATES P.A.
CANYON RIDGE APARTMENT HOMES
PRELIMINARY DEVELOPMENT PLAN
CANYON CREEK BLVD, LENEXA, KANSAS

OVERALL PRELIMINARY SITE PLAN
SHEET C1.0

FUTURE TRAIL CONNECTION



NO.	REVISION DATE	DESCRIPTION
1	11/15/24	ISSUED FOR PERMIT
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DETAILED SITE PLAN WEST

SHEET
C1.1

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(UNPLATTED)
CITY OF LENEXA

BLDG. 8
12 UNITS
STYLE B
16,840 S.F.

BLDG. 10
14 UNITS
STYLE A
23,008 S.F.

BLDG. 9
14 UNITS
STYLE A
23,008 S.F.

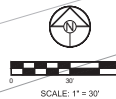
BLDG. 14
(STORM SHELTER)
12 UNITS
STYLE A
23,008 S.F.

BLDG. 13
14 UNITS
STYLE A
23,008 S.F.

BLDG. 12
14 UNITS
STYLE A
23,008 S.F.

BLDG. 11
14 UNITS
STYLE A
23,008 S.F.

KANSAS
STATE



PREPARED BY:

SCHLAGEL & ASSOCIATES P.A.

CANYON RIDGE APARTMENT HOMES
PRELIMINARY DEVELOPMENT PLAN
CANYON CREEK BLVD LENEXA, KANSAS

NO.	REVISION DATE	DESCRIPTION
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DETAILED SITE PLAN WEST(2)

SHEET
C1.2

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SCHLAGEL
 ASSOCIATES, P.A.
 1100 WEST 34th STREET, SUITE 100
 CHICKENIA, MISSOURI 64619
 (417) 492-5100
 WWW.SCHLAGELASSOCIATES.COM

PREPARED BY:
 SCHLAGEL & ASSOCIATES, P.A.

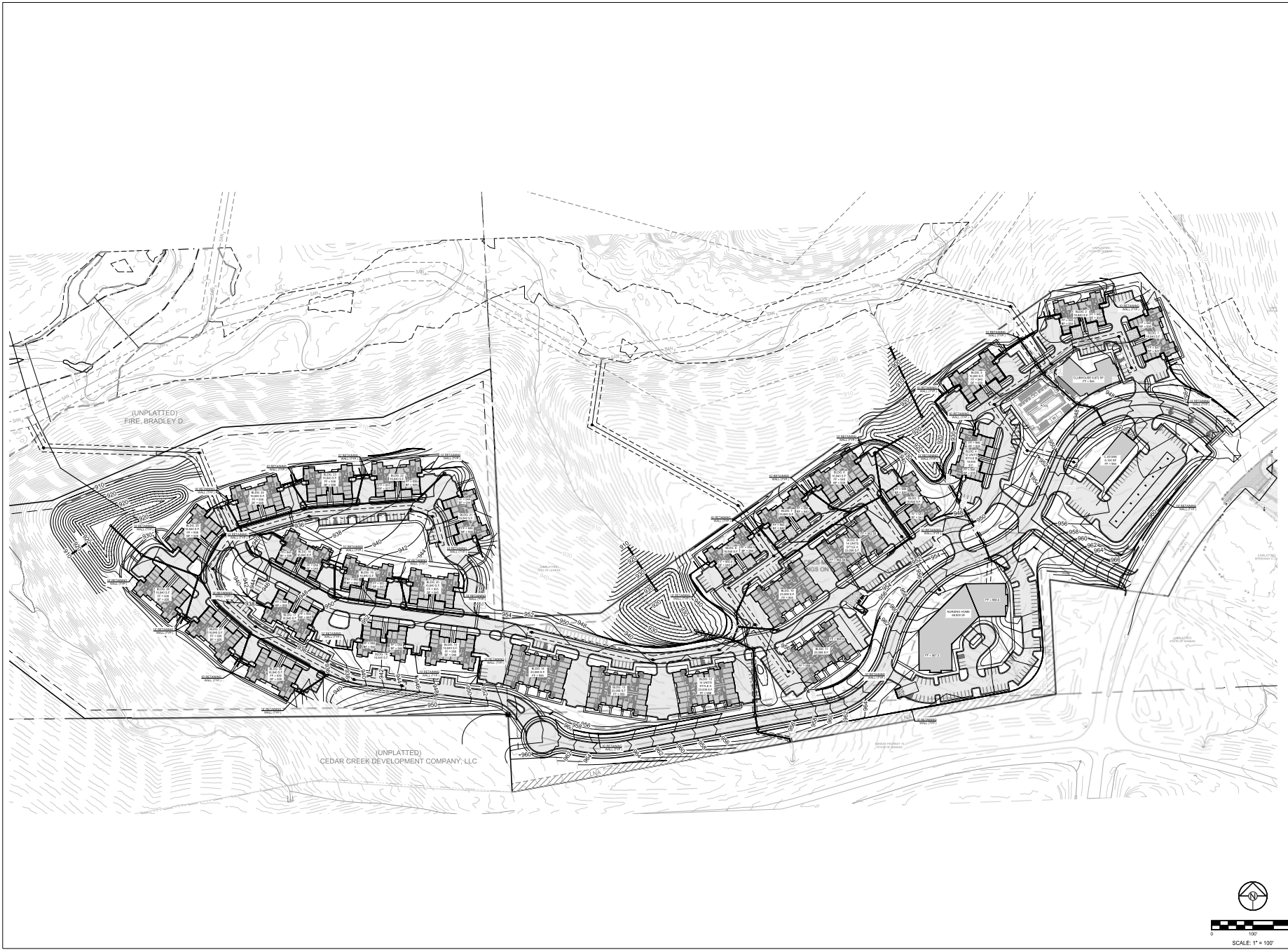
**CANYON RIDGE APARTMENT HOMES
 PRELIMINARY DEVELOPMENT PLAN
 CANYON CREEK BLVD LENEWA, KANSAS**

REVISION DATE	DESCRIPTION

DETAILED SITE PLAN EAST

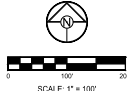
SHEET
C1.3

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(UNPLATTED)
FIRE, BRADLEY D.

(UNPLATTED)
CEDAR CREEK DEVELOPMENT COMPANY, LLC



PREPARED BY:

SCHLAGEL & ASSOCIATES P.A.

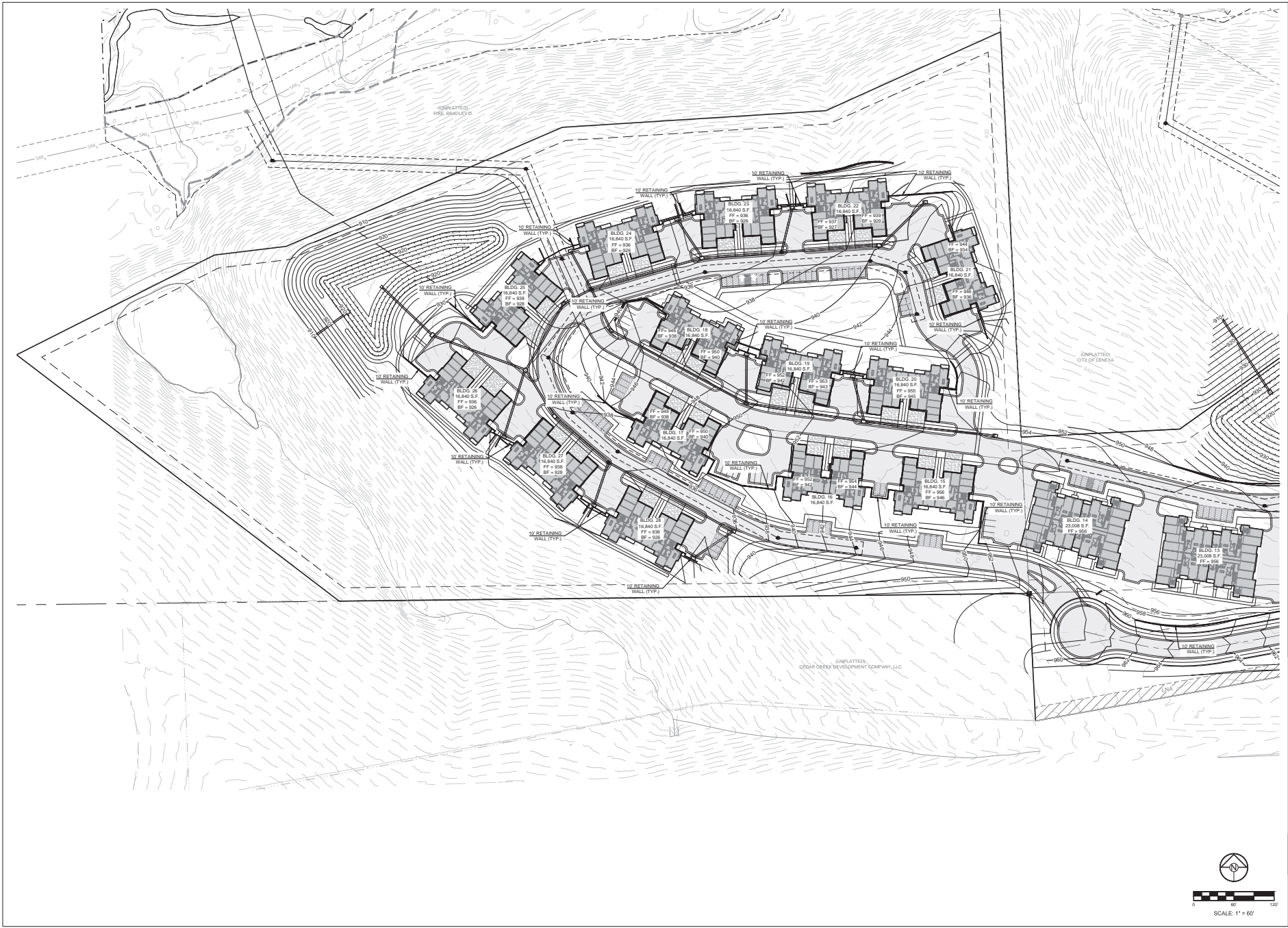
CANYON RIDGE APARTMENT HOMES
PRELIMINARY DEVELOPMENT PLAN
CANYON CREEK BLVD LENEXA, KANSAS

REVISION DATE	DESCRIPTION

OVERALL
PRELIMINARY
GRADING PLAN

SHEET
C2.0

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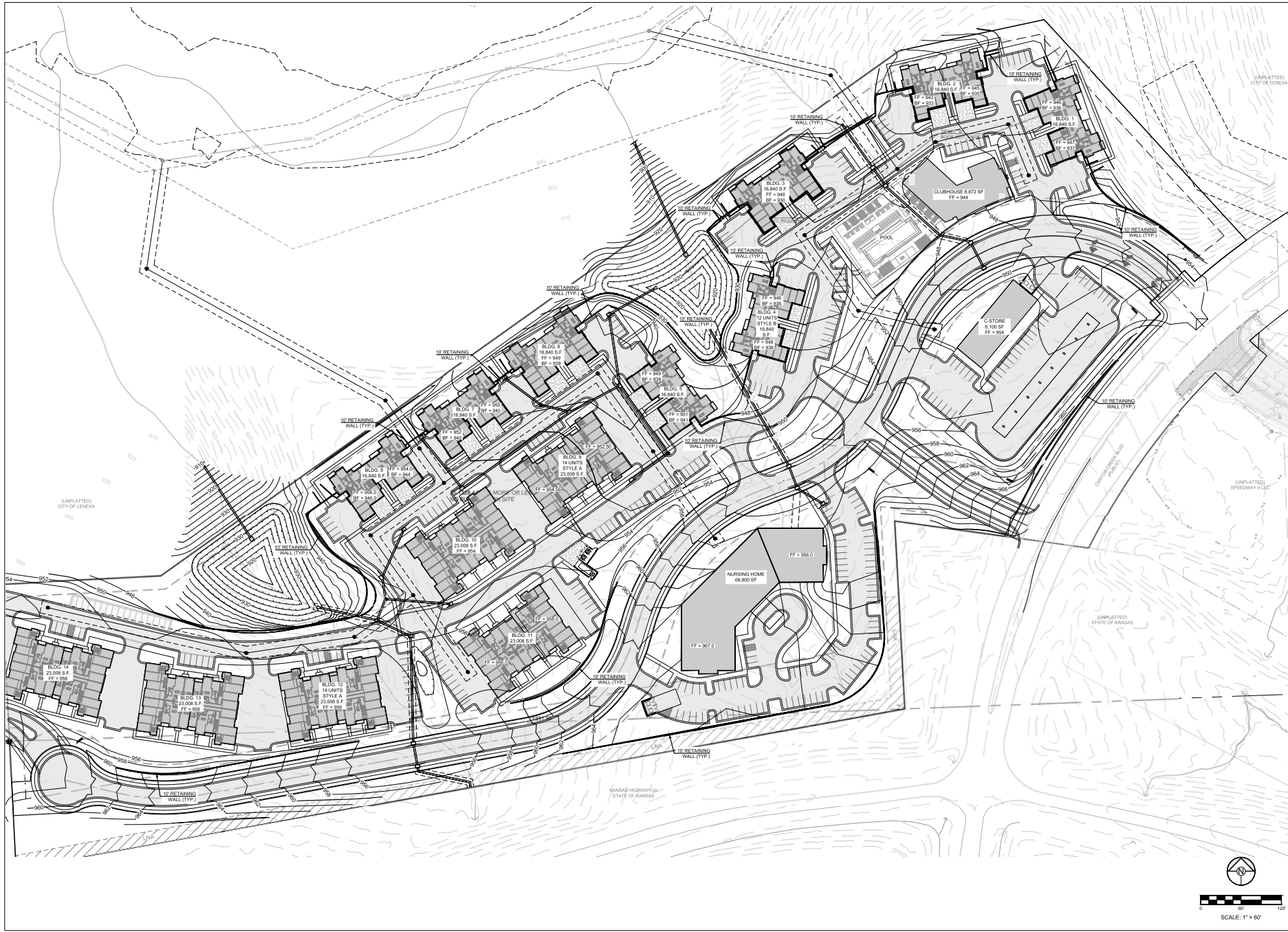
SCHLAGEL
 ARCHITECTS
 1125 S. W. 15th St., Suite 110
 Ft. Lauderdale, FL 33311
 (954) 470-5158 • Fax: (954) 462-8400
 WWW.SCHLAGELASSOCIATES.COM

PREPARED BY:
 SCHLAGEL & ASSOCIATES, P.A.

CANYON RIDGE APARTMENT HOMES
 PRELIMINARY DEVELOPMENT PLAN
 CANYON CREEK BLVD LENEAX, KANSAS

NO.	REVISION DATE	DESCRIPTION

DETAILED GRADING PLAN WEST
 SHEET
C2.1





SCHLAGEL ASSOCIATES, P.A.

REGISTERED PROFESSIONAL ARCHITECTS
REGISTERED PROFESSIONAL ENGINEERS
1101 WEST WYOMING AVE. SUITE 200
DENVER, CO 80202
TEL: (303) 440-4400
WWW.SCHLAGELASSOCIATES.COM

PREPARED BY: SCHLAGEL & ASSOCIATES, P.A.

CANYON RIDGE APARTMENT HOMES PRELIMINARY DEVELOPMENT PLAN

CANYON CREEK BLVD LENEKA, KANSAS

REVISION	DATE	DESCRIPTION

DESIGNED BY: [Name]

DRAWN BY: [Name]

CHECKED BY: [Name]

DATE PLOTTED: [Date]

PLOT NUMBER: [Number]

SHEET NUMBER: [Number]

DETAILED GRADING PLAN EAST

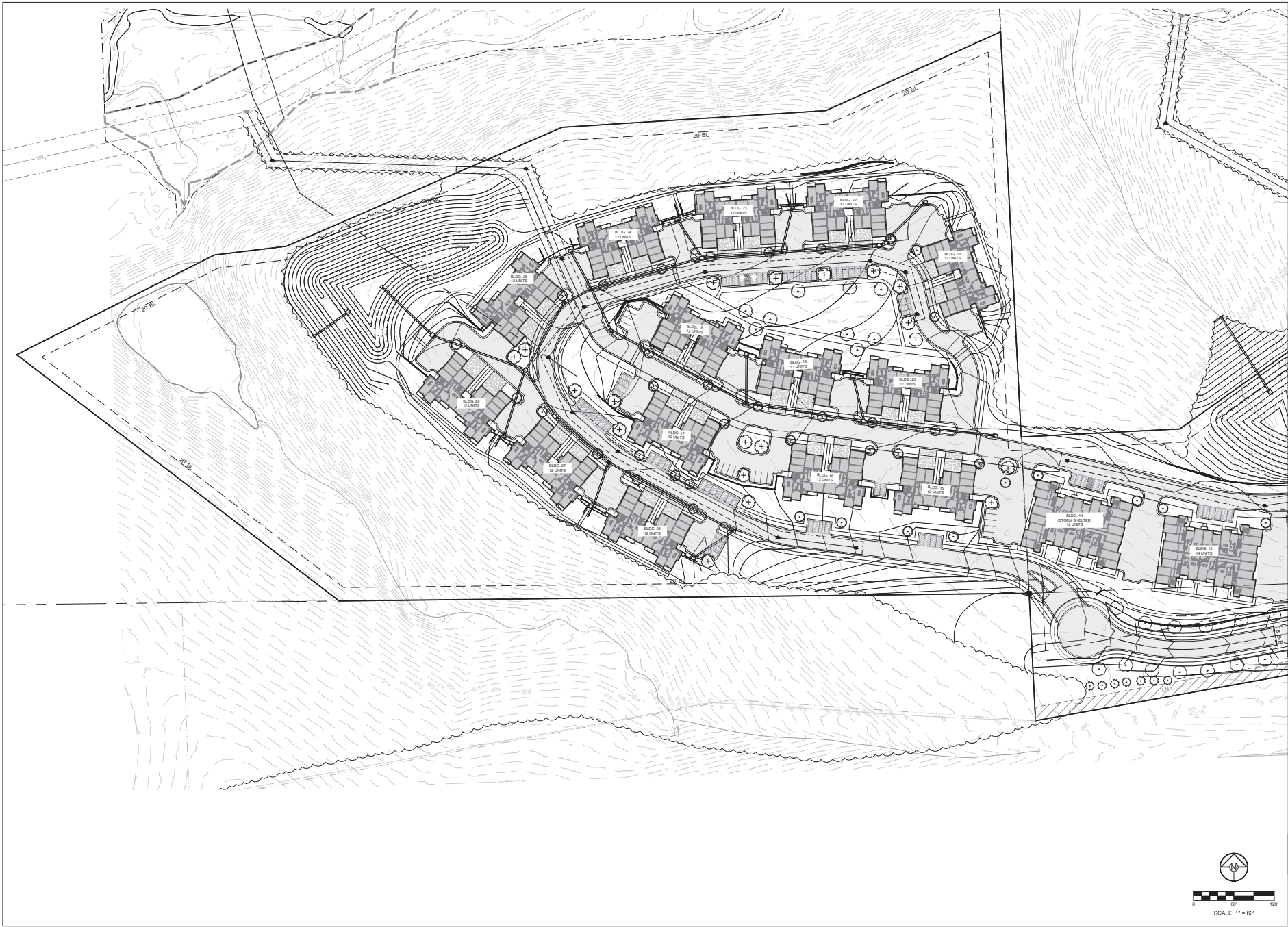


SCALE: 1" = 60'



SHEET **C2.2**

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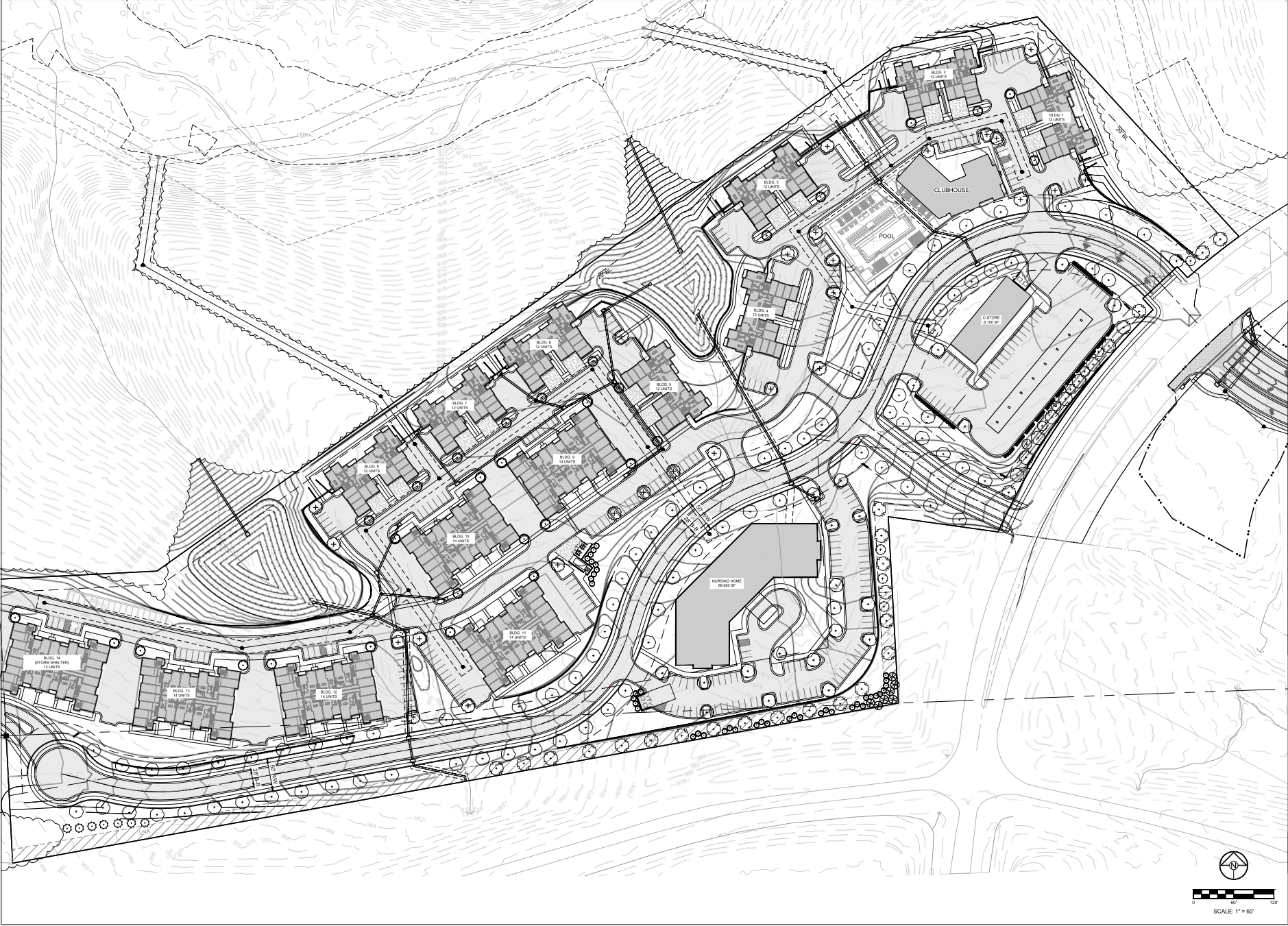
PREPARED BY:
SCHLAGEL & ASSOCIATES, P.A.

**CANYON RIDGE APARTMENT HOMES
PRELIMINARY DEVELOPMENT PLAN**
CANYON CREEK BLVD LENEZA, KANSAS

NO.	DESCRIPTION DATE
1	ISSUED FOR PERMITS 12/29/2023
2	FOR COMMENTS 12/29/2023
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25	FOR COMMENTS 12/29/2023

DETAILED
LANDSCAPE
PLAN WEST

SHEET
L2.0



REVISION DATE	DESCRIPTION

FINISH SCHEDULE			
SYMBOL	MATERIAL	MANUFACTURER	COLOR / TYPE
MS-1	WOODRY	TBD	LIGHT BROWN
MS-2	WOODRY	TBD	
CS-1	CASH STONE	TBD	
STC-1	STUCCO	TBD	OFF WHITE
STC-2	STUCCO	TBD	LIGHT GRAY/BROWN
STC-3	STUCCO	TBD	
STC-4	STUCCO	TBD	GRAY
LP-1	LAMCOMPOSITE SIDING	TBD	GRAY
LP-2	LAMCOMPOSITE SIDING	TBD	LIGHT BROWN
MT-1	METAL	TBD	DARK BRONZE
MT-2	METAL	TBD	GARAGE DOOR
RP-1	ROOFING	TBD	
RP-2	METALCOMPOSITE ROOF	TBD	DARK BRONZE
SP-1	SPANDREL	TBD	DARK BRONZE FRAME
SP-2	SPANDREL WINDOW	TBD	DARK BRONZE FRAME
SP-3	STONE LINER	TBD	

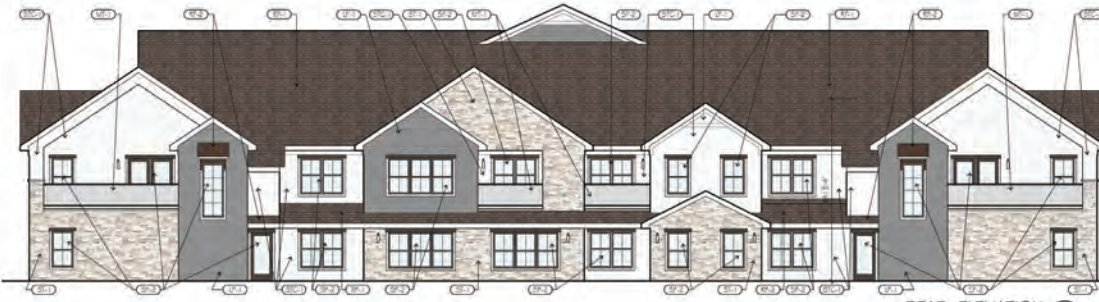




FRONT ELEVATION ④
SCALE: 1/8" = 1'-0"



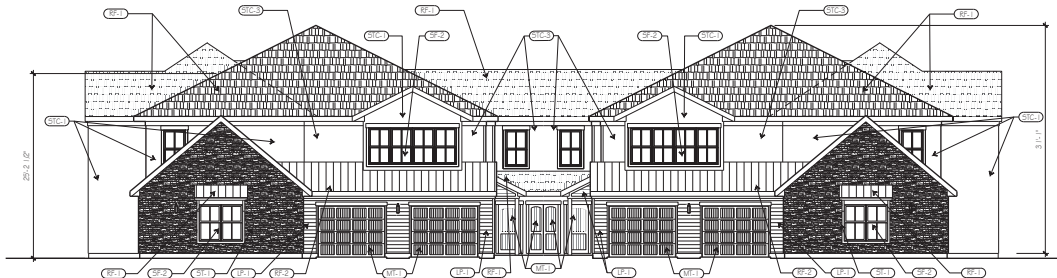
SIDE 2 ELEVATION ②
SCALE: 1/8" = 1'-0"



REAR ELEVATION ③
SCALE: 1/8" = 1'-0"



SIDE 1 ELEVATION ①
SCALE: 1/8" = 1'-0"



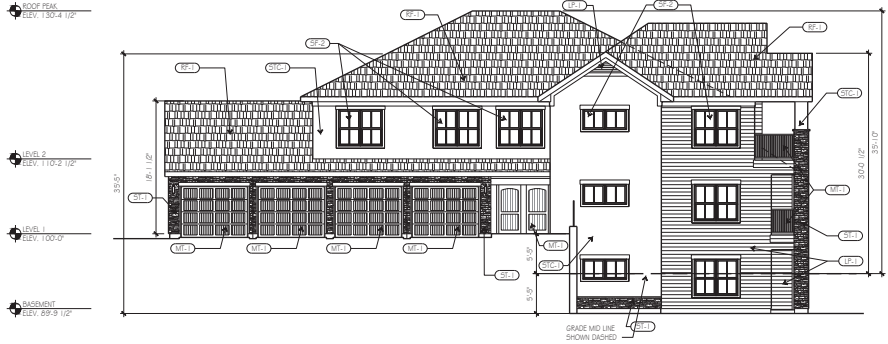
FRONT ELEVATION ④
SCALE: 1/8" = 1'-0"



SIDE 2 ELEVATION ②
SCALE: 1/8" = 1'-0"



REAR ELEVATION ③
SCALE: 1/8" = 1'-0"



SIDE 1 ELEVATION ①
SCALE: 1/8" = 1'-0"

NOTE:
- SEE SHEET A200 FOR MATERIAL
/ FINISH SCHEDULE

A201 BUILDING ELEVATIONS - WALK OUT

SCALE: 1/8" = 1'-0"

CANYON CREEK APARTMENTS | ODDO DEVELOPMENT | 10.30.2023



FRONT ELEVATION ④
SCALE: 1/8" = 1'-0"



SIDE 2 ELEVATION ②
SCALE: 1/8" = 1'-0"



REAR ELEVATION ③
SCALE: 1/8" = 1'-0"



SIDE 1 ELEVATION ①
SCALE: 1/8" = 1'-0"

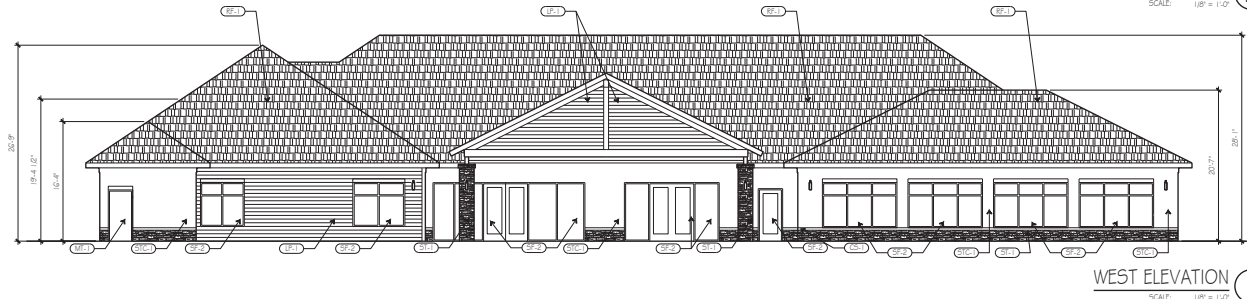
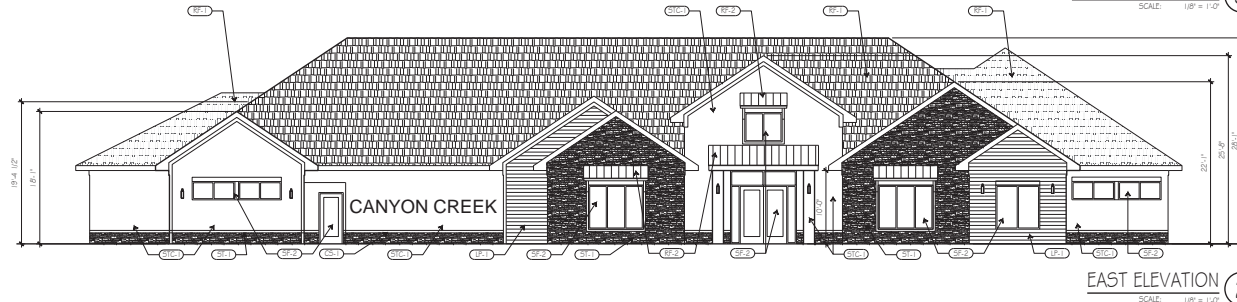
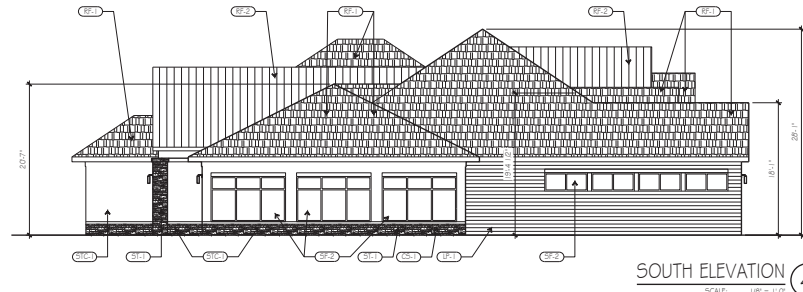
NOTE:
SEE SHEET A200 FOR MATERIAL
/ FINISH SCHEDULE

A201 BUILDING ELEVATIONS - WALK OUT

SCALE: 1/8" = 1'

CANYON CREEK APARTMENTS

ODDO DEVELOPMENT | 10.30.2023



NOTE:
- SEE SHEET A200 FOR MATERIAL
/ FINISH SCHEDULE

A202 BUILDING ELEVATIONS - CLUBHOUSE

CANYON CREEK APARTMENTS | ODDO DEVELOPMENT | 10.30.2023



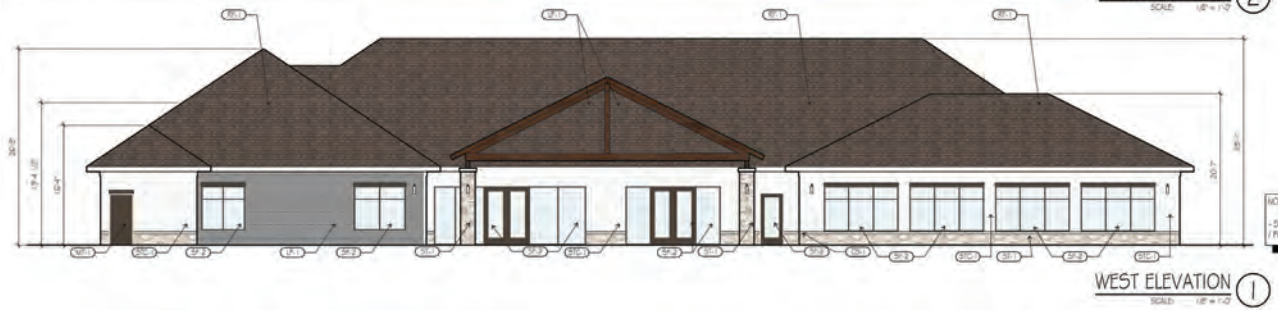
SOUTH ELEVATION ④
SCALE: 1/8" = 1'-0"



NORTH ELEVATION ③
SCALE: 1/8" = 1'-0"



EAST ELEVATION ②
SCALE: 1/8" = 1'-0"



WEST ELEVATION ①
SCALE: 1/8" = 1'-0"

NOTE:
- SEE SHEET A200 FOR MATERIAL
FINISH SCHEDULES.

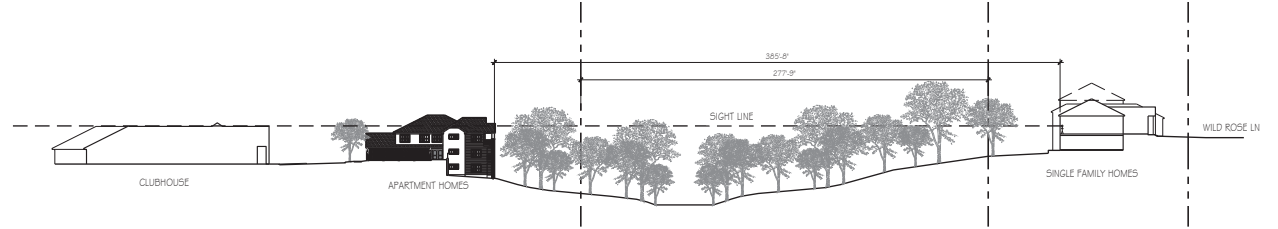
A202 BUILDING ELEVATIONS - CLUBHOUSE

SCALE: 1/8" = 1'-0"

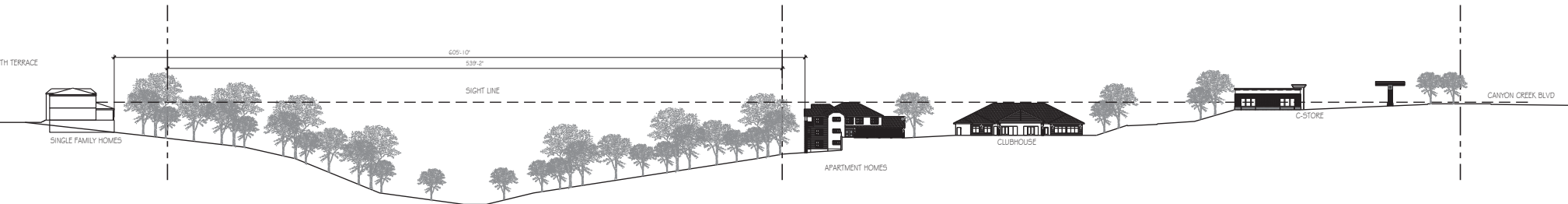




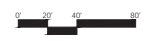
KEY PLAN ③
SCALE: NTS



SITE SECTION ②
SCALE: 1" = 40'

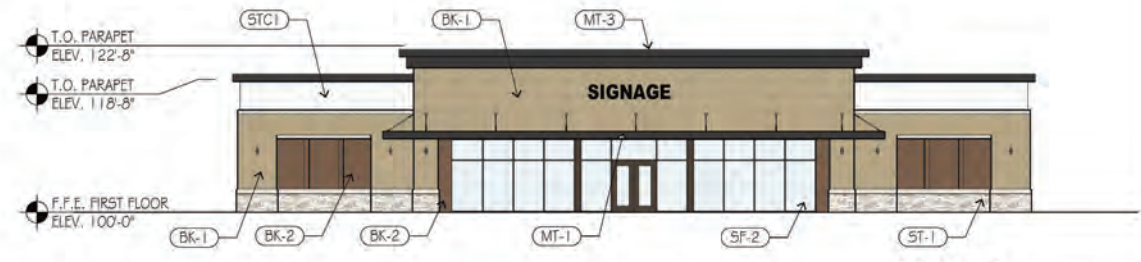
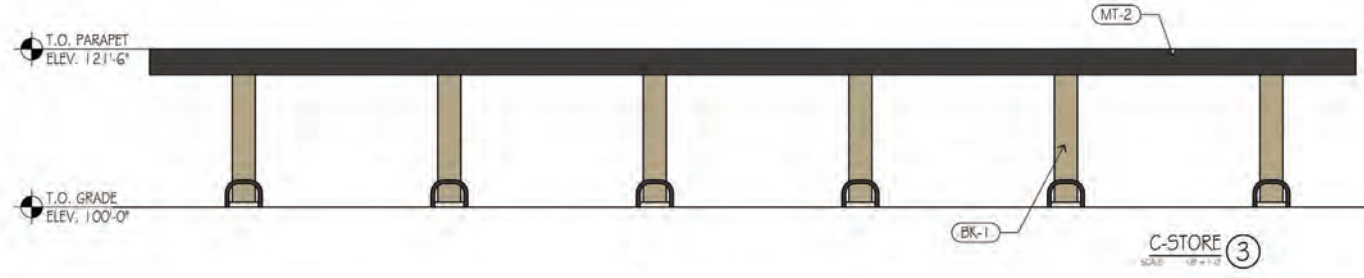


SITE SECTION ①
SCALE: 1" = 40'



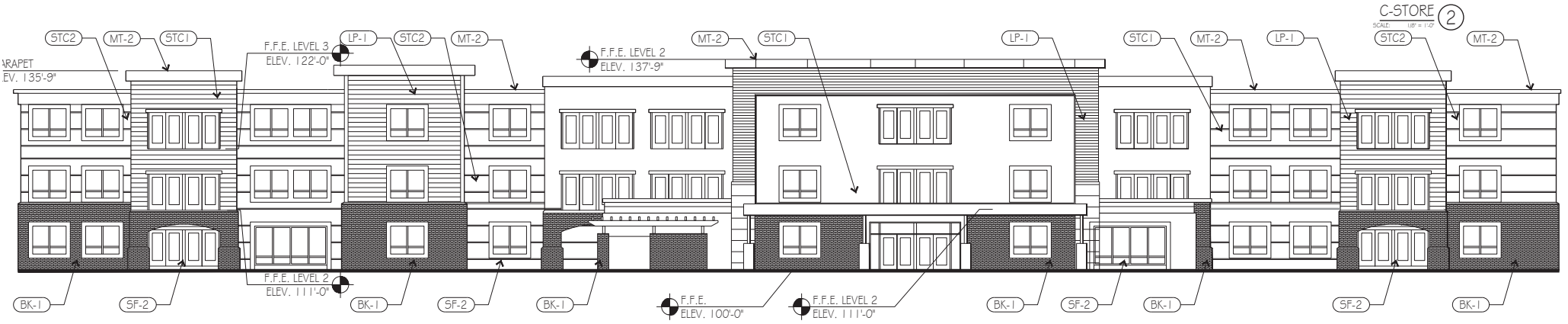
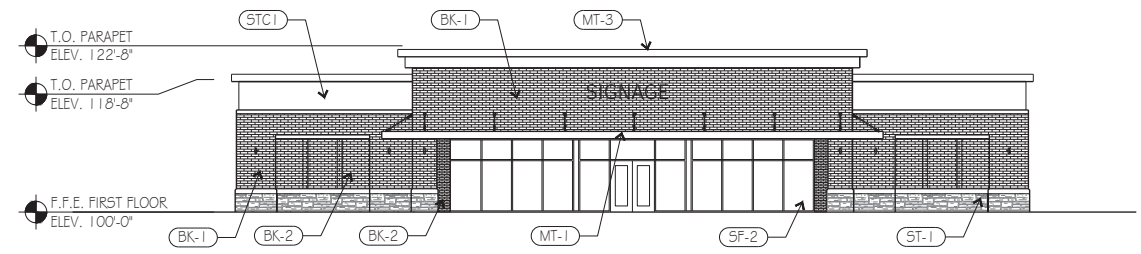
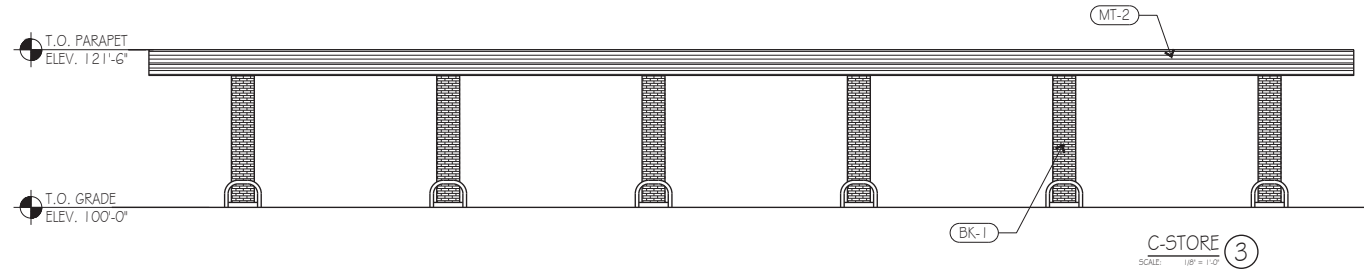
A300 SITE SECTION

SCALE: 1/8" = 1'



NOTE:
 -SEE SHEET A200 FOR MATERIAL / FINISH SCHEDULE.
 NURSING HOME 1
 SCALE: 1/8" = 1'-0"

A203 BUILDING ELEVATIONS



NOTE:
 - SEE SHEET A200 FOR MATERIAL
 / FINISH SCHEDULE

NURSING HOME ①
 SCALE 1/8" = 1'-0"

A203 BUILDING ELEVATIONS



CANYON RIDGE

Design Standards

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Purpose of These Standards

The Canyon Ridge Apartment Homes PUD Design Standards serve to provide a structure for future developers and property owners to follow as they prepare to start and continue to work through the development process. These standards are established to help achieve a desired aesthetic for the Canyon Ridge Apartment Homes PUD. They are in place to ensure that development within Canyon Ridge begins with a high standard that continues throughout the years to provide and maintain a cohesive appearance throughout the development.

The Canyon Ridge Apartment Homes PUD Design Standards book serves as a central resource for the City of Lenexa as well as developers, providing information specific to the various Property Owners and Tenants throughout Canyon Ridge. In addition, the Canyon Ridge Apartment Homes PUD Design Standards book provides information to guide developers through general site planning, architectural and landscape design, sustainability, lighting, storm water management, and signage. These items are further supplemented by the City of Lenexa Municipal Codes and Ordinances.

Acknowledgements

Design Team and Contact Information

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Lenexa, KS 66219	Lenexa, KS 66219	Lenexa, KS 66215
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Introduction

The intent of the Canyon Ridge Apartment Homes PUD Design is to create a walkable park-like neighborhood mixed-use environment through the use of enjoyable landscaped pathways connecting nodes, landscaped park areas and features. The development is situated on the Northwest corner of K-10 Highway and Canyon Creek Boulevard. The development is a planned mixed-use environment following the planning goals of Lenexa and will serve as a more park-like transitional development to the neighboring residential uses.

Canyon Ridge Apartment Homes by its location and neighboring area provides for a logical and evolutionary community environment and uses to meet and serve both multifamily and single family residential needs in adjacent neighborhoods and within the new mixed-use development.

Canyon Ridge Apartment Homes is intended to be a neighborhood Mixed-Use Development that serves its surrounding community on a daily basis accessed both by vehicular and pedestrian means. Various planned uses are grouped and oriented to the streets with walkable street frontages and are connected by a wider walkable landscaped path with multiple park like settings and amenity features along the way. This natural approach is one of the most influential environmental features in the entire development being situated along a natural ridge and stream corridor. Through extensive pedestrian pathways, streetscaping, pocket parks, and the creation of a more naturalized landscape will help to integrate nature with the built environment.



General Site Planning & Circulation Guidelines

Pedestrian Oriented Design Standards

Purpose

These standards are intended to provide for pedestrian connections between buildings, to the public sidewalk and provide for pedestrian-scale public space and amenities at the entrance of buildings.

Sidewalks within Development Sites

Each buildings main entrance shall be connected by a sidewalk to the main entrance of other buildings on the site, and to the adjacent public sidewalk network. Each connecting sidewalk path must also provide a crosswalk of the same pathway width. (See Diagram)

Location and Width

Where feasible, these sidewalks can be routed through landscaped islands in parking lots.

Provision of Crosswalks

Crosswalks shall be provided where sidewalks cross streets and drives. Crosswalks along 10' sidewalk circulation path shall be of a contrasting pavement treatment.

Provision of Pedestrian-Scale Public Space

Each building shall provide a public space, such as a plaza, courtyard, or garden within the vicinity of the pedestrian entrance(s).

All Commercial Buildings shall provide seating nodes where appropriate with the public roadways sidewalk system. This Node shall provide pedestrian interest and opportunity for rest as part of the pathway system. This node shall provide seating, or other unique node marker features.

Size

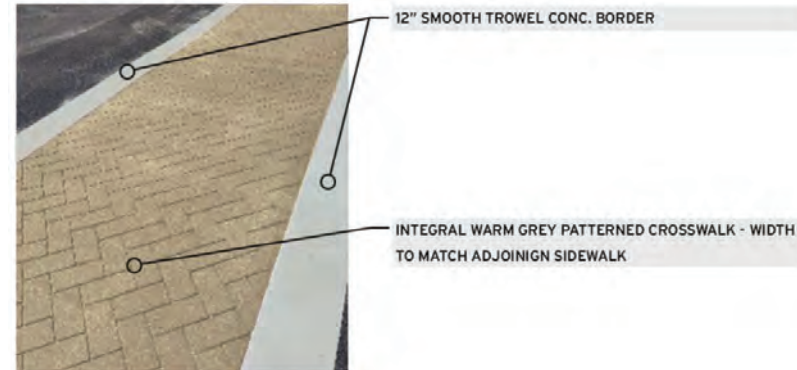
Public spaces within a site shall be 10 square feet for each parking stall provided, or 200 square feet, whichever is greater. The public space will be defined by the combination of paved areas, associated plantings, and architectural features.

Amenities

Public spaces shall include seating in the form of benches or ledges and accent and seasonal plantings. The public space should include at least one of the following:

- A fountain
- Upgraded textured paving
- Sculpture or other artwork
- A portico, trellis, or other architectural shade feature covering part of the seating area
- Equipment Screening and Outdoor Displays and Sales
- Decorative walls and pillars to establish the boundaries and corners

Typical Crosswalk Design



Architectural Standards

Summary

The primary purpose of the architectural design guidelines is to provide a framework to ensure the quality of the buildings proposed for the development.

These guidelines are to guide the quality of the materials selected, the proportion of the building massing, the definition of architectural elements, and to encourage high design standards.

Accessibility

This document does not specifically address issues of accessibility or universal design as the local building codes and standards will dictate the requirements for any given type of development. Exemplary projects within any given district will consider the needs of all users and provide design solutions that provide every user with the same experience regardless of physical capability.

Permitted Uses

Permitted uses within the Canyon Ridge Apartment Homes PUD shall be those uses listed in the Title 4 of Lenexa Unified Development Code for Zoning Districts NPO, RP4, and CP2 with listed restrictions and special use requirement.

The following uses exclude:

- Church or place of worship
- Auto service
- Funeral home
- Laundry, self-service
- Cemetery
- Hospital
- Auction services
- Arcade, commercial
- Bar or nightclub
- Entertainment, outdoor
- Parking, commercial
- Self Service storage

Description

Neighborhood scale retail shop / C-store buildings are generally one and two story buildings providing services and/or goods for residents.

Major Elements

Building Entry

Vestibule entries are encouraged to have a defining architectural feature (such as roof pitch or canopies).

Fenestration

A minimum of 30% of primary street facing façade shall be fenestration.

Maximize buildings visual transparency along the Main street facing façade.

Windows used for displays should not be covered with graphics except as permitted by the sign criteria. Any interior tenant walls should be a minimum of 3' from storefront glazing.

Awnings & Canopies

Awnings and canopies are to be durable and UV stable material.

Awnings and canopies may project over the sidewalk with a minimum clear height of 8'-0"

Mechanical Equipment

At the ground level: outdoor mechanical, electrical and communication equipment shall be located behind the building with Architectural compatible screening material from any public way and/or vehicular or pedestrian street.

At the roof level: outdoor mechanical, electrical and communication equipment shall be located within the building massing or at the highest level of the building with no projection above the lowest parapet. Note all roof top equipment shall be fully screened for public view.

Loading docks, trash dumpsters, utility meters should be located in inconspicuous area and screened. The screening shall be incorporated into the overall design of the building.

Walls and landscaping should be primary means to hide utilities and where permitted by utilities, any visible equipment is to be painted to blend with building architecture.

Parking

Parking may be provided in a surface lot, underground structure, above ground structures, tuck under parking or combination thereof.

Any above ground parking structures exposed to the public view shall be designed to meet the Design Guidelines for exterior designs herein and where possible, shall be shielded by storefronts or other building uses.

Architectural Image & Character

Retail Shops (neighborhood) principal use is smaller scale retail development, to provide a limited amount of the daily, short trip, needs of adjoining residential area.

Franchise architecture should be adapted to meet the intent of the guideline.

Architectural design shall create harmony through use of differing texture, material shadow lines and contrasting shapes to create visual interest.

Use of traditional materials and colors shall be consistent throughout the development to provide continuity.

Use of building material colors, façade design and roof lines shall be consistent on all sides.

Building Mass & Roof Forms

Large retail uses face the challenge of breaking down the scale of the building to pedestrian scale. No long blank walls are permitted and the walls shall be articulated both horizontally and vertically using projections, windows, canopies, bases and vertical wall elements and projections.

Roof

While flat roof may be the primary roof form in the retail (15,000 s.f. and larger) district, a pitched roof used to reinforce the retailers branding may be allowed. It is also acceptable (encourages) that vestibule entries have a defining architectural feature (such as roof pitch or canopies).

Streetscape

Pedestrian Access

Frontal approach sidewalk to main entry shall be no less than 7'-0" wide. Side approach sidewalk, perpendicular to the main entry, shall be no less than 8'-0" feet wide, which may also include landscaping so long as the walkable surface of the sidewalk is no less than 6'-0" wide.

Wall Articulations & Projections

Locate activity generating and vibrant program along the perimeter and maximize visual transparency. There shall be no blank, unarticulated walls exceeding a building bay. Building bays shall generally not exceed 30'-40'

or a reasonable pattern or rhythm that is appropriate to the size of the building. Further subdivision of bays is encouraged- e.g. horizontal, vertical datum lines. Façade bays may be delineated by the following scaling elements:

- Recesses
- Reveals
- Continuous projections
- Diversity in material
- Change in module rhythm
- Shadow trim



Roof

Flat Roof

Canopy

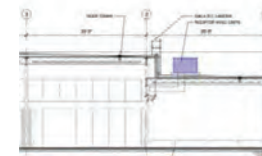
Canopy is 8'-0" or taller above walkway

Pedestrian Façade

Main Entrance located along the street façade

Transparent Entrance

Entry is transparent and invites customers into the business



Mechanical Equipment

Roof level equipment shall be integrated, not visible



Windows

Street facing façade has 30% or more windows.

Transparent Entrance

Entry as 75% or more transparent glass.

Pedestrian Access

12'-0" or wider side approach walkway.

Building Materials & Colors

Materials Encourages

- Laminated wood (timber) and steel
- Architectural metal panel, fiber cement siding and panel (integral color, non-painted)
- Architectural grade concrete, precast, or cast stone
- Glazing (storefront, curtain wall, windows)
- Stone
- Fired clay brick with a veneer depth of at least 3"
- Stucco & EIFS
- Tile

Prohibited Materials

- Standard Concrete or split block
- Vinyl
- Fiberboard
- Non-architectural grade concrete
- Asphalt shingles (except for premium grade laminate style)

Lighting

Refer to lighting guidelines for additional requirements, utilize energy efficient light fixtures.

Provide lighting to illuminate the entries, drop off canopy, outdoor spaces, parking entry and other walking surfaces.

Non-recessed types of light fixtures are to conform to the building design. Lamps shall be of the same color temperatures



Transparent Entrance
Entry has 30% or more transparent glass.

Pedestrian Access
Side approach min. 8'-0"



Canopy
Canopy is 8'-0" or higher above walkway

Windows
Street facing façade windows are encouraged



Transparent Entrance
Entry has 30% or more transparent glass.

Pedestrian Access
Frontal approach min. 7'-0"



Canopy

Canopy is 10'-0" or higher above walkway

Transparent Entrance
Entry has 30% or more transparent glass.



Equipment Screening and Outdoor Display and Sales

Outdoor Storage and Loading docks

Outdoor storage of any products, materials, debris, garbage, carts, or equipment of any kind is prohibited except as provided herein. All loading docks and receiving areas shall be located either behind or to the side of the principal structures screened from view of all public and private streets and adjoining properties through a combination of screen walls, fences, and landscaping.

Trash, Grease, and Recycling Containers

All trash, grease, and recycling containers and dumpsters must be fully contained within a building or otherwise confined within a walled permanent enclosure with opaque gates. Said enclosures must be either incorporated as part of or located behind the main structure in an inconspicuous area and be of sufficient height to completely screen from view the containers. The enclosure and gates must be of durable materials that match the finish materials of the primary structure. Wood or composite material fencing is not an acceptable enclosure material except for the enclosure doors or gates. Landscaping should be added around the enclosure to soften its visual impact.

Back-up Power Generators, Ground-Mounted HVAC Equipment, Chillers, Solar Equipment, Towers, & Satellite Dishes

All backup power generators and ground-mounted HVAC equipment, including chillers, must be fully screened from view of public and private streets and adjoining properties. Screening shall be accomplished by a combination of screen walls and landscaping. Said equipment should be located behind the principal structures in areas to minimize the noise impact on adjoining properties. All equipment must be designed, enclosed, and/or muffled to produce minimal noise and shall comply with the City's noise control regulations.

Any solar collectors or photovoltaic panels shall not be visible from any public street, must be incorporated into the roof structure of an approved principal structure, and shall not protrude above any parapet walls.

Communication towers and wind turbines are prohibited within the Canyon Ridge Apartment Homes PUD. Satellite dishes 24 inches in diameter and smaller should be located so not to be visible from a public or private street. All other satellite dishes must be completely screened from view.

Utility Meters and exposed utility conduits, pipes, and cables

Utility meters shall be located within an enclosure or otherwise located in an inconspicuous area and screened from view with a combination of screen walls and landscaping. All utility conduits, pipes, cables, and roof access ladders shall be fully concealed within the structure.

Public and Private Utilities and Easements:

When practical, all public utility easements should be located along the rear of properties and outside of any required buffer parks so not to limit the ability to plant and maintain landscaping, especially along the street frontage. Above ground utility boxes, transformers, and equipment should also be located in the rear of properties and behind the principal structures to minimize their visual impact. When possible, transformers should be enclosed within the trash enclosure screen walls and landscaping, including ornamental grasses, should be planted around these structures-subject to any required safety clearances.



Lighting Standards

Design Guidelines

- A. Required lamps- all lamps shall be LED or others with similar qualities to conserve energy, reduce glare, and provide for improved color correct vision. Lamps for drives and parking fixtures shall maintain a color temperature range between 4,000 and 5,000 Kelvin. Lamps for pedestrian scale fixtures, including bollards and wall lighting, shall maintain a color temperature range between 3,500 and 4,000 Kelvin.
- B. Required Exterior Lighting Fixtures- all exterior lighting fixtures shall be full cut-offs. No portion of the lamp, lens, or diffuser shall be visible from the side or top of any shield, or otherwise protrude from the bottom of the shield. No exterior lighting fixture shall emit light at or above a horizontal plane that runs through the lowest point of the shield.
- C. Commercial and Industrial
 1. Architectural and Decorative Lighting
 - a. Limited building-mounted lighting may be used to highlight specific architectural features or primary customer or building entrances. Floodlights are only permitted provided all light emitted is contained by the building or by an eave or protruding structure.

- b. Lighting fixtures shall be located, aimed, and shielded to minimize the glare that is emitted on objects other than a building's façade or landscape walls.
- c. Building-mounted neon lighting may only be used when the lighting is recessed, or contained inside a cap or architectural reveal.
- d. An exterior lighting fixture that emits less than 1800 lumens shall not be required to be a full cut-off fixture, provided that the lamp itself creates no glare or has an opaque covering.

2. Street Lighting

- a. Private Internal Drive Lights: All private street light fixtures shall measure no more than (27) twenty-seven feet from grade to the top of the lighting fixture, and shall be a contemporary architectural style similar to and consistent with the fixtures as listed in the Light Fixture Schedule. All private internal drive lights must be reviewed and approved by the Lenexa Community Development Department prior to installation. The use of lighting fixtures that direct light upward into the air is strictly prohibited.
- b. Public Street Lights: All lighting on public streets shall conform to the current City of Lenexa Standard Specifications.



Roadway and Parking Fixture



Pedestrian Fixture: Architectural Area Lighting



Pedestrian Fixture: Lumec

3. Site and Parking Lot Lighting

- a. The mounting height for lighting fixtures shall not exceed (30) thirty feet from grade to the top of the lighting fixture.
- b. The maximum average maintained foot-candles for a parking lot lighting fixture shall be three foot-candles. The maximum lighting level for a parking lot lighting fixture shall be ten foot-candles.
- c. The maximum horizontal foot-candle measurement at any property line shall be two foot-candles. The maximum maintained vertical foot-candle at any adjoining property line shall be two foot-candles, as measured at five feet above grade.

4. Canopy Lighting

- a. The average maintained foot-candles under a canopy shall be twenty (20) foot-candles. Areas outside the canopy shall be regulated by the guidelines and standards outlines above. Permissible fixtures for canopy lighting include:
 - i. Recessed fixtures that incorporate a lens cover that is either recessed or flush with the bottom surface of the canopy.
 - ii. Indirect lighting where light is emitted upward and then reflected down from the underside of the canopy. Such fixtures shall be shielded to ensure that no light is emitted at or above a horizontal plane that runs through the lowest point of the canopy.

5. Pedestrian Walkway Lighting

- a. Pedestrian walkways shall be lit by pedestrian-level, bollard-type lighting (4ft. height max.), pole lighting (14 ft. height max.), or other low, glare-controlled fixtures that are mounted on building or landscape walls.
- b. Pedestrian walkways may be located such that their proximity to street lighting fixtures provides illumination in lieu of walkway fixtures. An average luminance of one foot-candle shall be maintained with the minimum allowable luminance being half a foot-candle.

6. Landscape Accent Lighting

- a. Low level, ground mounted landscape lighting fixtures should be placed a minimum of twelve feet from the back of curb line of a travel way.
- b. Lighting fixtures shall be located, aimed and shielded to minimize the glare that is emitted on objects.

Map 91-2

Application	Fixture	Mounting Height	Pole	Color	Color Temperature Range
Roadway	Kim 'Altitude'	Private 30' Max	Round, Tapered	RAL7043	4,000-5,000 Kelvin
		Public: As determined by photometric plan			
Parking		30" Max		RAL7043	4,000-5,000 Kelvin
Pedestrian	Luminis 'Eclipse Maxi'	14' Max	Round, Tapered	RAL7043	3,500-4,000 Kelvin
Pedestrian Bollard	Forms and Surfaces 'Helio Bollards'	4' Max	N/A	RAL7043	3,500-4,000 Kelvin

Signage Types

1. Monument Signs
2. Primary Building Signage
3. Other Signage

1. Monument Signage

Monument signage is permitted in the Cedar Creek development for the overall development, individual lots, & individual buildings as described in this section.

A. Development Entrance Monument Sign

1. One development monument sign may be located on the primary street frontage of the development
2. Sign height shall not exceed five feet in height, above the average finished grade in the immediate area of the sign, at any point on the sign face or assembly.
3. The sign will be allowed to be a maximum of 72 square feet of signage, per side
4. The sign will be allowed to be a maximum of 10 feet in height.



(A. Development Entrance Monument Sign)

B. Tenant Monument Signs

1. One monument sign may be located on the primary street frontage of the out-lot.
2. Sign height shall not exceed five feet in height, above the average finished grade in the immediate area of the sign, at any point on the sign face or assembly.
3. A minimum of 200 linear feet of street frontage of any one out-lot is required
4. Sign area of a monument sign shall not exceed 24 square feet per side when the frontage length is between 200 and 300 linear feet.
5. Sign area of a monument sign shall not exceed 36 square feet per side when the out-lot frontage length exceeds 300 linear feet.



(B. Tenant Monument Sign)

Monument Signage Requirements:

Service Stations or other retail gasoline outlets:

1. If a monument sign is permitted, a maximum of 65% of the monument sign area may be dedicated to fuel types and pricing.
2. Interchangeable letter/number signs will not be permitted

2. Primary Building Signage

Signage affixed to buildings is permitted as described in this section.

10. Signs shall be individual internally illuminated letters or graphics, sculpted signs with solid or haloed background. Standard back lit cabinet signs and lighted backgrounds are prohibited.

A. Wall Signs

1. Up to 3 identification façade signs may be utilized, 1 per façade, on facades that are architecturally finished to a degree similar to that of the building front. Façade signs shall not be placed on building sides or rears that are directly adjacent to and face residential areas.
2. Each sign may be no larger than 5 percent of the total area of the façade upon which it is placed. Average letter height shall not exceed one eighth of the height of the façade upon which it is placed, and logos shall not exceed one quarter of the height of the façade upon which they are placed. A letter size of 18 inches is permitted regardless of building height and no letter or logo may exceed a total height of six feet.
3. The front of a building shall be considered to be the façade wall that contains the main entrance. If the front of the building faces away from the street frontage, a wall sign may be erected on the rear or sidewall of said building which faces a street at the same rate as allowed above. At no time shall more than two walls be used for computing allowable sign area.
4. Number of signs allowed, as listed above, will be allowed per each business with a maximum number of one sign per wall. However, for multi-tenant buildings, a blade sign, and wall sign, may be located on the same wall by tenant entrances. Sign sizes will be limited by the regulations within these sign criteria.
5. No wall or marquee sign shall project out from the building more than 18 inches.
6. No sign shall project above the upper-most roofline, or parapet of a building.
7. Multi-tenant buildings may have more than one sign may be erected, provided the signs are located on an exterior wall adjacent to the tenant's premises, and the total size of all signs is still within the requirements set forth above.
8. For convenience stores, in addition to the regulations stated above, one sign on a canopy of a convenience store or gasoline pump use may be permitted. Said sign shall be no more than 10 percent of the canopy's façade, per city ordinance. And the sign on the canopy will be allowed only in lieu of the building sign that was to be located on the same side of the building as the proposed canopy sign.
9. Building Identification signs shall be of metal material on opaque wall and be of dis-similar color and finish, translucent material on opaque wall, or vinyl letters applied to transparent glass surface. Painted signs are not allowed.

3. Other Signage

A. Building Address and Signs for After Hours of Operation

Building Addresses shall comply with the edition of the international Codes (I-CODES) as currently adopted by the reference in the City of Lenexa Municipal Code

1. Exceptions:

Color of the numbers may deviate from black or white for just cause when approved by the code official

Hours of operation may be displayed as letters applied directly to the store windows and doors, and shall not exceed one square foot.

2. Seasonal variations to the hours of operation may be displayed on temporary window signs for the duration of the change.
3. No flashing or neon signs will be permitted

B. Flags

The following flags are permitted, so long as they are flown in accordance with protocol established by the congress of the United States for the Stars and Stripes:

1. Flag of the United States of America
2. Flag of the City of Lenexa
3. State of Kansas
4. Foreign nations having diplomatic relations with the United States
5. And other flags adopted or sanctioned by the city of Lenexa

All other banners are regulated as follows:

1. A flag identifying a corporate business commercial enterprise, educational institution, or any other entity or organization is allowed provided it satisfies the following requirements:
 - A. The size of the flag shall not exceed 40 square feet
 - B. The flag shall be flown horizontally
 - C. Maximum of three flags will be allowed.
2. Flag shall be flown in conjunctions with, and at the same time as, a flag of the United States and the State of Kansas.
3. Flag shall be displayed on a separate flagpole located in close proximity to and no higher than the flagpoles of the United States, and the State of Kansas, and in no event higher than thirty (30) feet. One commercial flag shall be allowed in connection with any non-residential lot of record.
4. No flag of a commercial nature or purpose may be flown in a residential zone
5. All flags shall be kept in good repair at all times.

C. Menu Boards and Signage

Each Drive through restaurant shall be permitted one free standing or wall mounted menu board per drive through window
Menu board shall adhere to the following requirements:

1. Menu Board shall not exceed 32 square feet
2. Menu Board shall not exceed 8 feet in height above the finished grade
3. Menu board shall be located adjacent to and oriented toward the drive through lane.
4. A pre-menu board may be considered to be allowed if queuing lane length, location and configuration justifies the additional sign.

D. Window Signs:

Multi-Tenant Buildings:

1. Signs, graphics, logos, or lettering, or similar treatments shall be allowed on the inside, or outside, of window glass provided that they cover no more than 20% of the overall window area of any window and are separately illuminated.

Single tenant buildings:

1. Signs, graphics, logos, or lettering, or similar treatments shall be allowed on the inside, or outside, of window glass provided that they cover no more than 20% of the overall window area of any window and are separately illuminated.

Signs permanently affixed to the external side of windows shall be:

1. A mid or premium grade vinyl
2. Image on vinyl shall be a digital image using solvent coloring; or other similar type material and shall be maintained in good order.

G. Illuminated window signs:

One illuminated window sign may be permitted for each business based upon the following:

1. The sign shall only be illuminated during business hours
2. The sign shall be located on the interior side of the window
3. The sign shall meet safety requirements by having a backing and self-contained transformer. All components shall be UL approved.
4. Text, logos, and color, are not restricted
5. The sign shall not flash, change color, change lighting intensity, or move.
6. Size of the signs will be limited per Article G of this section.

H. Awning Signs:

Canopies and awnings may contain signage provided the following requirements are met:

1. If signage is parallel to the building façade, its area shall be deducted from the maximum allowable area of fascia/wall signs
2. If at right angles to the façade, it will substitute for allowable blade or projecting signs.

3. Awning materials will not be limited, however awnings, and their associated signage should be professional in appearance, and should fit in with the building and surroundings in the area.
4. Awning signs are limited to be no more than 4 square feet per awning.



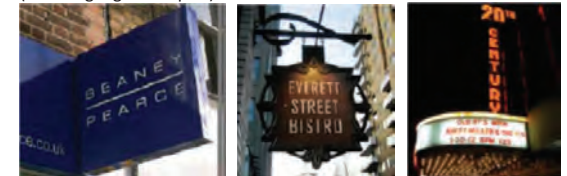
(Acceptable window signage)

J. Blade Signs:

One blade sign shall be allowed per establishment, provided the following criteria has been met:

1. Blade signs shall not exceed 4 square feet per side.
2. Maximum projection of any blade sign shall be 2'-6" from the façade face. No portion of the sign may exceed that maximum dimension.
3. Blade signs may be one fixed piece or a combination of arm and sign face.
4. The maximum height to the top edge of the blade sign shall be no more than 12 feet from the finished grade or sidewalk below.
5. The lowest portions of the blade sign must not be any lower than 8 feet from the finished grade or sidewalk below

(Blade signage examples)



Blade Sign

Blade Sign

Marquee Sign

Signage Requirements

General & additional requirements applicable to all signage.

Service Stations or other retail gasoline outlets:

1. If a monument sign is permitted, a maximum of 65% of the monument sign area may be dedicated to fuel types and pricing.
2. Interchangeable letter/number signs will not be permitted

Signage Lighting

Monuments sign lighting may incorporate:

1. Surface mounted lettering and graphics that are lit by lighting components applied to the back side creating a halo effect (also known as back lit or reverse channel signs)
2. Incorporate punch through translucent lettering and graphics that are lit by components mounted internal to the sign body (standard channel letter signs and the like)
3. Utilize ground mounted directional lighting with glare controlled fixtures, located aimed and shielded to minimize the glare that is emitted on objects other than the sign face
4. No direct or indirect glare or reflections from the sign or its lighting will be allowed to fall in the vehicular lanes to where it could impair vehicle operators.

Directional sign Lighting may incorporate:

1. Surface mounted lettering and graphics that are lit by lighting components applied to the back side created a halo effect (also known as back lit or reverse channel signs)
2. Incorporate punch through translucent lettering and graphics that are lit by components mounted internal to the sign body (standard channel letter signs and the like)
3. Utilize ground mounted directional lighting with glare-controlled fixtures, located aimed and shielded to minimize the glare that is emitted on objects other than the sign face.
4. No direct or indirect glare or reflections from the sign or its lighting will be allowed to fall in the vehicular lanes to where it could impair vehicle operators.

Wall mounted sign Lighting may incorporate:

5. Surface mounted lettering and graphics that are lit by lighting components applied to the back side created a halo effect (also known as back lit or reverse channel signs)
1. Incorporate punch through translucent lettering and graphics that are lit by components mounted internal to the sign body (standard channel letter signs and the like)
2. Utilize ground mounted directional lighting with glare-controlled fixtures, located aimed and shielded to minimize the glare that is emitted on objects other than the sign face.

3. No direct or indirect glare or reflections from the sign or its lighting will be allowed to fall in the vehicular lanes to where it could impair vehicle operators.

Projecting and/or Blade Sign Lighting may incorporate:

- a) Surface mounted lettering and graphics that are lit by lighting components applied to the back side created a halo effect (also known as back lit or reverse channel signs)
 - b) Incorporate punch through translucent lettering and graphics that are lit by components mounted internal to the sign body (standard channel letter signs and the like)
 - c) Utilize ground mounted directional lighting with glare-controlled fixtures located, aimed, and shielded to minimize the glare that is emitted on objects other than the sign face.
6. No direct or indirect glare or reflections from the sign or its lighting will be allowed to fall in the vehicular lanes to where it could impair vehicle operators.

Signage Map



Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Melissa Drummond <mdrummond@kcexec.com>

Date: December 26, 2023 at 10:00:48 PM CST

To: Bill Nicks <bnicks@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Joe Karlin <jkarlin@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Julie Sayers <jsayers@lenexa.com>, Craig Denny <cdenny@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Mark Charlton <mcharlton@lenexa.com>

Subject: Lenexa Re-Zoning Request for Request for Canyon Ridge Apartments NW corner of K-10 & Canyon Creek Blvd

Lenexa City Council Members,

I'm sending this note to encourage you to reject a proposal to build a large 28 multi-story building apartment complex, convenience store, and assisted living complex in western Lenexa. This is the 2nd attempted development of this land in 5 years. Surrounding neighborhoods canvassed and rallied support of over 170 signatures representing 75 million in home values in the 2-week notification period last time, and we have again been given less than 2-weeks to oppose this rezone effort. (January 8th is the Planning Commission Meeting).

This project requires land zoned for single family homes to be rezoned to support the proposal. Lenexa residents chose to live and invest in this part of Lenexa due to the published master land use plan. I have lived in Lenexa nearly my entire life and moved to this area specifically for the wooded landscape and the atmosphere of this section of Lenexa. A developer presenting a plan for this type of development should not override the investment of many hundreds of homeowners in this area. Additionally, the proposed building plat location is largely unfavorable requiring 10-foot retaining walls around nearly the entire complex, which will greatly raise the building height and visibility. Further, I have shared with the Lenexa Police Department my concerns about the unfavorable traffic conditions on Prairie Star Parkway, in particular, due to St. James Academy. St. James Academy is allowed to have drop-off/pick-up traffic sitting in the intersections and roundabouts on Prairie Star Parkway. I am concerned about the additional traffic conditions that this proposal may add to Prairie Star Parkway. The nearby villas, expansion to Arbor Lake, and new park being built will already add quite a bit more traffic to the area. Finally, an additional convenience store is completely unnecessary as there are Casey's <2 and <4 miles away with a Quick Trip being built on 83rd and K-7.

This proposal is an eyesore and truly unfitting for the western entrance of Lenexa. Lenexa has many areas already zoned for such a development and would be much better suited and zoned near walkable amenities around the City Center area or Ridgeview interchange. Again, you are urged to honor the published master land use plan and reject this proposal.

For awareness, I am attaching my letter from July 2018 when a similar proposal was put in front of the Lenexa Council for review and consideration.

I appreciate your time and attention to this request.

Kind Regards,

Melissa Drummond

25939 W 96TH TER, Lenexa, KS

Melissa Drummond, PMP, CSM

[\(913\) 269-6057](tel:9132696057)

mdrummond@kcexec.com

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Andrew Drummond <Andrew@kcexec.com>

Date: December 26, 2023 at 9:40:23 PM CST

To: Bill Nicks <bnicks@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Joe Karlin <jkarlin@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Julie Sayers <jsayers@lenexa.com>, Craig Denny <cdenny@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Mark Charlton <mcharlton@lenexa.com>, Julie Sayers <jsayers@lenexa.com>

Subject: Lenexa Re-Zoning Request for Request for Canyon Ridge Apartments NW corner of K-10 & Canyon Creek Blvd

Lenexa City Council Members,

I'm sending this note to encourage you to reject a proposal to build a large 28 multi-story building apartment complex, convenience store, and assisted living complex in western Lenexa. This is the 2nd attempted development of this land in 5 years. Surrounding neighborhoods canvassed and rallied support of over 170 signatures representing 75 million in home values in the 2-week notification period last time, and we have again been given less than 2-weeks to oppose this rezone effort. (January 8th is the Planning Commission Meeting).

This project requires land zoned for single family homes to be rezoned to support the proposal. Lenexa residents chose to live and invest in this part of Lenexa due to the published master land use plan. We moved to Lenexa (15-year resident) from Overland Park specifically for the wooded landscape and the atmosphere of this section of Lenexa. A developer presenting a plan for this type of development should not override the investment of many hundreds of homeowners in this area. Additionally, the proposed building plat location is largely unfavorable requiring 10-foot retaining walls around nearly the entire complex, which will greatly raise the building height and visibility. Finally, an additional convenience store is completely unnecessary as there are Casey's <2 and <4 miles away with a Quick Trip being built on 83rd and K-7.

This proposal is an eyesore and truly unfitting for the western entrance of Lenexa. Lenexa has many areas already zoned for such a development and would be much better suited and zoned near walkable amenities around the City Center area or Ridgeview interchange. Again, you are urged to honor the published master land use plan and reject this proposal.

I appreciate your time and attention to this request.

Thank you,
Andrew Drummond
25939 W. 96th Terrace Lenexa, KS
816.529.7500

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Barbara Eidt <beidt50@att.net>
Sent: Wednesday, December 27, 2023 6:51 PM
To: Bill Nicks <bnicks@lenexa.com>
Subject: ODDO Proposed development

Hi Bill

We are currently residents of canyon creek point. After carefully reviewing the ODDO plan documents for a another massive apartment development at k10 and canyon creek blvd, we are requesting you consider our STRONG opposition to this project.

Reasons:

This HUGE development massively impacts the entire section of Lenexa, negatively impacting the noise levels, traffic and natural beauty of our section of lenexa.

The plan includes a huge convenience store, directly opposite the one already in the works at k10 and canyon creek blvd.

The substantial increase in traffic resulting from this many apartments and a q trip style gas store will negatively impact the existing residents. in to all the added traffic, the noise levels will spoil a beautiful section of lenexa.

There will be an incred negative impact on the natural beauty of our entire area by removing natural trees and adding a massive amount of noise pollution to this beautiful section of lenexa- which was planned as home owner residential.

There are multiple alternatives available within a few short miles that are already in the works- such as the one across canyon creek Blvd, or the massive complex at Ridgeview and college....

This development conflicts with the existing master plan- for no valid reason. "Panasonic" can not be the primary reason repeatedly used to destroy our residential area!

A similar plan was successfully opposed by residents several years ago. Your lenexa residents consistently do NOT want this type of development so close to our homes.

PLEASE listen to your residents and reject this incredibly massive, Invasive, and inappropriate change to what is a peaceful home owner residential area.

Respectfully

Barbara Eidt
26122 W 96th Street
Lenexa ks 66227
Beidt50@att.net

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Fred Gower <gowerfj@gmail.com>
Sent: Wednesday, December 27, 2023 12:16 PM
To: Bill Nicks <bnicks@lenexa.com>
Subject: Re-Zoning proposal - Canyon Ridge Apartments

December 27, 2023

Dear Councilman Nicks,

I am writing to express my concern and objection to the re-zoning request for the Canyon Ridge Apartment Homes located approximately at the northwest corner of K-10 Highway & Canyon Creek Boulevard.

I reside at 26245 West 96th Terrace, Lenexa, Ks 66227 in the Canyon Creek Point subdivision. I purchased lot 53 at Canyon Creek point in October of 2018 based solely on the panoramic views and wildlife located in the area. As very few spaces in Lenexa offered the views available at Canyon Creek Point, the lot prices were considerably higher than those in surrounding neighborhoods. Many neighbors and I in Canyon Creek Point invested heavily in a secluded area surrounded by city parks and greenspaces.

The proposed rezoning request which includes 28 High density multi-story apartments, convenience store and assisted living center will completely destroy what so many of us have come to believe is the most beautiful spot in Lenexa. The plans show the apartment complex cresting the forested hillside from K-10 and down the valley putting the backside of all apartment buildings in the direct site line of all Canyon Creek Point homes. Picture our view today with a hillside covered with native trees that is highly traveled by wildlife with a view of a hillside stripped of vegetation and replaced by multi-storied apartments.



I attached several quotes describing Canyon Creek Point giving you an idea as to why we chose to live in Lenexa.
“The natural beauty of this area cannot be overstated.”

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

“Canyon Creek Point is situated atop a high ridge with deep stream valleys on two sides. City-owned parkland and a natural conservation area surround the community, allowing sweeping views of nothing but nature, and assuring residents that those views will remain forever unspoiled.”

“The community is surrounded by city-owned parkland and a natural conservation area, providing residents with panoramic vistas of nothing but nature and promising inhabitants that those views would remain undisturbed in perpetuity.”

“The purpose of The Lenexa Foundation is to support community beautification and maintenance, promote social welfare, **promote environmental conservation**, advance education and science, and promote the arts.”

It is my request that you and all city council members as well as the Lenexa Planning Commission reject the proposed re-zoning of this area and leave it as is for the sake of the area residents and wildlife that live here.

Very truly yours,

Fred Gower

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

Jeffrey and Alicia Klein
26213 W 96th Terrace
Lenexa, KS 66227

December 29, 2023

Courtney Eiterich
Lenexa Planning Commission
17101 West 87th St. Pkwy.
Lenexa, KS 66219

Subject: Concerns Regarding Proposed Development at the Northeast Corner of Hwy 10 and Canyon Creek Blvd.

Dear [City Council Member's Name],

We hope this letter finds you well. Our names are Jeffrey and Alicia Klein, and we are residents of Canyon Creek Point. We are writing to express our deep concerns about the proposed development in the heavily forested area 600 feet beyond our southern border.

We understand that the proposed development necessitates rezoning the current agricultural, community commercial, and general office zones to high-density residential. While we appreciate the need for urban planning and development, we believe there are significant reasons to reconsider this proposal in light of the following concerns:

Zoning Regulations: The area's current zoning aligns with the city master plan, reflecting a thoughtful consideration of the community's needs. Rezoning to high-density residential may violate existing zoning regulations, and we urge the City Council to thoroughly review the compatibility of this proposal with the current zoning laws.

Traffic and Infrastructure: Introducing high-density residential units in the proposed development may lead to increased traffic congestion and put undue stress on our existing infrastructure, including roads and schools. We request a comprehensive traffic impact assessment to understand and address these potential issues.

Property Values: There is a legitimate concern among residents that the removal of trees and the drastic change in land use may adversely affect property values in our neighborhood. Research suggests that green spaces and mature trees contribute positively to property values, and we encourage the City Council to consider these potential impacts.

Environmental Impact: The proposed development's plan to remove all trees from the heavily forested area raises significant environmental concerns. Destruction of this habitat could have far-reaching consequences for the local ecosystem and wildlife. We urge the City Council to conduct a thorough environmental impact assessment before approving any rezoning.

Noise and Privacy: Introducing high-density residential units may result in increased noise levels and compromise the privacy of existing residents. We request that the City Council consider the potential impact on the quality of life for those near the proposed development.

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

Aesthetic Concerns: The current character of our neighborhood is complemented by the presence of the adjacent forested area. Clearing this space for high-density residential units may alter the area's aesthetic appeal. We ask the City Council to evaluate the visual impact on our community carefully.

Community Input and Engagement: Ensuring that the community's concerns are heard and considered is crucial in any development process. We urge the City Council to facilitate transparent communication and public hearings to allow residents to voice their opinions and contribute to the decision-making process. In conclusion, We kindly request that the Lenexa City Council thoroughly assess the potential implications of the proposed development in the forested area south of our neighborhood. Considering the points raised above will not only help preserve the character of our community but also contribute to the long-term well-being of our residents and the environment.

Thank you for your attention to this matter. We look forward to a thoughtful and considerate evaluation of our community's concerns.

Sincerely,



Jeffrey and Alicia Klein
816-591-4644
jeffrgklein@gmail.com

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Susan Cameron <susankc1953@icloud.com>
Date: December 29, 2023 at 1:55:49 PM CST
To: Julie Sayers <jsayers@lenexa.com>
Subject: Refining

Lenexa Planning Commission - Re-Zoning Request for 28 High Density Apts Canyon Ridge Apartments NW corner of K-10 & Canyon Creek Blvd.

Please please vote NO!

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Jim Keller <jimekeller@hotmail.com>
Date: December 28, 2023 at 8:52:20 PM CST
To: Julie Sayers <jsayers@lenexa.com>, Mark Charlton <mcharlton@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, "cwilliamson@lenexa.comcherron@lenexa.com" <cwilliamson@lenexa.comcherron@lenexa.com>
Cc: Bill Nicks <bnicks@lenexa.com>, Joe Karlin <karlin@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Craig Denny <cdenny@lenexa.com>
Subject: ODDO Rezoning Request

Lenexa Mayor & City Council Members,

Last night, we had the opportunity to hear from the ODDO Development leadership team about a proposed 28-building, 342-unit multi-story apartment complex (Canyon Ridge Apartment Homes), assisted living center, and convenience store/gas station development at the northwest corner of K-10 and Canyon Creek Boulevard, which they have submitted to the Council for rezoning consideration. We urge you to **deny this request** and retain the integrity and spirit of the current Master Land Use Plan for the following reasons.

As new Lenexa residents, we gave considerable weight in our decision to purchase land and build our current home in Canyon Creek Point based upon the Master Land Use Plan and the assurances made to us in the sales process by Prime Development that the land and views surrounding our neighborhood would be preserved. Our subdivision is less than ½ mile from K-10, and we appreciate that the natural landscape and elevation of the land between our homes and the highway shields us from views of K-10 and mitigates considerable traffic noise. However, with the upcoming expansion of K-10 to six lanes, we fear it will be intolerable if the protective ridge with its trees and thick vegetation is stripped from the ridge and apartment buildings will allow sightlines to expose the interstate.

We learned at the meeting that this is the second attempted development and at the time, the surrounding neighborhoods were able to successfully canvas and solicit feedback from homeowners representing then roughly \$75 million in home value. After five years, there are considerably more homes and much higher home values for you to consider; however, with limited time, we fear you will not have the full weight of resident input to consider before your January 8th meeting.

Furthermore, it is our understanding that a convenience store is already planned for the northeast side of that interchange, and with two Casey's less than two and four miles from that area, and a planned Quick Trip at 83rd and K-7, another convenience store is not needed nor desired. It will only serve to increase traffic on and off of K-10 and onto our currently serene parkway and as a security concern, introduce more non-resident traffic into our neighborhoods.

In addition, we learned from the ODDO team that the proposed site for their development is very narrow, causing them to squeeze a large, high density apartment complex into the area between the floodplain valley and the easement adjacent to K-10. We agree! In addition to the natural beauty, this space, which hosts many native animals and is a unique and desired feature for those of us living north of K-10 and west of Canyon Creek Parkway, is a key reason we chose to live here. This development and other development all around the intersection of K-10 and Canyon Creek Parkway will put them at risk and cause us to lose a vital component of what makes western Lenexa so unique and desirable.

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

This project requires land currently zoned for single family homes to be rezoned to support the proposed commercial development. Especially for those of us situated within 600 feet of the proposed apartment complex, with more transient residents and little incentive to maintain the exterior of their apartments, single-family homes are more desirable and would preserve the aesthetics and continuity of the surrounding neighborhoods. Even a quiet, low-rise general office (CPO) complex, if incorporating the natural surroundings and beauty of the area, (on the order of Corporate Woods in Overland Park) would be more desirable, and would also provide a neat, professional, and more pleasing introduction to western Lenexa to travelers along K-10 than what is being proposed.

Finally, Lenexa has many areas already zoned for this type of development which would be much better suited and less problematic to surrounding neighborhoods, such as the areas around Lenexa City Center with its access to amenities, and the Ridgeview/K-10 interchange.

For these reasons, and many others that we and our fellow Canyon Creek Point neighbors articulated to the ODDO development team, we respectfully request that you continue to honor the published Master Land Use Plan and reject this proposal.

We look forward to more amenable ideas and recommendations for the use of this land that will meet the needs of the city and its residents, while protecting the investments and desires of the existing landowners.

We appreciate your time and attention to our concerns.

Kind Regards,

James and Robin Keller
25923 W. 96th Terrace
Lenexa, KS 66227

Jim Keller
913-908-8360

All that is necessary for the triumph of evil is for good men to do nothing.

Edmund Burke 1729-1797

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Tammy Wainwright <kctammy2009@gmail.com>
Date: December 28, 2023 at 7:58:15 PM CST
To: Julie Sayers <jsayers@lenexa.com>
Subject: Re: Lenexa Planning Commission - Re-Zoning Request for 28 High Density Apts Canyon Ridge Apartments NW corner of K-10 & Canyon Creek Blvd.

Mayor Julie Sayers,

I'm emailing you about the proposal to build a large apartment complex with 28-high density apartment buildings, an assisted living center and convenience store and gas station on the northwest corner of Canyon Creek Parkway and K-10 in western Lenexa. This project, Canyon Ridge Apartments, will require rezoning to support the proposal and **I implore you to reject this massive project and protect our neighborhoods.**

This is the second attempt in the past five years to rezone this land and push through a high density project in our suburban neighborhood. The proposed project will remove acres of trees, destroy wildlife habitats and increase traffic and noise, lowering the quality of life for residents in the Canyon Creek neighborhoods, many who moved to this area for the natural beauty surrounding our homes. Property values will decline. Five years ago, residents rallied and in less than two weeks got the support of over 170 residents (representing \$75 million in property values) to oppose this type of development. Again, we have less than two weeks to make our voices be heard, since the Lenexa Planning Commission will meet on Jan. 8, 2024, to hear the developer's plea and opposition from many Canyon Creek area residents.

Again, **I urge you to reject this proposal** and protect the Lenexa residents who chose to live and invest in this part of Lenexa due to the published master land use plan. Many of us moved to this area specifically for the wooded landscape and the atmosphere of this section of Lenexa. A developer seeking a profit should not override the investment of tax-paying homeowners in this area lush with natural beauty. Additionally, the proposed building plat location is largely unfavorable requiring 10-foot retaining walls around nearly the entire complex, which will greatly raise the building height and visibility. As a resident, seeing the removal of hundreds of mature trees so a developer can profit, is a travesty and will negatively impact our climate.

This development will be an eyesore and not one the residents want to have at the western entrance of Lenexa. Other areas in our city, many already zoned for apartments, assisted living and a convenience store, would be better suited for this development than a single-family neighborhood.

Please honor the published master land use plan, show support to the residents of the Canyon Creek neighborhoods and reject this proposal.

Please let me know how you plan to vote.
Thank you.

Sincerely,
Tammy Ljungblad Wainwright
Brian Wainwright
26058 W. 96th St.
Lenexa, KS 66227
Canyon Creek Point residents

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Gary Link <gflink69@gmail.com>
Sent: Friday, December 29, 2023 2:16 PM
To: Bill Nicks <bnicks@lenexa.com>
Subject: Canyon Creek West Development

I live in Canyon Creek just north of the proposed apartment and assisted living development. In two words, I object. This development is entirely too dense with the number of apartment buildings plus the assisted living center. The noise from the apartments and commercial area as well as the assisted living complex (regular EMT traffic and sirens) as well as light pollution will have a negative impact on the residences just to the north. Additionally, there is a planned new multi-family complex and commercial development less than a half mile east across Canyon Creek Blvd.

It appears that the K10 corridor is becoming lined with apartments from Ridgeview to Cedar Creek Parkway. I believe these developments will degrade our home investments. Thanks for your consideration.
Sent from my iPad

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Rick Vaughn <rvaughn763@gmail.com>
Date: December 31, 2023 at 1:38:10 PM CST
To: Julie Sayers <jsayers@lenexa.com>
Subject: Oppose the Proposed Canyon Ridge Apartments Development

Dear Mayor Sayers:

I am writing to ask you to oppose the proposed Canyon Ridge Apartments development located at the northwest corner of K-10 Highway and Canyon Creek Boulevard in western Lenexa. This proposed development is a 28 multi-story building apartment complex, convenience store/gas station and assisted living complex that would be developed by Oddo Development (Rick Oddo). The project requires land zoned for single family homes to be rezoned to support this proposal.

My wife and I bought our retirement home in this area of Lenexa two and a half years ago because we loved the peaceful, quiet environment and the beautiful conservation area surrounding our neighborhood, which has abundant green space and lots of wildlife. Our neighborhood (Canyon Creek Point) skews older with many retired couples who picked this area for similar reasons. We love watching for deer, turkeys, and even bobcats right in our back yard and being able to see the stars in the sky at night above the expanse of trees. My wife and I often joke that we no longer need to go on vacation, because it feels like we are on vacation just sitting on our deck. The nature in this area is truly amazing!

When we bought our home, we did so with the knowledge that the current zoning in the Lenexa master land use plan prevents this proposed type of major development. We felt we could trust the master plan and hope the council does not vote to rezone the area. We moved from Overland Park to get away from the noise and light pollution and were willing to give up close proximity to retail, grocery, etc. in order to be somewhere more peaceful and closer to nature. If the land is rezoned and the development is approved, we fear that it will change all of this for the worse.

Here are some of our more specific comments and concerns regarding the project:

1) We understand and support the need for housing diversity across Lenexa and believe the current zoning in and around our neighborhood already supports an impressive balance of multi-family and single-family developments. In terms of multi-family developments similar to the one being proposed, within one mile of our house there is already The Mansions at Canyon Creek (with 220 apartment/condo units), Mize Hill, which is currently under construction (162 units of duplexes/twin villas), and Canyon Creek Apartments which was recently approved (with 212 units). This totals 594 multi-family units already approved in our neighborhood. Lenexa's Comprehensive Plan mentions that "effective planning and management of development has ensured multi-family housing is diffused throughout the city and is not spatially concentrated." If you add the 345 proposed units, the total number of multi-family units within one mile of each other would be 939, which would be about 6.5% of the total in Lenexa if my math is correct (per the Comprehensive Plan - 12,252 existing units plus 2,281 more needed by 2030 = 14,533). This seems spatially concentrated in a city of over 34 square miles. Are the areas currently zoned for multi-family use maxed out? If not, why are we not adding additional multi-family units in those zones? Is it really necessary to rezone our area?

2) Additional noise and light pollution would result from this project for our area. The development plan calls for the removal of most of the trees on the land. Currently those trees not only provide beautiful green space, but also help to buffer the noise from K-10. Once they are removed, the reverberations off the buildings will likely

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

be much louder than we have now. When K-10 is expanded, it will be even worse (which also makes me wonder if it is even a good location for an assisted living facility, with all the noise from the highway and busy intersection). It also appears the buildings would have lots of outside lighting in front and back that would cause light pollution in our neighborhood (and in other surrounding neighborhoods).

3) The project includes a convenience store/gas station, but one has already been approved to go in right across the street in the Canyon Creek Apartments development. When asked, the developer, Mr. Oddo, was apparently not aware this was the case. Additionally, we already have a Casey's on Prairie Star Parkway (less than 2 miles away) and a new Quik Trip is being built at K-7/83rd street (less than 4 miles away). It doesn't seem like another gas station is needed in the area.

4) We also have a strong concern for what this project would do to the surrounding environment and in particular the green space, trees, and the wildlife. Lenexa's Comprehensive Plan makes some important statements about the need to protect and conserve our natural resources and to take environmental changes seriously. The removal of trees and green space with an increase in noise and light pollution will have a negative impact on the wildlife in the area, and they will almost certainly start to disappear.

Thank you for taking the time to read all this, I know you have a lot on your plate. I think you may know our daughter Lindsay Vaughn through politics. She is always telling us how important it is to reach out to our elected officials and be actively engaged in our community. We really love our home and the sanctuary it has become for us, and the developer made us feel relatively powerless to change the outcome of this project. We hope in appealing to you and the city council that our perspectives and the stories of others in our neighborhood will be considered as you evaluate the proposed development. We truly appreciate your consideration of our request.

Sincerely,

Rick Vaughn
25955 W. 96th Terrace
Lenexa, KS 66227
PH: 816.674.6547
Email: rvaughn763@gmail.com

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Kate Flax <ksflax@yahoo.com>
Sent: Friday, December 29, 2023 9:34 AM
To: David Dalecky <ddalecky@lenexa.com>
Subject: Re: Canyon Creek Apartment Home Plans

Thank you, David.

My main concern is the proximity of this development, and the type of development being proposed, in relation to our neighborhood. I heard the edge of the development will come within 600 feet of our homes which especially impacts the west side of the Canyon Creek by the Park section where I live. My home faces west on Wild Rose street and will look directly at a gas station and large apartment buildings. The homes in the Canyon Creek by the Park section range from 650k to over a million dollars and it's absurd to me that it's being considered to put another apartment complex and even worse, a gas station, when you have this level of homes. I don't know of any other subdivision around this area that has this level of homes that is surrounded by apartments and a gas station.

I have two teen girls and the thought of our home being within 600 feet of random people coming in and out of a gas station is a scary thought. We built our home with the understanding this would be a safe and clean area to live. Adding this development will result in more traffic, more noise, more trash and reduced safety.

I am strongly against this development, and very much appreciate you taking these comments and combining them with those of other concerned residents to share with the Planning Commissioners and City Council Members.

Thank you,
Kate Flax

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Michael Szczygiel <mszczflgtrp69@gmail.com>

Sent: Wednesday, January 3, 2024 8:49 AM

To: Julie Sayers <jsayers@lenexa.com>; Bill Nicks <bnicks@lenexa.com>; Mark Charlton <mcharlton@lenexa.com>; jcarlin@lenedxa.com; ceitirich@lenexa.com; Melanie Arroyo <marroyo@lenexa.com>; Chelsea Williamson <cwilliamson@lenexa.com>; Craig Denny <cdenny@lenexa.com>; Chris Herron <cherron@lenexa.com>

Subject: Canyon Creek Zoning Changes

My wife and I built a home in Canyon Creek in 2010. A major component of our decision-making process was our need to select a site, which based on extant zoning, would be compatible with our chosen lifestyle. Naturally, the characteristics of the neighborhood, determined in part by the type and number of structures, absence of commercial enterprises and population density, were and are of paramount importance. The proposed zoning changes are such that if they would have been in existence on 2010 we would not have chosen to build in Canyon Creek. These changes will destroy our right to enjoy a product we purchased, our home, in the manner in which we intended; effectively creating the equivalent of a "bait and switch." We appreciate your consideration.

Michael Szczygiel (Segal)
Cathy Moffett
25204 W 97th Terrace

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Suzanne Luke <suzannedahle@hotmail.com>
Date: January 3, 2024 at 2:31:58 PM CST
To: Julie Sayers <jsayers@lenexa.com>
Subject: Apartments at K-7 and Canyon Creek Blvd

Hello Julie,

Welcome to your new position as Mayor of Lenexa! We voted for you and we're glad you're in there.

I am a resident of Canyon Creek by the Lake and I'm really concerned about the proposal to build high density apartments at the corner of K-7 and Canyon Creek Blvd. We purchased in this neighborhood because of the beautiful feel of the land, the quietness of the neighborhoods, and overall atmosphere...and we definitely paid for those things! We currently have a neighborhood atmosphere similar to that of Cedar Creek whose values have remained high and strong throughout the years. Adding high density apartments would decrease that atmosphere and therefore decrease our values and the amount of taxes the City of Lenexa would receive. The entire landscape and feel would change. We need to keep our values in line with those of Cedar Creek and therefore, we need to keep the atmosphere the same by not allowing high density apartments to be built.

Also, Canyon Creek Elementary is already busting at the seams with houses still being built. Adding high density apartments would also affect my children's education because the sizes of the classrooms would increase.

Our area of Lenexa is NOT the right place for high density apartments. It was never part of the master plan and those living in these areas do not want it.

Please consider these points when addressing the proposed development at the meeting on January 8th.

Thank you!

Suzanne Luke
9776 Shady Bend Circle
Lenexa, KS 66227

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Vanessa Calcara <vanessagower@gmail.com>
Date: January 3, 2024 at 2:45:15 PM CST
To: Vanessa Calcara <vanessagower@gmail.com>
Subject: Proposed Rezoning in Western Lenexa

Lenexa City Council Members,

I am reaching out to encourage you to reject an upcoming proposal to build a large apartment complex, convenience store and assisted living complex near my home in western Lenexa. Since moving to this area in 2014, we have faced two other rezoning attempts on the outskirts of our neighborhood...and both times neighbors have rallied together to stop them and to preserve what little bit of nature we have left. My family chose Canyon Creek almost 10 years ago based on the published master land use plan--among other things. We enjoy being out of the hustle and bustle and love our natural views and wooded surroundings. There are so many areas in Lenexa that are already zoned for this type of use...so why ruin the natural beauty around us and remove the only barrier we have from K-10?

Please vote to honor the published master land use plan and reject this proposal.

Thank you,
Vanessa Calcara

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

On Aug 27, 2018, at 12:57 PM, Patrick C. Miller
pat.miller.travel@gmail.com> wrote:

Bill and Tom,

I am a resident of Canyon Creek by the Park, and have lived in this neighborhood since 2010. Over the course of the last eight years my wife and I have seen the issue of re-zoning raised in a nonchalant manner several times by the City of Lenexa. To be frank, I am very tired of those discussions, and I want to make sure my voice is heard. I did not invest into the vision of a neighborhood only to have that vision repeatedly changed by the Lenexa Planning Commission. I know it doesn't really matter to a lot of city planners; however, to someone who has invested literally hundreds of thousands of dollars into a home and neighborhood it does.

There are discussions underway, which support re-zoning an area at the intersection of K-10 and Canyon Creek Boulevard with "high density apartments". I am against these plans 100%. We already have hundreds of apartments directly to the north of us. I don't feel we also need hundreds of apartments to the southwest of us.

I was told the mayor believes "the silent majority supports this re-zoning effort, while only a vocal minority is against it". I can't confirm this statement by the mayor is accurate, but I have no reason to believe the neighbor who conveyed this remark to me is being dishonest. Unless the mayor has actually knocked on every door in this neighborhood, I don't know how he could make this claim. I've never spoken to any neighbor, who supported re-zoning our neighborhood either now or in the past.

In sum, I am against this re-zoning and I encourage both of you to vote against it (please). I intend to attend this meeting tonight -- even though it means working a full day and missing part of my daughter's birthday celebration tonight.

Please confirm your receipt of this email.

V/r,

Pat Miller
9632 Zarda Drive
Lenexa, KS 66227
316-737-3791

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: michelle.moseman@prodigy.net

Date: January 3, 2024 at 10:09:50 PM CST

To: Julie Sayers <jsayers@lenexa.com>, Bill Nicks <bnicks@lenexa.com>, Mark Charlton <mcharlton@lenexa.com>

Cc: Joe Karlin <jkarlin@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Craig Denny <cdenny@lenexa.com>, Chris Herron <cherron@lenexa.com>

Subject: K-10 and Canyon Creek - Canyon Ridge Apartment Homes

Happy New Year Lenexa City Council Members and Mayor Sayers,

I have been a resident of Lenexa residing in the Canyon Creek by the Park neighborhood (Ward 2) since 2010. However, I am growing quite concerned over repeated attempts to rezone and change the vision for this beautiful area near K-10 and Canyon Creek. We did not invest in the vision of a neighborhood/city only to have that vision repeatedly changed by the Lenexa Planning Commission.

For historical context, we had this exact.same.conversation in 2018 when there was a proposal to rezone K10 and Canyon Creek with apartments (see attached 2018 email with Bill Nicks). We were opposed to this rezoning back then and we remain opposed to this rezoning now. **What changed?** Why is Lenexa revisiting this rezoning issue after it was rejected previously?

Reasons why I oppose the rezoning:

1. K10 and Cedar Creek was meant to be the “western gateway” or “front door” to Lenexa. The city’s first impression should not be apartments. It is not consistent with Canyon Creek (north side of K10), nor is it consistent with Cedar Creek (south side of K10).
2. My sister’s family *and* former co-workers live in Canyon Creek by the Point and their backyard view will change from picturesque wooded trees to apartment buildings.
3. We were all sold on a vision and we don’t want Lenexa to be known as bait and switch with an evolving (degrading) vision for this area.
4. My 3 elementary aged children don’t need the added safety risks due to increased traffic that apartments will bring to Canyon Creek Pkwy.
5. We already have apartments (Mansions at Canyon Creek) adjacent / immediately to the North of our subdivision. We don’t need apartments in 360 degrees surrounding our neighborhood.
6. I (not the city planners) invested in this community / neighborhood and I want to protect my investment by preserving my home value. Every few years, we keep degrading the quality of the build around us.

Although tangential, I think it is important for you to understand why Canyon Creek residents are so sensitive to rezoning. We’ve had a parade of quality degradations since we moved in.

7. In 2012, Clay Blair purchased the Canyon Creek property and loosened the requirements for concrete tile roofs and all-stucco exterior walls.
8. During the summer of 2014, the owner of the land at the southeast corner of 99th Street and Canyon Creek Boulevard, across the street from the Canyon Creek pool complex, intended to build entry-level “starter” homes with prices starting in the \$200’s. Many Canyon Creek homeowners expressed concern that this kind of housing would have an adverse impact on property values in Canyon Creek. Clay Blair

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

agreed with us and wrote the attached letter underscoring the concerns that entry-level homes would decrease property values.

9. In 2018, we fought the rezoning of K10 and Canyon Creek to high density apartments.

It is convenient to assume that it is a vocal minority opposed and a silent majority supports, but that is categorically false. I'm attaching the 170+ signatures that were submitted in 2018. Meanwhile, in 2024, there is a new change.org petition being circulated as we speak. I have personally spoken to at least 20 households (= 40 voting members of Lenexa) tonight and 100% of them are opposed to the apartments as well. I work fulltime, so my time is limited and I can't hit every household in the city, but these results speak for themselves. When, in politics, do we have 100% agreement on any issue?

Finally, I am disappointed with the poor timing of this entire discussion occurring within a week of the holidays. That said, I will be sitting at 6 basketball and soccer games on Saturday and will be doing my civic duty to ensure every neighbor is educated on this topic.

Bill, you've been a great advocate for us in the past. Always listening. I hope we can count on you and the others I've copied for your continued support and understanding of these concerns.

Mayor Sayers, you've said you wanted to do the right things, for the right reasons, and in the right way. I trust you will oppose the rezoning since it isn't the right thing to do to residents who were sold on a vision of K10 and Canyon Creek being the "western gateway" of Lenexa – meaning we should be maintaining the integrity of that vision, quality of the area, and representing the wishes of the constituents that you serve.

Please confirm acknowledgement of this email and ensure it is shared with those who will be voting on this issue. Several neighbors, including myself, will see you on Monday, January 8th to continue the discussion.

Sincerely,
Michelle Miller
Canyon Creek by the Park resident

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Marcia Bledsoe <mdmoseman@gmail.com>
Sent: Wednesday, January 3, 2024 7:34 PM
To: Bill Nicks <bnicks@lenexa.com>
Subject: Opposition to Lenexa's Re-Zoning Proposal

Dear City Council Member Bill Nicks,

I am writing this letter to encourage you to reject a proposal to build a large 28 multi-story building apartment complex, convenience store and assisted living complex in western Lenexa. My husband and I spent our entire life savings to build our dream home in Canyon Creek Point a little over a year ago and the main reason we picked our specific plot was due to the surrounding nature and the peaceful secluded view. Our decision to move to Lenexa and make it our forever home was based solely on the Lenexa Master Land use plan. We love nature and would like our 1 year old daughter to be raised in this area. The Lenexa Master Land use plan (as you are aware of) called for this area to be single-family and agricultural. We hope that the city council members will be empathetic to us and the many families like us that will be affected by re-zoning this area. We believed in the city's leaders that they would continue to preserve these trees and wildlife for all the single-family residents to enjoy. It would be heartbreaking and disappointing to tear it all down in the name of commercialism.

Additionally, our neighbor informed us this is the 2nd attempted development of this land in 5 years. Previously, surrounding neighborhoods canvassed and rallied support of over 170 signatures representing 75 million in home values in the 2-week notification period last time, and we have again been given less than 2-weeks to oppose this rezone effort. (January 8th is the Planning Commission Meeting). A developer presenting a plan for this type of development should not override the investment of many hundreds of homeowners in this area. Plus, the proposed building plat location is largely unfavorable requiring 10-foot retaining walls around nearly the entire complex, which will greatly raise the building height and visibility. Finally, an additional convenience store is completely unnecessary as there are Casey's <2 and <4 miles away with a Quick Trip being built on 83rd and K-7.

This proposal is an eyesore and truly unfitting for the western entrance of Lenexa. Lenexa has many areas already zoned for such a development and would be much better suited and zoned near walkable amenities around the City Center area or Ridgeview interchange. Again, we strongly urge you to honor the published Lenexa Master Land use plan and reject this proposal.
I appreciate your time and attention to this request.

Thank you,

Marcia Bledsoe

26261 W 96th Ter, Lenexa KS

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Susan Cameron <susankc1953@icloud.com>
Subject: Rezoning
Date: December 29, 2023 at 2:57:36 PM EST
To: jkarlin@lenexa.com

Lenexa Planning Commission - Re-Zoning Request for 28 High Density Apts Canyon Ridge Apartments NW corner of K-10 & Canyon Creek Blvd.

Please please vote NO!

Sent from my iPhone

Canyon Ridge (RZ23-07, PL23-12P)
Public Comment Emails Received for the January 8, 2024 Planning Commission Meeting

From: Marcia Bledsoe <mdmoseman@gmail.com>
Sent: Wednesday, January 3, 2024 7:21 PM
To: Julie Sayers <jsayers@lenexa.com>
Subject: Opposition to Lenexa Re-Zoning Proposal

Mayor Julie Sayers,

I enjoyed reading about you in Lenexa's January 2024 Towntalk magazine. It's exciting to see you are a designer and uplifting to hear you want "all parts of Lenexa to remain healthy, beautiful and connected." I am writing this letter to encourage you to reject a proposal to build a large 28 multi-story building apartment complex, convenience store and assisted living complex in western Lenexa. My husband and I spent our entire life savings to build our dream home in Canyon Creek Point a little over a year ago and the main reason we picked our specific plot was due to the surrounding nature and the peaceful secluded view. Our decision to move to Lenexa and make it our forever home was based solely on the Lenexa Master Land use plan. We love nature and would like our 1 year old daughter to be raised in this area. The Lenexa Master Land use plan (as you are aware of) called for this area to be single-family and agricultural. We hope that you and the city council members will be empathetic to us and the many families like us that will be affected by re-zoning this area. We believed in the city's leaders that they would continue to preserve these trees and wildlife for all the single-family residents to enjoy. It's refreshing to see that your goal is "to make sure residents continue to see an exceptional quality of life." It would be heartbreaking and disappointing to tear it all down in the name of commercialism.

Additionally, our neighbor informed us this is the 2nd attempted development of this land in 5 years. Previously, surrounding neighborhoods canvassed and rallied support of over 170 signatures representing 75 million in home values in the 2-week notification period last time, and we have again been given less than 2-weeks to oppose this rezone effort. (January 8th is the Planning Commission Meeting). A developer presenting a plan for this type of development should not override the investment of many hundreds of homeowners in this area. Plus, the proposed building plat location is largely unfavorable requiring 10-foot retaining walls around nearly the entire complex, which will greatly raise the building height and visibility. Finally, an additional convenience store is completely unnecessary as there are Casey's <2 and <4 miles away with a Quick Trip being built on 83rd and K-7.

Since you are a designer, you can see what an eyesore this proposal is and truly unfitting for the western entrance of Lenexa. Lenexa has many areas already zoned for such a development and would be much better suited and zoned near walkable amenities around the City Center area or Ridgeview interchange. Again, we strongly urge you to honor what you stated in the Towntalk magazine by "doing the right things, for the right reasons and in the right way" by upholding the published Lenexa Master Land use plan and reject this proposal.

I appreciate your time and attention to this request.

Thank you,
Marcia Bledsoe
26261 W 96th Ter, Lenexa KS

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Kaylee Johnson <kaylee.johnson2@gmail.com>

Sent: Friday, January 5, 2024 9:03 AM

To: Julie Sayers <jsayers@lenexa.com>; Bill Nicks <bnicks@lenexa.com>; Mark Charlton <mcharlton@lenexa.com>

Subject: Lenexa Resident Opposed to Canyon Ridge Apartments

Mayor Sayers, Mr. Charlton and Mr. Nicks,

I am a Lenexa resident who lives in Canyon Creek by the Lake, here in Lenexa. We are aware of a recent proposal to rezone the Northwest Corner of K-10 and Canyon Creek Blvd. We were here 5 years ago when a similar request was made, and we were part of the opposition requesting this not occur. Our elected officials listened to us then, and we hope you, as our elected officials today, can listen to us know.

This rezoning request is an attempt to overturn land zoned for single family homes and goes against the Lenexa Master Plan that we along with MANY other homeowners used to make a decision to invest significant money to live here in Lenexa. I am respectfully urging the council to reject this proposal.

When my husband and I purchased our home in Canyon Creek by the Lake 7 years ago, we chose this Western edge of Lenexa due to the city's plan. We love the feel of living near the 'country' while still allowing our 3 kids to attend amazing schools and having access to all the city has to offer. It is vital to retain green space and maintain this area, currently zoned as single-family homes, in order to keep our home values up. This proposal directly contradicts the vision Lenexa put in place and what was promised to those of us who chose to be residents of this very special part of the city.

Lastly, the Northeast side of Canyon Creek Blvd has already been approved for some major development, the addition of 28 high density apartment type buildings adjacent to this major development would completely change the look and feel coming into our beautiful neighborhoods.

Again, as our elected officials, and knowing that Canyon Creek residents have already opposed this rezone in the past, I urge the council to prevent this type of rezoning.

Thank you for your consideration,

Kaylee & Aaron Johnson

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Steve Bennett <stevewildcat13@gmail.com>

Sent: Friday, January 5, 2024 10:08 AM

To: Julie Sayers <jsayers@lenexa.com>

Subject: Rezone Development- NWC of K-10 & Canyon Creek Blvd

Honorable Ms. Sayers:

I live in Canyon Creek Point.

Our development backs up the the proposed rezoning of the property at the NWC of K-10 and Canyon Creek Blvd.

Our neighborhood is greatly concerned of the City's consideration to rezone this for apartments and a gas station.

The are in question is full of wildlife, a creek and the proposed apartments will span almost 40' up in the air.

And there is another gas station planned right across the street that the City has already approved.

This is not an appropriate development for this area.

It is going to be further impacted by the planned expansion of K-10.

With the City's core value being to have green space, including a lot of parks, etc, this seems to go against this value with all the removal of habitat in the area.

Speaking of the K-10 expansion, we understand the need for this, but not the precedence of it being a toll road. We pay gas tax for repair and expansion for such things and more and more this is happening creating more financial impact to travelers.

Respectfully, people are getting tied in the troubling economy and political environment of this over reach.

Appreciate in advance you taking the time to listen.

Sincerely,

Steve Bennett
25891 W. 96th
816-730-0751

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Melinda Marquess <mmarquess1@att.net>

Date: January 6, 2024 at 1:35:47 PM CST

To: Julie Sayers <jsayers@lenexa.com>

Subject: Rezoning on NW corner of K-10 and Canyon Creek Blvd

Hi Mayor Sayers,

As Lenexa's new mayor, please do NOT allow the rezoning of K-10 and Canyon Creek Blvd to allow more apartment buildings, etc. and for those who built and moved to the Canyon Creek developments to lose the natural barrier of the noise from K10 and the natural beauty. We're already getting office buildings, etc on the NE corner of K-10 and Canyon Creek Blvd which will contribute to a lot more traffic.

This proposal requires multiple plats to be rezoned from RP1 and CPO which rejects the published Lenexa Master Land use plan which we used when we bought in this part of Lenexa.

Canyon Creek residents need your support in this matter.

Sincerely,
Melinda Marquess
913-940-1381

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

Topic: Proposed Canyon Ridge Apartment Homes Development

January 7, 2024

Dear Mayor Julie Sayers,
Mayor of the City of Lenexa

We are Susie and Dennis Burket, three-year residents of Canyon Creek Point (26229 W96th Ter). We have several major concerns to express to you with the desired outcome that you **vote against** the proposed rezonings, distance variances and construction at K-10 and Canyon Creek Road.

Concerns: (1) The proposed development does not support the Lenexa Comprehensive Plan - current zoning is appropriate and should not be changed; and (2) there is not enough time for residents to understand the interrelated effects of this proposal – all decisions related to the proposal must be delayed until after analysis, review, and approval of the proposed 2024 Lenexa Comprehensive Plan.

The proposed rezonings, safe distance variances, and required 10-ft retaining walls needed to squeeze in the maximum number of housing units on heavily wooded, sloped, and isolated land next to K-10 do not fit the Lenexa vision for future or current residents. Future residents of this proposed shortsighted development would be living in a loud, crowded, and isolated area with no sense of community and connection. Current residents would experience a loss of neighborhood character with a view of K-10 through three-story equivalent buildings sitting on required 10-ft retaining walls in a stripped area with additional swaths of land stripped through the Wetland/Wildlife Sanctuary to connect utilities. The scale of required woodland stripping would cause wildlife currently living in this area to be further compressed into an area already full of wildlife seeking sanctuary. Past City Councils got it right, the current zoning fits with the shared vision of Lenexa.

Decisions related to this impatient and ill-timed proposal should be delayed by the City Council until residents can get a good picture of the possible outcomes and the effects on the lives of current/future residents. Time is needed for residents to review the proposed 2024 Comprehensive Plan. There is too much not known about the interrelationships and possible outcomes between: other approved and proposed residential developments and projects in proximity; proposed K-10 interchange expansion and highway widening; school district capacity; property values and taxes; traffic and noise; future parks/off-leash areas; deforestation of thick red cedar woodland; and wetlands/wildlife sanctuary and resident wildlife.

In conclusion, current zoning in the proposed development area is correct for current/future residents, and wildlife. Residents need time to study the interrelationships between many factors and effects from the rushed developer's proposal. We extend an offer for you to visit us in our Canyon Creek Point home and view the proposed development area from our deck.

Regards,
Susie and Dennis Burket
913.593.5835

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Daholt <d2aholt@sbcglobal.net>
Date: January 6, 2024 at 7:11:15 PM CST
To: Courtney Eiterich <ceiterich@lenexa.com>
Subject: Canyon Ridge Apartments

Dear Council Person,

We understand that over the holidays an apartment complex at K10 and Canyon was approved by the Planning commission.

There has been no time for people in Canyon Creek to evaluate that. Apartments at that location was rejected previously and need to be rejected again for same reasons as back then including not in accordance with the master plan for city and county. There was supposed to be single family homes at the location.

Please reject the project in total. No one in Canyon Creek wants apartments at that location. There are plenty of other places for apartments.

Thank you very much

Don and Diane Aholt
25008 W 98 Street
Lenexa, KS 66227
Sent from my iPad

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: "Alonso, Joe" <Joe.Alonso@saint-gobain.com>

Date: January 7, 2024 at 12:52:42 PM CST

To: Courtney Eiterich <ceiterich@lenexa.com>

Subject: **FW: Oppose the development of 28 apartment buildings, a convenience store, and an assisted living center on the northwest corner of K-10 and Canyon Creek Blvd.**

Mrs. Eiterich

I am a resident of Canyon Creek Point subdivision and oppose the proposed rezoning and planned development of 28 apartment buildings, a convenience store, and an assisted living center on the northwest corner of K-10 and Canyon Creek Blvd. We were assured by the developer when we moved in that our views would remain forever unspoiled. I believe if the council approves the rezoning, we will incur the following issues.

1. The destruction of the wetlands.
2. The disturbance of wildlife i.e. owls, deer, coyotes, squirrels, rabbits, raccoons, wild turkeys, etc. in our backyard.
3. The added pollution and noise.
4. Additional dust from the construction and the blasting effecting my home.
5. Additional traffic and congestion.

The reason we moved into the sub-division is because of the beauty and the assurance that our views would remain forever unspoiled.

Please help keep our community beautiful. Vote to not rezone and plan a development of 28 apartment buildings, a convenience store, and an assisted living center on the northwest corner of K-10 and Canyon Creek Blvd.

Thank you,

Joe Alonso

Senior Director, Gypsum Sales West

20 Moores Road
Malvern, PA 19355 - USA
Tel.: 913-579-6722
certainteed.com

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: IAN CUTTS <cuttsinmemphis@aol.com>

Date: January 7, 2024 at 1:46:18 PM CST

To: Julie Sayers <jsayers@lenexa.com>, Mark Charlton <mcharlton@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Chris Herron <cherron@lenexa.com>, Bill Nicks <bnicks@lenexa.com>, Joe Karlin <jkarlin@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Craig Denny <cdenny@lenexa.com>

Cc: icmedcon@gmail.com

Subject: Proposal to build 28 multi story apartments/ convenience store/assisted living NW K10 and Canyon Creek Boulevard

Dear Planning Committee,

We are writing to formally express our strongest objections to the proposed development at NW K10 and Canyon Creek Boulevard. As residents of Canyon Creek Point we have significant concerns regarding not only the development itself but it's potential impact on the bio diversity of the surrounding area including our own home and the well being of surrounding residents.

The proposed construction will create years of noise, dust, vibration and pollution from equipment not only affecting our health and well being but the unique surrounding ecology. Our subdivision is flanked by wetlands which are highly productive and biologically diverse systems. We have a responsibility to maintain ecosystem productivity. In addition, the use of hazardous materials or ground contamination will disrupt the environment from this significant high density development.

Furthermore, the removal of 45 acres of dense woodland bordering our wetlands will displace existing wildlife including hawks, eagles, deer, coyote and create a large increase in noise from traffic.

What will be the impact on drainage from this high density site with its numerous structures?

The development will cause a considerable increase in traffic attempting to leave and to join K10.

We purchased our property in CCP not only based on the merits of the building but more importantly because of the serenity of the area! It was described as a neighborhood surrounded by nature. " City owned parkland and a natural conservation area surrounding the community allowing sweeping views of nothing but nature and assuring residents that those views will remain forever unspoiled" is what we were sold. Two years later that is under threat.

Where is the integrity?

Whilst we understand the need for more affordable housing we request that this development is located in a more suitable area.

Sincerely,
Gill and Ian Cutts
Sent from my iPad

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Brian Stevens <cerbds@gmail.com>

Date: January 7, 2024 at 2:06:04 PM CST

To: Julie Sayers <jsayers@lenexa.com>

Subject: Concerns about zoning change

Mayor Sayers,

It has come to my attention only very recently that the City of Lenexa is considering zoning changes and possible approval of a plan to build multifamily units at K10 and Canyon Creek Parkway.

I am a homeowner in Canyon Creek Point. Judging from the maps I have seen, the new structures will be within 600 feet from my back patio / deck and likely some of the habitat and trees will be removed much closer than that. We have lived in our home that we built for 4.5 years.

When we bought our lot, noise from K10 and what might someday be done with the land behind us were really our 2 only concerns. While we did not do everything that we maybe could have to understand the plans for the land, we did explore websites and talk with people about it. Prime development told us (and had posted on their website) that nothing would be done with the land, "ever". We did find information that made it seem likely that a convenience store and/or some offices would might someday be built there, we never imagined and quite frankly we don't see how it is being considered, that apartment complexes that will be close to 40' tall (with a 10' retaining wall holding it up making it more like 50' tall) would be build right in plain site of our back windows.

In addition, finding out about this right before Christmas and now understanding that the planning committee will make a decision on 8-January and the council will vote shortly after that seems very intentional to push this through without allowing homeowners like me to spend the time to understand the process, make recommendations and allow us to understand the impact and what options we have to make those minimal. I request that we be given time to do what we need to do to make this situation acceptable to all.

The noise is my biggest concern at this point. I know it is easiest and that the number of structures for this plan takes up most of the acreage, but why do all of the trees need to be removed? My understanding is that they may need to remove all of the trees clear down to the creek. Those trees are a natural sound barrier and without them, I fear that every semi truck that goes by will be something I can hear in my kitchen.

While that may not seem like something you need to be concerned with, I feel that the city has to take into consideration what the plan for the city was when I bought my lot just over 5 years ago. Zoning for single family homes, a convenience store and possibly some offices is very different than 350+ apartments along with a nursing home. When the City of Lenexa did planning and the plan was published, I have to imagine that the plan included the number of people in the area and what that meant long term. Now, without redoing the studies and understanding if these proposed zoning changes (along with others in our area that have added more apartments than originally planned) will have any impact on the overall plan, a decision is being rushed through.

Thank you for considering my concerns.

Cheers,
Brian Stevens

816-588-1265
cerbds@gmail.com

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Daholt <d2aholt@sbcglobal.net>
Date: January 6, 2024 at 7:11:15 PM CST
To: Courtney Eiterich <ceiterich@lenexa.com>
Subject: Canyon Ridge Apartments

Dear Council Person,

We understand that over the holidays an apartment complex at K10 and Canyon was approved by the Planning commission.

There has been no time for people in Canyon Creek to evaluate that. Apartments at that location was rejected previously and need to be rejected again for same reasons as back then including not in accordance with the master plan for city and county. There was supposed to be single family homes at the location.

Please reject the project in total. No one in Canyon Creek wants apartments at that location. There are plenty of other places for apartments.

Thank you very much

Don and Diane Aholt
25008 W 98 Street
Lenexa, KS 66227
Sent from my iPad

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Brad Krehbiel <thermoguy1@gmail.com>

Date: January 7, 2024 at 2:27:06 PM CST

To: Julie Sayers <jsayers@lenexa.com>

Subject: Proposed Development at K-10 & Canyon Creek Blvd.

Hello, Julie,

First, congratulations on your election as Mayor. The City of Lenexa, in my opinion, has always been a leader in quality-of-life and aesthetics, and your background in design and construction should only reinforce that. I look forward to seeing what the City can accomplish under your leadership!

I recently became aware of this proposed project (Convenience store, nursing home, and 346 apartment units) from other concerned citizens in our area (Canyon Creek and Canyon Creek Point). It seems that the Developer is trying to fast-track approval of this project. There was a hastily-arranged "informational meeting" for the neighborhood on December 28, which I was not available to attend, and now the project is on the agenda for the Planning Commission meeting on January 8. Luckily, some of my neighbors WERE able to attend the December meeting, and have made us aware of this project.

I have read through the (very lengthy) packet of information for the Planning Commission meeting and have noted that the recommendation of Staff is to approve this project and send it to the City Council for the February 6 meeting. If recent history is any predictor, the Planning Commission will rubber-stamp its approval, with maybe a couple of stipulations. I am planning to attend the meeting to offer my input.

Assuming this project is sent your way in February, I'd like to address some possible points of discussion for the Council:

1. The existing Future Land Use Plan calls for this area (and for some distance to the West) to be used for "Office/Employment Center" purposes. Although the Developer (in a January 4, 2024 letter to the Planning Commission and Council) cites studies proposing changing this future use designation to "High-density Residential," this has not been done. The proposed rezoning should be considered with respect to the plan in existence today.
2. There is nothing in the packet indicating that any kind of environmental impact or conservation study for the site and surrounding area has been conducted or proposed. It is very probable that the wetland area to the north of the site will be adversely affected by this project, either during site development or construction.
3. The Exhibit purporting to show "sightlines" from the existing homes in Canyon Creek Point and Canyon Creek by the Lake Subdivisions (Drawing A300, packet page 101) is very misleading. The existing deciduous trees in the valley are shown as tall as 60 feet, which they decidedly are not. Even if they were that tall, this blockage would occur only when there are leaves on the trees. Many of these trees are Hedge trees (Osage Orange), which are the first to drop leaves in the Fall and the last to re-leaf in the Spring.
4. This project will generate large amounts of dust and construction noise for many years, especially during the months when the prevailing winds are from the south. Although this is

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

inevitable during any construction project, we would hope that the City would require the Developer to have dust and noise mitigation plans in place if this project is approved.

5. This project may involve blasting to remove rock (not addressed in the packet). If so, how does the Developer plan to communicate with and protect the surrounding neighbors?

My neighbors and I would strongly encourage the Council to deny this project, but if it is your inclination to approve it, we would appreciate any help you can give us to make sure the project does not impact the area more than absolutely necessary. Thank you for your time and consideration.

Sincerely,

Brad Krehbiel, PE

26009 W 96th St

Lenexa, KS 66227

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Matt Kaminsky <mattkaminsky68@gmail.com>

Date: January 7, 2024 at 4:30:24 PM CST

To: Julie Sayers <jsayers@lenexa.com>, Mark Charlton <mcharlton@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Chris Herron <cherron@lenexa.com>, Bill Nicks <bnicks@lenexa.com>, Joe Karlin <jkarlin@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Craig Denny <cdenny@lenexa.com>

Subject: Proposed Apartment Complex to the South of Canyon Creek Point

To the Mayor and Lenexa Planning Commission,

We are Matt and Rochelle Kaminsky and have lived at 26133 West 96th Terrace in Canyon Creek Point since April 2020. Our home is currently one of many homes whose backyard will backup to the proposed Apartment Complex. We have many concerns about this development being passed because of the reasons listed below.

1. Prior to buying this property we looked at the Master Plan and it showed that it would not be developed as multi-family. That was one of the main reasons why we bought this property so we wouldn't have to look at apartments and we would have this quiet, forested beautiful sanctuary in our backyard.
2. Currently we get deer, bobcats, coyotes and many types of birds in our backyard including hawks, owls, pileated woodpeckers, and turkeys. With the destruction of the land and potential damage to the wetlands we are very concerned how it would affect the wildlife in our area.
3. We were told and under the assumption that this area would never be developed.
4. As a former contractor specializing in concrete construction for 20 years I'm very concerned about any blasting and what it will do to the foundation of our home.
5. Another concern is with the excavation of the trees and topsoil the damage from the runoff it will have into the stream and ecosystem in the valley.
6. The potential effect it will have on our home's resale value.

We appreciate your time and consideration,

Matt and Rochelle Kaminsky

--

Matt Kaminsky

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Tracy Thomas <tjthomas13212@gmail.com>

Date: January 6, 2024 at 10:10:40 PM CST

To: Mark Charlton <mcharlton@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Chris Herron <cherron@lenexa.com>, Bill Nicks <bnicks@lenexa.com>, Joe Karlin <jkarlin@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Craig Denny <cdenny@lenexa.com>, Julie Sayers <jsayers@lenexa.com>

Subject: Request for Continuance on Proposed Development and Rezoning Plans - Canyon Ridge Apartment Homes (R23-07, PL 23-12P)

Good evening Mayor Sayers and members of the Lenexa City Council.

Today, Saturday January 6, nearly 80 residents that represent neighborhoods near K-10 and Canyon Creek Blvd., met to discuss the proposed rezoning and planned development of 28 apartment buildings, a convenience store, and an assisted living center on the northwest corner of K-10 and Canyon Creek Blvd. **(Proposed Development and Rezoning Plans - Canyon Ridge Apartment Homes (R23-07, PL 23-12P)).**

We were first made aware of this proposal on December 20. Further details were provided at a meeting at Otto Development on December 28. The proposed plans were available for review this past Thursday. As such, we have had only one full business day to review the plans prior to consideration by the Planning Commission on Monday. Starting on December 20, through current date, which involves two major holidays, and two weekends, not to mention many with expertise on such issues were out on holiday break, we have had an extremely short amount of time to prepare. The rezoning and proposed plans are very complicated, and also quite concerning to area residents. As such, we will ask for a continuance on both issues at the Planning Commission meeting on Monday, January 8 at 7:00 p.m. Additionally, there is an expectation of bad weather, which causes us further issues.

It is the strong desire of our group to provide you, Mayor Sayers, and members of the City Council fully vetted and factual responses to our many concerns regarding this rezoning and proposed project. We feel the fair thing is for a continuance of both issues at the Planning Commission, and also the City Council level. A 30 day continuance provides us time to do our diligence, which we certainly hope is a request you will view favorably.

Any comments or questions, please feel free to reach out to me.

Very truly yours,

Tracy Thomas
26197 W. 96th Terrace
Lenexa, KS 66227
tjthomas13212@gmail.com
913-638-8133

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Lisa Mizell <lmizell@cpckc.org>
Sent: Sunday, January 7, 2024 4:13 PM
To: Mark Charlton <mcharlton@lenexa.com>
Subject: Proposed development off K10

Councilman Charlton,

We are sending this note to encourage you to reject a proposal to build a large 28 multi-story building apartment complex, convenience store, and assisted living complex in western Lenexa. This is the 2nd attempted development of this land in 5 years. Surrounding neighborhoods canvassed and rallied support of over 170 signatures representing 75 million in home values in the 2-week notification period last time, and we have again been given less than 2-weeks to oppose this rezone effort. (January 8th is the Planning Commission Meeting).

This project requires land zoned for single family homes to be rezoned to support the proposal. Lenexa residents chose to live and invest in this part of Lenexa due to the published master land use plan. We have lived in Lenexa since 2019 and moved to this area specifically for the wooded landscape and the atmosphere of this section of Lenexa. A developer presenting a plan for this type of development should not override the investment of many hundreds of homeowners in this area. Additionally, the proposed building plat location is largely unfavorable requiring 10-foot retaining walls around nearly the entire complex, which will greatly raise the building height and visibility. Finally, an additional convenience store is completely unnecessary as there are Casey's <2 and <4 miles away with a Quick Trip being built on 83rd and K-7.

We moved here from a much more urban and busy part of Johnson County. Our plan is to retire in this home surrounded by nature and the peaceful landscape. That is what we were promised when we purchased our home and truly hope you will help the City of Lenexa keep that promise.

This proposal is an eyesore and truly unfitting for the western entrance of Lenexa. Lenexa has many areas already zoned for such a development and would be much better suited and zoned near walkable amenities around the City Center area or Ridgeview interchange. Again, you are urged to honor the published master land use plan and reject this proposal.

We appreciate your time and attention to this request.

Kind Regards,
Dave and Lisa Mizell
26130 West 96th St.
Lenexa, KS 66227
Canyon Creek Point

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Randy Lewis <lewisrandy.lewis@gmail.com>
Date: January 8, 2024 at 12:45:13 PM CST
To: Julie Sayers <jsayers@lenexa.com>
Subject: **Proposed rezoning of land on the northwest corner of Highway 10 and Canyon Creek Development**

Dear Mayor Sayers,

My family and I live in Canyon Creek Point which is north of the proposed rezoning and development of the land on the northwest corner of highway 10 and Canyon Creek Blvd.

We bought our home with the understanding that the existing views would not be disturbed. We reviewed the existing zoning of the land surrounding the development and felt development under the existing zoning would have minimal impact on the wetlands and surrounding area.

Our concerns are as follows:

- 1) Impact to the wetlands and the native wildlife (e.g.; bats, owls, bobcats, deer etc,) and the native vegetation.
- 2) The deforestation of the entire construction area which will impact the wetlands and natural environment.
- 3) Impact on air quality during the extended construction time.
- 4) The removal of a natural sound barrier of trees. The sound is already becoming an issue and with the proposed widening of highway 10 it will only become worse.
- 5) The developers commitment as documented in their marketing collateral that the views would remain as is.

I realize that the City Council will ultimately approve or decline this rezoning request but also want you to be aware of the concerns of some of your constituents.

Sincerely,

Randy and Cynthia Lewis
9601 Wild Rose Lane
Lenexa, KS 66227

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Julie Else <jelse0770@gmail.com>

Sent: Monday, January 8, 2024 2:09 PM

To: Julie Sayers <jsayers@lenexa.com>

Subject: Please oppose the zoning request for Canyon Creek Blvd

Dear Mayor Sayers,

As a resident of Canyon Creek Point, we are writing to ask you to oppose the proposed rezoning of Canyon Creek Boulevard and K10. As you already know this area was zoned for single-family homes, office space and agriculture. With the current rezoning proposal, 45+ acres of wooded terrain would be leveled and destroy habitat, noise barrier from K10 to multiple existing Canyon Creek neighborhoods as well as views of this beautiful valley. With the possible upcoming expansion of K10, the highway will be even closer to our neighborhoods and noise and sight lines for the future need to be considered. The destruction of habitat and deforestation for this area would be devastating to the local population of wildlife. We have seen eagles, owls, fox, coyote, bobcat, turkeys that all live in this area.

West Lenexa has a different feel and is a peaceful retreat. My family moved here and invested in a home in this area specifically because of the woods, trees, quiet and wildlife after 25 years in Overland Park. Lenexa's master plan for the area and the developer of our neighborhood assured us that this area's natural beauty and views were protected. We chose to be 15-20 minutes from the hustle and bustle of the city. Even my college-aged kids said it feels like "we are out of the chaos here." We ask that the city keeps to the Future Land Use Map and honors the investments of hundreds of homeowners who have already invested in this area of Lenexa.

We have high hopes that you as the mayor and the City Council will keep the the natural beauty and integrity of the area safe and as it was intended and originally planned. We as investors and home owners are here to stay.

Thank you for your time and service,

Greg + Julie Else
9559 Landon St
Lenexa KS

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Gina R <g.marie.ross@gmail.com>
Sent: Monday, January 8, 2024 2:20 PM
To: Julie Sayers <jsayers@lenexa.com>
Subject: Concerned Citizen

Hello Mayor Sayers,

I am writing today with the upmost respect and deep concern regarding the proposed rezoning and proposed planning of the Canyon Ridge Apartments on the Northwest corner of K-10 and Canyon Creek Blvd.

My family moved to Canyon Creek by the lake from Colorado about three years ago. We were in awe of the beauty of the area and how quiet our new neighborhood was. This was a huge draw for us coming from the busy and overcrowded area of Colorado.

When we moved here we were assured that the developer promised to keep the natural beauty and views. This did not include large apartment buildings and commercial buildings. Rezoning this land goes against what we were promised when we invested into this area.

In addition to rezoning issues, I am highly against further deforestation, eliminating 45 acres of wooded terrain. This should be a critical concern to our city planners as it leads to loss of biodiversity, increased carbon emissions, soil erosion and degradation, water cycle disruption, stormwater issues and increased flooding risks and further displacement of natural wildlife.

The additional noise, traffic and pollution that this proposed property will bring to our area is concerning as we many of us who reside in canyon creek, invested in an area that was promised to remain a natural sanctuary.

We also do not have the room in our current educational institutions to support more people in this area. I worry that our children's education will suffer and that excellent teachers will burnout do to needless overcrowding.

Please consider this when hearing the proposal. I speak on behalf of myself, family and neighbors when I say we do not want these proposed plans to pass.

Thank you for your consideration,

Gina Ross
Canyon Creek by the Lake & Lenexa Resident
303-809-3336
24935 W. 98th St.

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Cary Daniel <cdaniel@nextaff.com>
Sent: Thursday, January 4, 2024 4:04 PM
To: Mark Charlton <mcharlton@lenexa.com>
Subject: Canyon Creek Point

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hey Mark,

Hope all is well, and you had a good holiday season. Congrats on Ward 2 City Council – you got my vote!

I was reaching out regarding the Oddo Development proposed rezoning off Canyon Creek & K10. Not sure you can give one, but if you can, In your opinion, what is the best offense to defeating this rezoning?

The argument most have, like me, is they did their research before buying in this development based on Lenexa Future Use Plan and Lenexa zoning. I also look at the Johnson County zoning. That neighborhood already has considerable highway noise and a 28 multi-level apartment complex with parking would in my guess eliminate all or most of the trees helping with that noise. The other consideration is obviously the view of potential CDO or CP2 vs an apartment building.

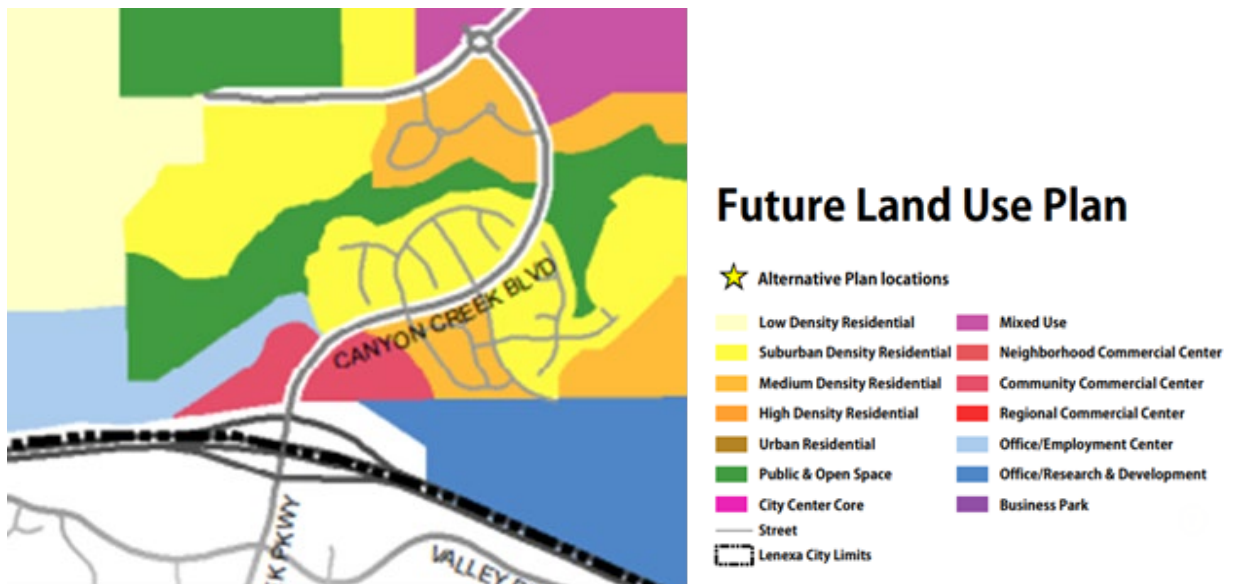
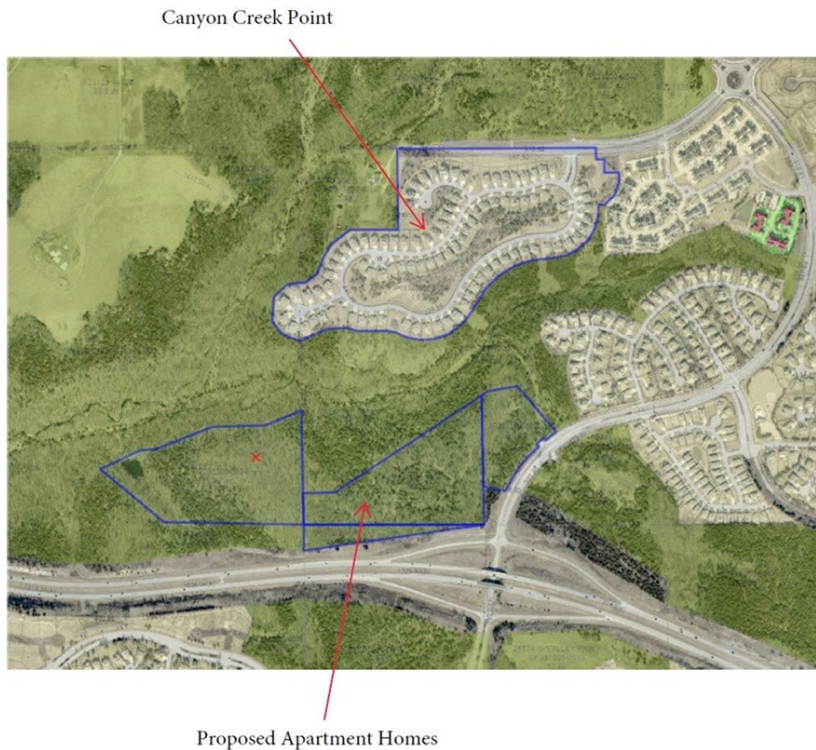
I'm trying to figure out the most effective argument that you've seen be affective so we can present a reasonable and logical argument vs neighbors crying and talking about the birds that will die...

Thanks!

Cary Daniel (913-484-7840)

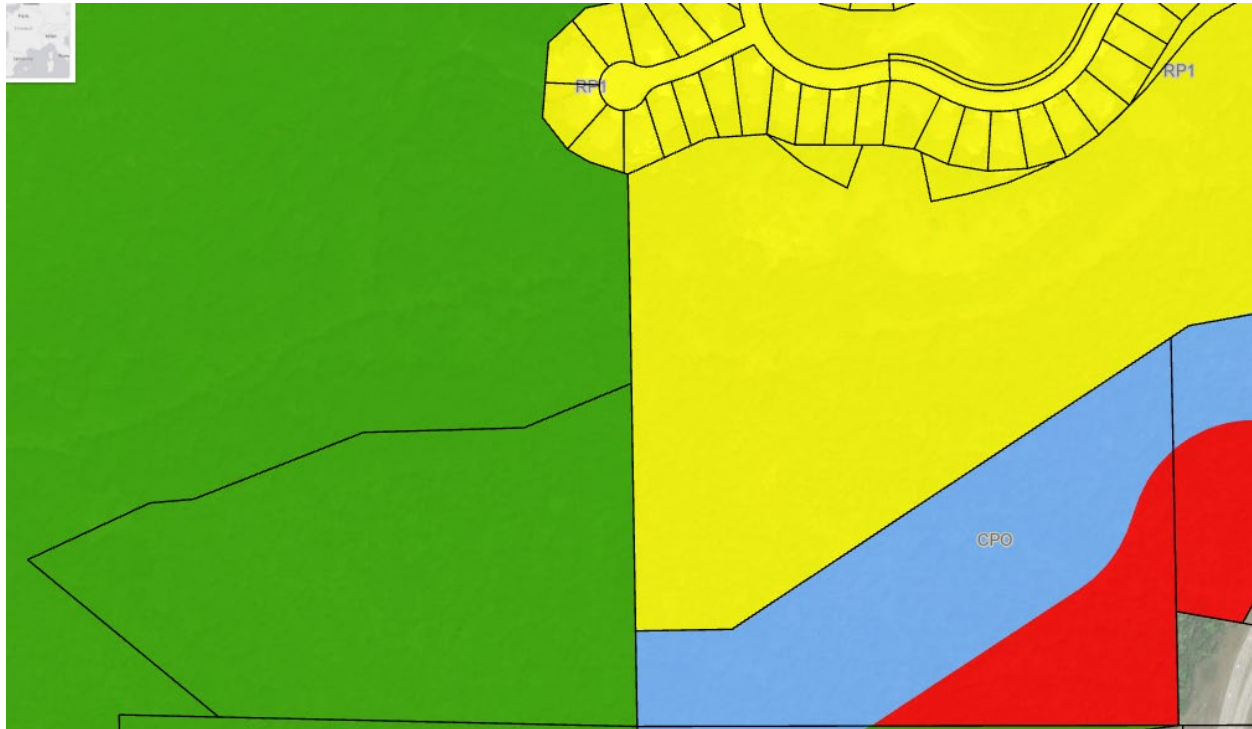
Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting



Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting



Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

ODDO Development has submitted a proposal to build 28 multi-story apartment buildings, convenience store and assisted living center on the NW corner of K-10 & Canyon Creek Blvd. This proposal requires multiple plats to be rezoned from RP1 (planned single family residential) and CPO - General Office. This proposal rejects the published Lenexa Master Land use plan which many used when building/buying in this area of Lenexa.

The Lenexa Planning Commission will be meeting Monday January 8, 2024 7 p.m. to make a decision on the re-zoning.

If you would like to voice your opinion on this matter, you are encouraged to attend the meeting in person or send an email to the Lenexa Council Members.

The planning commission meeting will be held at Lenexa City Hall starting at 7:00PM located at City Hall (17101 W. 87th Street Parkway Lenexa KS).

Julie Sayers (mayor)
jsayers@lenexa.com
913.477.7567

Bill Nicks (ward 2)
bnicks@lenexa.com
913.424.9228

Mark Charlton (ward 2)
mcharlton@lenexa.com
913.675.2428

Joe Karlin (ward 1)
jkarlin@lenexa.com
913.477.7560

Courtney Eiterich (ward 1)
ceiterich@lenexa.com
913.522.9423

Melanie Arroyo (ward 3)
marroyo@lenexa.com
913.279.0407

Chelsea Williamson (ward 3)
cwilliamson@lenexa.com
913.303.9929

Craig Denny (ward 4)
cdenny@lenexa.com
816.225.5891

Chris Herron (ward 4)
cherron@lenexa.com
913.477.7550

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

Future Land Use vs. Zoning

Land use and zoning, though related, are not the same. Land use is intended to set the framework for guiding future development. Current land use is how the land is being used right now and future land use is how the land is envisioned to be used in the future. Land use is often described in more generalized terms, such as low-density residential and multi-family residential, or regional commercial and neighborhood commercial. For this reason, while there is a relationship between land use and zoning, there is not a one-for-one relationship between the two classifications.

Zoning is the “tool” municipalities use to regulate land use. Zoning refers to how the property can be used and specific regulations for the development of the property. It is through zoning regulations found within the City of Lenexa’s Unified Development Code that items relating to the use of land, height and size of buildings, size of lots, setbacks, and parking, to name a few, are regulated.

When considering a request to rezone property, part of the review, and one of the factors, includes an analysis of how the land is currently being used and how the land is designated on our Future Land Use Map. The staff analyzes the rezoning request against thirteen criteria known as the Golden Criteria named after the land use case Donald Golden vs City of Overland Park. Other criteria relate to the character of the neighborhood, affect to nearby properties, the environment, safety of the street network, and adequacy of required utilities and services, to name a few.

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Laura McNeese <lmcneese@gmail.com>

Sent: Monday, January 8, 2024 2:34 PM

To: Mark Charlton <mcharlton@lenexa.com>

Subject: Proposed development Canyon Creek Parkway and K10

We are not able to attend tonight's meeting, but we would like to voice our concerns regarding the proposed ODDO development.

We have lived in Lenexa since 1998. From that time we have always been very impressed with the care and concern that the city provides in regards to zoning, planning and development.

We moved from Falcon Ridge to Canyon Creek Point in 2019 and built here with the knowledge that the growth would occur near us, but we had confidence that the potential for that growth would be under the guise of the ever vigilant city planners. The requested zoning changes would practically be a 180 degree change from the current zoning, and would shatter the commitments promised by our developer when we purchased our home.

Please do not allow the proposed zoning changes for the northwest corner of K-10 and Canyon Creek Parkway.

Respectfully,

Laura and John McNeese

9563 Landon St, Lenexa, KS 66227

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Larry Riggins <larryriggins@live.com>

Date: January 9, 2024 at 4:31:47 PM CST

To: Julie Sayers <jsayers@lenexa.com>, Mark Charlton <mcharlton@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Chris Herron <cherron@lenexa.com>, Bill Nicks <bnicks@lenexa.com>, Joe Karlin <jkarlin@lenexa.com>, Melanie Arroyo <marroyo@lenexa.com>, Craig Denny <cdenny@lenexa.com>

Subject: Canyon Ridge (RZ23-07, PL23-12P) - ODDO Rezoning Request

Larry & Lartrell Riggins

9570 Wild Rose Ln

Lenexa, KS 66227

Date: January 9, 2024

Subject: Canyon Ridge (RZ23-07, PL23-12P) – ODDO Rezoning Request

To: Lenexa Mayor & City Council Members

We share the concerns submitted by other Canyon Creek Point (CCP) residents and residents from neighboring developments. After reviewing the most recent ODDO developer remarks, we have a couple of comments:

ODDO: Most real estate and planning experts understand there is no need for office development in this area of Lenexa (or nearly anywhere else in Johnson County post-Covid) and considering the steep grades of the subject property it is nearly impossible to develop office buildings on the subject property.

It is interesting that is nearly impossible to construct office buildings, but apartments are suitable for the same property. My understanding is as one resident has stated: “The plans show the apartment complex cresting the forested hillside from K-10 and down the valley putting the backside of all apartment buildings in the direct site line of all Canyon Creek Point homes”. The backside of the certain apartments will be three stories tall for the walkout purposes with a large retaining wall to overcome the steep grade. It seems highly unlikely that the remaining few trees in the project will cover the site line from the CCP homes. It is unfortunate that the size of this project requires the entire tree buffer to K-10 to be eliminated to fit an apartment complex onto a very challenging track of land.

ODDO: Importantly, the Kansas Supreme Court has stated that rezoning may not be based on a “plebiscite of the neighborhood” but must be based on the interests of the community at large.

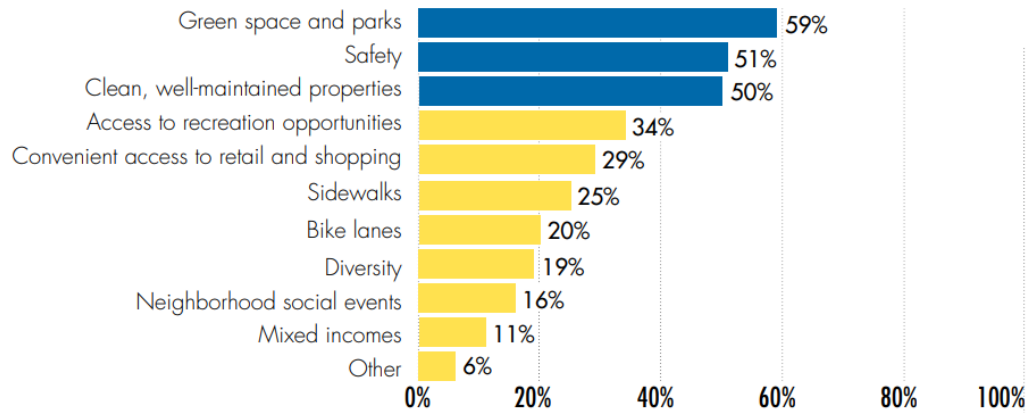
Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

Lenexa’s vision for 2040 will be realized through strong community partnerships

Through an online survey, we asked Lenexans what factors should be considered and are most important for maintaining and creating healthy neighborhoods in 2040. Their responses are below.

What factors are most important for maintaining and creating healthy neighborhoods in 2040?



SOURCE: Where to Focus — Survey 2. Appendix B

20 • HEALTHY PEOPLE

K-10 as a highway was completed in 1984. The proposed 45-acre rezoning site has remained undisturbed for at least 40 years and now has mature trees and a wetland that attracts several wildlife species.

Lenexa’s stated vision shows that green space and parks are at the top of the latest community survey as most important for maintaining and creating healthy neighborhoods in 2040. The area just north of CCP is reserved for a future city park. The proposal means almost all the 45 acres of wooded terrain would be cleared for this project. This leaves a narrow band of conservation area south of CCP for a planned Lenexa walking trail that would have close site-lines to the apartments. The long-term goal of the future city park, in conjunction with the trail that leads back to nearby Mize Lake, is to promote and make available this area as green space to the larger community. The project, as proposed, would detract from this vision.

We respectfully request that you continue to honor the published Master Land Use Plan and reject this proposal.

Sincerely,

Larry & Lartrell Riggins
Canyon Creek Point residents

Canyon Ridge (RZ23-07, PL23-12P)

Post Packet Public Comment Emails Received After January 8, 2024 Planning Commission Meeting

From: Lee Stucky <leestucky@icloud.com>

Date: January 14, 2024 at 6:40:56 PM CST

To: Melanie Arroyo <marroyo@lenexa.com>, Chelsea Williamson <cwilliamson@lenexa.com>, Courtney Eiterich <ceiterich@lenexa.com>, Joe Karlin <jkarlin@lenexa.com>, Craig Denny <cdenny@lenexa.com>, Chris Herron <cherron@lenexa.com>

Subject: Opposition to Canyon Creek Oddo Development Proposal

Dear Lenexa City Council Members,

We built our home in Canyon Creek by the Park in 2007. Much has changed over the past 16 years, but the basic premise of life in Canyon Creek - living amidst nature's beauty, wildlife, and tranquility - has been well preserved. It is exactly why we made the decision to live here, and why we have enjoyed and appreciated every moment of it.

Now comes the proposal from Oddo Development calling for a multi-building, multi-story, high density apartment complex, assisted living center, and convenience store. At its closest proximity, it will come within 600 feet of many established Canyon Creek homes, while destroying 45 acres of prime woodland beauty immediately adjacent to the community.

The rezoning required for this project is counter to the City of Lenexa's master plan for the area and violates the developer's commitment to protect and maintain the natural beauty of it.

The negative impacts of such a project are numerous and wide ranging from immediate to long-term. To name a few, these concerns include:

- Environmental: Destruction and damage to woodlands, wetlands, and wildlife, as well as increased drainage and pollution issues;
- Construction: Prolonged heavy equipment operation, noise, wind blown dust, and potential peripheral demolition and blasting damage to residential foundations;
- Traffic: Construction-related road damage, increased usage due to density and retail component, and increased congestion at K-10/Cedar Creek/Canyon Creek ramps;
- Property Values: Cumulative effects of the above will negatively impact existing home and development values in the long term.

We strongly oppose this project and urge the Mayor and City Council to reject this proposal in consideration of the long term impacts on the environment, quality of life in Canyon Creek, and the image it conveys for the City of Lenexa.

Respectfully,

Lee and Sharon Stucky
25712 W. 97th Street
Canyon Creek by the Park

Paul E. LaForge
9858 Garden St.
Lenexa, KS 66227
pelaforge@gmail.com
KS Engineer #7319

January 30, 2024

Lenexa KS Planning Commission
City of Lenexa, KS
17101 W. 87th St. Pkwy
Lenexa, KS 66219

CMRRR 7021-0350-0001-2552-3723

RE: Pending Planning Commission Applications for February 5, 2024

RE: Canyon Ridge Apartment Homes located approximately at the northwest corner of K-10 Highway & Canyon Creek Blvd. – Rezoning & Preliminary Plan for a multifamily residential use, assisted living, and retail use

Lenexa KS Planning Commission:

Area Resident.

Paul E. LaForge; 9858 Garden St.; Lenexa, KS 66227

Resident's Opening Statement.

This resident is a current area resident. The applicant has made multiple deviation requests. This resident is not against development of the land as long as Lenexa Zoning laws, the Lenexa Comprehensive Plan and Lenexa UDC codes are met. **The current owner/applicant bought the land knowing the current Lenexa Zoning, the Lenexa Comprehensive Plan and the Lenexa UDC code requirements.**

4. The extent to which the proposed use will detrimentally affect nearby property.

Item 4 reads. "...The proposed uses will not detrimentally affect nearby property of any greater impact than if the site were to be developed with office and retail uses as the site is *now zoned*. It is Staff's opinion the proposed uses are compatible with the existing and planned uses in the vicinity..."

Response. The Staff is not being honest in making such a statement. The applicant's proposed deviations will detrimentally affect nearby property. The applicant is openly requesting a "*rezoning*". The applicant's proposed deviations are not adequate. The "Staff's opinion" statement is both misleading and simply wrong.P

Resident Requested Action. Deny applicant's rezoning request.

Apartment Parking (Title 4 UDC, § 4-1-D-1-C)

Applicant proposal is for **566 stalls** (519 for units, 43 for guest parking).

Applicant “alleges” 635 stalls are required.

Applicant states that its 566 stalls are 69 stalls short of applicants “alleged” 635 stalls.

Response. Applicant does not state how many bedrooms are in each unit.

Lenexa UDC requires **606 stalls** if all are 1 bedroom units.

Lenexa UDC requires **692 stalls** if all are 2 bedroom units.

Lenexa UDC requires **779 stalls** if all are 3 bedroom units.

Also, the applicant drawing shows “*Potential locations for additional parking stalls*” but applicant does not show how many. Having looked at the drawings, these “additional parking stalls” must be part of the original construction. This resident also questions whether the additional parking stalls can even be built were shown.

Resident Requested Action. Deny applicant’s deviation request.

Nursing Home Parking (Title 4 UDC, § 4-1-D-1-C)

Applicant drawing simply shows 85 stalls.

Applicant does not state how many beds are in the nursing home.

Applicant does not state how many employees are anticipated.

Response. Lenexa UDC stipulates 1 stall per 3 beds, plus 1 space per employee.

Resident Requested Action. Deny applicant’s deviation request.

Convenience Store Floor Area (Title 4 UDC, § 4-3-C-3)

Applicant has requested a deviation from 5,000 SF to 6,100 SF.

Response. Lenexa UDC stipulates a maximum of 5,000 SF for a convenience store. Once this door is opened a Pandora’s Box is opened - the next applicant will want 10,000 Sf, the next applicant 20,000 SF – ditto – until a shipping center is requested.

Resident Requested Action. Deny applicant’s deviation request.

Freeway Setback (Title 4 UDC, § 4-1-B-26-C-1)

Applicant has requested a freeway setback from 100 LF to 28 LF, with a corresponding decrease in the landscape buffer from 100 LF to 28 LF.

Response. Lenexa UDC requires a setback of 100 LF with a corresponding landscape buffer. The existing Lenexa UDC required setbacks help to control freeway noise. Nothing good can or will come from reducing the freeway and landscape buffer set back of 100 LF.

Resident Requested Action. Deny applicant’s deviation request.

Gasoline Pump Island Queue Space (Title 4 UDC, § 4-1-D-1-N-1)

Applicant has requested a deviation from 50 LF to 25 LF.

Response. Lenexa UDC stipulates 50 LF from each end of the pump island. The 50 LF accommodates large vehicles. Reducing Queue space to 25 LF will block the road and will cause safety hazards with the pumps so close to the roadway.

Resident Requested Action. Deny applicant’s deviation request.

Salt Lake Area Study on Impact to Single Family Home Values.

Applicant has included a study for "Suburban Salt Lake County" in support of its position.

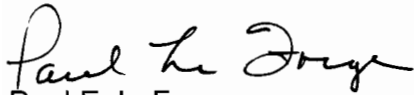
Response. I have been to Salt Lake, UT area. I live in Lenexa, KS. The applicant's use of a Salt Lake area study as a comparison to Lenexa, KS is simply absurd - Suburban Salt Lake County, UT is NOT Lenexa, KS.

Resident Requested Action. Ignore the Salt Lake Area study.

Area Resident's Closing.

It is this resident's opinion that the applicant's deviation requests are to maximize the applicant's profits from the income generated from the sale of the property, but without regard to current zoning, the Lenexa UDC codes and the impact upon the adjacent property owners. If Lenexa were to approve the applicant's deviation requests, Lenexa's actions would result in a negative impact on the value of the current homes in the Lenexa area. **This resident requests that all of the applicant's deviation be denied.**

Sincerely,



Paul E. LaForge
9858 Garden St.
Lenexa, KS 66227

Cc:

Lenexa KS City Clerk at: cityclerk@lenexa.com
Scott McCullough at: smccullough@lenexa.com
Julie Sayers at: jsayers@lenexa.com

Neighbors opposed to Canyon Ridge Apartment Homes



Started January 3, 2024

Why this petition matters

Started by [Fred Gower](#)

Who is impacted? Every homeowner in Canyon Creek (Point, Highlands, Lake and Park), Canyon Ridge subdivision, and anyone currently using the Canyon Creek Blvd exit.

What is at stake? The immediate effect will be loss of all natural barriers between Canyon Creek homes and K-10 highway, increasing noise levels and the elimination of native trees and wildlife. There are also safety concerns that come with the increased traffic. Canyon Creek neighborhoods span across Canyon Creek Blvd, and children often have to cross this road to and from school, to access the park and pool, visit friends, etc. **This proposal goes against the Lenexa Master Plan, which current homeowners used when deciding to invest in this part of the city.**

Why is now the time to act? Left as is, homeowners continue to have access to the beauty that Lenexa states it wants to preserve. If rezoned, all surrounding neighbors will have a direct site line to the backside of 2-story + walkouts (in effect, 3-story) multi-unit apartments. K-10 & Canyon Creek Blvd was meant to be the "western gateway" or "front door" to Lenexa. If rezoned, you will see the stark difference between Olathe's priorities (south side of K-10: preserved natural beauty) and Lenexa's priorities (north side of K-10: loss of native trees and wildlife replaced with apartments, assisted living and a gas station).

Area residents were only notified of the developers request to re-zone the area on December 18th, wanting approval in less than 30 days. We believe the timing of this request was intentional with the hope that most residents would not notice as this is right in the middle of the busy holiday season.

576

Signatures

1,000

Next Goal

Support now



Neighbors opposed to Canyon Ridge Apartment Homes

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petition_signatures_jobs_37816043_20240131222219

Name	City	State	Postal Code	Country	Signed On
Vanessa Calcara	Lenexa	KS		US	2024-01-03
Fred Gower	Lenexa	KS	66227	US	2024-01-03
Chris Calcara	Kansas City	MO	64114	US	2024-01-03
Dennis Burket	Prairie Village	KS	66208	US	2024-01-03
Roslyn Brittain	Leawood	KS	66206	US	2024-01-03
mandy Pennebaker	Lenexa	KS	66227-7300	US	2024-01-03
Jennifer Tran	Lenexa	KS	66227	US	2024-01-03
Colleen Gower	Olathe	KS	66062	US	2024-01-03
Heather Day	Olathe	KS	66062	US	2024-01-03
KIM GODWIN	Olathe	KS	66062	US	2024-01-03
Michael Day	Olathe	KS	66062	US	2024-01-03
Ron Ramsour	Lenexa	KS	66227	US	2024-01-03
Kathleen Mulligan	Kansas City	KS	66102	US	2024-01-03
Becky Longfellow	Lenexa	KS	66227	US	2024-01-03
Chad Mellick	Lenexa	KS	66227	US	2024-01-03
Lois Maxwell	Lenexa	KS	66227	US	2024-01-03
Judith McNish	Lenexa	KS	66227	US	2024-01-03
Darci Guerrein	Lenexa	KS	66227	US	2024-01-03
Larry Riggins	Lenexa	KS	66227	US	2024-01-03
Barb Ramsour	Lenexa	KS	66227	US	2024-01-03
Lisa Mizell	Lenexa	KS	66227	US	2024-01-03
Tyler Kippes	Olathe	KS	66061	US	2024-01-03
Kelley Chapman	Olathe	KS	66062	US	2024-01-03
Andrew Drummond	Lenexa	KS	66227	US	2024-01-03
Melissa Drummond	Lenexa	KS	66227	US	2024-01-03
Joe Guerrein	Lenexa	KS	66227	US	2024-01-03
Matthew Zelenc	Lenexa	KS	66227	US	2024-01-03
Lisa Vaughn	Lenexa	KS	66227	US	2024-01-03
Robin Keller	Lenexa	KS	66227	US	2024-01-03
Adam Hansen	Lenexa	KS	66227	US	2024-01-04
Lyndsy Zelenc	Lenexa	KS	66227	US	2024-01-04
Gina Ross	Lenexa	KS	66227	US	2024-01-04
Todd Ross	Lenexa	KS	66227	US	2024-01-04
Thomas DeMaria	Olathe	KS	66227	US	2024-01-04
Wes Simmons	Lenexa	KS	66215	US	2024-01-04
Richard Miller	Lenexa	KS	66219	US	2024-01-04

Suzanne Luke	Lenexa	KS	66227	US	2024-01-04
Tammy Wainwright	Lenexa	KS	66227	US	2024-01-04
Alan Baker	Olathe	KS	66061	US	2024-01-04
Susanne Burket	Lenexa	KS	66227	US	2024-01-04
Nicole Ferrell	Lenexa	KS	66227	US	2024-01-04
Areli Quinones	Olathe	KS	66227	US	2024-01-04
Mike Carter	Lenexa	KS	66220	US	2024-01-04
Jenna Means	Lenexa	KS	66227	US	2024-01-04
Kristin Kippes	Lenexa	KS	66227	US	2024-01-04
Thomas & Michele Carrigan	Lenexa	KS	66227	US	2024-01-04
Angie Nelson	Lenexa	KS	66227	US	2024-01-04
David Nelson	Lenexa	KS	66227	US	2024-01-04
Connor Navrude	Lenexa	KS	66227	US	2024-01-04
Melinda Gibson	Olathe	KS	66062	US	2024-01-04
Danielle Navrude	Lenexa	KS	66227	US	2024-01-04
Daniel Quinones	Kansas City	KS	66103	US	2024-01-04
Kristen Potter	Overland Park	KS	66212	US	2024-01-04
Allyson Putnam	Kansas City	MO	64133	US	2024-01-04
Jeffrey Oswald	Lenexa	KS	66220	US	2024-01-04
Amy Kaufmann	Olathe	KS	66062	US	2024-01-04
Rochelle Kaminsky	Lenexa	KS	66227	US	2024-01-04
Matt Kaminsky	Lenexa	KS	66227	US	2024-01-04
Michelle Miller	Lenexa	KS	66227	US	2024-01-04
Tina Nonoyama	Olathe	KS	66062	US	2024-01-04
Laura Mordica	Lenexa	KS	66227	US	2024-01-04
Barb Eidt	Lenexa	KS	66227	US	2024-01-04
Justin Eastwood	Lenexa	KS	66227	US	2024-01-04
Leslie Marvin	Olathe	KS	66061	US	2024-01-04
Marcia Bledsoe	Lenexa	KS	66227	US	2024-01-04
Stephanie Rector	Lenexa	KS	66227	US	2024-01-04
Natalie Eidt	Lenexa	KS	66227	US	2024-01-04
Patricia Pound	Olathe	KS	66061	US	2024-01-04
Nicole Thomas	Olathe	KS	66061	US	2024-01-04
Mike Marvin	Olathe	KS	66061	US	2024-01-04
Jeff Folks	Lenexa	KS	66220	US	2024-01-04
Marjorie Lampton	Shawnee	KS	66226	US	2024-01-04
Brittany Sacks	Lenexa	KS	66227	US	2024-01-04
Stuart Pollack	Lenexa	KS	66215	US	2024-01-04

Chris Rector	Lenexa	KS	66227	US	2024-01-04
Melissa Frock	Olathe	KS	66062	US	2024-01-04
Lori Sosna	Shawnee	KS	66218	US	2024-01-04
Jim Baird	Lenexa	KS	66227	US	2024-01-04
Patrick Miller	Lenexa	KS	66227	US	2024-01-04
Rae Baird	Overland Park	KS	66212	US	2024-01-04
Gary Brittain	Kansas City	MO	64184	US	2024-01-04
Gabe Wiechman	Olathe	KS	66062	US	2024-01-04
Chad Boling	Lenexa	KS	66227	US	2024-01-04
Denise Wiechman	Lenexa	KS	66227	US	2024-01-04
Mike Smith	Lenexa	KS	66227	US	2024-01-04
Jeff Godwin	Olathe	KS	66062	US	2024-01-04
Jessica Grier	lenexa	KS	66227	US	2024-01-04
Caitlin Skala	Olathe	KS	66061	US	2024-01-04
Amanda Morgan	Lenexa	KS	66227	US	2024-01-04
Joshua Dreesen	Overland Park	KS	66213	US	2024-01-04
David Bledsoe	Overland Park	KS	66207	US	2024-01-04
Kelsey Ingold	Lenexa	KS	66227	US	2024-01-04
Tim Durkin	Kansas City	MO	64110	US	2024-01-04
Gina Calhoon	Kansas City	MO	64121	US	2024-01-04
Toby Leach	Overland Park	KS	66212	US	2024-01-04
Christiane Branstrom	Lenexa	KS	66227	US	2024-01-04
James Shenoy	Lenexa	KS	66227	US	2024-01-04
Dan Robinson	Lenexa	KS	66220	US	2024-01-04
Karen Watkins	Olathe	KS	66061	US	2024-01-04
Steven Branstrom	Olathe	KS	66062	US	2024-01-04
Elizeth Gonzalez	Phoenix		85008	US	2024-01-04
Earl Watkins	Olathe	KS	66061	US	2024-01-04
Britany Gordon	Lenexa	KS	66227	US	2024-01-04
Heather Suelflow	Lenexa	KS	66215	US	2024-01-04
Holly Myers	Lenexa	KS	29466	US	2024-01-04
Josh Suelflow	Lenexa	KS	66215	US	2024-01-04
Karen Shenoy	Canyon Creek by the Park, Ler	KS	662227	US	2024-01-04
Jill Rew	Lenexa	KS	66215	US	2024-01-04
Joseph Czyz	Lenexa	KS	66227	US	2024-01-04
Lyndsey Stuber	Olathe	KS	66061	US	2024-01-04
Randy Lewis	Lenexa	KS	66227	US	2024-01-04
Andrea Kelley	Lenexa	KS	66227	US	2024-01-04

Nick Gordon	Kansas City	MO	64113	US	2024-01-04
Lauren Schneider	Eudora	KS	66025	US	2024-01-04
Megan Goodyear	Lenexa	KS	66227	US	2024-01-04
Adam Frock	Lenexa	KS	66227	US	2024-01-04
Caren Oswald	Lenexa	KS	66220	US	2024-01-04
Shaun Burnison	Lenexa	KS	66227	US	2024-01-04
Jon Inwood	Brooklyn	NY	11226	US	2024-01-04
Kelly Burnison	Lenexa	KS	66227	US	2024-01-04
Michaela Rush	Olathe	KS	66062	US	2024-01-04
Tammy Forgey	Lenexa	KS	66227	US	2024-01-04
Tracy Thomas	Lenexa	KS	66227	US	2024-01-04
Erika Rikhiram	Clermont	FL	34711	US	2024-01-04
jill angelichio	charlotte	NC	28204	US	2024-01-04
Yonatan Aguilar	King George		22485	US	2024-01-04
Rick Vaughn	Lenexa	KS	66227	US	2024-01-04
Melissa Gower	Prairie Village	KS	66208	US	2024-01-04
Lindsay Carter	Lenexa	KS	66220	US	2024-01-04
Max Bruce	Lenexa	KS	66227	US	2024-01-04
Terri and Steve Bennett	Lenexa	KS	66227	US	2024-01-04
Jake Ellis	Lenexa	KS	66227	US	2024-01-04
Kim Pauli	Lenexa	KS	66227	US	2024-01-04
Lisa Johnson	Lenexa	KS	66227	US	2024-01-04
Robyn Reid	Lenexa	KS	66227	US	2024-01-04
Gary Reid	Lenexa	KS	66227	US	2024-01-04
Ashley Thornton	Shawnee	KS	66217	US	2024-01-04
Kim Smith	Prairie Village	KS	66208	US	2024-01-04
Alyne Millert	Lenexa	KS	66227	US	2024-01-04
Broni cherian	Olathe	KS	66061	US	2024-01-04
George Mordica	Lenexa	KS	66227	US	2024-01-04
Joy Palangi	Lenexa	KS	66227	US	2024-01-04
Michele MorganCarrigan	Kansas City	KS	66106	US	2024-01-04
Alfonso Aldave	Kansas City	KS	66227	US	2024-01-04
Adrian Kelley	Lenexa	KS	66227	US	2024-01-04
Matthew Lewis	Lenexa	KS	66227	US	2024-01-04
Kelsey Lewis	Lenexa	KS	66227	US	2024-01-04
Brenda Cunningham	Olathe	KS	66061	US	2024-01-04
Kayla South	Kansas City	KS	66102	US	2024-01-04
Susan Burkholder Cameron	Olathe	KS	66062	US	2024-01-04

Patricia Mathews	Lenexa	KS	66227	US	2024-01-04
Lesley Walden	Lenexa	KS	66227	US	2024-01-04
Randall Stark	Prairie Village	KS	66208	US	2024-01-04
Steve Powell	Olathe	KS	66062	US	2024-01-04
Sreeraman Rangarajan	Waipahu	HI	96797	US	2024-01-04
Cynthia Neumayer	Olathe	KS	66061	US	2024-01-04
Tracy King	Lenexa	KS	66227	US	2024-01-04
Anthony Lawson	Lenexa	KS	66227	US	2024-01-04
Ron David	Olathe	KS	66061	US	2024-01-04
James Byers	Florence	AZ	85132	US	2024-01-04
Brett Bales	Kansas City	KS	66106	US	2024-01-04
Donna David	Olathe	KS	66061	US	2024-01-04
Cyndee Lewis	Lenexa	KS	66227	US	2024-01-04
Carole Munns	Lenexa	KS	66227	US	2024-01-04
Lisa and Willie Scott	Lenexa	KS	66227	US	2024-01-04
Linda Powell	Olathe	KS	66062	US	2024-01-04
Cheryl Greenough	Lenexa	KS	66227	US	2024-01-04
Cheri Couture	Lenexs	KS	66227	US	2024-01-04
Gillian Cutts	Overland Park	KS	66214	US	2024-01-04
Melissa Feltz	Lenexa	KS	66227	US	2024-01-04
Judy Farrell	Lenexa	KS	66227	US	2024-01-04
Dan Miller	Olathe	KS	66062	US	2024-01-04
Steve Bennett	Lenexa	KS	66227	US	2024-01-04
Ian Cutts	Lenexa	KS	66227	US	2024-01-04
Mary Woltkamp	Lenexa	KS	66227	US	2024-01-04
Patricia Krehbiel	Lenexa	KS	66220	US	2024-01-04
Donna Garrett	Stevensville		21666	US	2024-01-04
Patricia Hunt	Lenexa	KS	66227	US	2024-01-04
William R. Hensley	Olathe	KS	66061	US	2024-01-04
Jeannette Paige	Lenexa	KS	66227	US	2024-01-04
Tyler Booth	Lenexa	KS	66215	US	2024-01-04
Joe Alonso	Lenexa	KS	66227	US	2024-01-04
Lori Barrett	Prairie Village	KS	66208	US	2024-01-04
Janet Alonso	Lenexa	KS	66227	US	2024-01-04
Leslie Bales	Overland Park	KS	66212	US	2024-01-05
Jerry Walters	Olathe	KS	66062	US	2024-01-05
Kim Galbreath	Lenexa	KS	66227	US	2024-01-05
Melody Baker	Lenexa	KS	66227	US	2024-01-05

Laura McNeese	Lenexa	KS	66227	US	2024-01-05
Kirk Calhoon	Lenexa	KS	66227	US	2024-01-05
Vonda Kay Brown	Olathe	KS	66062	US	2024-01-05
Jane Moseman	Omaha	NE	68130	US	2024-01-05
Yanci Moran	Houston		77073	US	2024-01-05
Larry Molder	Olathe	KS	66061	US	2024-01-05
Catherine Dextraze	Lenexa	KS	66227	US	2024-01-05
Andrew Floyd				US	2024-01-05
Andy Dextraze	Lenexa	KS	66227	US	2024-01-05
Kent Siemens	Olathe	KS	66062	US	2024-01-05
Dianne Walker	Prairie Village	KS	66208	US	2024-01-05
Aaron Johnson	Lenexa	KS	66227	US	2024-01-05
Adeline Powers	Elkhorn		68022	US	2024-01-05
Darci Deskin	Lenexa	KS	66227	US	2024-01-05
Steve Wyman	Lenexa	KS	66227	US	2024-01-05
Tom Deskin	Overland Park	KS	66204	US	2024-01-05
Ron & Patricia McNelis	Overland Park	KS	66213	US	2024-01-05
Alana Preziosi	Swedesboro		8085	US	2024-01-05
Whitney Webb	Lenexa	KS	66227	US	2024-01-05
Christy Wichtendahl	Shawnee	KS	66227	US	2024-01-05
Kelly Roney	Lenexa	KS	66227	US	2024-01-05
Sabrina Markese	Olathe	KS	66061	US	2024-01-05
Alexandria Hills	Lenexa	KS	66227	US	2024-01-05
Megan Johnson	Lenexa	KS	66227	US	2024-01-05
Maria Holdenried	LENEXA	KS	66227	US	2024-01-05
Ronnie Kotz	Olathe	KS	66061	US	2024-01-05
Tresa Riggins	Lenexa	KS	66227	US	2024-01-05
Sydney Ash	Phoenix	AZ	85003	US	2024-01-05
Sam Baba				Saudi Ara	2024-01-05
Alleigh Taylor	Lenexa	KS	66227	US	2024-01-05
Lui yu	Miami		33132	US	2024-01-05
Lily Jones	Lenexa	KS	66215	US	2024-01-05
Lowell J. Tawney	Lenexa	KS	66227	US	2024-01-05
Madeline Maldonado	Waukegan		60085	US	2024-01-05
Melissa Weatherspoon	Lenexa	KS	66227	US	2024-01-05
Linda Winter	Olathe	KS	66061	US	2024-01-05
Brian Stevens	Lenexa	KS	66227	US	2024-01-05
Montira Kotz	Lenexa	KS	66227	US	2024-01-05

Wayne Walker	Prairie Village	KS	66208	US	2024-01-05
Hanna Flaming	Olathe	KS	66062	US	2024-01-05
Gregory Roe	Lenexa	KS	66227	US	2024-01-05
Kaylee Johnson	Lenexa	KS	66220	US	2024-01-05
Nicole Barker	Lenexa	KS	66227	US	2024-01-05
Stanton Barker	Lenexa	KS	66227	US	2024-01-05
jesus Segura	Chicago		60651-3944	US	2024-01-05
Candy Johnson	Prairie Village	KS	66208	US	2024-01-05
Catherine Montgomery-Malone	Lenexa	KS	66227	US	2024-01-05
Jerry Johnson	Lenexa	KS	66227	US	2024-01-05
Bruce Kotz	Lenexa	KS	66227	US	2024-01-05
Debra Meyer	Lenexa	KS	66227	US	2024-01-05
Laura Hatcher	Lenexa	KS	66227	US	2024-01-05
Ken Hunt	Lenexa	KS	66227	US	2024-01-05
James Keller	LENEXA	KS	66227	US	2024-01-05
Clare McGinness	Lenexa	KS	66227	US	2024-01-05
marcella stevens	Olathe	KS	66062	US	2024-01-05
Jay Ramsdell	Olathe	KS	66227	US	2024-01-05
Jennifer Walker	Olathe	KS	66061	US	2024-01-05
Brad Krehbiel	Lenexa	KS	66227	US	2024-01-05
Cindy Ritchie	Olathe	KS	66061	US	2024-01-05
Patti Finn	Lenexa	KS	66227	US	2024-01-05
Sachet Ohio				US	2024-01-05
Josue Chávez	Los Angeles		90004	US	2024-01-05
Karen Beckwith	Lenexa	KS	66227	US	2024-01-05
William Ross	Lenexa	KS	66220	US	2024-01-05
Melissa Harmon	Lenexa	KS	66227	US	2024-01-05
Brian Wainwright	Olathe	KS	66062	US	2024-01-05
Dianne Brown	Shawnee	KS	66226	US	2024-01-05
Carol Hudson	Chicago		60644	US	2024-01-05
Erin Morgan	Olathe	KS	66061	US	2024-01-05
LORI HOTZEL	Olathe	KS	66062	US	2024-01-05
Eddie Regan	Denver	CO	80238	US	2024-01-05
Kevin Thornton	Shawnee	KS	66226	US	2024-01-05
Guiselle Isidro	Astoria		97103	US	2024-01-05
Michelle Fairchild	LENEXA	KS	66227-7262	US	2024-01-05
John Marshall	Lenexa	KS	66227	US	2024-01-05
Matt Regan	Overland Park	KS	66221	US	2024-01-05

Skyler Fairchild	Lenexa	KS	66227	US	2024-01-05
Clayton Neal	Overland Park	KS	66227	US	2024-01-05
Paige Mulgrew	Lenexa	KS	66227	US	2024-01-05
Lea Ann Littell-West	Lenexa	KS	66220	US	2024-01-05
Stephen Munns	Olathe	KS	66062	US	2024-01-06
Michelle Folks	Lenexa	KS	66220	US	2024-01-06
Bruce Austin	Kansas City	MO	64137	US	2024-01-06
Lori Brannan	Olathe	KS	66062	US	2024-01-06
Manoj Devalla	Olathe	KS	66062	US	2024-01-06
Jeff Kerr	Miamisburg		45342	US	2024-01-06
Rebecca Hook	Lenexa	KS	66227	US	2024-01-06
Jose A Garcia	Atlanta		30319	US	2024-01-06
Patricia Curran	Lenexa	KS	66227	US	2024-01-06
Mark Smith	Lenexa	KS	66215	US	2024-01-06
Carolyn Tomes	Olathe	KS	66227	US	2024-01-06
Renee States	Lenexa	KS	66227	US	2024-01-06
Shirley Robinette	Olathe	KS	66061	US	2024-01-06
Karen Janes	Lenexa	KS	66227	US	2024-01-06
Karen Johnson	Lenexa	KS	66227	US	2024-01-06
Janet Link	Lenexa	KS	66227	US	2024-01-06
Carol Cooley	Lenexa	KS	66227	US	2024-01-06
Leslie Rollins	Lenexa	KS	66215	US	2024-01-06
Amy Hosier	Omaha	NE	68137	US	2024-01-06
Dave Hosier	Omaha	NE	68137	US	2024-01-06
Amy Bartkoski	Olathe	KS	66227	US	2024-01-06
Castlen Hunt	Lenexa	KS	66227	US	2024-01-06
Michael Bartkoski	Lenexa	KS	66227	US	2024-01-06
Deborah Stone	Lenexa	KS	66227	US	2024-01-06
Melissa Tawney	Olathe	KS	66062	US	2024-01-06
Mike Steggerda	Lenexa	KS	66227	US	2024-01-06
Dawn Steggerda	Lenexa	KS	66227	US	2024-01-06
Kim Leyva	Georgetown		41008	US	2024-01-06
Kate Flax	Olathe	KS	66062	US	2024-01-06
Melinda Marquess	Lenexa	KS	66220	US	2024-01-06
Scott Marquess	Lenexa	KS	66227	US	2024-01-06
Rich and Theresa Jones	Lenexa	KS	66215	US	2024-01-06
Paul LaForge	Lenexa	KS	66215	US	2024-01-06
Eric Kelter	Olathe	KS	66061	US	2024-01-06

Paul McCluskey	Lenexa	KS	66227	US	2024-01-06
Rob Oylar	Kansas City	MO	64131	US	2024-01-06
Ron Pentecost	Lenexa	KS	66227	US	2024-01-06
Jeannie McCluskey	Lenexa	KS	66227	US	2024-01-06
Donna Nichols	Lenexa	KS	66227	US	2024-01-06
LaTrice Cobbins	Lenexa	KS	66227	US	2024-01-06
Robert Johnson	Olathe	KS	66061	US	2024-01-06
Sarah Supiran	Lenexa	KS	66227	US	2024-01-06
David Gunter	Olathe	KS	66062	US	2024-01-06
Sharon Colbert	Prairie Village	KS	66208	US	2024-01-06
Sharon Stucky	Lenexa	KS	66227	US	2024-01-06
Debra Jones	LENEXA	KS	66227	US	2024-01-06
Don Aholt	Kansas City	MO	64110	US	2024-01-06
Mary Carter	Kansas City	MO	64108	US	2024-01-06
Jay Greenough	Wichita	KS	67209	US	2024-01-06
Adam Kaluba	Burleson		76028	US	2024-01-06
Tamara Shepley	Kansas City	MO	64124	US	2024-01-06
Brad and Pam Mohr	Lenexa	KS	66227	US	2024-01-06
Jill Lackey	Lenexa	KS	66227	US	2024-01-06
Paul Christianson	Olathe	KS	66062	US	2024-01-06
wilkelna bourdeau	Lynn		1905	US	2024-01-06
Gary Link	Olathe	KS	66062	US	2024-01-07
Nicole Weddige	Lenexa	KS	66220	US	2024-01-07
John Shepley	Overland Park	KS	66212	US	2024-01-07
Richard Richard	Atlanta		30340	US	2024-01-07
Silvia Gunter	Lenexa	KS	66227	US	2024-01-07
Brian Lester	Olathe	KS	66061	US	2024-01-07
Trinity Morris	Detroit	MI	48219	US	2024-01-07
Ashley Lutjemeier	Lenexa	KS	66220	US	2024-01-07
Stephanie Niemeier	Lenexa	KS	66227	US	2024-01-07
Sandra Arcuri	New York		10118	US	2024-01-07
Brittany Phillips	Fayetteville		28314	US	2024-01-07
Laura Gascogne	Shawnee	KS	66216	US	2024-01-07
Anastcia Roberts	Brooklyn		11210	US	2024-01-07
Sherry McKee	Lenexa	KS	66227	US	2024-01-07
Bryon Larson	Olathe	KS	66061	US	2024-01-07
Jeffery Forgey	Lenexa	KS	66227	US	2024-01-07
Christian Marie	Lenexa	KS	66215	US	2024-01-07

Jeff Abernathy	Lenexa	KS	66227	US	2024-01-07
Pam Mosher	Lenexa	KS	66227	US	2024-01-07
Meghan Saylor	Lenexa	KS	66227	US	2024-01-07
Lee Stucky	Lenexa	KS	66227	US	2024-01-07
John Harrington	Olathe	KS	66062	US	2024-01-07
Erica Harrington	Chicago	IL	60625	US	2024-01-07
Lance Saylor	Lenexa	KS	66227	US	2024-01-07
Pamela Flick	Lenexa	KS	66227	US	2024-01-07
Shawn Flick	Lenexa	KS	66227	US	2024-01-07
Larrie Nichols	Overland Park	KS	66212	US	2024-01-07
Bryant Brown	Chicago	KS	60602	US	2024-01-07
Susan Wailes	Lenexa	KS	66227	US	2024-01-07
Norm Waters	Lenexa	KS	66227	US	2024-01-07
Julie Else	Lenexa	KS	66227	US	2024-01-07
Jonathan Walker	Lenexa	KS	66227	US	2024-01-07
Danielle Gallegos	Lenexa	KS	66227	US	2024-01-07
Nathaniel Gallegos	Lenexa	KS	66227	US	2024-01-07
Patty Clinkinbeard	Lenexa	KS	66227	US	2024-01-07
Kay Pentecost	Lenexa	KS	66227	US	2024-01-07
Alex Laime	West Roxbury		2132	US	2024-01-07
Gregory Else	Lenexa	KS	66227	US	2024-01-07
Trudi Stark	Prairie Village	KS	66208	US	2024-01-07
Krista Else	Olathe	KS	66061	US	2024-01-07
Wayne Mathews	Lenexa	KS	66227	US	2024-01-07
Sharon Caffrey	Olathe	KS	66062	US	2024-01-07
Jacqueline Pederson	Lenexa	KS	66227	US	2024-01-07
Amy Abernathy	Lenexa	KS	66227	US	2024-01-08
Pamela Walker	Kansas City	MO	64112	US	2024-01-08
Terrence Doyle	Overland Park	KS	66212	US	2024-01-08
Kendall Tomes	Lenexa	KS	66227	US	2024-01-08
Noah Andemichael	White Plains	MD	20695	US	2024-01-08
Jan Price	Lenexa	KS	66227	US	2024-01-08
Katie Price	Lenexa	KS	66227	US	2024-01-08
Alora Thompson	Seattle		98109	US	2024-01-08
Anderson Kintu	Atlanta		30319	US	2024-01-08
Marilyn Hall	Florence		85132	US	2024-01-08
MICHAEL TREGONING	Ottawa		61350	US	2024-01-08
Cheryl Aston	Raymore	MO	64083	US	2024-01-08

Lori Franklin	Lenexa	KS	66227	US	2024-01-08
William Murray	Lenexa	KS	66227	US	2024-01-08
rosemary torossian	Sherman Oaks		91423	US	2024-01-08
Bryan Obi	Carrollton	TX	75007	US	2024-01-08
Allie Trompeter	Lenexa	KS	6622766218	US	2024-01-08
Stacey OKeefe	Xenia	OH	45385	US	2024-01-08
Ben Petersen	Lenexa	KS	66227	US	2024-01-08
Leah Petersen	Olathe	KS	66061	US	2024-01-08
Walden Hodges	Lenexa	KS	66227	US	2024-01-08
Michele Bingham	Charlotte		28206	US	2024-01-09
Jerrica Martin	Aiea		96701	US	2024-01-09
joce garcia	Dallas		75238	US	2024-01-09
Kara Horigan	Olathe	KS	66277	US	2024-01-09
Nadia Alexis	Orlando	FL	32807	US	2024-01-09
Andrea Percy	Lenexa	KS	66227	US	2024-01-09
Katrina Taylor	Lake City		32024	US	2024-01-09
Nikki Graham	Sarasota		34236	US	2024-01-09
Matthew Gelsheimer	Olathe	KS	66061	US	2024-01-09
Marion West	Olathe	KS	66061	US	2024-01-09
Mirsad Cekovic	New York		10118	US	2024-01-09
Eduardo Levia	San Francisco	CA	50321	US	2024-01-09
Mary Jo Kostus	Lenexa	KS	66227	US	2024-01-10
Sam Sharp	Kansas City	KS	66103	US	2024-01-10
Maleah Owsley	Indianapolis		46202	US	2024-01-10
Kofi Addo	Cerritos		90703	US	2024-01-10
robert cobb	Ormond Beach		32174	US	2024-01-10
Juan Morales	Newark		7105	US	2024-01-10
Stephen Farrell	Lenexa	KS	66227	US	2024-01-10
Amanda Kushner	West Chester		19380	US	2024-01-10
Raven Koki	New York		10605	US	2024-01-10
Dinsdale Jackson				US	2024-01-11
Jennifer Montoya	Orem		84058	US	2024-01-11
Breauna Gant	Olathe	KS	66062	US	2024-01-11
Joshua Curphey	Peterborough		PE7	US	2024-01-11
Ruba Saleh	Maryville		37803	US	2024-01-11
Kathleen Stone	Wake Forest	NC	27587	US	2024-01-11
Taylor Nixon	Graham		27253	US	2024-01-11
Kyle Powers	Towson		21286	US	2024-01-11

Kevin Zou	Philadelphia		19120	US	2024-01-11
Adnan Bibi	Austin		78705	US	2024-01-11
Austin Ward	Corvallis	OR	97330	US	2024-01-11
Yurai Dina	Las Vegas		89115	US	2024-01-11
Jessica Downing	Bridgeton		8302	US	2024-01-12
Janell Washington	Washington		20012	US	2024-01-12
Carolyn Hall	lenexa	KS	66227	US	2024-01-12
Shawn Scholz	Kansas City	KS	66103	US	2024-01-12
Melissa Pierson	Kansas City	MO	64114	US	2024-01-12
Kristina Goodman	Minneapolis		55408	US	2024-01-12
Scott Grier	Orlando	FL	32801	US	2024-01-12
Amanda Reed	Lenexa	KS	66227	US	2024-01-12
Joanne BURCHARD	Lenexa	KS	66220	US	2024-01-12
Mason Scholz	Overland Park	KS	66212	US	2024-01-12
Logan Scholz	Kansas City	KS	66227	US	2024-01-12
Jeanne Boucek	Lenexa	KS	66215	US	2024-01-12
Afton Bingman	Lenexa	KS	66215	US	2024-01-12
Marissa Bundy	Lenexa	KS	66227	US	2024-01-13
Adam Bundy	Dallas	TX	75207	US	2024-01-13
Shawn Luke	Lenexa	KS	66227	US	2024-01-13
Lexi Parish	Lenexa	KS	66227	US	2024-01-13
Terrence Doyle	Lenexa	KS	66227	US	2024-01-13
Heath Reed	Olathe	KS	66061	US	2024-01-13
Melissa Ruiz	Lenexa	KS	66215	US	2024-01-13
Alisa Ford	Lenexa	KS	66227	US	2024-01-13
Bret Chapman	Lenexa	KS	66227	US	2024-01-13
Aaron Niemeier	Lenexa	KS	66227	US	2024-01-13
Shirley Pavlovich	Lenexa	KS	66215	US	2024-01-13
Severin Risner	Portland		97206	US	2024-01-13
David Gryszowka	Olathe	KS	66062	US	2024-01-13
Anne Healy	Lenexa	KS	66227	US	2024-01-13
Cery Castillo	Houston		77043	US	2024-01-13
Jill Achanzar	Atco		8004	US	2024-01-13
Donene Pollack	Kansas City	MO	64184	US	2024-01-13
Lou salced	Cleveland		77327	US	2024-01-13
Emily Antolovic	Glenview		60026	US	2024-01-14
Aiden Bell	Lake Orion		48362	US	2024-01-14
Lisa Bakeman				US	2024-01-14

Mm Rr	Bnd		Ye	US	2024-01-14
Christine Jones	Omaha		68105	US	2024-01-14
Debra Wallace	OLATHE	KS	66061	US	2024-01-15
Di Shi	Shawnee	KS	66218	US	2024-01-15
Shuaishuai Lambkin	Lenexa	KS	66227	US	2024-01-15
Sukhpreet Grewal	Olathe	KS	66061	US	2024-01-15
Rada Lewis	Lenexa	KS	66227	US	2024-01-15
Jodi Easter	Lenexa	KS	66227	US	2024-01-15
Brooke Lennington	Lenexa	KS	66215	US	2024-01-15
Terry Evans	Topeka	KS	66614	US	2024-01-15
Jack Hruska	Lenexa	KS	66227	US	2024-01-15
Durrel Harper	Lenexa	MO	66227	US	2024-01-15
Corey McIntyre	Olathe	KS	66061	US	2024-01-15
Krista Phelps	Port Huron		48060	US	2024-01-15
Dana Burgess	Arvada		80004	US	2024-01-15
Cindy Steck	Olathe	KS	66062	US	2024-01-15
Kyla Alsman	Cape Coral	FL	33904	US	2024-01-15
Livvy Briece	Kansas City	MO	64133	US	2024-01-15
Tizzi Blackburn	Lenexa	KS	66227	US	2024-01-15
Lisa Bowen	Olathe	KS	66062	US	2024-01-15
Gale Hansen	Olathe	KS	66062	US	2024-01-15
Melissa Leach	Kansas City	KS	66109	US	2024-01-15
Michelle jarman Jarman	Mexico City		3020	Mexico	2024-01-15
Katie Czyz	Olathe	KS	66061	US	2024-01-15
Brett Childers	Lenexa	KS	66227	US	2024-01-15
Carrie DeMaria	Olathe	KS	66062	US	2024-01-15
Tammy Bangs	Lenexa	KS	66227	US	2024-01-15
Misty Jared	Baxter		38544	US	2024-01-15
Pablo Unzueta	Chicago		60628	US	2024-01-15
Eric Easter	Olathe	KS	66062	US	2024-01-15
Meghan Magistro	Lenexa	KS	66227	US	2024-01-15
Carter Saul	Grove City		43123	US	2024-01-15
Azael Amador	San Antonio		78245	US	2024-01-16
Butter Man	Frisco		75933	US	2024-01-16
Derek Brown	San Antonio		78256	US	2024-01-16
David Magistro	Kansas City	MO	64184	US	2024-01-16
Christina Alexander	Olathe	KS	66061	US	2024-01-16
Michelle Abella	Overland Park	KS	66214	US	2024-01-16

Julian Nardelli	McKinney		75071	US	2024-01-16
Hunter Geiger	Houston		77005	US	2024-01-16
Junior Enriquez	San Antonio		78249	US	2024-01-16
Kiara Williams	Mount Joy		17552	US	2024-01-16
Carlos Maldonado	San Antonio		78222	US	2024-01-16
Andrés Naranjo	Lewisville		75067	US	2024-01-16
Khloe Obaya	San Antonio		78237	US	2024-01-16
alijah Sylve-Matautia	Converse		78109	US	2024-01-16
Yo No	McKinney		75070	US	2024-01-16
Nygil Setzer	Newbraunfels		78130	US	2024-01-16
Anita Max Wynn	San Antonio		78245	US	2024-01-16
Kristi Holthaus	Shawnee	KS	66226	US	2024-01-16
hi hi	Flower Mound		75022	US	2024-01-16
khmerapmbz@gmail.com munir balo	Elk Grove Village		60007	US	2024-01-16
Tom Crowder	Lenexa	KS	66227	US	2024-01-16
Constance Phipps	Lenexa	KS	66227	US	2024-01-18
Edward Shires	Lenexa	KS	66227	US	2024-01-18
Ben Byers	Lenexa	KS	66215	US	2024-01-18
Howard Wilcox	Olathe	KS	66062	US	2024-01-18
Jonathan Swinney	Olathe	KS	66062	US	2024-01-18
Nube Pinos	Brooklyn		11233	US	2024-01-18
Linda INFANTE	West Granby		6090	US	2024-01-19
Dennis Szabo	Lenexa	KS	66227	US	2024-01-19
Monica Montgomery	Kansas City	MO	64106	US	2024-01-19
Evan Ferrante	Overland Park	KS	66212	US	2024-01-19
Mark Mader	Olathe	KS	66062	US	2024-01-19
Justin Kaufman	Fort Wayne		46806	US	2024-01-20
Ben Harder	Lenexa	KS	66220	US	2024-01-20
Carl Creamean	Olathe	KS	66061	US	2024-01-20
Stephen Havel	Olathe	KS	66061	US	2024-01-20
Valerie Schroeder	Olathe	KS	66062	US	2024-01-20
Jamee Patrick	Orange Park		32073	US	2024-01-21
Stephanie Perez	Chino		91710	US	2024-01-21
Michael Edwards	Shawnee	KS	66226	US	2024-01-21
Sarah Edwards	Shawnee	KS	66226	US	2024-01-21
Stiven Cabrera	Atlanta		30297	US	2024-01-21
Kathy Snelgrove	Olathe	KS	66062	US	2024-01-21
Tina Williamson	Springfield		65804	US	2024-01-21

Paula Sapata	Wichita Falls		76302	US	2024-01-22
Jose Rodriguez	Lenexa	KS	66227	US	2024-01-22
Raj Patel	Lenexa	KS	66227	US	2024-01-22
Kimberly Curtis	Lenexa	KS	66220	US	2024-01-22
Kyle Nicolson	Rancho Cucamonga	CA	91730	US	2024-01-23
Shannon Perr	Lenexa	KS	66227	US	2024-01-24
Kelsey Lee	Citrus Heights		95621	US	2024-01-24
Kayla Fritchey	Topeka	KS	66614	US	2024-01-26
Akihisa Nonoyama	Olathe	KS	66061	US	2024-01-27
Randy Retherford	Lenexa	KS	66227	US	2024-01-30
Kelly Shea	Kansas City	KS	66102	US	2024-01-30
Nicholas Payne	Kansas City	KS	64124	US	2024-01-30
Floyd Shoup	Overland Park	KS	66210	US	2024-01-30
Alan Parker	Olathe	KS	66061	US	2024-01-30
Heather Jones	Olathe	KS	66061	US	2024-01-30
Kathleen Pirie	Olathe	KS	66061	US	2024-01-30
Charles Pirie	Olathe	KS	66061	US	2024-01-30
Melissa Fox	Shawnee	KS	66216	US	2024-01-30
Lisa Fox	Kansas City	MO	64132	US	2024-01-30
Melanie Beck	Olathe	KS	66061	US	2024-01-30
Lori Poland	Olathe	KS	66061	US	2024-01-31
Timothy Fogarty	Olathe	KS	66061	US	2024-01-31
Melody Luschei	Olathe	KS	66061	US	2024-01-31
Julie Whitley	Olathe	KS	66062	US	2024-01-31
Abby Baird	Olathe	KS	66061	US	2024-01-31
Stacy Stephens	Olathe	KS	66061	US	2024-01-31
Allison Wollenhaupt	Olathe	KS	66061	US	2024-01-31
Lisa Morrison	Olathe	KS	66062	US	2024-01-31
Mandy Perkins	Olathe	KS	66062	US	2024-01-31
Kelley Chilcoat	Overland Park	KS	66213	US	2024-01-31
Brian Chilcoat	Saint Joseph	MO	64501	US	2024-01-31
Patrick Gallagher	Olathe	KS	66061	US	2024-01-31
Bootsie Martin	Olathe	KS	66061	US	2024-01-31
Angela Tunnell	Kansas City	KS	64184	US	2024-01-31
Doug Tunnell	Overland Park	KS	66213	US	2024-01-31
Gary Milligan	Kansas City	MO	64118	US	2024-01-31
Krishna Rangarajan	Kansas City	IL	66109	US	2024-01-31
Dennis Eaton	Omaha	NE	68137	US	2024-01-31

Mike Johanning	Olathe	CO	66061	US	2024-01-31
Denise Brooks	Olathe	KS	66061	US	2024-01-31
Stephanie Volpe	Olathe	KS	66061	US	2024-01-31
Mike Mulligan	Lenexa	KS	66227	US	2024-01-31
Victoria Worrel	Olathe	KS	66061	US	2024-01-31
Jack Brake	Olathe	KS	66062	US	2024-01-31
Erin Corona	Olathe	KS	66061	US	2024-01-31
Sandra Askey	Solidaridad		77725	Mexico	2024-01-31



January 4, 2023

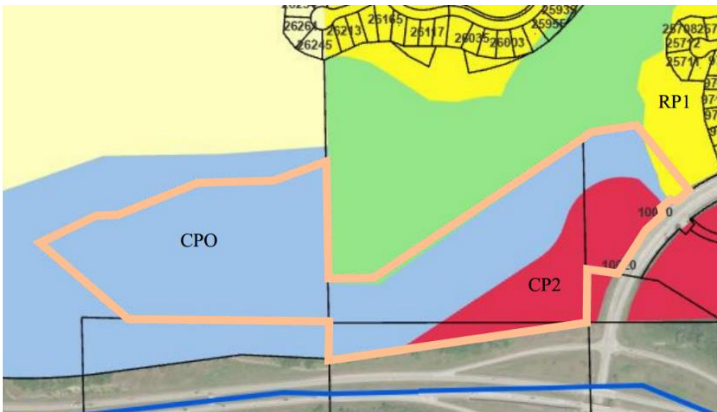
RE: CANYON RIDGE APARTMENT HOMES – NW K-10 & CANYON CREEK BLVD

Dear Planning Commission and City Council Members:

On behalf of the development team, we have worked very hard with the City’s Community Development Department to design a very thoughtful and appropriate mixed-use project at the NW corner of Canyon Creek Blvd (Major Arterial) and K-10 Highway (Freeway/Expressway which carries 70,000-80,000 vehicles per day). Intended to address a serious need for more affordable homes in Western Lenexa (as noted in the recent studies of the City’s Comprehensive Plan), the Project includes a Class-A, medium-density, multi-family residential community (big home/mansion style), a senior living residential community, and a convenience store facility near the main intersection. We understand that you have received several letters of opposition to our project from certain residents north of the Project. This letter is intended to address several of the primary concerns stated:

1. **Comprehensive Plan:** Many opponents claim they relied on the City’s Future Land Use Plan when they bought their homes and then *incorrectly* allege the Future Land Use Plan shows this area to be developed for agriculture and single-family “homeownership.” The current Future Land Use Map (shown below) and Comprehensive Plan has long called for this area to be developed as an “Office/Employment Center” (CPO zoning) or “Regional Commercial Center” (CP2 zoning). Examples include “both campus type development as well as single buildings and could include both single and multiple tenants.”

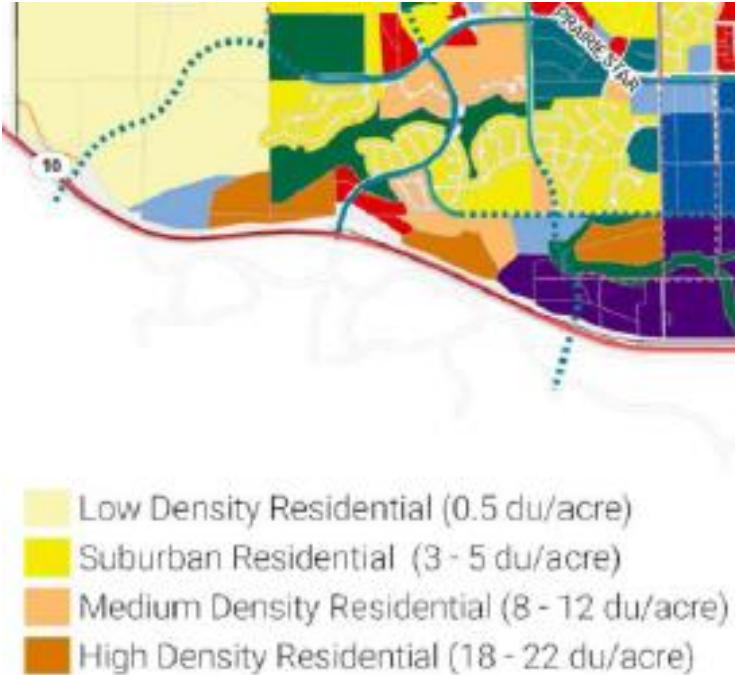
Current Future Land Use Map:



Most real estate and planning experts understand there is no need for office development in this area of Lenexa (or nearly anywhere else in Johnson County post-Covid) and considering the steep grades of the subject property it is nearly impossible to develop office buildings on the subject property. Recognizing these facts, City Planning is currently suggesting changes to the Comprehensive Plan and Future Land Use Map that will affect this area of Western Lenexa. We understand that at least some of the neighbors are aware of the pending changes through public workshops. In May 2023, the City

Planning Commission and the City Council held a public, joint planning, work session and discussed multiple scenarios for development of this area. Recognizing the need for more attainable and affordable housing and greater density in Western Lenexa, the City Staff and a hired independent planning consultant (Houseal-Lavigne) strongly suggested this area be changed from Office/Employment Center to High-Density Residential (see proposed map changes below) with retention of the commercial area adjacent to Canyon Creek Blvd. The May 2023 Presentation specifically states that “The location of multifamily housing flanking state highways has proven appropriate to buffer lower density residential uses from the highway.”

Changes to Future Land Use Map:



Opponents claim that more apartments are not needed next to them and there are better locations in other parts of Lenexa (Not in My Back Yard). They mention a previous apartment proposal on this same land they defeated nearly 6 years ago in 2018. But this Project is significantly different and better than the 2018 proposal for many reasons. It is lower in both height and density. The 2018 proposal for The Vistas at Canyon Creek was for RP-4 zoning, included 3/4-story buildings of contemporary design totaling 294 units on 21.689 acres with a density of 13.56 units/acre. The new Project is for PUD zoning (planned unit development - mixed-use) and includes only 2-story buildings with walkouts and traditional residential design totaling 346± units on 35.95 acres with a density of only 9.62 units/acre. The 2018 proposal was considered high-density under the Lenexa Zoning Regulations, but the new Project is considered medium-density (typically RP-3 zoning) and is more in line with the height and design of the residential subdivisions to the north.

- 2. Views/Aesthetics:** Many opponents claim they moved to this area based solely on the “panoramic views and wildlife located in the area ... and that those views would remain undisturbed in perpetuity.” This understanding is in direct conflict with the existing Future Land Use Map which calls for office buildings that could be as tall as 150’ or more (there is no height limitation in the CP-O district). They opposed the 2018 rezoning (in part) because it included 3/4-story buildings with much greater density (RP-4 density)

and did not like the contemporary design. The current proposed Project limits building heights to 2-story with walkouts and includes a traditional design which is much more in line with the single-family homes where they live. Residents should understand they have no legal right under law to a protected view shed in perpetuity and that the City cannot prohibit other landowners from developing their lands. A landowner has property rights to develop its land just like the landowner who developed their single-family subdivision and chased away wildlife. The proposed Project is sensitive to the neighborhood concerns which is why we are proposing 2-story, big home apartments buildings (aka "mansions" - where each resident has internal access to a garage). These are not tall, high-density buildings like proposed in 2018. Instead, these homes will be very similar in design to their single-family homes, but are merely larger in square footage, thus the name "mansion" style. They will be beautiful buildings and complimentary to their single-family homes.

Importantly, there is a huge natural preserve (i.e., Cedar Station Park) between their single-family subdivision and our Project which provides significant buffers measuring several hundreds of feet to over one thousand feet between our respective developments. There are no other multi-family projects in Lenexa with this same amount of large parkland and buffer distance adjacent to a single-family subdivision, and many of those other apartment developments are high-density, unlike the proposed Project which is medium density. See buffer map below.



The City recently approved an RP-4 apartment project immediately east of Canyon Creek Blvd (Canyon Creek Apartment Homes) which is closer to single-family homes where the same alleged impacts to views, traffic, and noise were raised, without any evidence to support those claims. The mansion style homes proposed with our Project would have the same low-profile as their single-family homes. The subject area ideally suited for multi-family use, just like the recently approved project across the street. It is next to a very busy state highway and provides a great transition to their single-family homes in terms of density and buffering noise from the highway. Moreover, there is no need for offices, and very little retail needed in the area, so the only other viable use is apartments. PUD zoning is much better than RP-4 or RP-3 zoning due to more flexible controls. The proposed community is virtually identical to

our Sonoma Hill community at 89th and Maurer Rd. which was unanimously approved a few years ago and that project is within 50' of single-family homes. Sonoma Hill did not hurt their property values, and neither will our Project.

- 3. Traffic/Noise:** Opponents claim that there are already traffic issues along Canyon Creek Blvd and K-10 Highway. One person mentioned that there was a wreck on K-10 Highway which backed up traffic on the highway. While true there is a lot of traffic along K-10 Highway (approximately 70,000 – 80,000 trips per day), and sometimes there are wrecks, but that is irrelevant to our Project. Future development along K-10 Highway (i.e., Panasonic battery plant, etc.) will add significantly more traffic to K-10 Highway than our Project. KDOT is already considering widening and other improvements along K-10 Highway that has nothing to do with our Project.

Canyon Creek Blvd is a 4-lane divided Major Arterial which carries virtually no traffic today. The Project is not expected to create any negative impacts to traffic along Canyon Creek Blvd. We had an independent traffic expert prepare a Traffic Impact Study (TIS) which has been submitted to the City and reviewed by the City's traffic engineers. The TIS shows that 85% of the traffic from the Project will enter and exit from K-10 Highway, so only a small fraction of the traffic will even use Canyon Creek Blvd going north of our Project. The Level of Service (LOS) at the intersections along Canyon Creek Blvd, as measured before and after the Project are nearly all "A", which is the best level possible. The only exceptions are the east bound internal movements at 101st Street and the first access drive which are "B", which is still considered excellent. In short, the Project will cause virtually zero traffic impacts to Canyon Creek Blvd.

Opponents claim that somehow the Project will increase noise from the highway with the alleged removal of "all trees." First, maintaining existing trees is valuable to our Project from a screening and aesthetic standpoint, and we intend to save every good tree possible. Second, we believe the buildings and the additional landscaping we install will likely mitigate sound better than trees that have no leaves 4 months out of the year. We are not removing the hill which is the main barrier to highway sound. Finally, the complaint about sound is exactly why it is a bad idea to build single-family homes adjacent to a highway. As evidenced in the May 2023 Presentation on suggested changes to the Comprehensive Plan, the City is keenly aware that highways do not make good neighbors to single-family homes and hasn't approved a single-family project next to a highway in many years. The neighbors' suggestion that the subject land be developed for single-family homeownership is bad planning.

- 4. Property Values:** A common complaint of opponents to apartments is the alleged negative impact to property values. This issue has been studied across the country as well as by the Johnson County Appraiser's office and been universally debunked. You can look at property values all over Lenexa and other cities in Johnson County which show no decrease to single-family homes values caused by nearby apartment projects. It also makes no difference the density of the apartment community. *See attached Study from the Kem C. Gardner Policy Institute, The University of Utah, School of Business, The Impact of High-Density Apartments on Surrounding Single-Family Home Values in Suburban Salt Lake County (February 2021)*. This Project will be constructed as a Class-A apartment community in terms of building design and construction, architectural design and materials, and amenities. The Project will be very similar to our Sonoma Hill at 89th and Maurer Rd. which also consists of big home/mansion style apartments. There has been no impact to property values for the single-family homes that are literally across the street within 50' of Sonoma Hill. This is the same for apartment projects all over Johnson

County. During the recent consideration of an apartment community, we are developing in Leawood called East Village, the Johnson County Appraiser's Office submitted a letter to the City which found no negative impacts from apartments to single-family homes – see letter below.



11811 S. Sunset Drive, Suite 2100
Olathe, Kansas 66061

November 18, 2021

Rick Oddo

RE: Leewood Apartments

This letter is sent to confirm our discussion the other day. In the 26 years I have been in the assessment practice, I have not observed a situation where the construction of an apartment complex negatively affected the single home market. Even in cities like Scottsdale and Paradise Valley, both located in Maricopa County.

I have heard the same concerns for commercial developments and special use properties, but the concern does not come to fruition.

Respectfully,

David Beau Boisvert

David Beau Boisvert RMA
County Appraiser

Some of the neighbors have also mentioned that the City should not approve a project they allege will impact the value of single-family property values totaling \$75 million. The economic value (i.e., tax revenues) that a project may bring is inappropriate to consider in land use decisions. Such factor is not part of the City's zoning criteria or allowed under Kansas zoning law. But since they have raised the issue, it is worth noting that the proposed Project will likely have a value exceeding \$100 million.

It is also worth mentioning that low density projects like single-family subdivisions, do not fully pay for city services they receive – see comment below from the [Johnson County Community Housing Study \(February 2021\)](https://ucsjoco.org/wp-content/uploads/2021/02/2021-Johnson-County-Housing-Study.pdf). <https://ucsjoco.org/wp-content/uploads/2021/02/2021-Johnson-County-Housing-Study.pdf>. In fact, that is one reason why the City believes that greater density is needed in Western Lenexa.

Smart Growth in Kansas City.

The topic of smart growth and infrastructure burdens on city governments, housing costs, and the taxpayers is not new to the Kansas City metro. Past discussions and lecture have unveiled interesting data on how metro cities should prioritize funding in the future.

A lecture sponsored by the Mid-America Regional Council in October, 2019 titled *Smart Growth KC How? Where?* brought in consultants from Urban3 and Strong Towns to show fiscal impacts of infrastructure growth. The insightful presentation showed that much of the traditional subdivision growth is not fiscally responsible, with six times more infrastructure growth than the growth in population. Subsidies are effectively built into infrastructure for large lot subdivisions. Or rather, the tax burden for all residents increases instead of only for those living in the large lot subdivisions. Instead, investing in existing neighborhoods first and more compact new development patterns can cover infrastructure costs and services.

View the presentation at <https://kclibrary.org/signature-events/smart-growth-kc-how-where>

5. **Convenience Store:** Opponents claim there is no need for a convenience store at this location, citing examples of other c-stores nearby (one proposed across the street and another nearly 4 miles away). For good reason, determination of *need* is not permitted under the City's zoning regulations or Kansas statutes when considering whether to rezone land. Whether the market can support 2 groceries stores, 2 pharmacies, 2 fast-food restaurants or 2 single-family subdivisions next to each other is not for the government to decide. Instead, the government establish which uses belong in which zoning districts based on establish land use criteria and not some speculative decision about whether the market can support such use. A convenience store is merely one, of a number of permitted uses allowed under CP2 zoning. The City has already determined that CP2 zoning is appropriate for portions of the subject land area.

6. **Neighborhood Input:** While neighborhood input is to be considered, it is not and should not be the basis for denying a rezoning application. Importantly, the Kansas Supreme Court has stated that rezoning may not be based on a “plebiscite of the neighborhood” but must be based on the interests of the community at large. *See e.g., Waterstradt v. Leavenworth*, 203 Kan. 317 (1969); *Taco Bell v. City of Mission*, 234 Kan. 879 (1984). So, notwithstanding that some neighbors are opposed, it is not a valid reason to deny the Project. Virtually all projects have some neighborhood opposition and if their approval was required then nothing would ever get built. Having said that, we are sensitive to their concerns and are proposing a very appropriate 2-story medium density project that is Class-A. The development of this Project will be no different than the development of the single-family subdivision where they now live, in terms of the removal of trees and wildlife. Just as their developer acted with care when building their subdivision, so do we intend to act when developing our Project. We intend to save every good tree possible and treat the land with respect.

While using hyperbolic terms and phrases to describe our Project such as, “high-density” (when it is clearly medium density), or “massive project” (when the density and height are very modest), has been effective in defeating other apartment projects (like was done in 2018) these exaggerations do not reflect reality in this case. It should be noted that these same tactics were highlighted in the Johnson County Community Housing Study regarding opposition to many apartment developments and are generally without bases – see below.

“The high amount of public opposition to housing projects in nearly every city. Stories of projects getting denied by Councils even though the project met code standards were mentioned in nearly every session. Opposition is not necessarily geared toward one product - apartments, attached, low-income, and other mixed-use arrangements have all faced opposition. Several reasons are cited by the public in opposition, although not based on provided facts or evidence: › Suggestions of traffic congestion. › Accusations of the project increasing property taxes. › Claims of detriment to neighborhood character.”

@ P. 70.

It appears the neighbors do not want anything built on this land, which cannot be the test.

We are looking forward to sharing this plan with you and seeing it come to fruition.



Improving the way we live.

Dejan Eskic
Senior Research Fellow

The Impact of High-Density Apartments on Surrounding Single-Family Home Values in Suburban Salt Lake County

New, dense housing continues to be a point of conflict in growing communities as concerns over negative impacts to home values dominate the discussion. This study quantifies how new apartment construction has impacted single-family home price acceleration over the last decade.

February 2021

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The Impact of High-Density Apartments on Surrounding Single-Family Home Values in Suburban Salt Lake County

Analysis in Brief

This study found apartments built between 2010 and 2018 have not reduced single-family home values in suburban Salt Lake County. In response to accelerating housing prices over the last decade, the market continues to shift to denser development to slow this trend. However, denser development continues to be a politically controversial topic on city council agendas as existing residents often bring up negative impacts on home values. Single-family homes located within 1/2 mile of a newly constructed apartment building experienced higher overall price appreciation than those homes farther away.

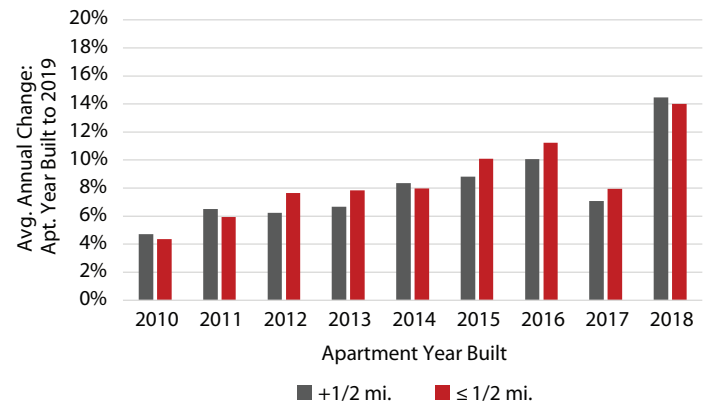
Key Findings

- **New Apartments Have Not Reduced Single-Family Home Values**—Between 2010 and 2019, homes located within 1/2 mile of a newly constructed apartment building experienced a 10.0% average annual increase in median value, while the value of those farther away increased by 8.6%. Only in the Southeast part of the county did homes more than 1/2 mile away from new apartment construction experience higher average price appreciation than those located $\leq 1/2$ mile.
- **Negative Impacts**—The only occurrence where negative price trends followed apartment construction was for homes near apartments built in 2010 and 2011. This resulted from the negative economic impacts brought on by the housing crash of the prior decade.
- **Higher Value per Square Foot**—Between 2010 and 2019, homes that are located $\leq 1/2$ mile of new apartments averaged

an 8.8% higher median value per square foot compared with those farther away. However, the total median market value of single-family homes averaged 4.7% greater for those that are located more than 1/2 mile away from new apartments.

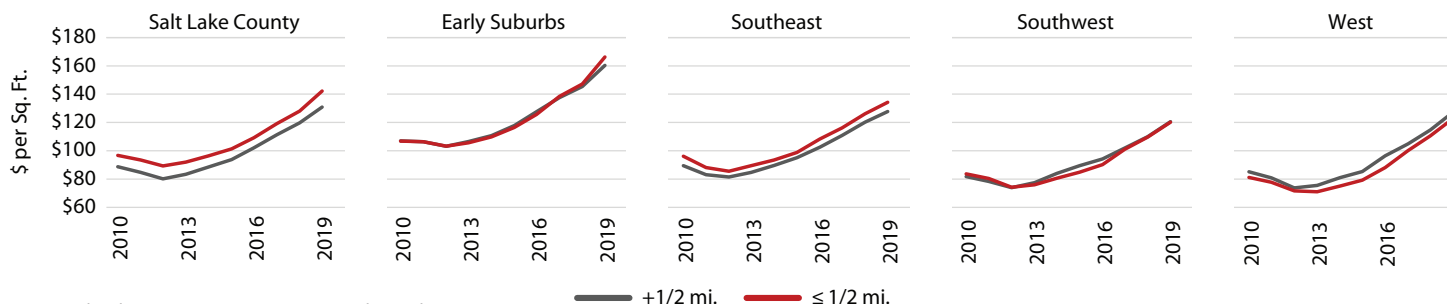
- **Homes Near Apartments Are Smaller and Older**—In suburban Salt Lake County overall, homes located within 1/2 mile of new apartments are approximately 270 sq. ft., or 11.1%, smaller than those farther away. Homes that are located $\leq 1/2$ mile of new apartments are seven years older on average than those located farther away and lot sizes average 0.02 acre smaller for homes located $\leq 1/2$ mile of new apartments.

Average Annual Change in Median Price, Year of Apartment Built to 2019, Salt Lake County



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Median Market Value per Square Foot of Single-Family Homes by Distance to Nearest Apartment



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Access full report at gardner.utah.edu

Introduction

Over the last decade, Utah has led the nation in the rate of population growth, resulting in a record demand for housing. While the housing oversupply of the 2000s was absorbed as the economy recovered from the recession in the early 2010s, supply in the new decade has struggled to keep up, leading to a housing shortage of 53,000 units in 2020. According to the National Association of Realtors, the year-over median sales price of a home in the Salt Lake metropolitan area increased by 12.3% in the first quarter of 2020. The Salt Lake metropolitan area ranked 16th of 182 metropolitan areas surveyed for a year-over price increase. Housing price increases were lower in 90% of the metropolitan areas surveyed.¹ Additionally, land improvement costs, such as excavation and utility work, increased by approximately 40% between 2007 and 2017, and building costs grew 23% in the same period.² Land prices have also soared with a limited supply across the Wasatch Front. The Wasatch Mountains to the east and the Oquirrh Mountains to the west limit the availability of developable land in Salt Lake County.

The combination of soaring demand and supply shortages continues to push the market to provide a more affordable housing product. This is typically done through density because the price of land is distributed across more units. Over the last decade, the market has shifted to denser development, with nearly 48% of all units being built as something other than single-family.

As denser projects continue to appear on city council agendas, opposition to them has grown, manifested in a rising Nimby (not in my back yard) sentiment.³ Amongst the grievances aired by those opposing denser development is an expected negative impact on property values. The question, “Does new apartment construction negatively impact single-family home values?” is challenging to answer because the housing market, over the last decade, has experienced historic price accelerations—it is rare to find a home whose value has decreased. Rather, this study attempts to quantify how new apartment construction has impacted single-family home price acceleration.

This study found apartments built between 2010 and 2018 have not reduced single-family home values. Compared by distance, single-family homes located within 1/2 mile of a newly constructed apartment building experienced higher overall price appreciation than those homes farther away. Measuring the median value of homes from the year the apartment was built to 2019 shows that homes located within 1/2 mile of an apartment experienced a 10.0% average annual increase, while the value of those farther away increased by 8.6%. This implies an additional 1.4 percentage points in annual price appreciation for homes closer to new apartment buildings (see Table 1). Similar results

Table 1: Average Annual Change in Median Price, Year of Apartment Built to 2019

Area	+1/2 mi.	≤1/2 mi.
Salt Lake County	8.6%	10.0%
Early Suburbs	7.6%	10.7%
Southeast	7.3%	6.8%
Southwest	7.7%	9.7%
West	10.5%	13.7%

Note: See Figure 1 for area designations.
 Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

are seen in most of the county, with the likely driver being that new apartment construction brings new demand and new dollars to a community and redevelops an older piece of property, thus bringing more vibrancy and “buzz” to the area.⁴

Literature Review

The academic literature leans towards showing multifamily, denser development having either no impact or a positive impact on single-family residential values. A study in King County, Washington, shows an increase in single-family home values for those located near denser development. The study also showed an increase in access to other land uses and parks, adding additional benefits.⁵

A study completed by the National Association of Homebuilders found that between 1997 and 1999, single-family values increased 2.9% for those homes within 300 feet of an apartment building, compared with an increase of 2.7% for those that weren’t located next to an apartment.⁶ Based on data from 1970 to 2000, a study published in 2003 by Harvard’s Joint Center for Housing Studies concluded that apartments posed no threat to surrounding single-family house values.⁷

A study from researchers at Virginia Tech University concluded that apartments with attractive design and landscaping increased the overall value of nearby detached housing, citing three possible reasons.⁸ These include, first, new construction serves as a potential indicator of positive economic growth; second, new apartments increase the pool of future homebuyers for current homeowners; and third, apartments with mixed-use development often increase the attractiveness of nearby communities as they provide more housing and amenity choices.⁹

An additional benefit is a decrease in traffic, not an increase as often thought. A study by the National Personal Transportation Survey found that doubling density decreases vehicle miles traveled by 38% since denser households typically own fewer vehicles.¹⁰

Methodology & Overview

The Salt Lake County Assessor’s market value data is used to measure new apartment construction effects on single-family homes. Two measures are used. First, the average annual rate of value change from the year the apartment was constructed to 2019 is used to measure the overall impact. Second, the year-over percent change of median market value is used to estimate annual fluctuations.

Because of data availability, only apartments built between 2010 and 2018 are used to measure these impacts. Single-family homes are divided into two categories, homes that are less than or equal to one-half mile ($\leq 1/2$ mi.) from new apartment construction, and those that are farther away ($+1/2$ mi.).

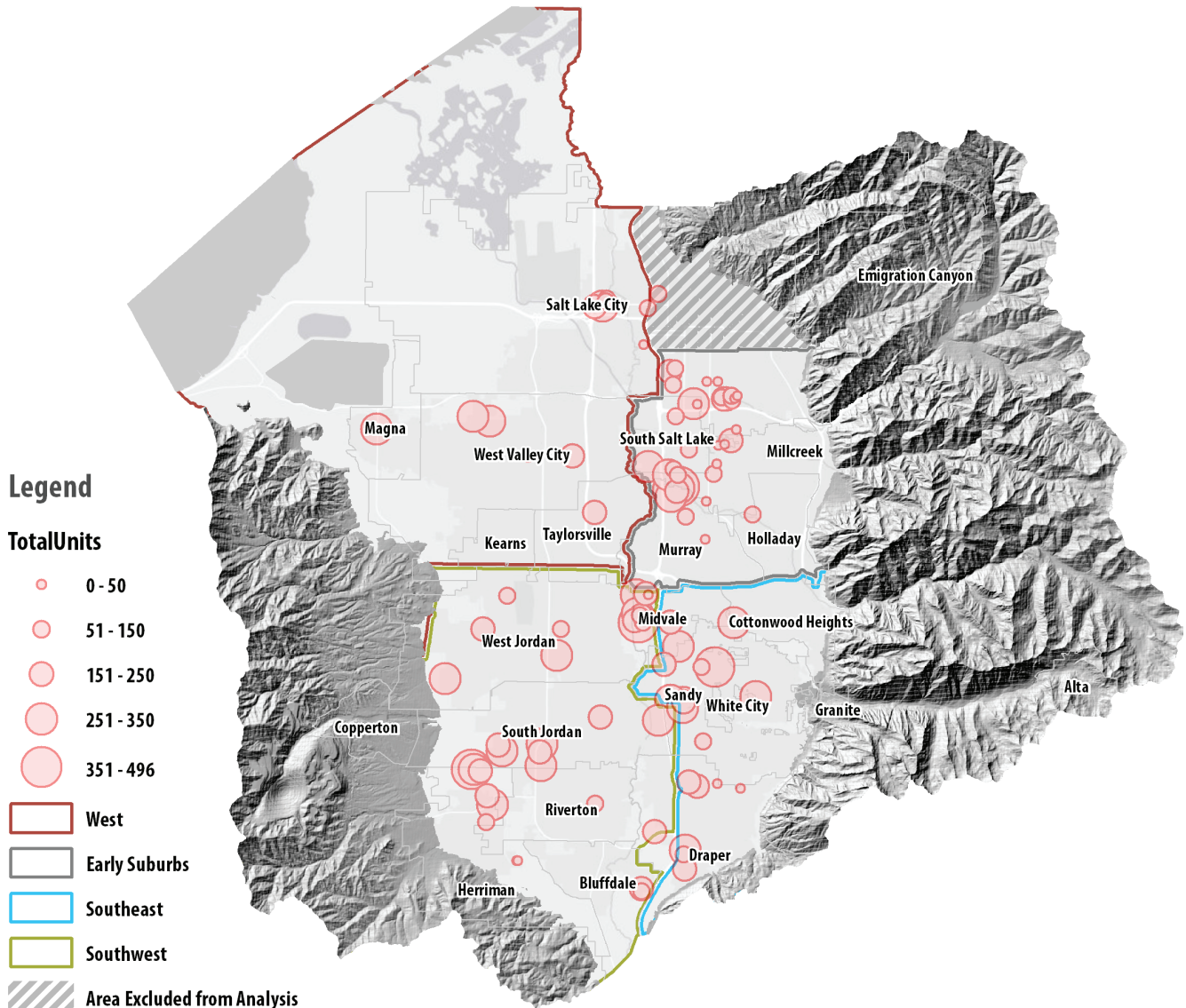
The five geographies covered by this study are shown in Figure 1. Because of a range of development activity and multiple factors not present in the suburban parts of the county,

the greater Salt Lake City downtown area is excluded from this study. The five geographies are based on Census tracts and consist of the following cities and townships:

- **Suburban Salt Lake County:** consists of the four geographies mentioned below.
- **West:** includes a part of Salt Lake City, Magna, West Valley City, Kearns, and Taylorsville.
- **Early Suburbs:** includes a part of Salt Lake City, South Salt Lake, Millcreek, Murray, and Holladay.
- **Southeast:** includes part of Midvale, Cottonwood Heights, Sandy, and part of Draper.
- **Southwest:** includes Bluffdale, Harriman, Riverton, South Jordan, West Jordan, and part of Midvale and Draper.

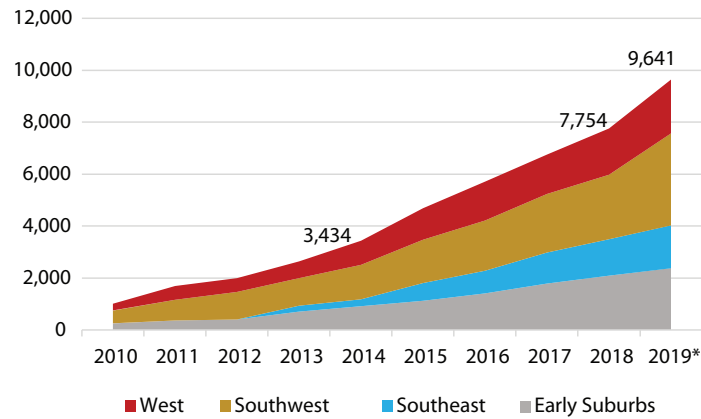
Apartment construction boomed in Salt Lake County during the last decade. Between 2010 and 2018, 7,754 units were

Figure 1: Areas of Analysis and Location of Apartments by Number of Units, 2010–2018



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 2: Cumulative Apartment Units Built, Salt Lake County
(Excluding greater downtown area)



*The data to measure impacts of apartments constructed in 2019 was unavailable at the time of this study.

Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

completed (see Figure 2). Another 1,887 units were delivered to the market in 2019 but are not included in this analysis as the data to measure their impacts are not yet available. By 2018, the county’s Southwest area accounted for 32.2% of total apartment units built since 2010, followed by the Early Suburbs area, accounting for 26.9%. The West area held 21.5% of new units built since 2010, and the Southeast area had the lowest share with 17.1% of units.

Table 2: Annual Apartment Units Built by Geographic Area
(Excluding greater downtown area)

Area	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019*
Salt Lake County	1,008	693	292	647	794	1,250	1,027	1,038	1,005	1,887
Early Suburbs	256	100	40	307	211	210	288	378	293	300
Southeast	0	0	0	228	42	416	181	330	211	239
Southwest	496	315	252	0	258	334	270	330	238	1,048
West	256	278	0	112	283	290	288	0	263	300

*The data to measure impacts of apartments constructed in 2019 was unavailable at the time of this study.

Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Table 3: Single-Family Characteristics by Geographic Area and Distance to New Apartments

Area	Distance to Apartment	# of Single-Family Homes	Median Bldg. Sq. Ft.	Median Age	Median Parcel Size (Acres)
Salt Lake County	+1/2 mi.	129,564	2,403	41	0.21
	≤1/2 mi.	27,829	2,134	48	0.19
Early Suburbs	+1/2 mi.	30,063	2,464	63	0.21
	≤1/2 mi.	11,383	1,824	77	0.16
Southeast	+1/2 mi.	28,378	2,866	41	0.23
	≤1/2 mi.	7,293	2,428	41	0.21
Southwest	+1/2 mi.	29,471	2,980	23	0.24
	≤1/2 mi.	5,005	2,892	19	0.22
West	+1/2 mi.	41,652	1,930	42	0.18
	≤1/2 mi.	4,148	1,788	61	0.18

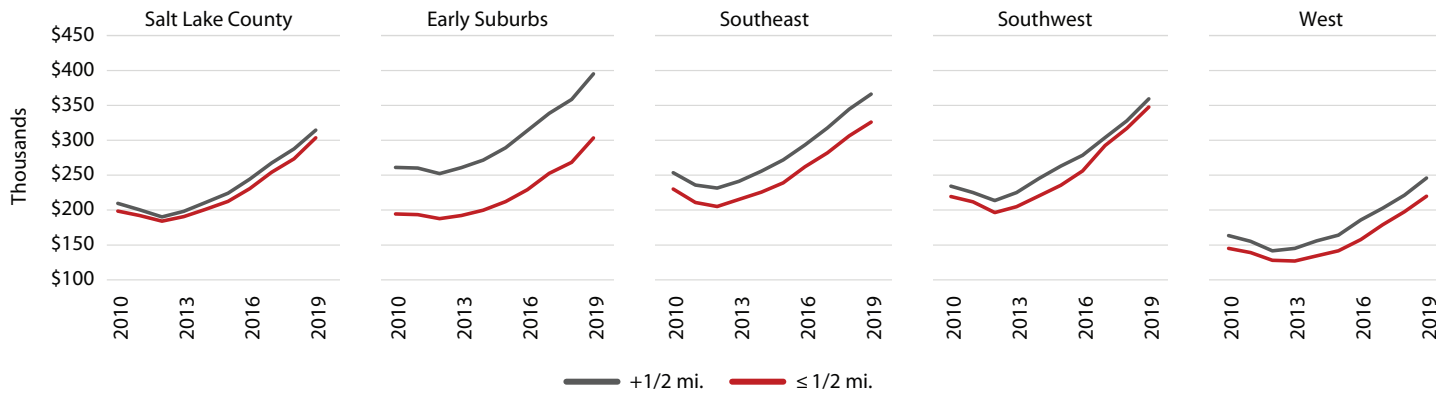
Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

In suburban Salt Lake County, 1,887 new apartment units completed construction and began leasing in 2019, a single-year record surpassing the 1,250 new units constructed in 2015 (see Table 2). In the Early Suburbs area, 2017 was a record year with 378 new units constructed. The Southeast area set its record in 2015, with 416 new units. The Southwest area holds the record for any single year, adding 1,048 new apartment units in 2019. The West area also reached its record in 2019 for single-year construction with the delivery of 300 units.

Key physical characteristics distinguish single-family units based on their proximity to new apartment construction and impact their value (see Table 3). The size of a home is a major factor driving market value. In suburban Salt Lake County overall, homes located within 1/2 mile of new apartments are approximately 270 sq. ft., or 11.1%, smaller than those farther away. The size difference is even greater for those homes located in the Early Suburbs area; homes ≤1/2 mile of new apartments are 640 sq. ft., or 26.0%, smaller than those that aren’t. Homes located in the Southeast area are 438 sq. ft. smaller or 15.3%, while those located in the Southwest area are nearly identical, with a size difference of only 88 sq. ft., or 3.0%. The difference in size for homes in the West area is 142 sq. ft., or 7.4%.

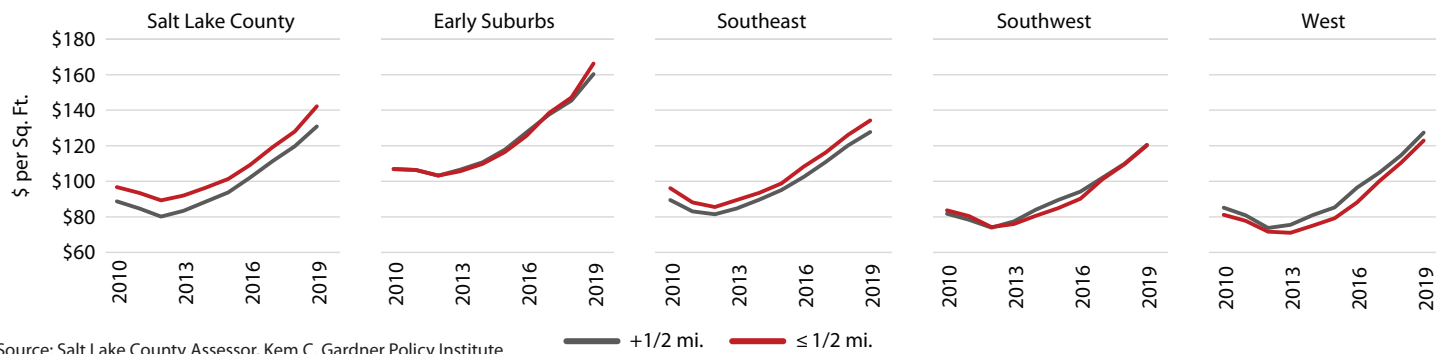
Home age is another factor influencing value, although remodeling and updates often negate this effect. Homes in suburban Salt Lake County that are located ≤1/2 mile of new apartments are seven years older on average than those located

Figure 3: Median Market Value of Single-Family Homes by Distance to Nearest Apartment



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 4: Median Market Value per Square Foot of Single-Family Homes by Distance to Nearest Apartment



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

farther away. Homes located $\leq 1/2$ mile in the Early Suburbs area are 14 years older than those that aren't. Southeast area homes are the same age, while those in the Southwest area that are located $\leq 1/2$ mile of new apartments are four years newer than those located farther. Homes in the West area average 19 years older, the largest age difference between homes that are $\leq 1/2$ mile of new apartments and those that are farther away.

Lot size is another key category that influences overall value. In suburban Salt Lake County, lot sizes average 0.02 acre smaller for homes located $\leq 1/2$ mile of new apartments. For homes located in the Early Suburbs area, lots are 0.05 acre smaller for homes $\leq 1/2$ mile from new apartments. Home lots in the Southeast, Southwest, and West areas are 0.02 acre smaller for those located $\leq 1/2$ mile of apartments.

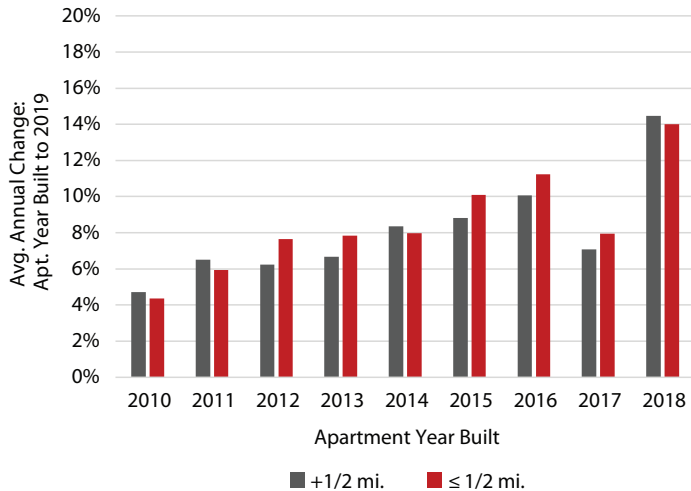
Results

The median market value of single-family homes is greater for those that are located more than 1/2 mile away from new apartments. Between 2010 and 2019, those that are farther than 1/2 mile averaged a 4.7% higher median value (see Figure 3). Homes located in the Early Suburbs area have the greatest discrepancies in values when compared by distance, with the difference averaging 34.6%. This is due to the fact that some of the most expensive and largest homes are located in the areas of Sugar House and Holladay. The average difference in value for homes located in the Southeast area over the last decade is 12.3%. Homes in the Southwest area show the median value

disparity lessening with time. Between 2010 and 2016 the difference by distance was 9.1%; however, the disparity narrowed to 3.5% between 2016 and 2019. This was driven by a 10.4% increase in median building square feet for homes within 1/2 mile of an apartment, leading to an overall increase in home values. The median value for homes in the West area has averaged 13.6% between 2010 and 2019.

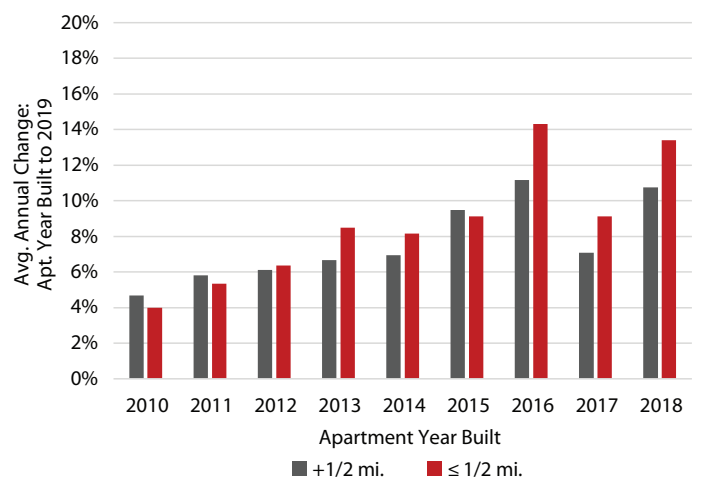
While the total median market value is greater for those single-family homes farther than 1/2 mile from new apartment construction, the opposite is true when measuring the median value per square foot (PSF). Between 2010 and 2019, homes

Figure 5: Average Annual Change in Median Price, Year of Apartment Built to 2019, Salt Lake County



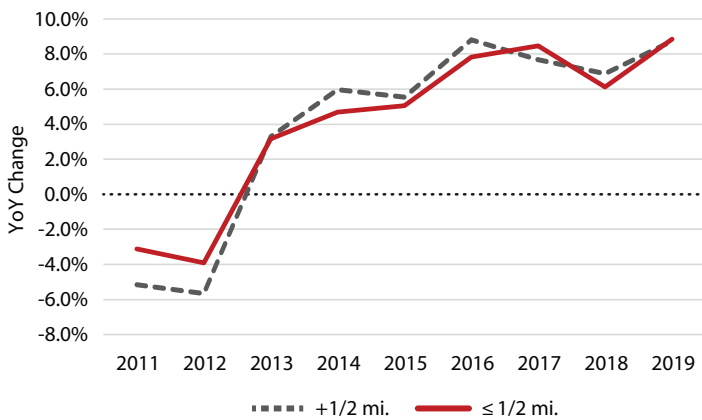
Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 7: Average Annual Change in Median Price, Year of Apartment Built to 2019, Early Suburbs



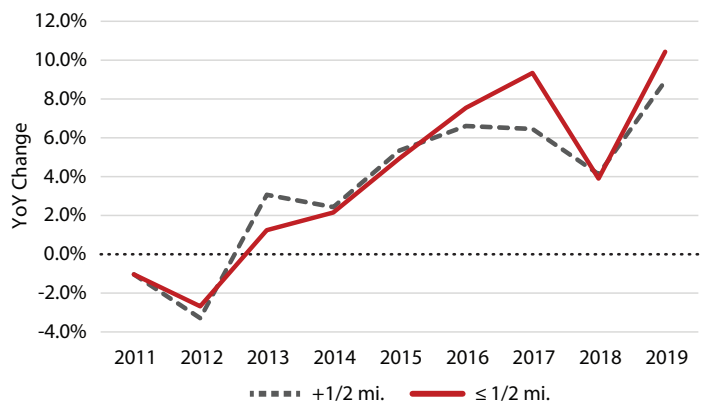
Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 6: Year-Over Change of Median Market Value, Salt Lake County



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 8: Year-Over Change of Median Market Value, Early Suburbs



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

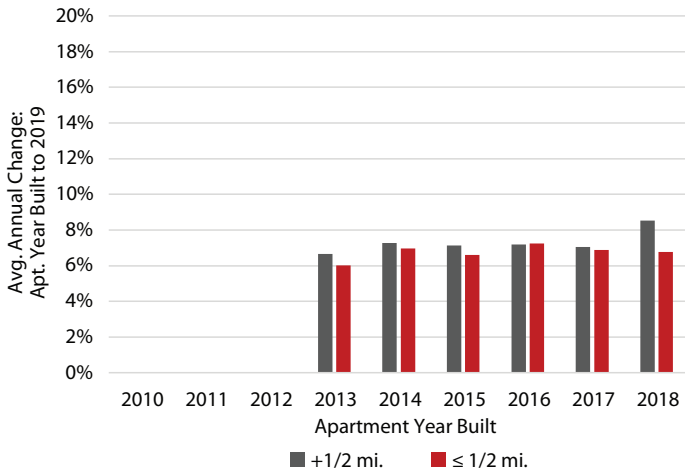
that are located ≤1/2 mile averaged an 8.8% higher PSF median value compared with those farther away (see Figure 4). Although the Early Suburbs area shows the highest discrepancy in total median market value in Figure 3, comparing values on a PSF basis shows there to be little to no difference between the two distances. PSF home values in the Southeast area averaged 5.3% higher for homes located ≤1/2 mile over the last decade. Similar to the trend seen in total median values, the PSF discrepancies in the Southwest favored homes that were farther away between 2013 and 2016, but shows no substantial difference since. The West area shows homes located ≤1/2 mile of a new apartment averaged 5.2% less in median value PSF over the decade when compared with homes farther away. The reason for this disparity is likely due to the homes' age. Homes located ≤1/2 mile of new apartments in the West area average 19 years older than those farther away.

The following sections present a summary of each individual study area's findings, starting with a summary for Salt Lake County.

Figures 5, 7, 9, 11, and 13 measure the average annual rate of value change from the year the nearest apartment was constructed to 2019. This measure is used to understand the overall impact new apartments have on existing single-family homes. Figures 6, 8, 10, 12, and 14 show year-over percent change of median market value to measure annual fluctuations.

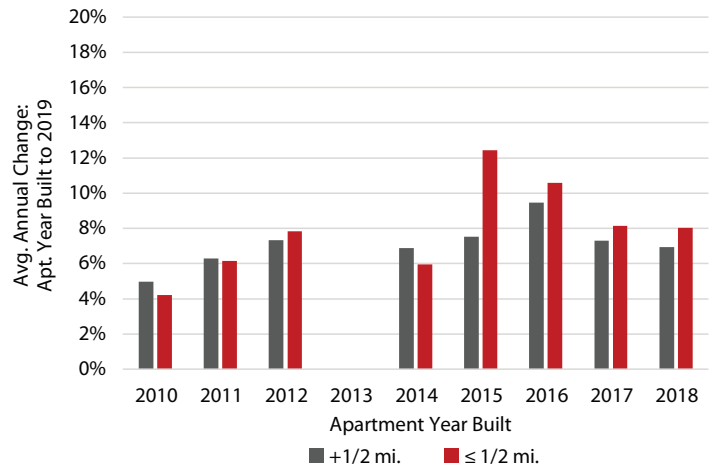
In suburban Salt Lake County, from the year of construction to 2019, single-family homes located ≤1/2 mile of a new apartment experienced a 10.0% average annual increase in value, while the value of homes farther away increased 8.6% on average annually (see Figure 5). Homes that were located more than 1/2 mile in 2010 and 2011 experienced a 1.9-percentage-point larger decline in their value than those that were closer to

Figure 9: Average Annual Change in Median Price, Year of Apartment Built to 2019, Southeast



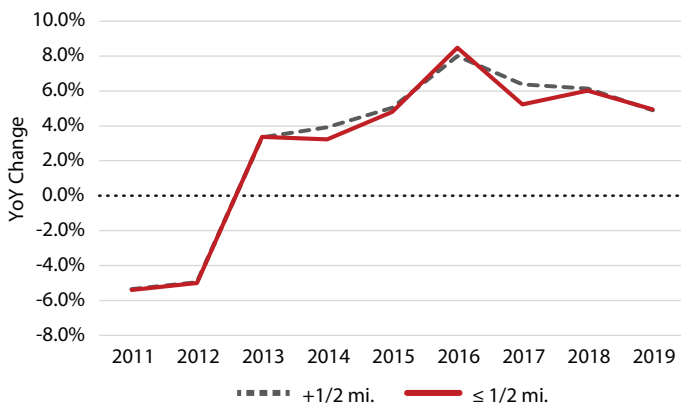
Note: There was no new apartment construction between 2010 and 2012.
 Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 11: Average Annual Change in Median Price, Year of Apartment Built to 2019, Southwest



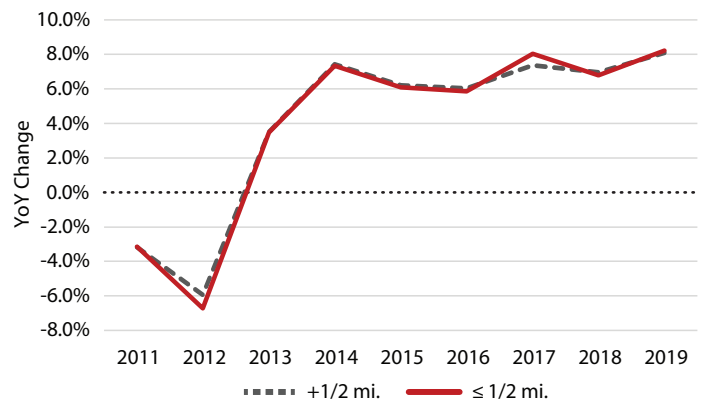
Note: There was no apartment construction in 2013.
 Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 10: Year-Over Change of Median Market Value, Southeast



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 12: Year-Over Change of Median Market Value, Southwest



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

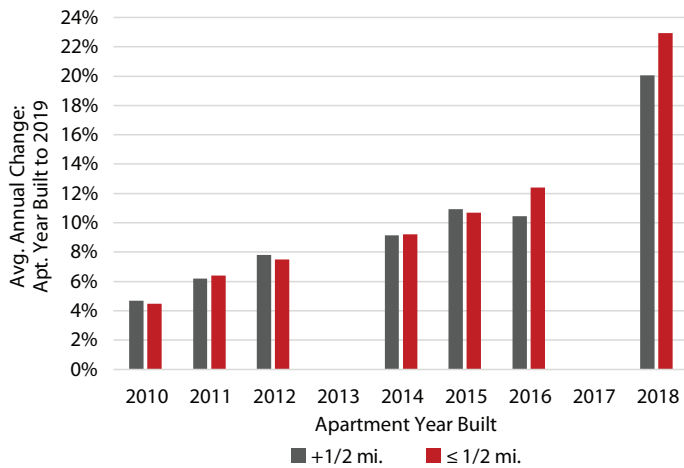
a new apartment building, showing that apartment proximity had a positive impact overall on preserving value during the recession (see Figure 6).

From the year of construction to 2019, homes in the Early Suburbs area that are located ≤1/2 mile of a new apartment experienced a 10.7% average annual increase in value, while the value for homes farther away increased 7.6% annually on average (see Figure 7). Year-over changes have shown some disparities over the last decade. Homes farther than 1/2 mile saw a more positive appreciation from 2012 to 2015, while homes located ≤1/2 mile outperformed those farther away between 2016 and 2019 (see Figure 8).

The Southeast area is the only instance where homes that are more than 1/2 mile away from new apartment construction experienced higher average price appreciation than those located ≤1/2 mile (see Figure 9). Homes farther away

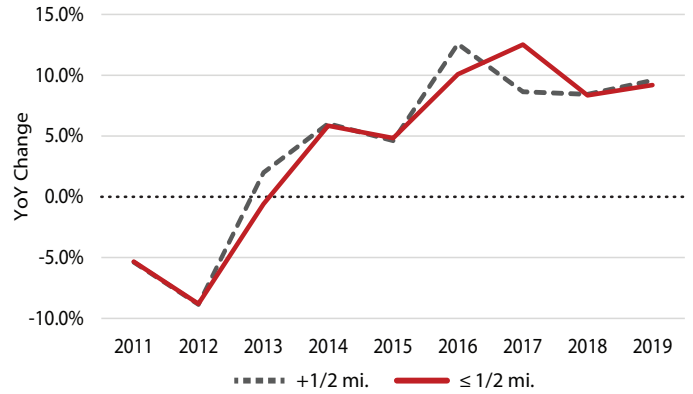
experienced an annual appreciation of 7.3% between year the apartment was constructed to 2019, and those located ≤1/2 mile saw their values increase 6.8% annually. The likely explanation for this discrepancy is that there is a higher concentration of larger retail development near those homes that are located ≤1/2 mile of apartments than in any other study areas. In the other three study areas, homes located ≤1/2 mile of an apartment were near an average of 20% less retail space when compared with homes farther away. In the Southeast area, there is 84% more retail space near homes that are closer to new apartment construction compared with those farther away. Year-over annual trends stayed similar for both distance categories with the exception of 2014 and 2017, when homes farther than 1/2 mile experienced slightly greater annual growth (see Figure 10).

Figure 13: Average Annual Change in Median Price, Year of Apartment Built to 2019, West



Note: There was no new apartment construction in 2013 and 2017.
 Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

Figure 14: Year-Over Change of Median Market Value, West



Source: Salt Lake County Assessor, Kem C. Gardner Policy Institute

In the Southwest area, from the year of construction to 2019, single-family homes located ≤1/2 mile of a new apartment experienced a 9.7% average annual increase in value, while the value for homes farther away increased 7.7% on average annually (see Figure 11). Median value year-over trends in the Southwest area show little or no difference between apartment proximities (see Figure 12).

Homes in the West area that are located ≤1/2 mile of a new apartment experienced a 13.7% average annual increase in value, while the value for homes farther away increased 10.5%

annually on average (see Figure 13). Year-over trends show some fluctuation through the last decade. Homes farther than 1/2 mile outperformed annual price growth in 2013, 2016, and 2019, while homes located ≤1/2 mile outperformed in 2017, with the remaining years showing relatively similar year-over price shifts (see Figure 14).

Conclusion

The public perception about high-density housing continues to be a point of conflict in growing communities across Utah and the country. While many stereotypes and generalizations about negative impacts are brought up in public settings, high density development does not actually appear to depress home values.¹¹ From the year an apartment was constructed to 2019, in Salt Lake County, single-family homes that were located within 1/2 mile of new apartment construction realized 1.4% more in annual price appreciation than those single-family homes that were located farther away. This is likely because new apartment construction brings new demand and new dollars to a community and redevelops an older piece of property, thus bringing more vibrancy and “buzz” to the area.

The challenges of housing affordability are not going away anytime soon. While density is a solution to alleviate costs, zoning is the mechanism that allows or denies it. Zoning regulations, more than any other local policies, govern the annual supply of single-family and multifamily housing. In recent years, the supply of housing has not met the demand, creating a housing shortage.¹² This shortage has tremendous impacts on Utah’s future. The shortage has also excluded many from homeownership, added to substantial increases in doubling-up of households, delayed marriages, and discouraged young people from forming new households.

Endnotes

1. National Association of Realtors
2. <https://gardner.utah.edu/wp-content/uploads/May2018HousingReport.pdf>
3. Haughey, R. "Higher Density Development Myths and Facts." Urban Land Institute - ULI. Washington, D.C. 2005
4. Haughey, R. "Higher Density Development Myths and Facts." Urban Land Institute - ULI. Washington, D.C. 2005
5. University of Washington. "Denser development is good for single-family home values." ScienceDaily. www.sciencedaily.com/releases/2012/06/120626151109.htm
6. National Association of Home Builders, "Market Outlook: Confronting the Myths about Apartments with Facts" (Washington, D.C.: Author, 2001), p. 4.
7. Alexander Hoffman, *The Vitality of America's Working Communities* (Cambridge, Massachusetts: Harvard University Joint Center for Housing Studies, 2003).
8. Arthur C. Nelson and Mitch Moody, "Price Effects of Apartments on Nearby Single-Family Detached Residential Homes," Working Draft (Blacksburg, Virginia: Virginia Tech University, 2003).
9. Arthur C. Nelson, "Top Ten State and Local Strategies to Increase Affordable Housing Supply," *Housing Facts & Findings*, vol. 5, no. 1.
10. Robert Dunphy and Kimberly Fisher, "Transportation, Congestion, and Density: New Insights," *Transportation Research Record*, 1996.
11. https://furmancenter.org/files/media/Dont_Put_It_Here.pdf
12. <https://gardner.utah.edu/wp-content/uploads/Best-Practices-Dec2020.pdf>



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CALL TO ORDER

Chairman Poss called the regular meeting of the Lenexa Planning Commission to order at 7:16 p.m. on Monday, January 8, 2024. The meeting was held in the Community Forum at Lenexa City Hall at 17101 W. 87th Street Parkway, Lenexa, Kansas.

ROLL CALL

COMMISSIONERS PRESENT

Chairman Chris Poss
Vice-Chairman Mike Burson
Commissioner Ben Harber
Commissioner Brenda Macke
Commissioner David Woolf
Commissioner John Handley
Commissioner Cara Wagner
Commissioner Curt Katterhenry

COMMISSIONERS ABSENT

Commissioner Don Horine

STAFF PRESENT

Scott McCullough, Director of Community Development
Stephanie Kisler, Planning Manager
Tim Collins, Engineering Construction Services Administrator
Andrew Diekemper, Assistant Chief – Fire Prevention
Steven Shrout, Assistant City Attorney II
Kim Portillo, Planner III
Dave Dalecky, Planner II
Logan Strasburger, Planner I
Will Sharp, Planning Intern
Gloria Lambert, Senior Administrative Assistant

APPROVAL OF MINUTES

The minutes of the December 4, 2023 meeting were presented for approval. Chairman Poss entertained a motion to **APPROVE** the minutes. Moved by Commissioner Katterhenry seconded by Commissioner Handley and **APPROVED** by a unanimous voice vote.

CONSENT AGENDA

1. **Brentwood East VI - Consideration of a final plat for a lot line adjustment for properties located at 14602 West 91st Terrace and 14606 West 91st Terrace within the RP-1, Planned Residential (Low-Density) District. PT24-02F**
2. **Vista Village, 3rd Plat - Consideration of a final plat to combine Lot 7 and Lot 8 of Vista Village, 1st Plat into a single lot to accommodate development of a restaurant with accessory microbrewery use on property located near the southeast corner of Prairie Star Parkway & Ridgeview Road within the PUD, Planned Unit Development District. PT24-01F**
3. **First Watch - Consideration of a revised final plan for exterior changes for a restaurant use on property located at 12242 West 95th Street within the CP-3, Planned Regional Commercial District. PL23-06F**
4. **Sar-Ko-Par Aquatic Center Sign - Consideration of a sign deviation to allow a monument sign to exceed the maximum allowable sign height for Sar-Ko-Par Aquatic Center located at 8801 Greenway Lane in the R-1, Residential Single-Family District. DV24-01**

Chairman Poss entertained a motion to **APPROVE** Consent Agenda Items 1-4. Moved by Commissioner Burson seconded by Commissioner Harber and carried by a unanimous voice vote.

REGULAR AGENDA

5. **Santa Fe Commerce Center - Consideration of rezoning and preliminary plan/plat for an industrial development on property located at 15620 Santa Fe Trail Drive. RZ24-01, PL24-01P**
 - a. **Consideration of a rezoning from the AG, Agricultural District to the BP-2, Planned Manufacturing District. RZ24-01**
 - b. **Consideration of a preliminary plan/plat for an industrial development. PL24-01P**

APPLICANT PRESENTATION

Dan Finn, Phelps Engineering, showed an aerial and gave the location of the 19-acre site that is currently zoned AG, Agricultural District but is requesting to rezone to BP-2, Planned Manufacturing District. Mr. Finn explained the property consisted of two new industrial buildings. He gave the location of auto parking for both buildings and explained where the truck stalls and docks would also be located. He said there will be an outdoor storage yard on the northwest corner of the development that will be screen with an 8-foot-tall vinyl fence, per the City's request. There will be four points of access to the site including new entrances and turn lanes. The stormwater detention tracks will be located at the north end of the site. The detention tracks will handle both stormwater and water detention for the development as well as the existing Fry Wagner development to the west. The stormwater plans will meet all BMP requirements set by the City of Lenexa. He said they are asking to vacate the public right-of-way at Lakeview Avenue and maintenance responsibility will be taken over by the private property owner. He noted that the owner and developer of the property is also owner of the western lot, currently occupied by Fry Wagner. Mr. Finn discussed the landscape plan and noted that ten feet of the property line will not be disturbed to ensure

that the existing tree line remains. He added, there will be additional berming installed to help screen the truck docks. A screen wall will be installed on the southern end of the docks to help with additional screening. He pointed out all the building materials that would be used and displayed architecture elevations that included store front entry areas on the office side and of the dock area. He said the applicant is requesting a deviation on the outdoor storage area and the location of the fence. Due to the uniqueness of the site, the curvature of the road and length of building they are requesting a 50-foot yard setback for the outdoor storage area. He discussed the three conditions from the Staff Report that included the public sidewalk along Santa Fe Trail Drive, the internal sidewalk connection linking the two buildings and the masonry columns into the vinyl fence to screen the outdoor storage area. He stated that with the future City trail they are providing a 10-foot trail easement along the entire frontage of the property. He said they are requesting that in lieu of the sidewalk being installed at this time, to instead provide private sidewalks into the right-of-way for future trail connections. He said they are also asking that internal sidewalks between buildings be removed from Staff's conditions as it is the expectation that each building will have a separate tenant. They also request that the condition for masonry columns on the northern side of the fence be removed, but agreed to install the columns along the Santa Fe Drive side of the fence.

STAFF PRESENTATION

Kim Portillo presented the Staff Report. Ms. Portillo noted the preliminary plan was also serving as the preliminary plat. She gave the site location and showed an aerial map of the property and stated that it borders the City of Olathe to the south. She presented the Future Land Use and Zoning Maps and said the applicant is requesting to zone the AG, Agricultural portion of the property to the BP-2, Planned Manufacturing District. She explained that the site plan consists of two industrial speculative buildings to be located on approximately 18-acres with an outdoor storage area and a stormwater tract in the northern corner. She talked about the proposed building materials and showed architectural drawings provided by the applicant. She spoke about the applicant's proposed landscape plan noting that it did not require any deviations; however, Staff and the applicant worked together to further improve the landscaping. She said the applicant is requesting a deviation related to the fence setback for the outdoor storage area and Staff is supportive of the 50-foot setback request as they have provided additional landscaping beyond requirements. It is Staff's recommendation that the applicant add masonry columns along the north and east fence lines. She gave an example, noting that a similar requirement was applied on Santa Fe Tow's prior application, which included a condition to add masonry columns to the outside of their fence. She talked about the recommended conditions relating to sidewalks. The first condition is a requirement that the applicant provide a 5-foot public sidewalk along Santa Fe Trail Drive with the current development. The applicant mentioned wanting to wait until the City builds the planned trail along Santa Fe Trail Drive, but there is no timeline of how long it would be before the trail would be built. Staff recommends the Planning Commission enforce the requirement for the public sidewalk. Staff is also requesting the required internal sidewalk connections from front entrance to front entrance of the individual industrial buildings. She presented a graphic of what the connection could potentially look like but stated that Staff is willing to work with the applicant in considering a different layout or different location for the internal sidewalks. Staff recommends the applicant install sidewalk connections to the public network and from building to building.

PUBLIC HEARING

Chairman Poss **OPENED** the Public Hearing and asked if anyone wished to speak on this item. No one from the audience came forward.

Chairman Poss entertained a motion to **CLOSE** the Public Hearing. Moved by Commissioner Katterhenry, seconded by Commissioner Woolf, and carried by a unanimous voice vote.

PLANNING COMMISSION DISCUSSION

Commissioner Katterhenry said he did not have a problem with the fence being beyond the face of the building as long as it meets the 50-foot setback. He agrees with Staff concerning the installation of internal sidewalks and the masonry on the fence.

Commissioner Woolf said that he is less concerned with internal sidewalks as long as there is a connector to the public sidewalks. He also feels the masonry should be installed wherever visible.

Commissioner Handley stated that the public sidewalks should be installed and would leave it up to Staff to decide where the internal sidewalks should be installed. He suggests requiring masonry columns on the first half of the third of the northern property line or to consider the first half of the southeast to northwest property line visual.

Chairman Poss asked if the fence would be installed on the property line. Ms. Portillo replied that it would be installed at a 10-foot setback from the north property line, inside the applicant's property. Chairman Poss asked if the trees are located on or inside the property line. Dan Finn replied that the precise tree location will be determined at final plan stage. Chairman Poss said it may not be necessary to install masonry columns along the fencing on the entire north side. Ms. Portillo said the Commissioners could recommend a condition for a greater expanse between masonry columns along the fence line. Chairman Poss said he agrees with the spacing that Staff recommended.

Chairman Poss asked that if the left-hand turn lane is deferred and who would install it in the future. Tim Collins said it is unknown at this time.

MOTION

Chairman Poss entertained a motion to recommend **APPROVAL** of rezoning property from AG to BP-2 for RZ24-01 – **Santa Fe Commerce Center** at 15620 Santa Fe Trail Drive and 15504 Santa Fe Trail Drive, for an industrial development.

Moved by Commissioner Handley, seconded by Commissioner Burson, and carried by a unanimous voice vote.

Chairman Poss entertained a motion to recommend **APPROVAL** of the preliminary plan/plat for PL24-01P – **Santa Fe Commerce Center** at 15620 Santa Fe Trail Drive and surrounding parcels, for an industrial development, with the following conditions:

1. A five-foot wide public sidewalk along Santa Fe Trail Drive shall be provided on the final plan and shall be constructed by the applicant in conjunction with the first building.
2. Internal sidewalk connections linking the two buildings within the development shall be provided on the final plan. The public sidewalk network can serve as this connection.
3. The final plan shall incorporate masonry columns into the white vinyl screening fence on the north and south fence lines at intervals of one column per three fence panels if 8-foot panels are used, or similar spacing, with a minimum of two columns worth of return on the north and south fence lines.

Moved by Commissioner Harber, seconded by Commissioner Woolf, and carried by a unanimous voice vote.

STAFF REPORT

Staff had nothing additional to report to the Commission.

ADJOURNMENT

Chairman Poss ended the regular meeting of the Lenexa Planning Commission at 7:45 p.m. on Monday, January 8, 2024.

DRAFT