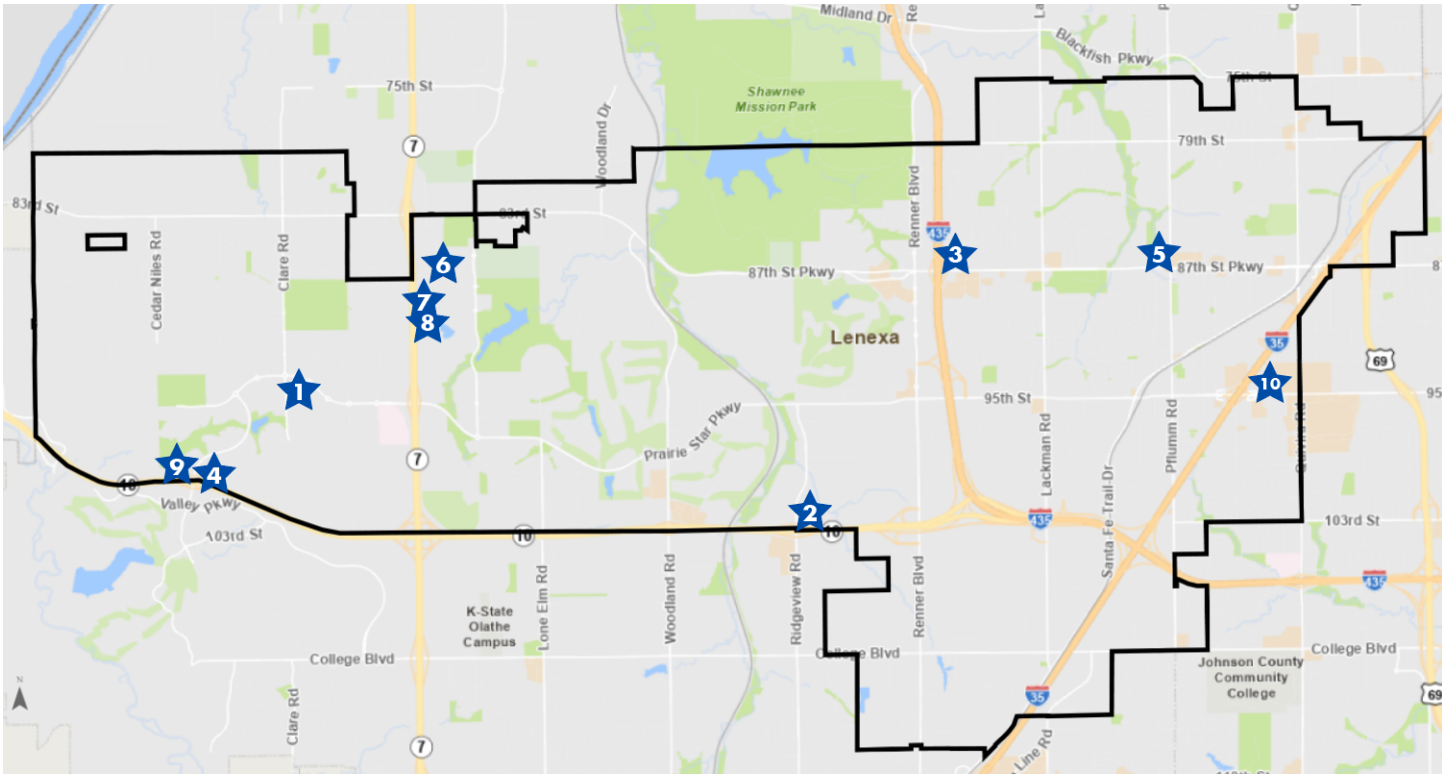


**AGENDA MAP**



**CALL TO ORDER**

**ROLL CALL**

**APPROVE MINUTES FROM THE NOVEMBER 6, 2023 MEETING**

**CONSENT AGENDA**

*All matters listed within the consent agenda have been distributed to each member of the Planning Commission for review, are considered to be routine, and will be enacted by one motion with no separate discussion. If a member of the Planning Commission or audience desires separate discussion on an item, that item may be removed from the consent agenda and placed on the regular agenda.*

- 1. St. James Academy Sports Field (Stadium) Improvements - Consideration of a revised final plan for construction of concession stands, restrooms, a press box, bleachers, and additional parking for property located at 24505 Prairie Star Parkway within the R-1, Planned Residential (Low Density) District. PL23-07FR**

2. **Towneplace Hotel at Vista Ridge - Consideration of a final plan for a new four-story hotel located near the northeast corner of K-10 Highway & Ridgeview Road within the CP-3, Planned Regional Commercial District. PL23-23F**
3. **Arise Sales Center - Consideration of a final plan for a new office building located near the northwest corner of W 87th Street Parkway & Maurer Road within the CP-2, Planned Community Commercial District. PL23-24F**

## REGULAR AGENDA

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4. **Cedar Canyon West Commercial - Consideration of a preliminary plan/plat for a multibuilding commercial development located near the southeast corner of future 100th Street & Canyon Creek Boulevard within the CP-2, Planned Community Commercial District. PL23-08P**
5. **Express Oil Change & Tire Engineers - Consideration of a preliminary plan for a new auto service center located at 8610 Pflumm Road within the CP-2, Planned Community Commercial District. PL23-10P**
6. **The Residences on Woodsonia - Consideration of rezoning and a preliminary plan/plat for a multifamily residential development on property located near future 86th Street between Woodsonia Drive & Monticello Road. PL23-11P, RZ23-08 (Public Hearing)**
  - a. **Consideration of a rezoning from the AG, Agricultural District to the RP-2, Planned (Intermediate-Density) District and the RP-4, Planned Residential (High Density) District. RZ23-08**
  - b. **Consideration of a preliminary plan for a multifamily residential development. PL23-11P**

## CONTINUED APPLICATIONS (NO DISCUSSION)

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7. **Copper Creek North - Consideration of a concept plan for a multifamily development located near the northwest corner of West 89th Street & Woodsonia Drive within the RP-3, Planned Residential (Medium-High) Density, RP-4, Planned Residential (High Density), and RP-5, Planned Residential (High Rise, High Density) Districts. PL23-04CP**
8. **Copper Creek South - Consideration of a concept plan for a multifamily development located near the southwest corner of West 89th Street & Woodsonia Drive within the RP-3, Planned Residential (Medium-High) Density, RP-4, Planned Residential (High Density), and RP-5, Planned Residential (High Rise, High Density) Districts. PL23-05CP**

9. **Canyon Ridge Apartment Homes - Consideration for a mixed-use development including multifamily residential, assisted living, and retail uses on property located near the northwest corner of K-10 Highway & Canyon Creek Boulevard. RZ23-07, PL23-12P**
  - a. **Consideration of a rezoning from the AG, Agricultural District to the RP-1, Planned Residential (Low Density), RP-3, Planned Residential (Medium-High Density), and CP-2, Planned Community Commercial Districts. RZ23-07**
  - b. **Consideration of a preliminary plan for a mixed-use development including multifamily residential, assisted living, and retail uses. PL23-12P**
  
10. **First Watch - Consideration of a revised final plan for exterior changes for a restaurant located at 12242 West 95th Street within the CP-3, Planned Regional Commercial District. PL23-06FR**

## STAFF REPORTS

## ADJOURN

## APPENDIX

### 11. **Draft Minutes - November 6, 2023**

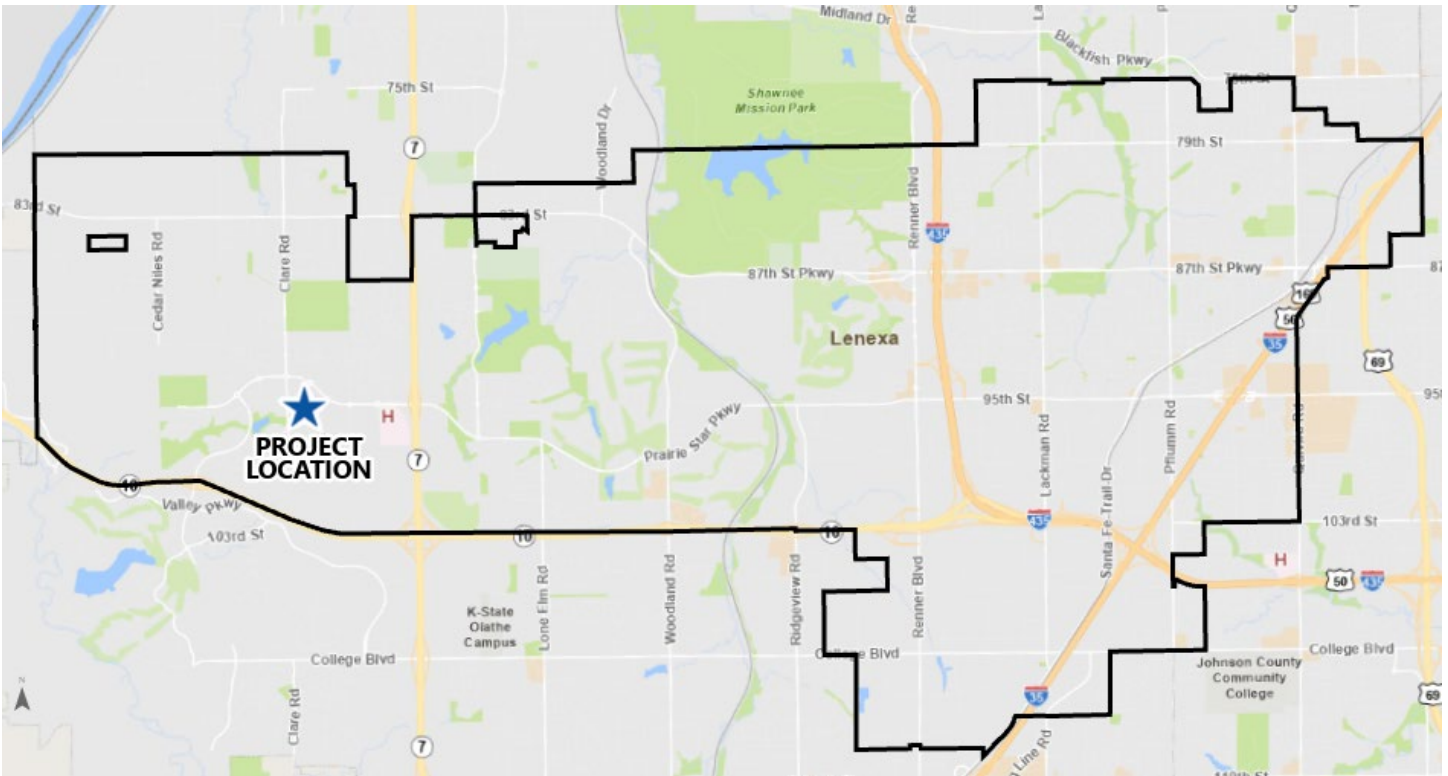
*If you have any questions about this agenda, please contact Stephanie Kisler, Planning Manager, at [skisler@lenexa.com](mailto:skisler@lenexa.com).*

*If you need any accommodations for the meeting, please contact the City ADA Coordinator at 913-477-7550 at least 48 hours prior to the meeting.  
Kansas Relay Service: 800-766-3777*

*Assistive Listening Devices are available for use in the Community Forum by request.*

# ST. JAMES ACADEMY STADIUM IMPROVEMENTS

<b>Project #:</b>	PL23-07FR	<b>Location:</b>	24505 Prairie Star Parkway
<b>Applicant:</b>	Andy Tylicki, St. James Academy	<b>Project Type:</b>	Revised Final Plan
<b>Staff Planner:</b>	Dave Dalecky and Will Sharp	<b>Proposed Use:</b>	High School Stadium Improvements



## PROJECT SUMMARY

The applicant proposes to construct sports field (stadium) improvements and additional parking lots at the St. James Academy campus located at 24505 Prairie Star Parkway. The stadium improvements will include concession stands, restrooms, a press box, bleachers, and two new parking lots. The new parking lots will provide 160 additional parking spaces adjacent to the stadium, which will result in 851 parking spaces overall within the campus. The applicant requests approval of a final plan for the development. The proposed final plan is consistent with the approved preliminary plan (PL09-01PR), which was approved by the Governing Body on May 19, 2009. This project does not require a Public Hearing.

## STAFF RECOMMENDATION: APPROVAL

## SITE INFORMATION

The St. James Academy campus is a 44-acre site located at the southeast corner of Prairie Star Parkway and Clare Road. The site has access from Clare Road and from Prairie Star Parkway. The initial preliminary plan for the campus was approved in 2003 and construction began that year with the main high school building. Incremental improvements have been made to the campus over time.

Previous approvals:

- SU03-02: Permanent Special Use Permit to allow a school in the R-1 Zoning District (ORD #4532)
- PL03-01: Preliminary Plan for the campus
- PL09-01PR: Revised Preliminary Plan for the campus
- PL23-02FR: Final Plan for a multipurpose building (under construction)

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
45.9	NA	R-1	Public/Open Space



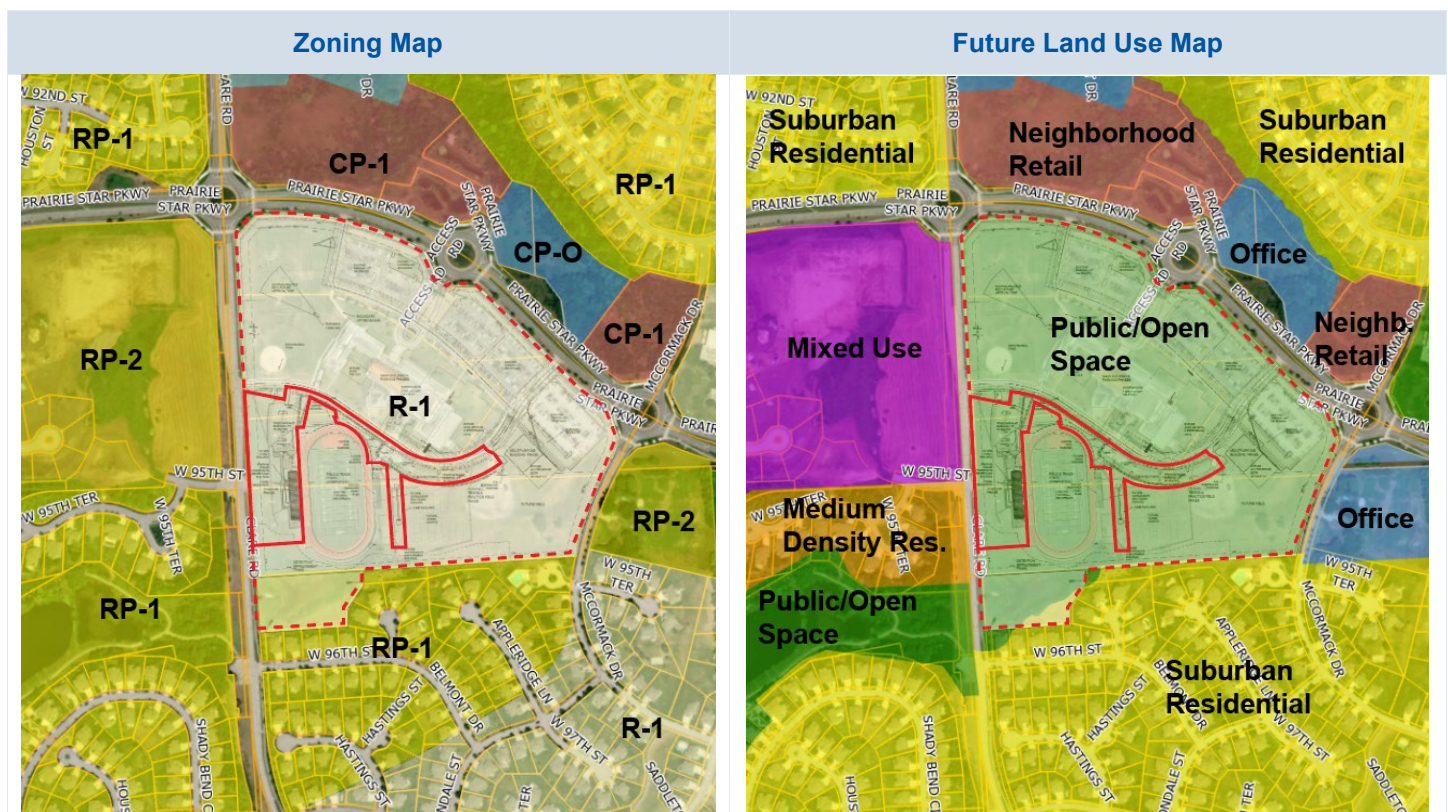
**Exhibit 1: Aerial image of subject site. Campus outlined with a dashed line and current phase is outlined with a solid line.**

## LAND USE REVIEW

St. James Academy is a private Catholic high school. The campus is in the R-1 Zoning District. A public or private school is permitted in the R-1 Zoning District by special use permit (SUP). A permanent SUP for the campus was approved in 2003 (SU03-02).

The proposed stadium improvements include an accessory structure for the St. James Academy high school campus. The accessory structure is a single feature and will include concession stands, restrooms, press box, and bleachers. The stadium will be used for regularly scheduled sporting events throughout the school year.

The proposed use is compatible with the existing land uses and development on adjacent sites.

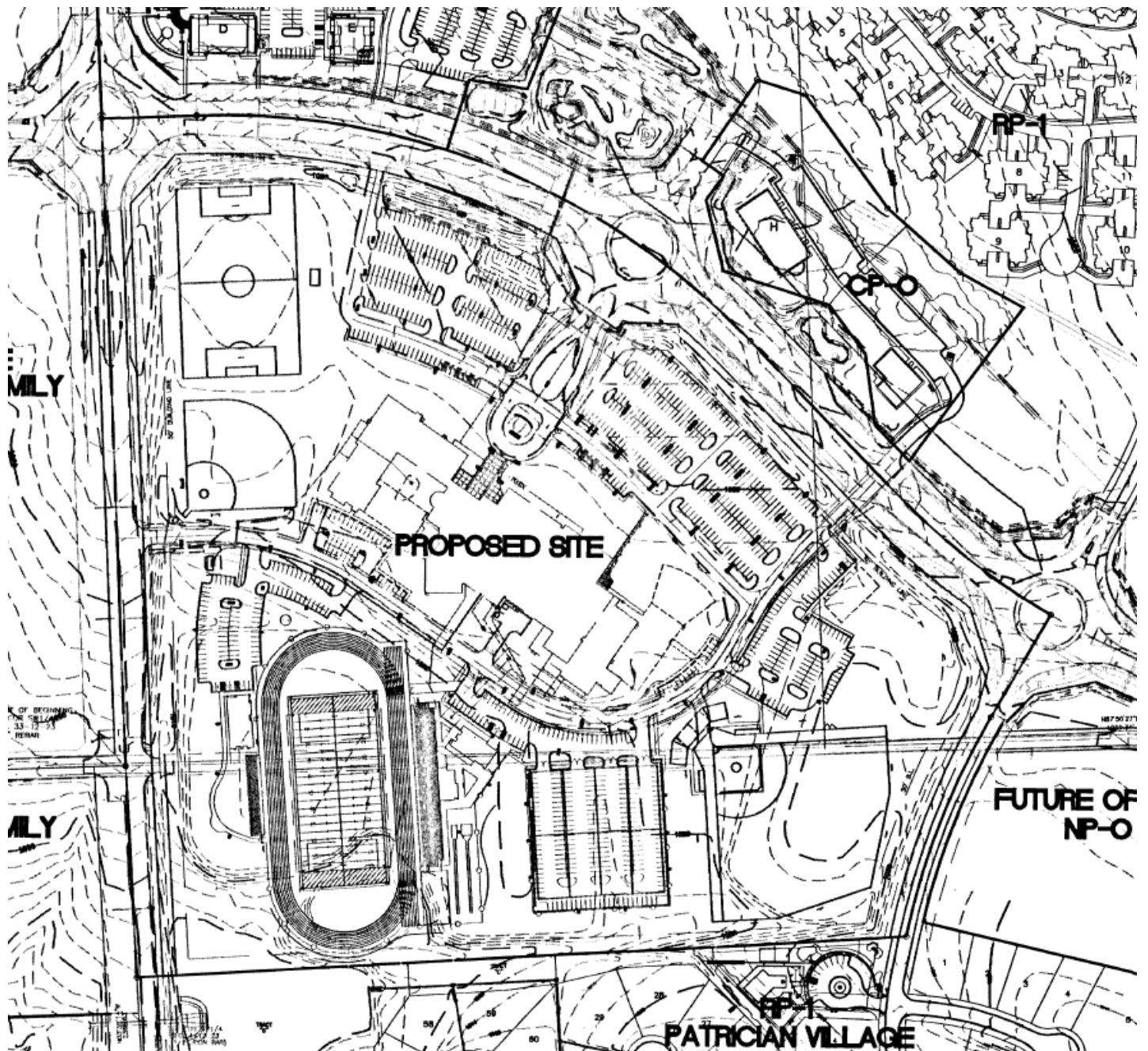


**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Public/Open Space	R-1	School
<b>North</b>	Neighborhood Retail and Office	CP-1	Undeveloped
<b>South</b>	Suburban Residential	RP-1	Single-Family Residential
<b>East</b>	Office	RP-2	Undeveloped
<b>West</b>	Med. Density Residential Mixed Use	RP-2	Single-Family Residential

## FINAL PLAN REVIEW

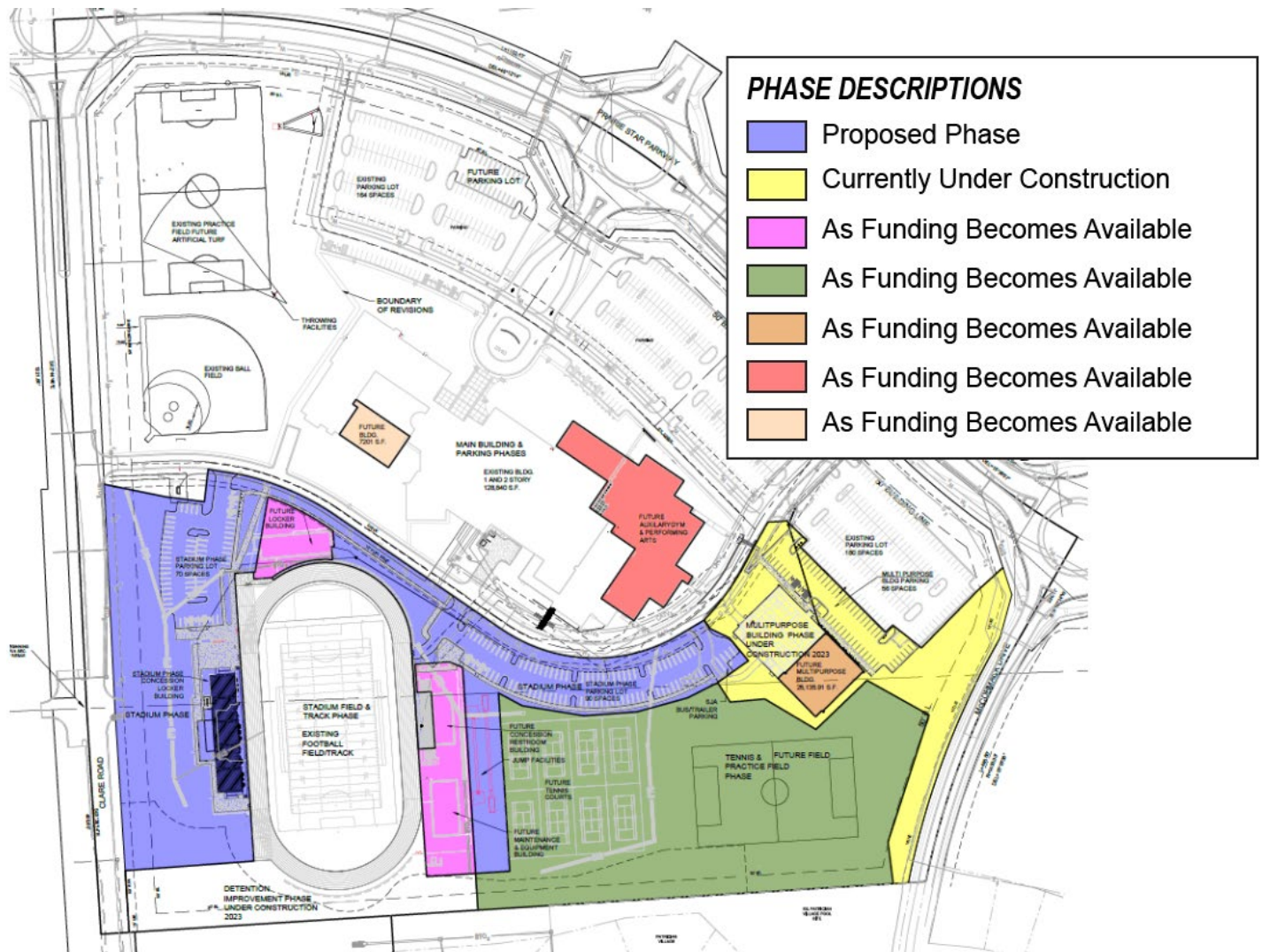
The applicant is requesting final plan approval for improvements to the existing field and track. The improvements are part of the incremental completion of the St. James Academy stadium. This phase will include new parking areas totaling 160 additional parking spaces to the campus. The campus includes a main high school building, parking areas, track and field stadium, additional sports fields, and a multipurpose building on the northeastern corner of the site. The proposed stadium improvements are consistent with the approved preliminary plan for the campus (PL09-01PR).



*Exhibit 2: Previously approved preliminary plan (PL09-01PR).*

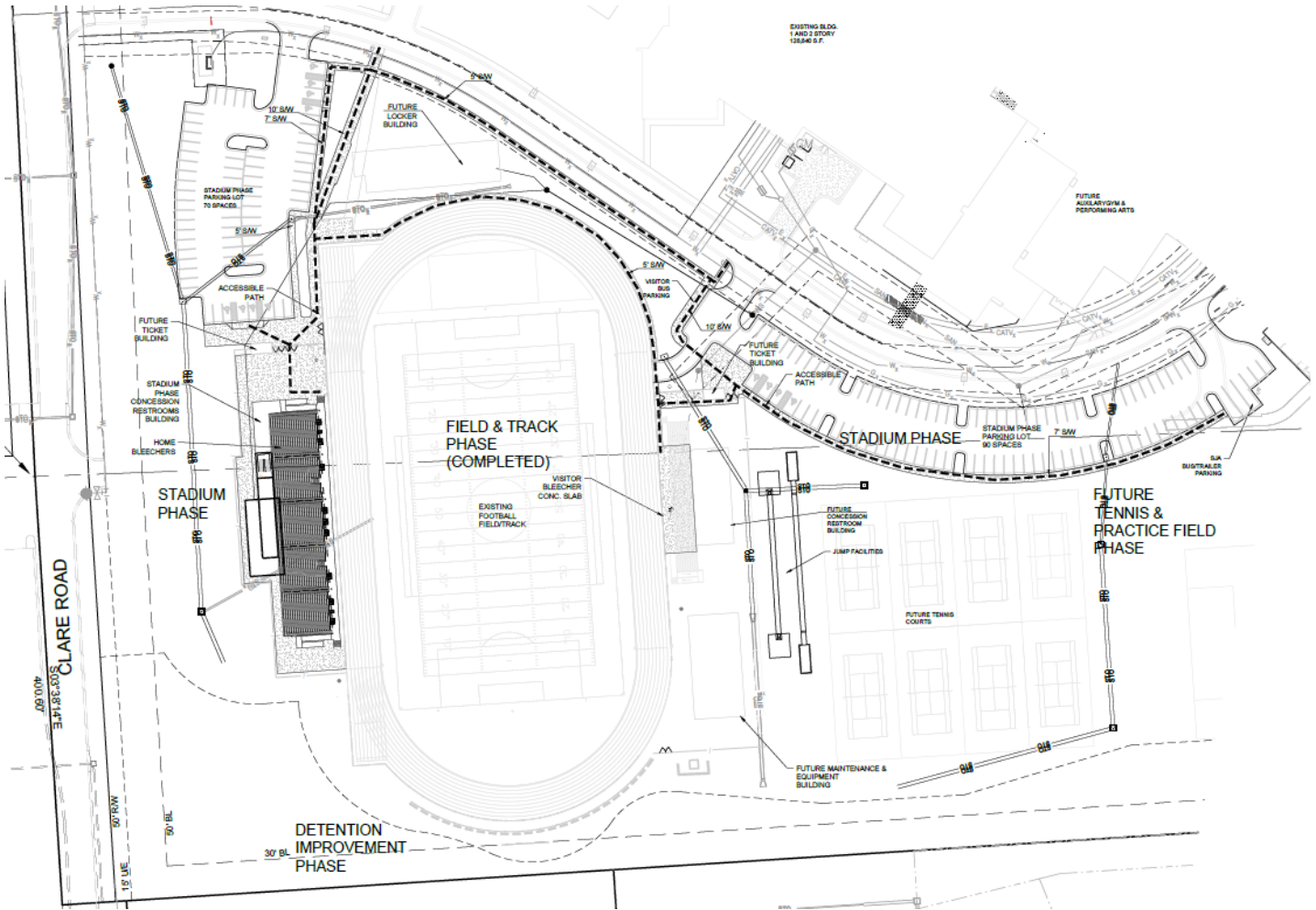
The proposed improvements with this phase include a 41-foot-tall bleacher structure with seating for 2,500 spectators. The two-story home bleachers also include a press box, concession stands, and restrooms.

Future improvements are planned for the site as shown on Exhibit 3.



**Exhibit 3: Current proposal (in purple) includes additional stadium parking, bleachers, and a press box.**





**Exhibit 4: Proposed site plan.**

**ACCESS, TRAFFIC, AND PARKING**

- Prairie Star Parkway and Clare Road provide access to the site. These are classified as Major Arterial roadways.
- Accessible sidewalks will be constructed that will connect the new parking lots to the stadium. Another accessible sidewalk will run along the internal access road and connect the two new parking lots.
- As the campus develops, any parking issues will be coordinated between City Staff and the school's administration.
- The site exceeds minimum parking requirements.

TABLE 2: PARKING ANALYSIS				
Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
High school	0.50 spaces for every student plus 1 space for every employee	576	851	+ 275

## STORMWATER

Required improvements to the wetland located at the southwestern corner of the property are currently underway. Additional components will be included and are planned for future phases.

## FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

## LIGHTING

This phase does not show additional site lighting. If new lighting is to be installed, a photometric plan is required and is subject to the City’s lighting requirements per Section 4-1-C-4-I of the UDC.

## LANDSCAPING

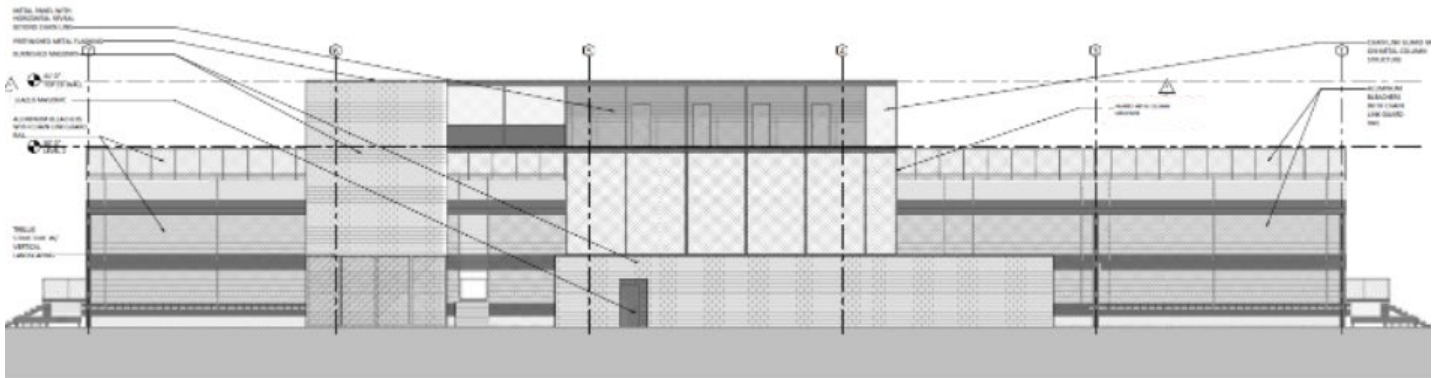
This phase of the campus’s development includes landscaping installed along Clare Road to the west of the stadium. Landscaping will also be installed in the islands of the two new parking lots, and along the interior access road.



**Exhibit 5: Landscape plan.**

**ARCHITECTURE**

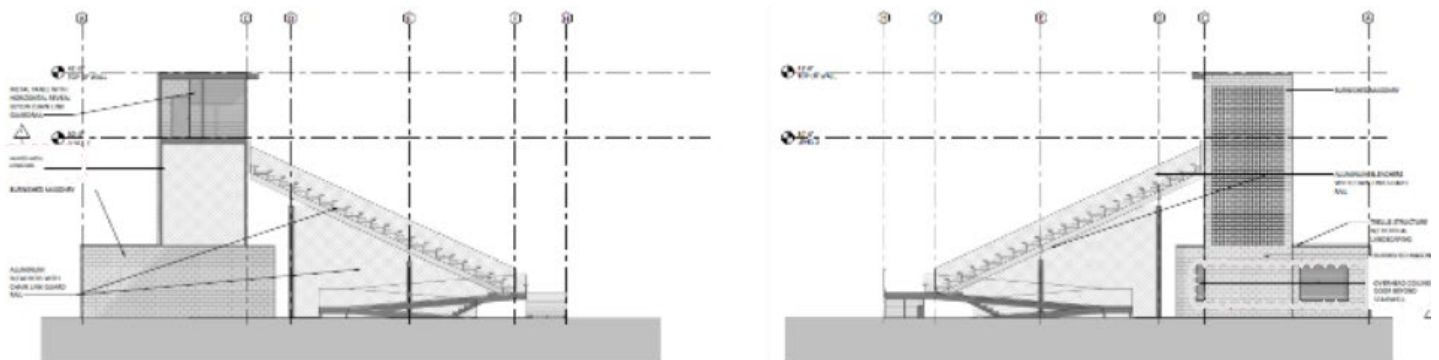
The new bleachers, concession stand, restrooms, and press box structure are 41 feet tall. The uppermost level is clad with metal panels, burnished masonry, and prefinished metal flashing. The press box has aluminum windows. The bleachers are made of aluminum with a chain-link guard rail. Other architectural features include a trellis structure and glazed masonry in the school’s colors. The landscape plan shows vines installed to grow on the trellis.



**Exhibit 6: Architectural plans, east elevation.**



**Exhibit 7: Architectural plans, west elevation.**



**Exhibit 7: Architectural plans, north (left) and south (right) elevations.**

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## DEVIATIONS

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The applicant is not requesting any deviations.

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## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

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## RECOMMENDATION FROM PROFESSIONAL STAFF

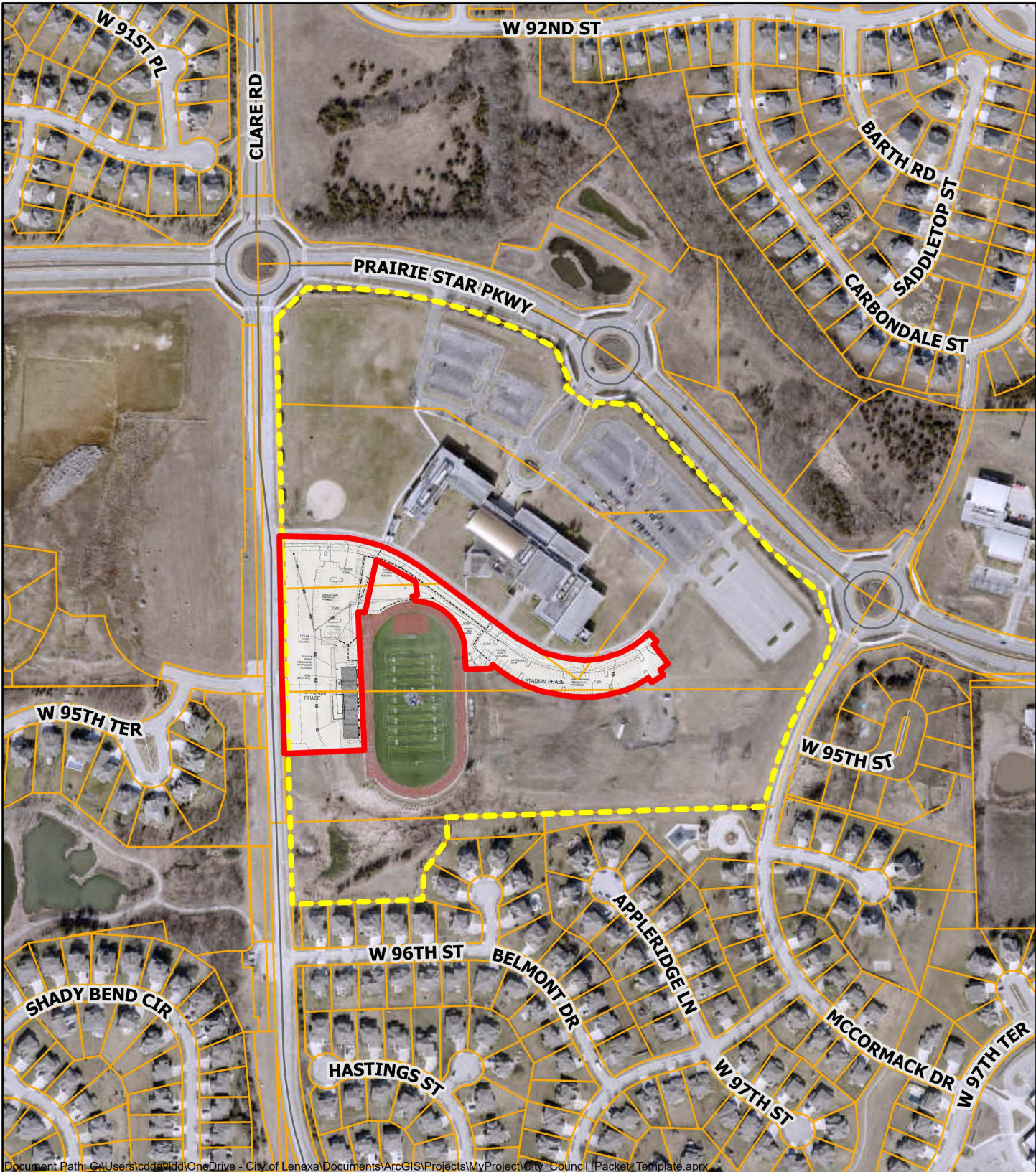
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★ **Staff recommends approval of the proposed Revised Final Plan for St. James Academy Stadium Improvements.**

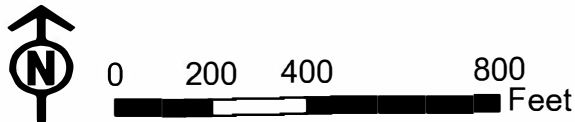
- The applicant proposes stadium improvements and additional parking at the St. James Academy high school campus. The stadium improvements include concession stands, restrooms, a press box, bleachers, and two new parking lots. The new parking lots will provide 160 additional parking spaces adjacent to the stadium.
- The project is consistent with Lenexa's goals through *Strategic Community Investment* to create *Vibrant Neighborhoods* and *Healthy People*.

## FINAL PLAN

Staff recommends **approval** of the revised final plan for PL23-07FR – **St. James Academy Stadium Improvements** at 24505 Prairie Star Parkway, for improvements to the private school's campus.



# St. James Academy Stadium Improvements PL23-07FR



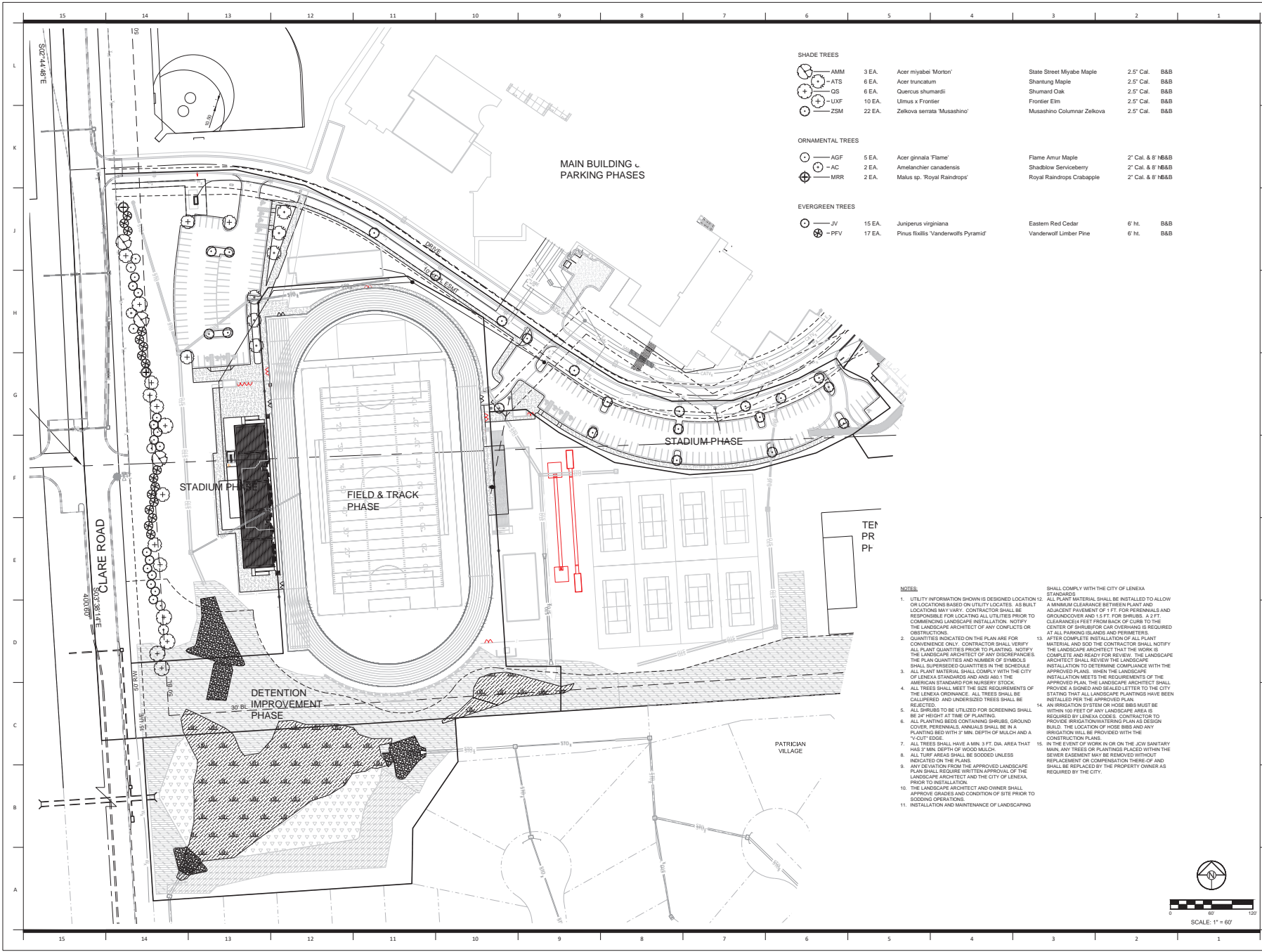












SHADE TREES			
AMM	3 EA.	Acer myriabil 'Morlan'	State Street Myrtle Maple 2.5' Cal. B&B
ATS	8 EA.	Acer truncatum	Shantung Maple 2.5' Cal. B&B
QS	6 EA.	Quercus shumardi	Shumard Oak 2.5' Cal. B&B
UXF	10 EA.	Ulmus x Frontier	Frontier Elm 2.5' Cal. B&B
ZSM	22 EA.	Zelkova serrata 'Musashino'	Musashino Columnar Zelkova 2.5' Cal. B&B

ORNAMENTAL TREES			
AGF	5 EA.	Acer ginnala 'Flame'	Flame Amur Maple 2' Cal. & 8' HB&B
AC	2 EA.	Amelanchier canadensis	Shadblow Serviceberry 2' Cal. & 8' HB&B
MRR	2 EA.	Malus sp. 'Royal Raindrops'	Royal Raindrops Crabapple 2' Cal. & 8' HB&B

EVERGREEN TREES			
JV	15 EA.	Juniperus virginiana	Eastern Red Cedar 6' Ht. B&B
PFV	17 EA.	Pinus filixilis 'Vanderwolfs Pyramid'	Vanderwolf Limber Pine 6' Ht. B&B

### St. James Academy - Stadium Additions

24505 Prairie Star Parkway  
Lenexa, KS 66227

**Project Number:** MultiStudio  
 St. James Academy High 4300 Pennsylvania  
 24505 Prairie Star Parkway Lenexa, KS 66227 913.254.4300 913.651.8555  
 www.jstholder.org multi.studio

**Civil engineer/landscape architect:** Schlagel  
 4820 W. 107th St Lenexa, KS 66115 913.492.8400  
 www.schlagelassociates.com www.bdc-enr.com

**mechanical/electrical engineer:** Smith & Bauer  
 25618 W 103rd St. Olathe, KS 66061 913.345.2327  
 www.smithbauer.com

Issue Date: October 30, 2023

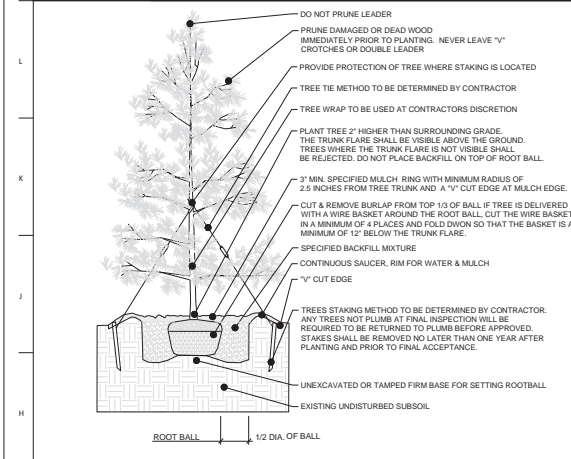
Revisions Number	Description	Date
1	CITY COMMENTS	11/28/2023

- NOTES:**
- LITILITY INFORMATION SHOWN IS DESIGNED LOCATION 12 OR LOCATIONS BASED ON UTILITY LOCATES. AS BUILT LOCATIONS MAY VARY. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO COMMENCING LANDSCAPE INSTALLATION. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS OR OBSTRUCTIONS.
  - QUANTITIES INDICATED ON THE PLAN ARE FOR CONVENIENCE ONLY. CONTRACTOR SHALL VERIFY ALL PLANT QUANTITIES PRIOR TO PLANTING. NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES. THE PLAN QUANTITIES AND NUMBER OF SYMBOLS SHALL SUPERSEDE QUANTITIES IN THE SCHEDULE.
  - ALL PLANT MATERIAL SHALL COMPLY WITH THE CITY OF LENEXA STANDARDS AND AGS AND THE AMERICAN STANDARD FOR NURSERY STOCK. ALL TREES SHALL MEET THE SIZE REQUIREMENTS OF THE LENEXA ORDINANCE. ALL TREES SHALL BE CALIPURED AND UNDRUGGED. TREES SHALL BE REJECTED.
  - ALL SHRUBS TO BE UTILIZED FOR SCREENING SHALL BE 34" HEIGHT AT TIME OF PLANTING.
  - ALL PLANTING BEES CONTAINING SHRUBS, GROUND COVER, PERENNIALS, ANNUALS SHALL BE IN A PLANTING BED WITH 1" MIN. DEPTH OF MULCH AND A "V-CUT" EDGE.
  - ALL TREES SHALL HAVE A MIN. 3 FT. DIA. AREA THAT HAS 3" MIN. DEPTH OF WOOD MULCH.
  - ALL TURF AREAS SHALL BE SOODED UNLESS INDICATED ON THE PLANS.
  - ANY DEVIATION FROM THE APPROVED LANDSCAPE PLAN SHALL REQUIRE WRITTEN APPROVAL OF THE LANDSCAPE ARCHITECT AND THE CITY OF LENEXA. PRIOR TO INSTALLATION.
  - THE LANDSCAPE ARCHITECT AND OWNER SHALL APPROVE GRADES AND CONDITION OF SITE PRIOR TO SOODING OPERATIONS.
  - INSTALLATION AND MAINTENANCE OF LANDSCAPING SHALL COMPLY WITH THE CITY OF LENEXA STANDARDS.
  - ALL PLANT MATERIAL SHALL BE INSTALLED TO ALLOW A MINIMUM CLEARANCE BETWEEN PLANT AND ADJACENT PAVEMENT OF 1 FT. FOR PERENNIALS AND GROUND COVER AND 1.5 FT. FOR SHRUBS. A 2 FT. CLEARANCE FEET FROM BACK OF CURB TO THE CENTER OF SHRUB FOR CAR OVERHANG IS REQUIRED AT ALL PARKING ISLANDS AND PERIMETERS.
  - AFTER COMPLETE INSTALLATION OF ALL PLANT MATERIAL AND SOOD THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT THAT THE WORK IS COMPLETE AND READY FOR REVIEW. THE LANDSCAPE ARCHITECT SHALL REVIEW THE LANDSCAPE INSTALLATION TO DETERMINE COMPLIANCE WITH THE APPROVED PLAN. WHEN THE LANDSCAPE INSTALLATION MEETS THE REQUIREMENTS OF THE APPROVED PLAN, THE LANDSCAPE ARCHITECT SHALL PROVIDE A SIGNED AND SEALED LETTER TO THE CITY STATING THAT ALL LANDSCAPE PLANTINGS HAVE BEEN INSTALLED PER THE APPROVED PLAN.
  - AN IRRIGATION SYSTEM OR HOSE BEES MUST BE WITHIN 100 FEET OF ANY LANDSCAPE AREA IS REQUIRED BY LENEXA CODES. CONTRACTOR TO PROVIDE BRIGAD CONCRETE AS DESIGN. BUILD. THE LOCATION OF HOSE BEES AND ANY IRRIGATION SHALL BE PROVIDED WITH THE CONSTRUCTION PLANS.
  - IN THE EVENT OF WORK IN OR ON THE JOV SANITARY MAIN, ANY TREES OR PLANTINGS PLACED WITHIN THE SEWER EASEMENT MAY BE REMOVED WITHOUT REPLACEMENT OR COMPENSATION THERE OF AND SHALL BE REPLACED BY THE PROPERTY OWNER AS REQUIRED BY THE CITY.

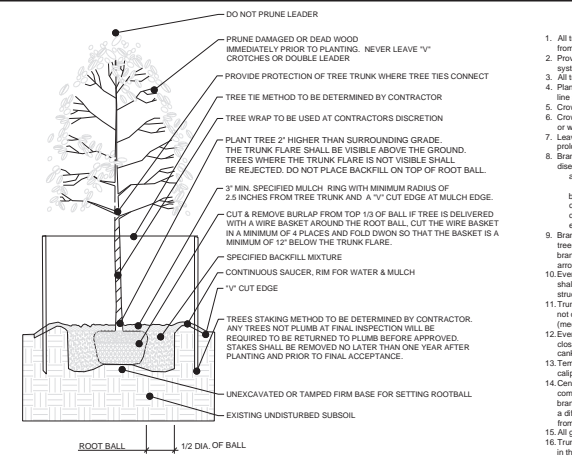
UNLESS A PROFESSIONAL SEAL WITH SIGNATURE AND DATE IS AFFIXED, THIS DOCUMENT IS PRELIMINARY AND NOT EXTENDED FOR CONSTRUCTION, RECORDING PURPOSES OR ANY OTHER PURPOSE.

**Stadium Phase Landscape Plan**  
**L1.1**  
 FDP Submittal

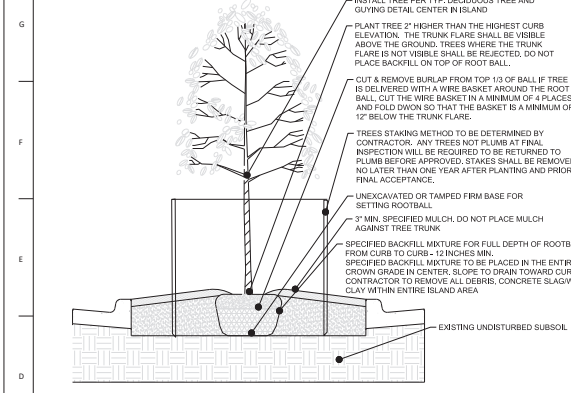




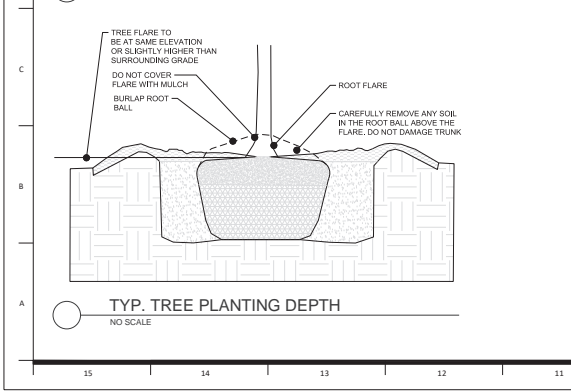
**TYP. EVERGREEN PLANTING & GUYING**  
NO SCALE



**TYP. DECIDUOUS PLANTING & GUYING**  
NO SCALE



**TYP. PARKING LOT ISLAND PLANTING**  
NO SCALE



- All trees shall comply with State and Federal regulations. Trees should be obtained from local sources but must meet the quality guidelines herein. Trees transported from out of the region shall meet all State and Federal regulations and be certified to be disease and insect free.
- Provide healthy stock, grown in a nursery and reasonably free of die-back, disease, insects, eggs, borers, and larvae. At the time of planting all plants shall have a root system, stem, and crown from that will restrict normal growth, establish and health for the expected life of the plant.
- All trees shall be nursery-grown.
- Plants shall be healthy with the color, shape, size and distribution of trunk, stems, branches, buds and leaves normal to the plant type specified. Tree quality above the soil line shall comply with the following:
  - Crown: The form and density of the crown shall be typical for a young specimen of the species or cultivar pruned to a central and dominant leader.
  - Crown specifications do not apply to plants that have been specifically trained in the nursery as topiary, espalier, multi-stem, clump, or unique selections such as contorted or weeping cultivars.
- Leaves: The size, color, and appearance of leaves shall be typical for the time of year and stage of growth of the species or cultivar. Trees shall not show signs of prolonged moisture stress or over watering as indicated by wilted, shriveled, or dead leaves.
- Branches: Shoot growth (length and diameter) throughout the crown should be appropriate for the age and size of the species or cultivar. Trees shall not have dead, diseased, broken, distorted, or otherwise injured branches.
  - Main branches shall be distributed along the central leader not clustered together. Potential main branches shall be evenly spaced and have appropriate space between them. They shall form a balanced crown appropriate for the cultivar/species.
  - Branch diameter shall not be larger than two-thirds (one-half is preferred) the diameter of the central leader measured 1 inch above the branch union.
  - The attachment of the largest branches (scaffold branches) shall be free of included bark.
  - Branches shall be distributed radially around and vertically along the trunk, forming a generally symmetrical crown typical for the species.
  - The attachment of scaffold branches shall be free of included bark.
- Branch structure: The better quality, large-maturing shade trees (lower extreme left) have all branches less than about two-thirds the trunk diameter. Poor quality shade trees (lower left center) have larger upright branches. Trees such as orange myrtle and other small-maturing trees can have several trunks. Trees with extensive defects in branches such as cracks and included bark (lower right) represent lesser quality than trees free of these potential problems. Included bark can be seen between the two growths below. Branches with bark inclusions are weakly attached to the tree and can split easy.
- Evergreen branch structure: The branch pattern should be dense, symmetrical and the branch stems should be evenly spaced completely around the trunk. The branches shall extend to within 12 inches of the ground and be along the full length of the trunk. Trees which are not symmetrical or that have an "open area" will be rejected. For structural integrity on evergreen trees, all side branches should be less than half the diameter of the adjacent trunk (less than one-third is preferred).
- Trunk: The tree trunk shall be relatively straight, vertical, and free of wounds that penetrate to the wood (properly made pruning cuts, closed or not, are acceptable and are not considered wounds), sunburned areas, conks (fungal fruiting bodies), wood cracks, sap leakage, signs of boring insects, galls, cankers, girdling ties, or lesions (mechanical injury).
- Evergreen tree trunk: Evergreen trees shall have a single trunk that is straight, vertical, and free of wounds that penetrate to the wood (properly made pruning cuts, closed or not, are acceptable and are not considered wounds), sunburned areas, conks (fungal fruiting bodies), wood cracks, sap leakage, signs of boring insects, galls, cankers, girdling ties, or lesions (mechanical injury). Codominant trunks (trunks of similar size) will not be accepted.
- Temporary branches, unless otherwise specified, can be present along the lower trunk below the lowest main (scaffold) branch, particularly for trees less than 1 inch in caliper. These branches should be no greater than 3/8-inch diameter. Clear trunk should be no more than 40% of the total height of the tree.
- Central Leader: Trees shall have a single(ome), relatively straight central leader and tapered trunk, free of co-dominant stems and vigorous, upright branches that compete with the central leader. Preferably, the central leader should not have been headed. However, in cases where the original leader has been removed, an upright branch at least 1/2 (one-half) the diameter of the original leader just below the pruning point shall be present. All trees are assumed to have one central leader tree unless a different form is specified in the plant list or drawings. If the central leader is broken or damaged during delivery or installation the tree shall be rejected and removed from the site. If the central leader dies within the warranty period the tree shall be replaced at the end of the warranty period.
- All graft unions, where applicable, shall be completely closed without visible sign of graft rejection. All grafts shall be visible above the soil line.
- Trunk caliper and taper shall be sufficient so that the lower five feet of the trunk remains vertical without a stake. Auxiliary stake may be used to maintain a straight leader in the upper half of the tree.
- Plant quality shall be normal to the plant type specified. Root observations shall take place without impacting tree health. Root quality at or below the soil line shall comply with the project Root Acceptance details and the following:
  - The roots shall be reasonably free of scrapes, broken or split wood.
  - The root system shall be reasonably free of injury from biotic (e.g., insects and pathogens) and abiotic (e.g., herbicide toxicity and salt injury) agents. Wounds resulting from root pruning used to produce a high quality root system are not considered injuries.
  - A minimum of three structural roots reasonably distributed around the trunk (not clustered on one side) shall be found in each plant. Root distribution shall be uniform throughout the root ball, and growth shall be appropriate for the species.
  - Plants with structural roots on only one side of the trunk (1 root) shall be rejected.
  - The root collar shall be within the upper 1 inch of the substrate/soil. Two structural roots shall reach the side of the root ball near the top surface of the root ball. The grower may request a modification to this requirement for species with roots that rapidly abscise, provided that the grower removes all stem girdling roots above the structural roots across the top of the root ball. Any excess soil shall be removed from the root ball so that the root flare is visible as indicated in the "Planting Depth Detail". The root collar shall be visible above the mulch layer.
  - The root system shall be free of stem girdling roots over the root collar or kninked roots from nursery production practices.
  - Plant Grower Certification: The final plant grower shall be responsible to have determined that the plants have been root pruned at each step in the plant production process to remove stem girdling roots and kninked roots, or that the previous production system used practices that produce a root system throughout the root ball that meets these specifications. Regardless of the work of previous growers, the plant's root system shall be modified at the final production stage, if needed, to produce the required plant root quality. The final grower shall certify in writing that all plants are reasonably free of stem girdling and kninked roots as defined in this specification, and that the tree has been grown and harvested to produce a plant that meets these specifications.
  - At time of observations and delivery, the root ball shall be moist throughout. Roots shall not show signs of excess soil moisture conditions as indicated by stunted, discolored, distorted, or dead roots.

**St. James Academy - Stadium Additions**

24505 Prairie Star Parkway  
Lenexa, KS 66227

Project Number: MultiStudio  
St. James Academy High 4200 Pennsylvania  
24505 Prairie Star Parkway Lenexa, KS 66227  
913-254-3200 616-931-8555  
www.jstuhdun.org multi.studio

Civil engineer/Landscape: Scholaj 4920 W. 107th St Lenexa, KS 66215 913-485-2127  
www.schlagassociates.com www.bdc-emrg.com

mechanical/electrical: Smith & Bowler 25614 W. 103rd St. Olathe, KS 66061 913-345-4144  
www.smithandbowler.com

Issue Date: October 30, 2023

Revisions	NUMBER	DESCRIPTION	CITY COMMENTS	DATE
				10/26/2023

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**Stadium Phase Landscape Details**  
**L2.0**

FDP Submittal



**St. James Academy - Stadium Additions**

2455 Prairie Star Parkway  
Lenexa, KS 66227

Project Number: 0223-2123

Client: St. James Academy High  
32525 Prairie Star Parkway  
Lenexa, KS 66227  
913.241.4576  
www.stjacademy.org

Architect: multistudio  
4200 W. 124th St.  
Lenexa, KS 66224  
913.241.4576  
www.multistudio.com

Structural Engineer: Mark R. Casper  
4500 W. 124th St.  
Lenexa, KS 66224  
913.241.4576  
www.multistudio.com

MEP Engineer: Keith F. Heuler  
4500 W. 124th St.  
Lenexa, KS 66224  
913.241.4576  
www.multistudio.com

Issue Date: October 30, 2023

Revisions

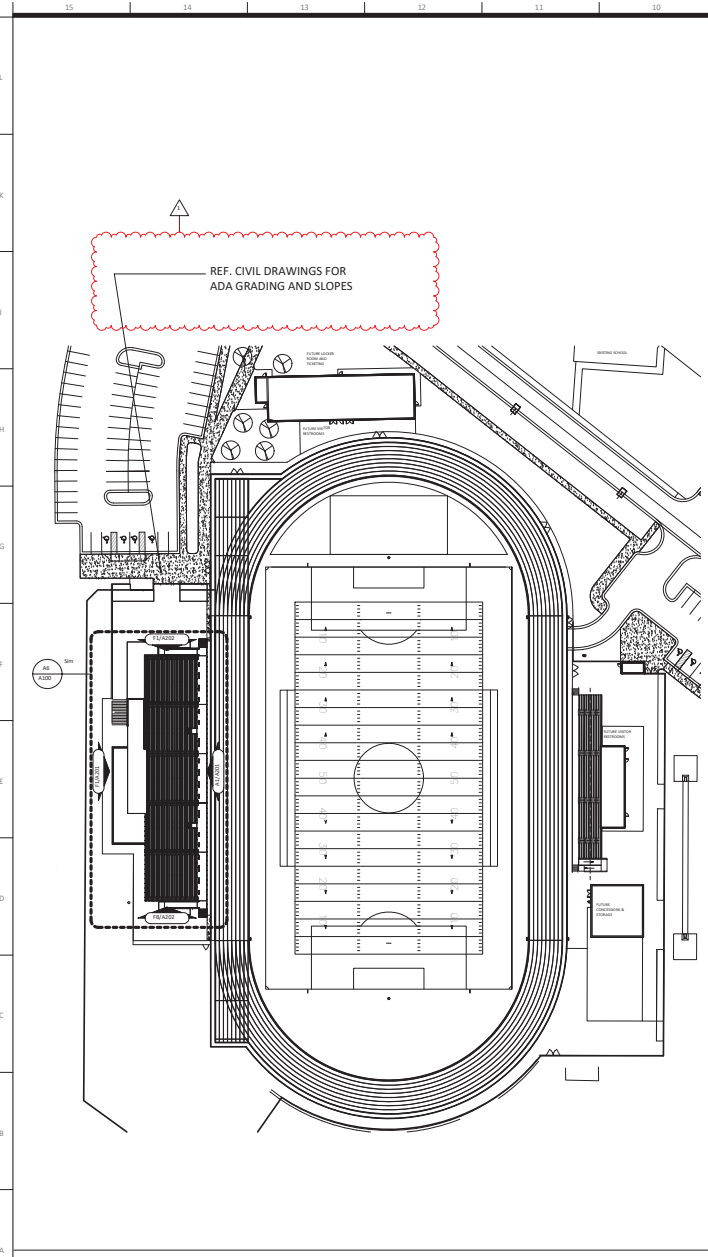
NUMBER	DESCRIPTION	DATE
1	ISSUE FOR PERMIT	11/02/2023

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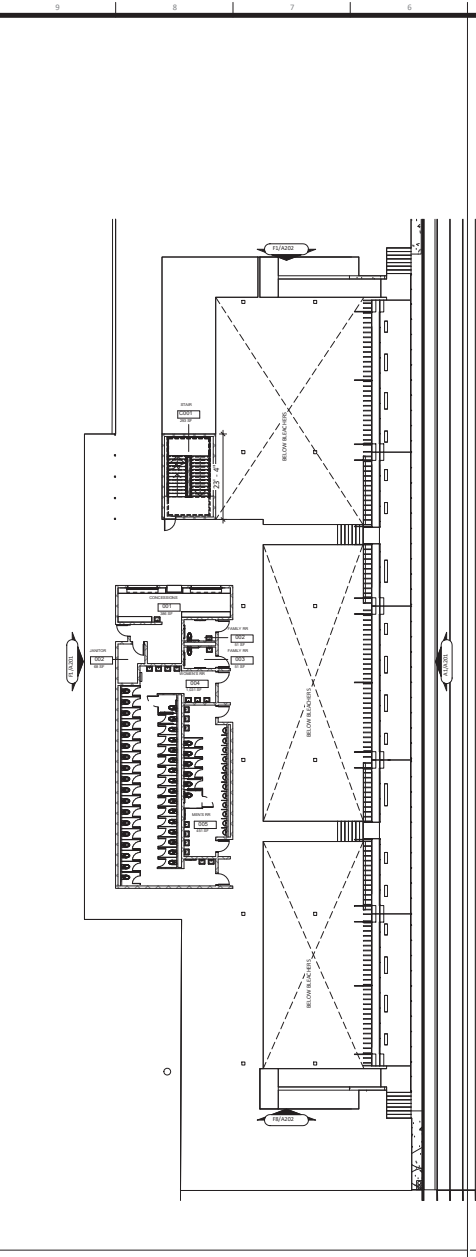


**SITE PLAN  
A100**

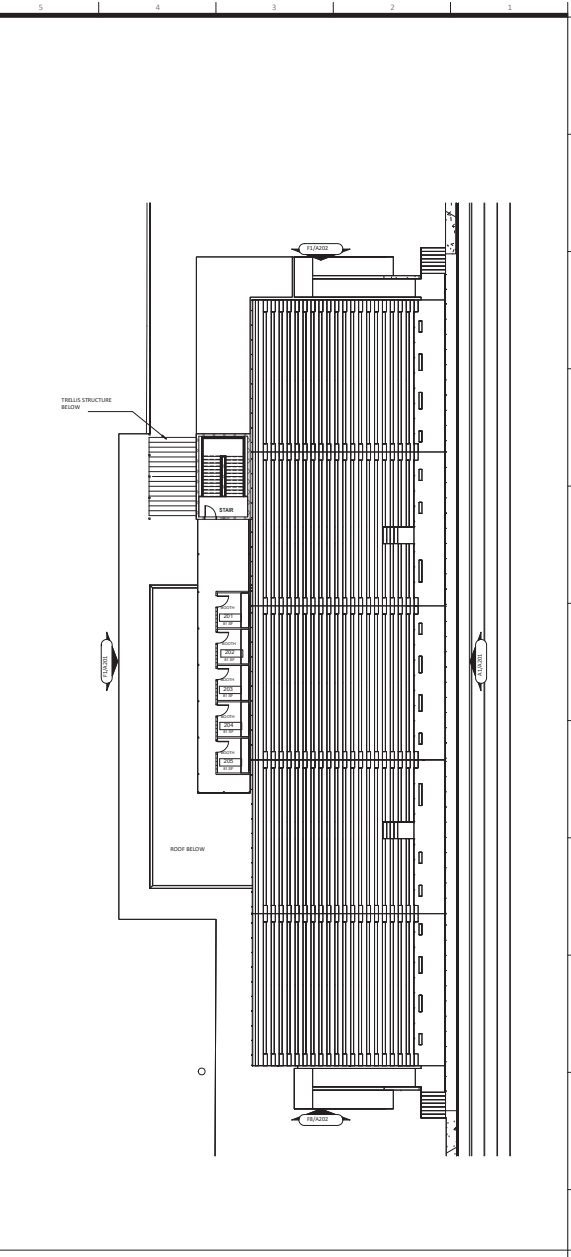
Design Development



Stadium - Site Plan **A10**  
1" = 50' 0"



Floor Plan - Level 1 **A6**  
1/32" = 1' 0"



Floor Plan - Level 2 **A1**  
1/32" = 1' 0"

St. James Academy - Stadium Additions

24505 Prairie Star Parkway  
Lenexa, KS 66227

Project Number: 0223-2023

architect: multistudio  
25225 Appleton Parkway  
Lenexa, KS 66227  
913.246.4576  
www.multistudio.org

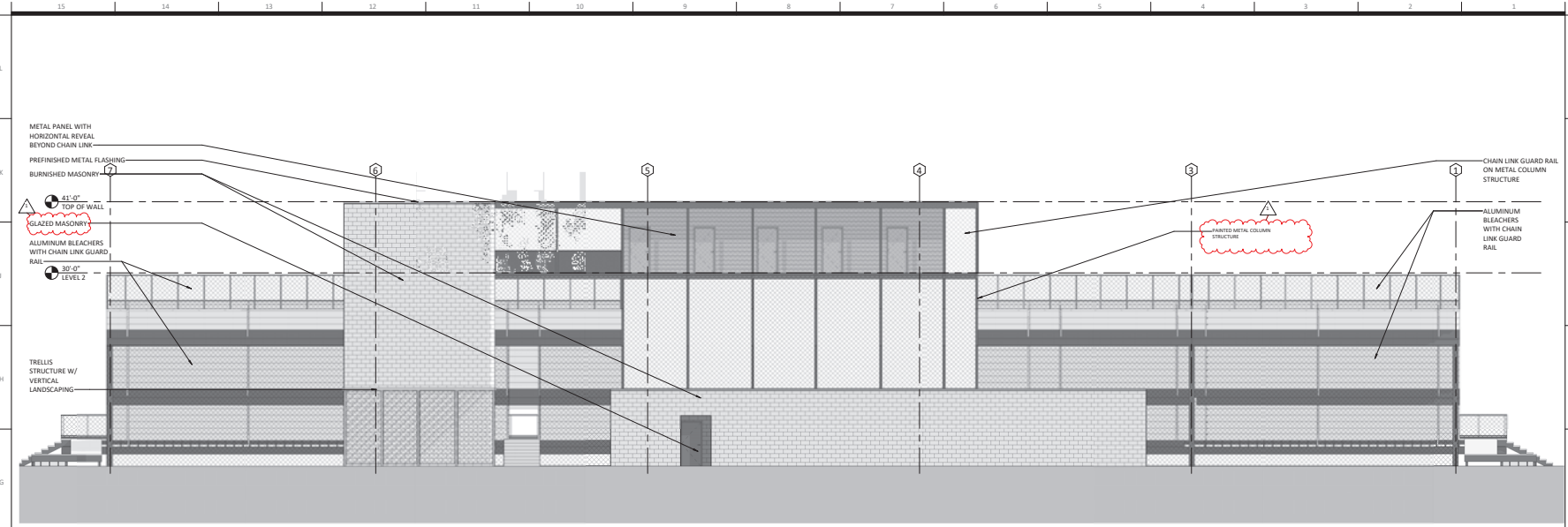
contract engineer: Mark R. Campbell  
4450 W. 127th St.  
Lenexa, KS 66215  
913.811.8800  
www.mrcampbellinc.com

contractor: Keith + Hatcher  
24240 W. 104th St.  
Overland Park, KS 66204  
913.246.1227  
www.keithhatcher.com

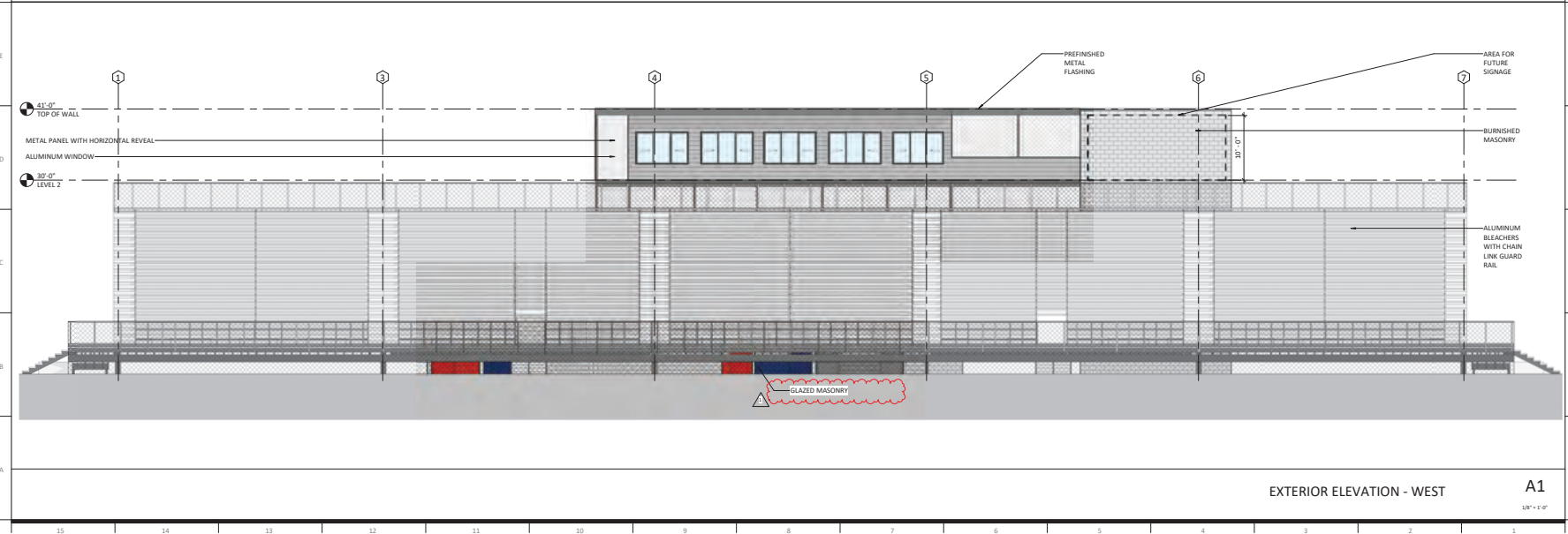
Issue Date: October 20, 2023

NUMBER	DESCRIPTION	DATE
1	FOR DEVELOPMENT	11/02/2023

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EXTERIOR ELEVATION - EAST  
F1  
1/8" = 1'-0"



EXTERIOR ELEVATION - WEST  
A1  
1/8" = 1'-0"

EXTERIOR ELEVATIONS  
A201

Design Development

**St. James Academy - Stadium Additions**

2455 Prairie Star Parkway  
Lenexa, KS 66227

Project Number: 0223-2025

architect:	multistudio
2455 Prairie Star Parkway Lenexa, KS 66227 913.224.4555 www.multistudio.org	2455 Prairie Star Parkway Lenexa, KS 66227 913.224.4555 www.multistudio.org
civil engineer/landscape architect:	mark b. coxhead
1400 W. 127th St. Lenexa, KS 66224 www.mbcengineering.com	4308 Buchanan Ave. Lenexa, KS 66241 913.224.4544 www.mbc-engineer.com

structural engineer:	keith r. haeuber
2455 Prairie Star Parkway Lenexa, KS 66227 www.stjamesacademy.com	4308 Buchanan Ave. Lenexa, KS 66241 913.224.4544 www.keithhaeuber.com

Issue Date: October 30, 2025

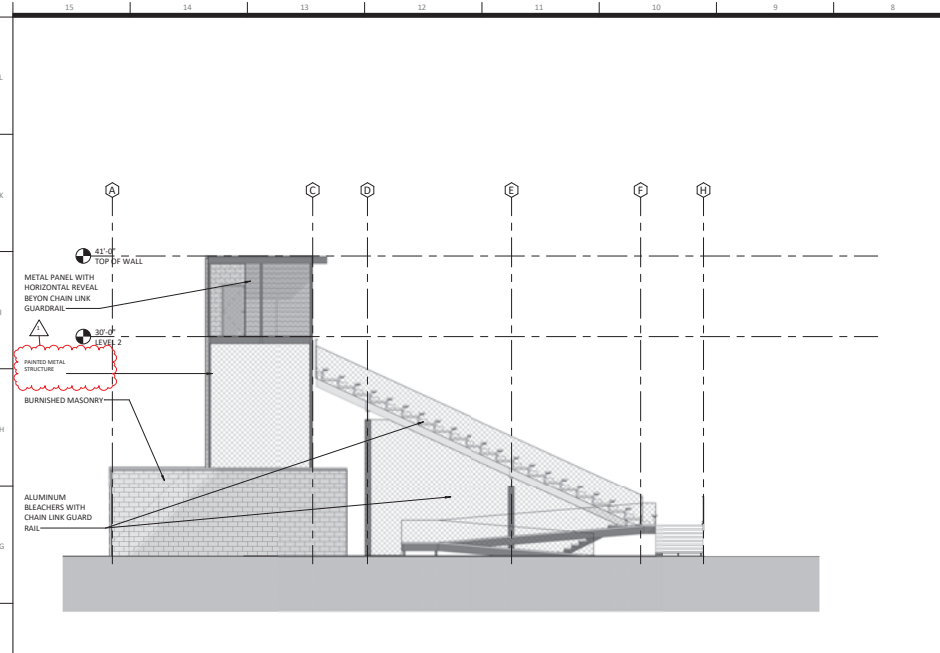
NUMBER	DESCRIPTION	DATE
1	ISSUE FOR CONSTRUCTION	11/20/2025

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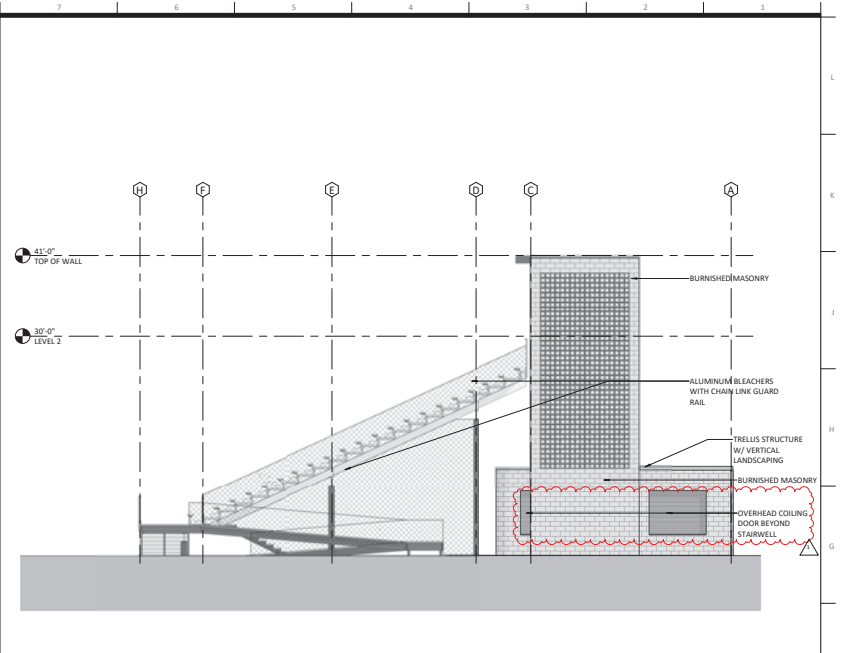


**EXTERIOR ELEVATIONS  
A202**

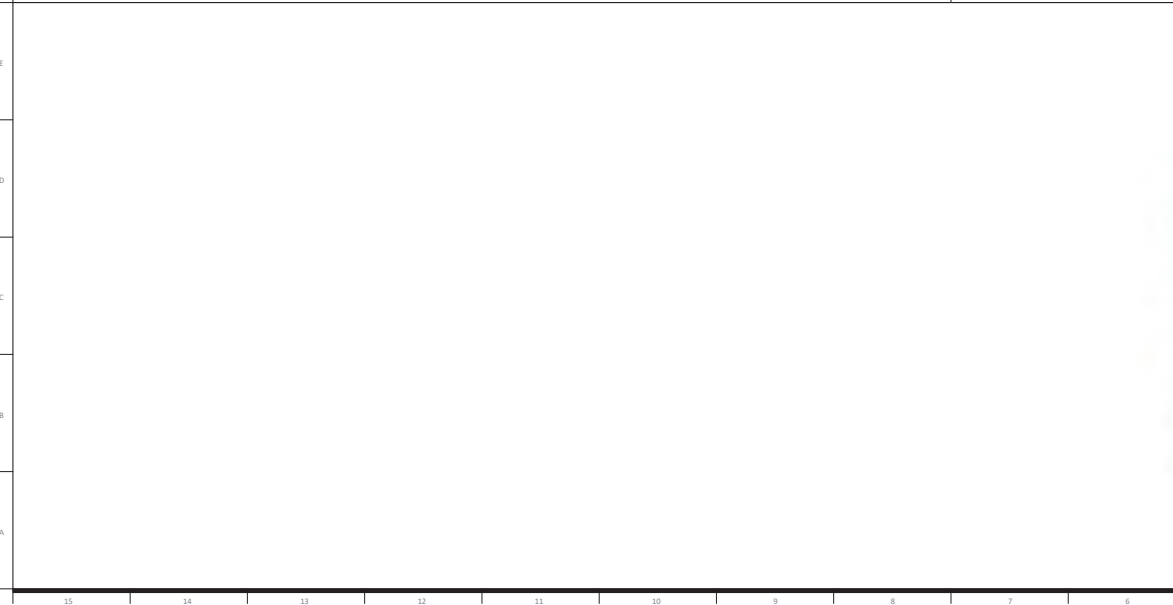
Design Development



EXTERIOR ELEVATION - NORTH  
F8  
1/8" = 1'-0"



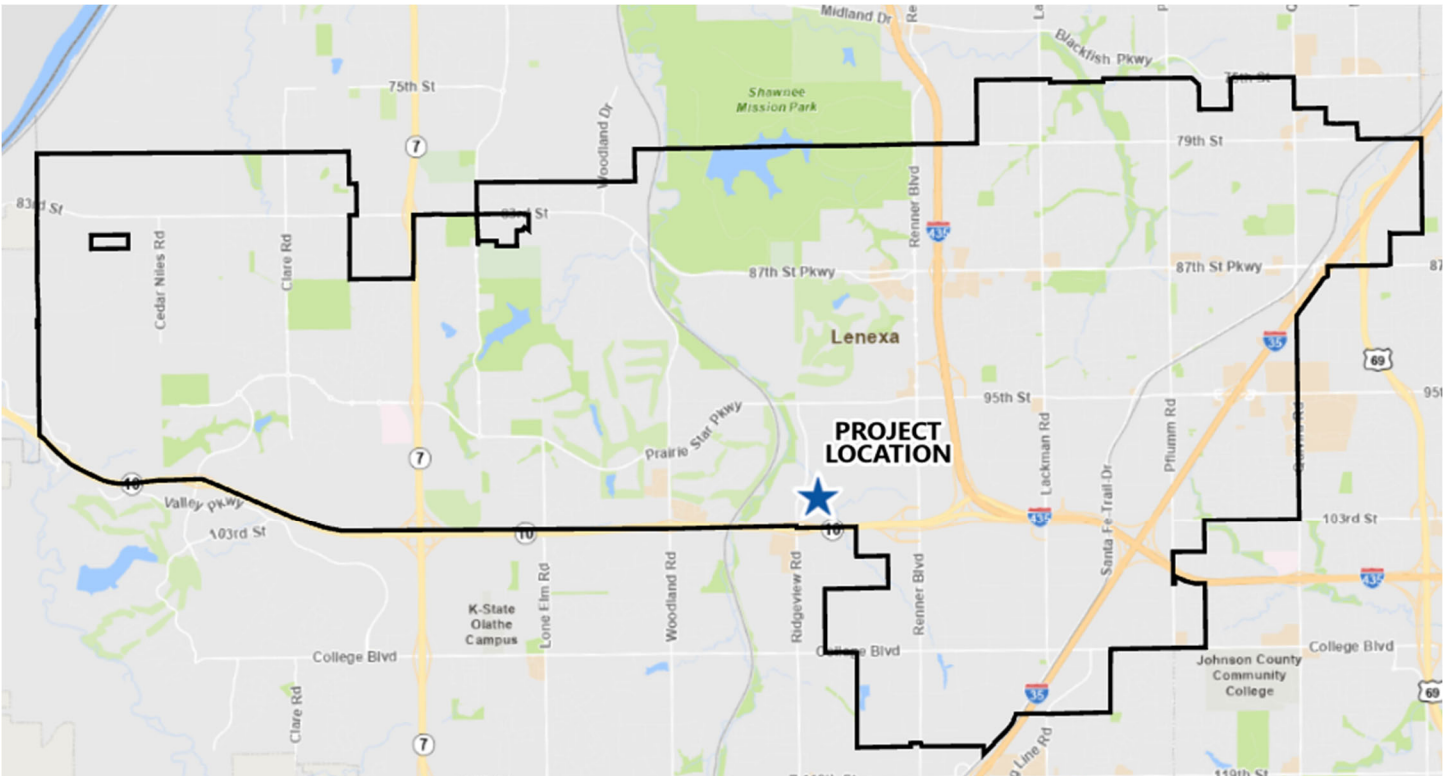
EXTERIOR ELEVATION - SOUTH  
F1  
1/8" = 1'-0"



MATERIAL PALETTE  
A1  
1/8"

# TOWNEPLACE HOTEL AT VISTA RIDGE

<b>Project #:</b>	PL23-23F	<b>Location:</b>	Northeast corner of K-10 Highway and Ridgeview Road
<b>Applicant:</b>	Chad Porter, Payne and Brockway	<b>Project Type:</b>	Final Plan
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	Hotel



## PROJECT SUMMARY

The applicant is requesting final plan approval for a 64,031 SF, four-story hotel with 102 rooms located at the northeast corner of K-10 Highway and Ridgeview Road on a 3.55-acre lot. The hotel is one of six out-parcel lots within the Vista Ridge development. The lot containing the hotel is along the easterly side of the site, which is at the “rear” of the development.

**STAFF RECOMMENDATION: APPROVAL WITH A CONDITION**

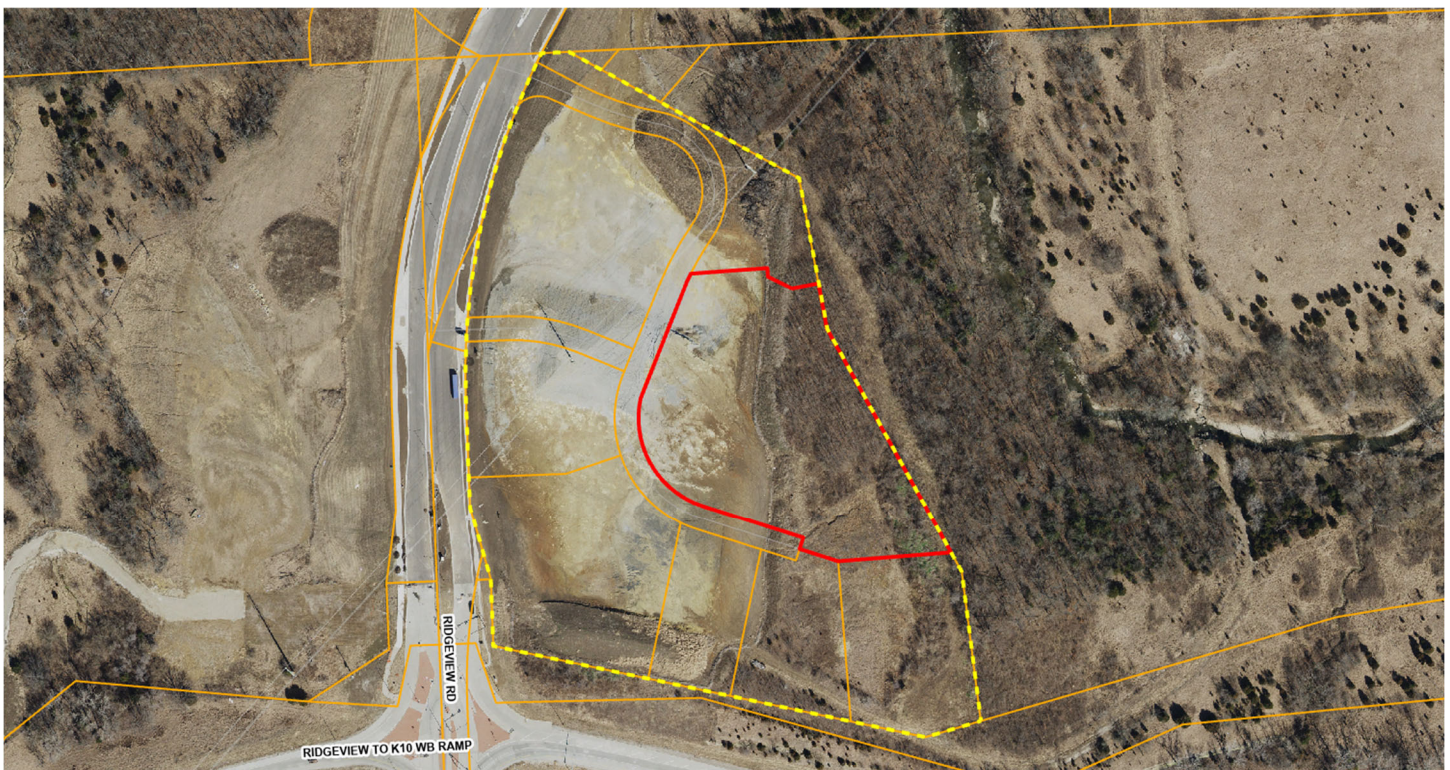
## SITE INFORMATION

Vista Ridge is a commercial development at the northeast corner of K-10 Highway and Ridgeview Road. The development has two points of access onto Ridgeview Road. The northernmost drive is a full-movement intersection, and the southerly drive is a right-in, right-out and left-in-only intersection.

The site has been zoned for commercial development since 2014 (RZ14-06). The development was originally called Mill Creek Village. This project was envisioned as a regional commercial center containing 170 acres with two “big box” retail stores, office buildings, and several out-parcel buildings. The most recent preliminary plan for Vista Ridge was approved in the spring of 2022 (PL22-01PR). The Vista Ridge development contains a total of 13 acres, eight lots, and nine buildings.

Vista Ridge and the Vista Village site, further north on Ridgeview Road, are associated developments as they are both owned by West Star Development and are planned to share aesthetic characteristics. The developer intends to use complimentary branding for the two developments, beginning with signs. Both use “Vista” as part of the name and both sites share architectural design guidelines. The buildings are not intended to all look identical but are to have the same level of detail and quality of materials. Vista Ridge is a smaller development than Vista Village and will have predominantly retail uses.

<b>LAND AREA (AC)</b> 3.55	<b>BUILDING AREA (SF)</b> 64,031	<b>CURRENT ZONING</b> CP-3	<b>COMP. PLAN</b> Mixed Use
-------------------------------	-------------------------------------	-------------------------------	--------------------------------

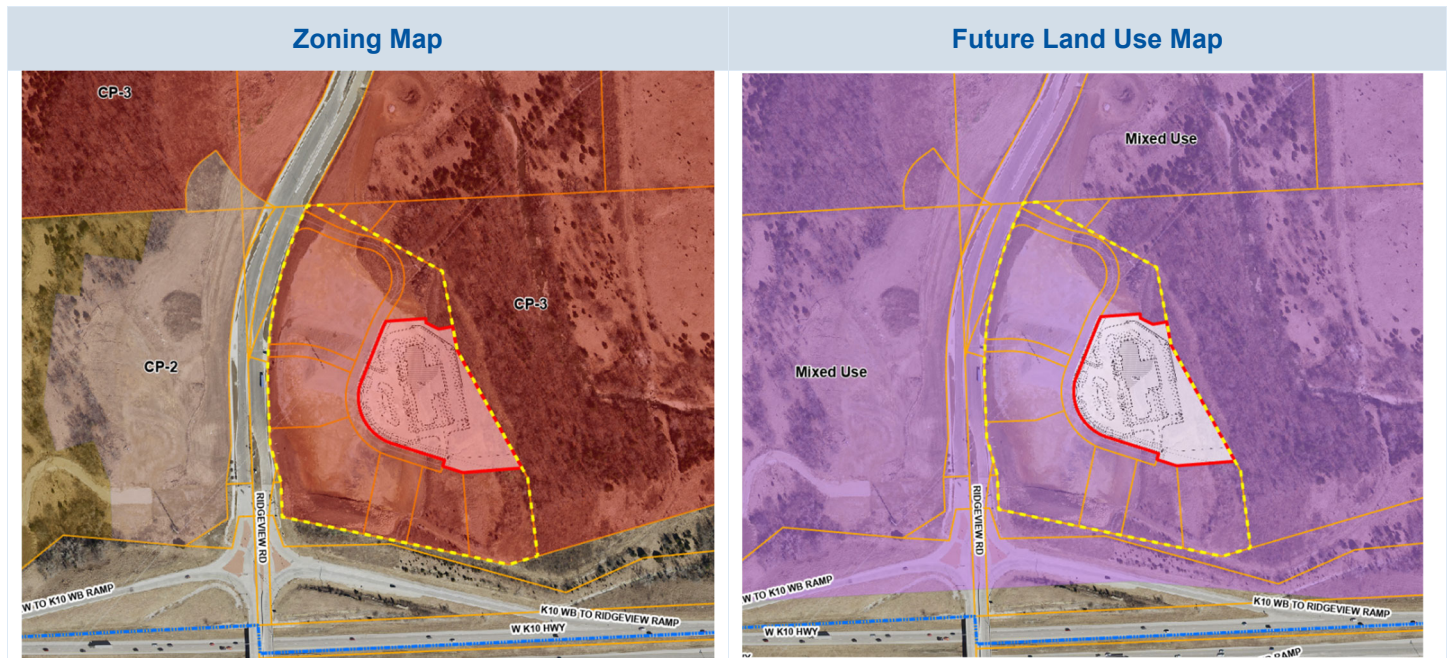


**Exhibit 1: Vicinity Aerial Image of Subject Site.**



## LAND USE REVIEW

A hotel is an allowed use in the CP-3 Zoning District. The initial preliminary plan for Vista Ridge showed a hotel on the southeasternmost lot of the development. This plan proposes the hotel on the lot to the north. This change is not a substantial change to the development. The adjacent property, not a part of the Vista Ridge development, is undeveloped land and is stream corridor which will remain undeveloped.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Future Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Mixed-use	CP-3, Planned Regional Commercial District	Undeveloped
<b>North</b>	Mixed-use	CP-3, Planned Regional Commercial District	Open space (stream corridor)
<b>South</b>	Mixed-use	CP-3, Planned Regional Commercial District	Undeveloped
<b>East</b>	Mixed-use	CP-3, Planned Regional Commercial District	Undeveloped
<b>West</b>	Mixed-use	CP-2, Planned Community Commercial District	Undeveloped

## FINAL PLAN REVIEW

The proposed final plan is for a 102-room hotel on a lot within the Vista Ridge commercial development. The preliminary plan shows a hotel on the lot to the south. Staff concludes relocating the hotel onto an adjacent lot is substantially consistent with the approved preliminary plan. The site is accessed from an internal drive at three access points. This internal drive functions as a private street providing access to several lots in Vista Ridge. The hotel is centrally located on the site allowing for vehicular circulation and parking around the entire building. Sidewalks are provided around the building and along the private drive with a location to cross the internal drive.



**Exhibit 2: Site Plan with Aerial.**

### TRAFFIC, ACCESS, AND PARKING

- The site provides well-defined internal site circulation with distinct drive aisles and parking areas.
- The site will have three points of access from the private drive within the Vista Ridge development.
- The Vista Ridge development has an internal sidewalk network to allow for visitors and customers to walk from site to site. This sidewalk will cross the internal drives at the “T” intersection. Staff requests a second location to cross the main internal drive. Staff will coordinate this detail with the applicant with the building permit submittal.

**TABLE 2: PARKING ANALYSIS**

Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Hotel or motel	1 space per rooming unit	102	105	3
<b>Totals</b>		<b>102</b>	<b>105</b>	<b>3</b>

### STORMWATER

The overall stormwater management for this site has been previously constructed with the overall commercial development.

### FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

### LIGHTING

The site will include lighting for pedestrian areas, building mounted lighting, and parking lot lighting. Parking lot light poles are 20 feet tall and are provided in parking lot islands and along the curb edge of the lot. The site lighting complies with [Section 4-1-C-4-I](#) of the Unified Development Code (UDC).

### LANDSCAPING

Landscaping will be installed along the property boundaries, around the perimeter of the lot, in parking lot islands, and around the building. The landscaping plan depicts the typical landscape design for a pad site in a commercial development. There are spaces around the building that do not show any landscaping. The plan meets the required number of plantings; however, some of the areas around the foundation of the building should be landscaped with small plant materials and seasonal ornamental plantings. Staff will coordinate this detail with the building permit submittal



*Exhibit 3: Landscape plan.*

## ARCHITECTURE

The building uses stacked stone, metal siding with a wood grain finish and two colors of Exterior Insulation and Finish Systems (EIFS) material as the primary exterior finish materials. The stacked stone is the predominant material continuing along the entire ground floor level as a base feature and extends vertically at select locations to the second floor and to the roofline of other sections. A transition detail is used for transition from the top of the stacked stone to EIFS. The vertical material changes occur at an “inside corner” of a wall plane articulation of the façade. The windows include a lintel detail on the parts of the building clad with stacked stone. This detail is strategic and creates character for the building.

A porte-cochere is at the front of the building for weather protection for customers of the hotel. The porte-cochere is a butterfly style awning which is higher at the edges and has a valley in the middle. The butterfly design repeats pitched roof details for elements at the roofline of the building. The columns of the porte-cochere are shown as painted metal. The metal columns have a modern appearance, but Staff recommends the columns incorporate a stone base for continuity with the building. A long metal awning extends over the main entrance and a row of windows on the front façade. This awning also uses metal posts. The columns for this awning should also incorporate a stone base. Staff will coordinate these details with the applicant with the building permit submittal.



**Exhibit 4: Front façade rendering.**

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## DEVIATIONS

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The applicant is not requesting any deviations for this final plan for a hotel.

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## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

---

## RECOMMENDATION FROM PROFESSIONAL STAFF

---

★ **Staff recommends approval of the proposed Final Plan Towneplace Hotel at Vista Ridge.**

- This project is consistent with Lenexa's goals through ***Responsible Economic Development*** to promote ***Inviting Places*** and ***Thriving Economy***.

## FINAL PLAN

Staff recommends **approval** of the final plan for PL23-23F – **Towneplace Hotel at Vista Ridge** near the northeast corner of K-10 Highway and Ridgeview Road for a hotel with the following condition:

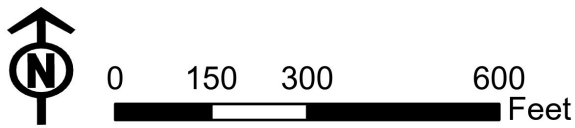
1. The building permit plans and construction of the hotel site shall incorporate the following:
  - a. A second sidewalk crossing at the south end of the lot;
  - b. Additional landscape plantings around the building; and
  - c. A stone base for the porte-cochere columns for continuity with the building.



Document Path: C:\Users\cddavidd\OneDrive - City of Lenexa\Documents\ArcGIS\Projects\MyProject\City Council Packet Template.aprx

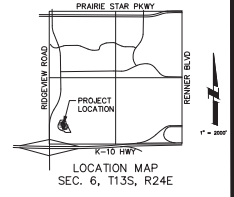
Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Towneplace Hotel at Vista Ridge PL23-23F





NO.	DATE	DESCRIPTION	BY
1	11-8-22	PER CITY COMMENTS	CMP
2			
3			
4			
5			
6			
7			



DEVELOPER / OWNER  
 TEN RIDGE, LLC  
 10550 S WARWICK ST  
 OLAHIE, KS 66061

- LEGEND**
- DENOTES PROPOSED BUILDING
  - DENOTES PROPOSED 8" REINFORCED CONCRETE
  - DENOTES PROPOSED 6" ASPHALT PAVEMENT
  - DENOTES PROPOSED CONCRETE SIDEWALK
  - DENOTES PROPOSED TYPE B CURB
  - DENOTES CONSTRUCTION LIMITS

- DEVELOPMENT NOTES:**
- SIDEWALKS SHALL BE CONCRETE.
  - CONTRACTOR SHALL REMOVE AND REPLACE EXISTING SIDEWALKS AS REQUIRED TO CONSTRUCT IMPROVEMENTS SHOWN ON THIS PLAN. REMOVAL OF EXISTING SIDEWALKS SHALL BE DONE BY SAWCUTTING AT JOINTS.
  - FIRE LANE DESIGNATION MAY BE REQUIRED FOLLOWING CONSULTATION WITH THE LENEXA FIRE DEPARTMENT. CURBS IN AGREED UPON AREAS SHALL BE PAINTED YELLOW. ACCORDINGLY, "NO PARKING FIRE LANE" SIGNS SHALL BE INSTALLED. SIGNS MUST BE DOUBLE SIDED AND VISIBLE FROM BOTH DIRECTIONS OF TRAVEL. (FC 2018, 503).
  - PRIVATE DRIVES SHOWN HEREON PROVIDE SHARED ACCESS FOR EACH LOT WITHIN THE DEVELOPMENT.
  - STORM SEWER DRAINAGES, WATER QUALITY, AND DETENTION SHALL BE DONE IN ACCORDANCE WITH THE PREVIOUSLY APPROVED PLAN AND STORM WATER LETTER, AND PER EXISTING STORMWATER FACILITIES ON-SITE.
  - OUTDOOR FEATURES SHALL BE PROVIDED IN ACCORDANCE WITH THE VISTA DESIGN GUIDELINES.
  - ALL SIDEWALK RAMPS SHOWN HEREON SHALL COMPLY WITH THE ADAAG 2010 STANDARDS.

**SITE DATA**

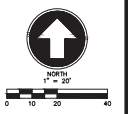
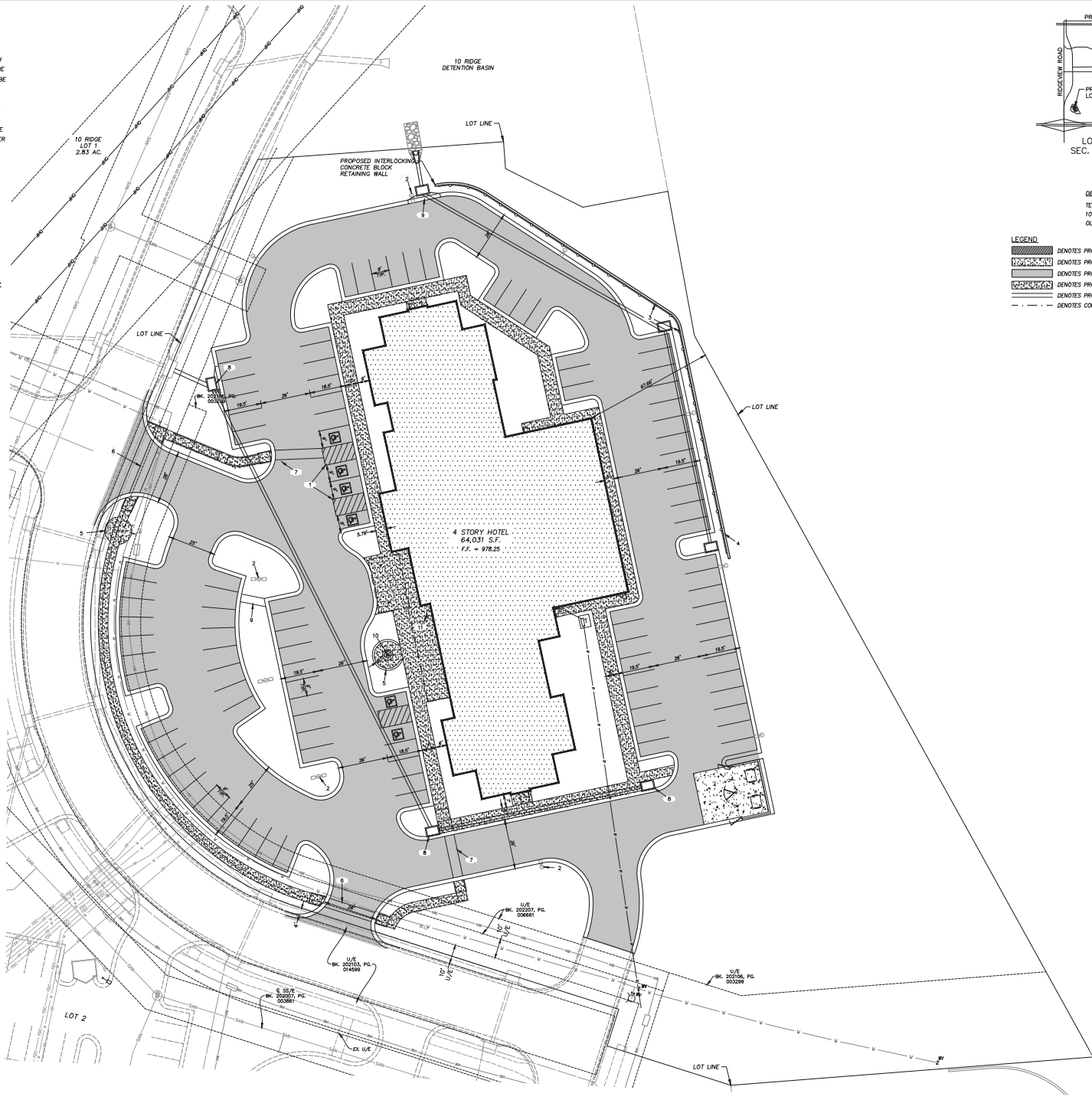
SITE	BUILDING AREA	LOT AREA	LOT NUMBER
HOTEL	64,031 SF	3.55 AC. +/-	7

SITE DEVELOPMENT DATA	
SIKIL AREA	3.55 AC. +/-
TOTAL BUILDING AREA	64,031 SF
PROPOSED PARKING (102 ROOMS)	102 SPACES
PROPOSED PARKING	102 SPACES
TOTAL IMPERVIOUS AREA	76,685 S.F. (44.94%)
TOTAL GREEN SPACE	15,353 S.F. (12.06%)
ZONING	CP3

- PARKING NOTES:**
- ALL REGULAR PARKING SPACES SHOWN HEREON ARE 9'x19'.
  - THE ANGLE OF ALL PARKING SPACES SHOWN HEREON IS 90°.

- CONSTRUCTION NOTES:**
- ADA PARKING SIGN.
  - PARKING LOT LIGHTS. SEE LIGHTING PLAN.
  - METAL FENCE.
  - CONCRETE BLOCK RETAINING WALL.
  - PEDESTRIAN PLAZA.
  - PAINTED CROSSWALK. SEE DETAIL SHEET C103.
  - CONCRETE CROSSWALK. SEE DETAIL SHEET C103.
  - STORM SEWER. SEE SHEET C200.
  - CONCRETE FLUME.
  - FLAG POLE.
  - BIKE RACK. SEE DETAIL SHEET C103.



**PLANT SCHEDULE**

SYMBOL	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
<b>ORNAMENTAL TREE</b>							
	CC	<i>Cercis canadensis</i>	Eastern Redbud Clump	6' Ht.	B&B / Cont.	3	
	MA	<i>Malus x 'Adirondack'</i>	Adirondack Crabapple	6' Ht.	B&B / Cont.	3	
	MR	<i>Malus x 'Royal Raindrops'</i>	Royal Raindrops Crabapple	6' Ht.	B&B / Cont.	2	
	QR	<i>Quercus robur x bicolor 'Nadler'</i>	Kindred Spirit Oak	6' Ht.	B&B / Cont.	5	
<b>SHADE TREE</b>							
	AM	<i>Acer niyabai 'State Street'</i>	State Street Maple	2' Cal.	B&B / Cont.	11	
	AT	<i>Acer truncatum 'Pacific Sunset' TM</i>	Pacific Sunset Maple	2' Cal.	B&B / Cont.	11	
	QS	<i>Quercus shumardi</i>	Shumard Red Oak	2' Cal.	B&B / Cont.	6	
	TD	<i>Taxodium distichum 'Shawnee Brave' TM</i>	Shawnee Brave Bald Cypress	2' Cal.	B&B / Cont.	3	
<b>EVERGREEN TREES</b>							
	TP	<i>Thuja plicata 'Green Giant'</i>	Green Giant Arborvitae	5' Ht.	B&B / Cont.	10	
<b>SHRUBS</b>							
	ish	<i>Aronia melanocarpa 'UCONNAM166' TM</i>	Low Scape Hedger Black Chokeberry	18" Ht.	Pot	84	
	dttq	<i>Chaenomeles x 'Double Take' TM</i>	Double Take Quince	18" Ht.	Pot	27	
	gj	<i>Juniperus virginiana 'Grey Owl'</i>	Grey Owl Juniper	18" Ht.	Pot	40	
	tes	<i>Rhus typhina 'Tiger Eyes'</i>	Tiger Eyes Sumac	18" Ht.	Pot	7	
	pvv	<i>Viburnum x pragensis</i>	Prague Viburnum	18" Ht.	Pot	34	
<b>PERENNIALS</b>							
	pdt	<i>Penstemon digitalis 'Peachbloss'</i>	Peachbloss Beardtongue	18" Ht.	Pot	101	
<b>GRASSES</b>							
	mag	<i>Miscanthus sinensis 'Gracillimus'</i>	Maiden Grass	18" Ht.	Pot	14	
	nws	<i>Panicum virgatum 'North Wind'</i>	Northwind Switch Grass	18" Ht.	Pot	61	

**LANDSCAPE CALCULATIONS:**

PER SECTION 4-1-D-2 OF THE LENEXA UDC. THE FOLLOWING IS REQUIRED:

**SECTION 4-1-D-2-J. MINIMUM STREET TREE REQUIREMENTS:**

STREET TREES - ONE TREE PER 40' OF PUBLIC OR PRIVATE STREET FRONTAGE  
 - LOOP ROAD = 509.36 LF / 40 = 13 TREES REQ. / 13 TREES PROVIDED

**SECTION 4-1-D-2-L. MINIMUM PERIMETER LANDSCAPE REQUIREMENTS:**

ALONG STREET FRONTAGE 2 SHADE TREES AND 12 SHRUBS REQ. PER 100 LF  
 - LOOP ROAD = 509.36 LF = 11 TREES REQ. / 11 PROVIDED, 62 SHRUBS REQ. / 78 PROVIDED

**NON-STREET FRONTAGE ONE SHADE TREE AND 9 SHRUBS PER 100 LF**

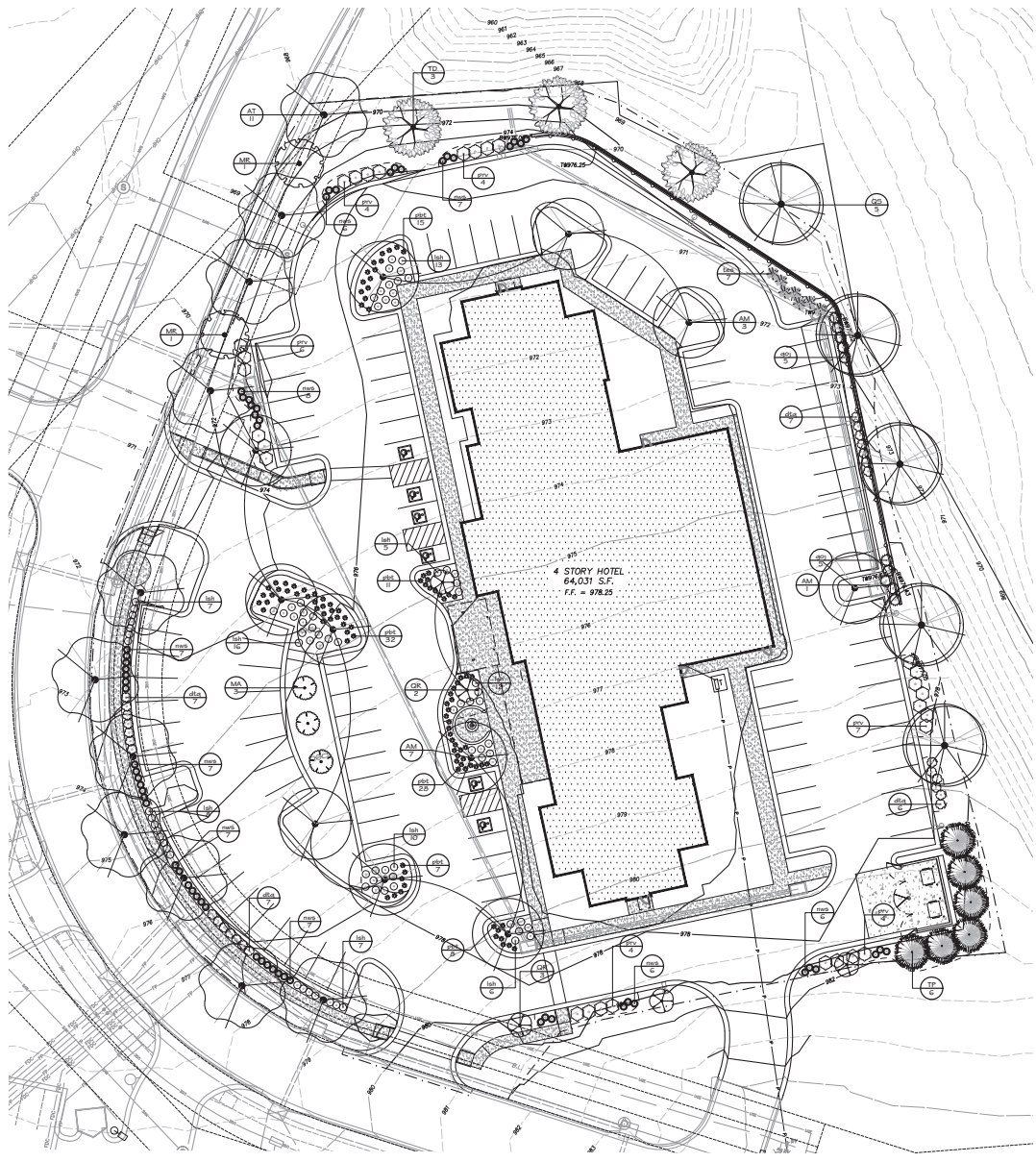
- N PL = 231.74 LF = 3 TREES AND 21 SHRUBS REQ. / 3 TREES AND 21 SHRUBS PROVIDED  
 - E PL = 404.23 LF = 5 TREES AND 37 SHRUBS REQ. / 5 TREES AND 37 SHRUBS PROVIDED  
 - S PL = 220.58 LF = 3 TREES AND 20 SHRUBS REQ. / 3 TREES AND 20 SHRUBS PROVIDED

**SECTION 4-1-D-2-M. INTERNAL PARKING LOT LANDSCAPE AREAS:**

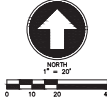
A MINIMUM OF 5% OF TOTAL PARKING LOT AREA TO BE LANDSCAPE ISLANDS.  
 - ONE TREE PER ISLAND / PROVIDED

**LANDSCAPE PLAN NOTES:**

- Existing underground (u/g), overhead (o.h.) utilities and drainage structures have been plotted from available information and therefore, their locations must be considered approximate only. It is the responsibility of the contractor to verify existence and location of all utilities before starting any work.
- No substitutions for variety or cultivars shall be accepted without first obtaining written approval from Owner's Representative.
- All plant material shall be of excellent quality, free of disease and infestation, and true to type, variety, size specified, and form per American Nursery Association Standards (2601).
- All planting beds shall receive 3" minimum depth of 2-4" Missouri Rainbow river rock on DeWitt Pro-5 weed fabric as detailed, unless otherwise noted. In landscape beds, mulch shall be a consistent 3" depth throughout and weed fabric shall not be visible. For trees planted in turf areas, a ring of died brown shredded hardwood mulch shall be formed into a saucer in a minimum ring twice the diameter of the rootball from the trunk with minimum depth of 3" water settled. Elevation of top of mulch shall be 1/2" below any adjacent pavement. River rock landscape beds shall have black 3/8" x 4" steel edging between landscape bed and turf. No edging shall be used between pavement and landscape bed. Hardwood mulch free rings shall have a natural edge.
- Contractor shall thoroughly water in each plant immediately following installation.
- Contractor shall be responsible for calculating all areas of sod and seed and the amounts of each needed for optimum coverage. Quantities on this plan are for reference only.
- Contractor shall install sod in all areas disturbed during construction not otherwise designated as another material. Sodded areas greater than 4:1 slope shall have sod secured in place with sod staples spaced evenly and adequately to hold sod in place. Sod shall be Excalibur Turf-type Tall Fescue Blend (or approved equal). Starter fertilizer with moisture management agent shall also be applied to topsoil at time of sodding.
- Proposed trees shall not be placed over existing or proposed utility service lines. It is the contractor's responsibility to understand utility locations and have them marked during tree planting operations. If utility is damaged during planting, contractor is responsible for notifying the general contractor and owner of utility and paying for repair of the damaged utility.
- A fully automated irrigation system will be supplied for this project by the Contractor. Design shall be provided by the contractor and approved by the landscape architect prior to ordering materials or installing any aspect of the irrigation system. Irrigation system shall consist of top, backflow, smart controller, heads, Sch. 40 pipe, valves, wire, flow sensing, weather station, and any other feature to give the most efficient and comprehensive system as deemed necessary by the owner and landscape architect. System shall cover all areas designated as turf or landscape beds. Turf, trees and shrubs shall all be on separate zones so that they can be watered at different rates. Submit all irrigation plans to the landscape architect for approval.
- No trees shall be planted within 15 feet of a street or parking lot light pole.
- All land area which is not paved or covered by buildings will be brought to finished grade, and planted with turf.



**1 PDP LANDSCAPE PLAN**  
 1" = 20'



PLOT DATE: 11/16/2023 23-0022 23-0022 Ten Ridge Towneplace Landscape PDP #117123.dwg - L100

NO.	DATE	DESCRIPTION	BY	CHK
1	11-17-23	SITE PLAN UPDATE		
2				
3				
4				
5				
6				



**Chad D. Weindand, PL, ASLA**  
 Landscape Architecture  
 15173 W. 157th Terrace, Olathe, Kansas 66062  
 913.644.3738 - cweindand74@gmail.com  
 Copyright 2023

10-2-23

**L100**

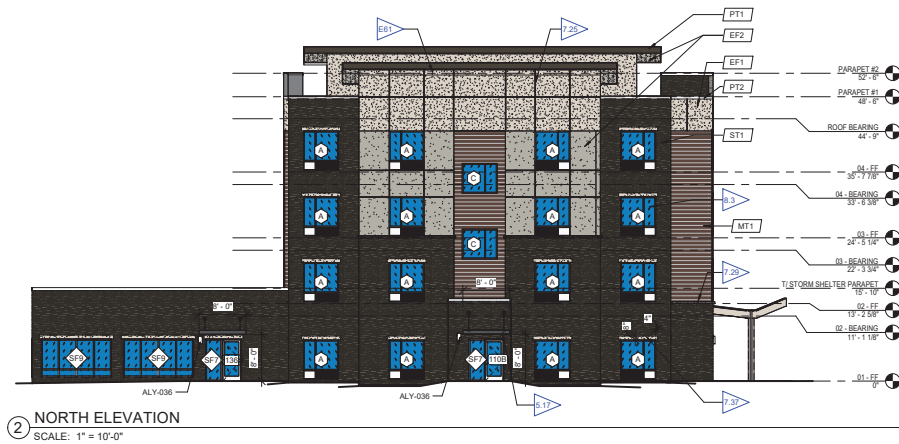




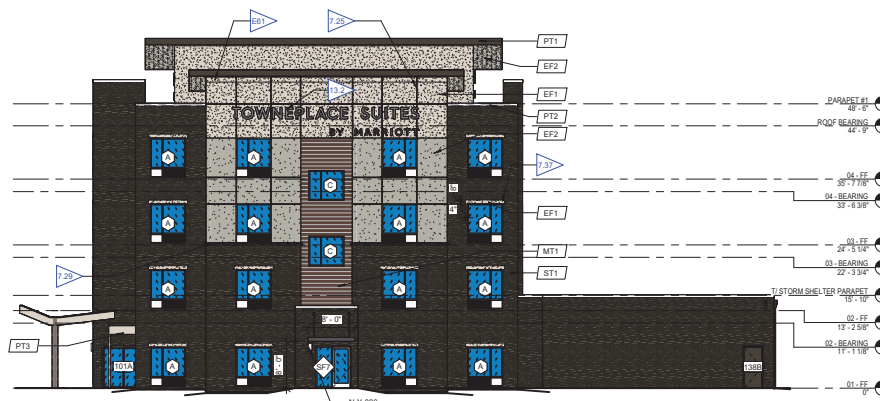
EXTERIOR MATERIAL FINISH SCHEDULE		
MARK	MATERIAL	DESCRIPTION
EF1	DRAINABLE EPS SYSTEM	DRYVIT - SAND/PEBBLE FINISH - "NATURAL WHITE"
EF2	DRAINABLE EPS SYSTEM	DRYVIT - SAND/PEBBLE FINISH - "GULL GRAY"
MT1	WOOD LOOK ALUMINUM SIDING	PERLIWAN WALNUT BLEND - BROWN
PT1	PAINTED OR PRE-FINISHED SURFACE	COLOR TO MATCH SHERWIN WILLIAMS "SEALSKIN"
PT2	PAINTED OR PRE-FINISHED SURFACE	COLOR TO BE WHITE
PT3	PAINTED OR PRE-FINISHED SURFACE	COLOR TO MATCH DRYVIT "NATURAL WHITE"
ST1	MANUFACTURED STONE	MOUNTAIN LEDGE - SIERRA

- EXTERIOR WINDOW COLOR: ALUMINUM - WHITE
- EXTERIOR STOREFRONT COLOR: ALUMINUM - WHITE
- EXTERIOR HM DOORFRAME COLOR: PAINTED TO MATCH SHERWIN WILLIAMS "SEALSKIN"
- EXTERIOR GLAZING: CLEAR (REFLECTIVE)
- SHEET METAL FINISH: COLOR TO MATCH ADJACENT MATERIAL
- MECH. LOUVERS: COLOR TO MATCH ADJACENT MATERIAL

KEYED NOTES	
5.17	PRE-MANUFACTURED ALUMINUM AWNING SYSTEM - ARCH/FAB OR EQ - INSTALL PER MFRG INSTRUCTIONS
7.25	VGROOVE JOINT - TYP.
7.29	EPS WATERTABLE - TYP.
7.37	DRAINABLE EPS HEADERS
8.3	ALUMINUM WINDOW W/ INTEGRAL PTAC LOUVER - INSTALL PER MFRG INSTRUCTIONS
13.2	SIGNAGE PER OWNER/FRANCHISE APPROVED SIGN VENDOR. GC TO COORDINATE POWER AND STRUCTURE REQUIREMENTS WITH SIGN VENDOR
E61	LINEAR UPLIGHT FIXTURE - PROVIDE CONTINUOUS BAND OF LIGHT TO ILLUMINATE BOTTOM OF SLOPFFIT - ALY-037



② NORTH ELEVATION  
SCALE: 1" = 10'-0"



① SOUTH ELEVATION  
SCALE: 1" = 10'-0"



CHASEN GARRETT  
ARCHITECTS  
7309 CHAD COLLEY BLVD. SUITE C  
BARLING, ARKANSAS 72821

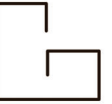
TOWNEPLACE SUITES  
VISTA RIDGE, LENEXA, KANSAS

SET STATUS	DESCRIPTION	DATE

SHEET NAME  
EXTERIOR ELEVATIONS

SHEET NUMBER  
**A202**

DATE: 11/29/2023  
PROJECT NO.: 22026



CHASEN GARRETT

ARCHITECTS

7809 CHAD COLLEY BLVD, SUITE C  
BARLING, ARKANSAS 72821



**TOWNEPLACE SUITES**  
VISTA RIDGE, LENEXA, KANSAS

SET STATUS  
DESCRIPTION DATE

SHEET NAME

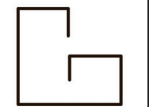
EXTERIOR RENDERING

SHEET NUMBER

**A203**

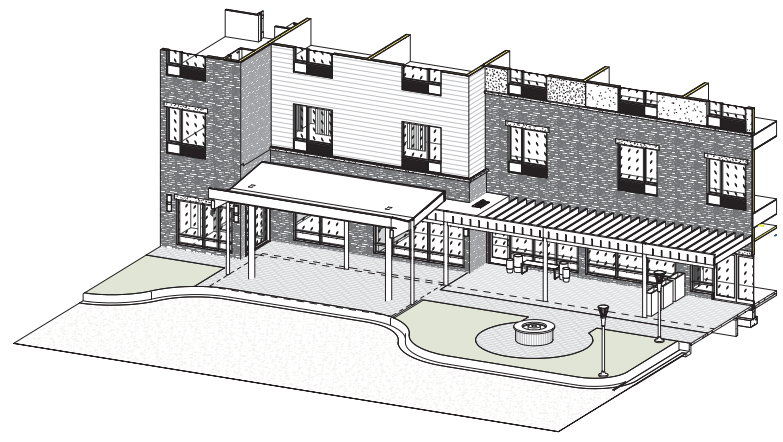
DATE 11/09/2023

PROJECT NO. 22026

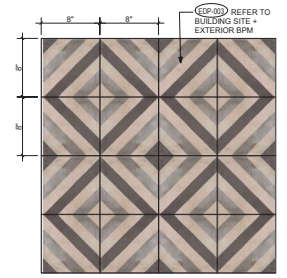


**CHASEN GARRETT**  
ARCHITECTS  
7309 CHAD COLLEY BLVD, SUITE C  
BARKING, ARKANSAS 72821

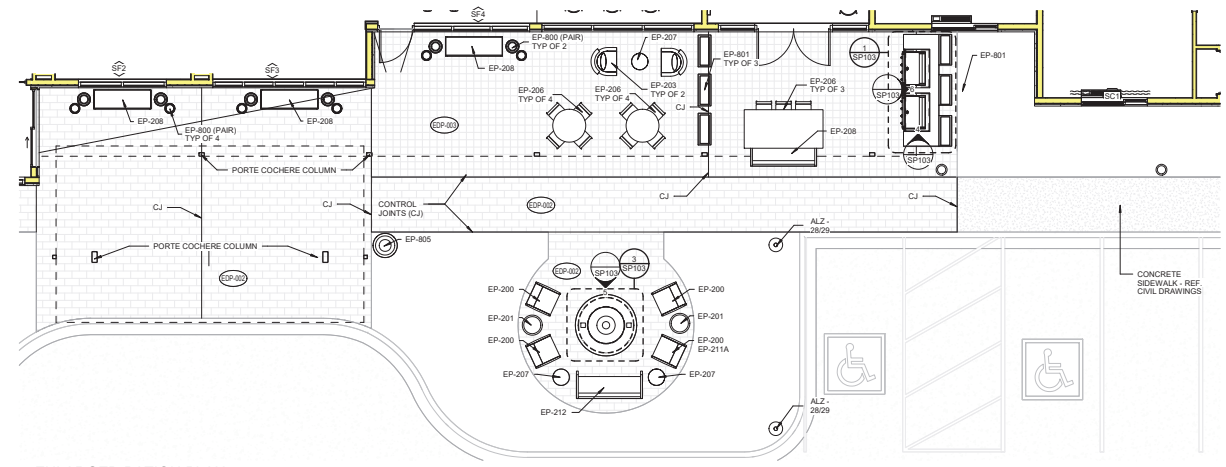
**TOWNEPLACE SUITES**  
VISTA RIDGE, LENEXA, KANSAS



③ PATIO VIEW  
SCALE:



② PATIO TILE  
SCALE: 1 1/2" = 1'-0"



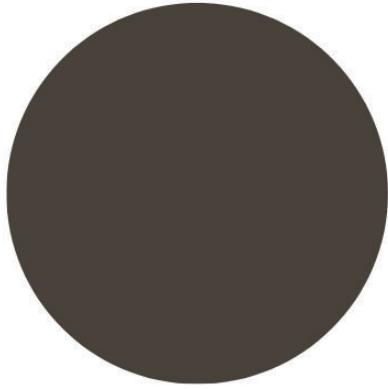
① ENLARGED PATIO PLAN  
SCALE: 3/16" = 1'-0"

SET STATUS	
DESCRIPTION	DATE

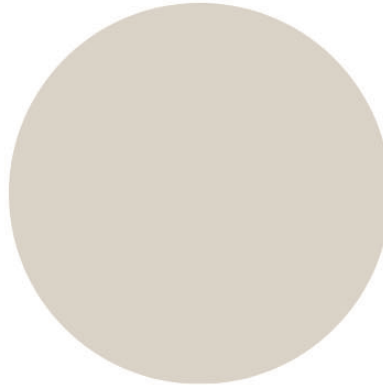
SHEET NAME  
**ENTRANCE PATIO PLAN**

SHEET NUMBER  
**SP105**

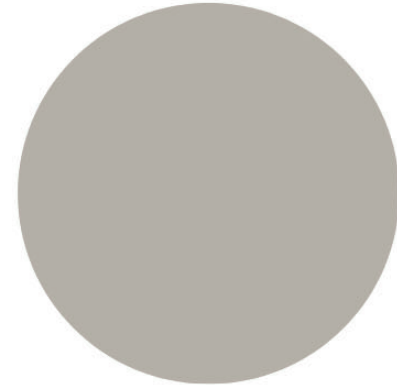
DATE: 11/29/2023  
PROJECT NO.: 22026



**PT1** - SHERWIN WILLIAMS - SEALSKIN



**EF1** - DRYVIT - NATURAL WHITE



**EF2** - DRYVIT - GULL GRAY



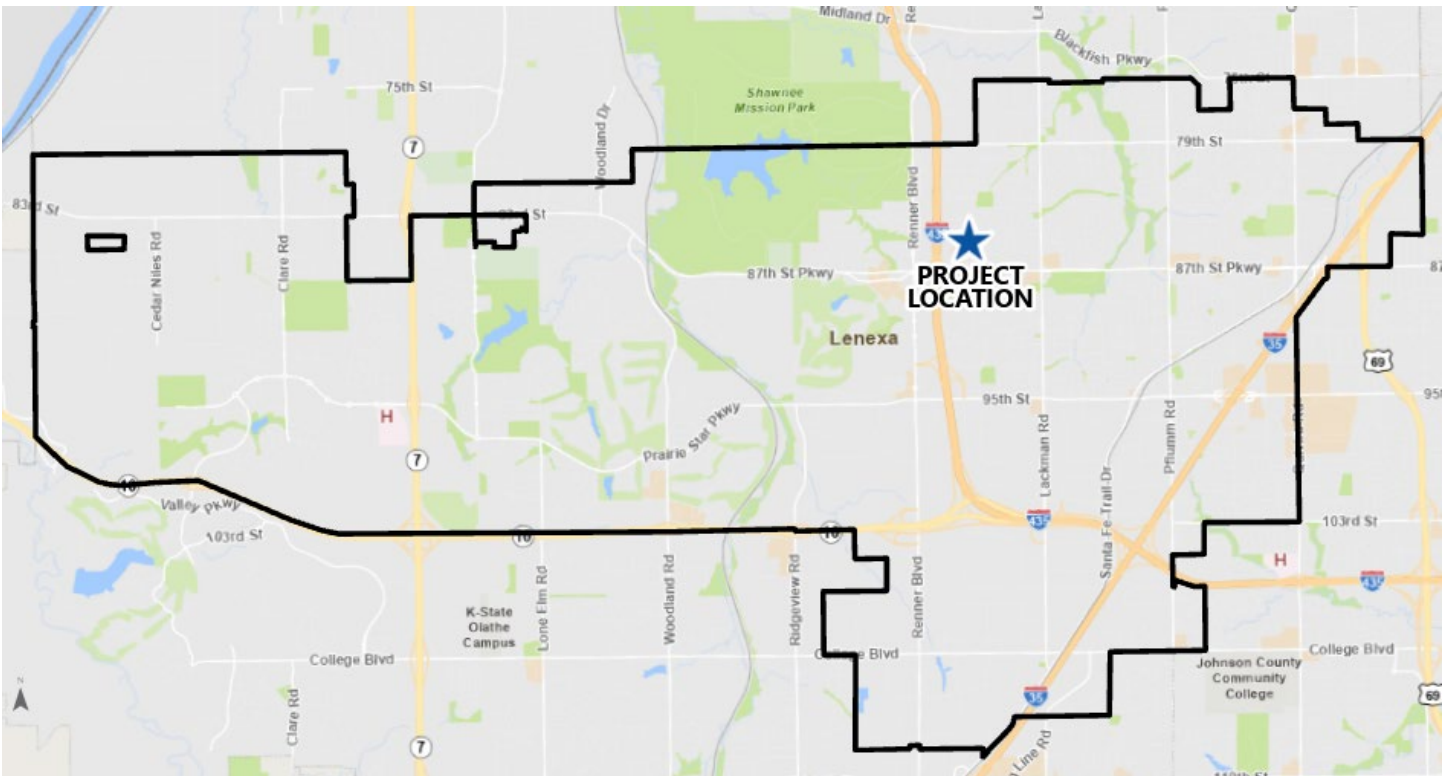
**ST1** - EL DORADO -  
MOUNTAIN LEDGE SIERRA



**MT1** - DIZAL FAUX WOOD - ALUMINUM  
SIDING - PERUVIAN WALNUT BROWN

## ARISE SALES CENTER

<b>Project #:</b>	PL23-24F	<b>Location:</b>	Near the northwest corner of W 87 <sup>th</sup> Street Parkway & Maurer Road
<b>Applicant:</b>	Dan Foster, Schlagel & Associates	<b>Project Type:</b>	Final Plan
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Office



### PROJECT SUMMARY

The applicant proposes to construct a 4,542 SF building with a planned future expansion of 1,250 SF near the northwest corner of W 87<sup>th</sup> Street Parkway and Maurer Road. The building will be used as a design and sales center for Arise Homes and is classified as an office use. At its greatest height, the structure will be 29 feet tall. The development includes construction of a new parking lot with 47 stalls and access through the commercial center to Maurer Road to the east. The applicant requests approval of the final plan and three deviations related to freeway setback and non-street perimeter setbacks. Staff supports the requested deviations. This project does not require a Public Hearing.

### STAFF RECOMMENDATION: APPROVAL

## SITE INFORMATION

The subject property is located in the vicinity of the northwest corner of W 87<sup>th</sup> Street Parkway and Maurer Road. This commercial center was originally developed in the late 1980s and parcels within it have been gradually platted since then. Lenexa Point, First Plat (PT16-25F) was approved in 2016 and Lenexa Point, Second Plat (PT23-24F) was approved in 2023. The current final plan proposal is for an unplatted piece of land between Lenexa Point, Second Plat to the north and Lenexa Point, First Plat to the south. The subject site is not platted but will need to be platted and recorded before a building permit is issued.

A preliminary plan for a drive-thru restaurant use (PL08-11P) was approved in 2008 but a final plan was never submitted.

A revised preliminary plan for this site was included as part of the final plan (PL16-11F) application for the dental office building located to the south. The revised preliminary plan showed a restaurant use for the site; however, the general layout was consistent with what is now being proposed. Staff made the determination to allow the current application to move forward to final plan stage based on the revised preliminary plan.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
1.12	4,542 (current) 1,250 (future)	CP-2	Community Retail

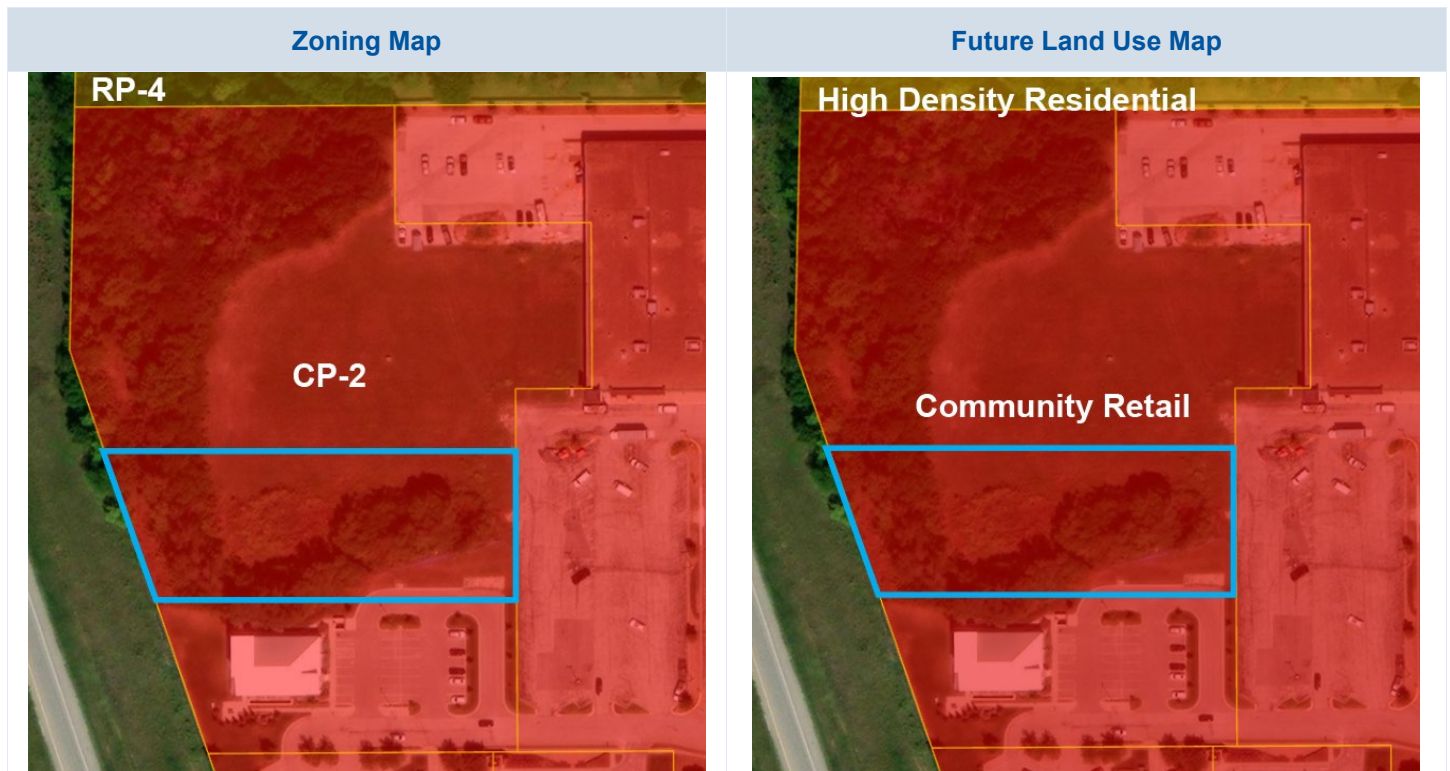


**Exhibit 1: Aerial image of subject site.**

## LAND USE REVIEW

The activities proposed at the Arise Sales Center fall under the category of office according to the Unified Development Code (UDC). Office is an allowed use in the CP-2, Planned Community Commercial Zoning District.

The subject parcel is designated as Community Retail on the Future Land Use Map in the Comprehensive Plan. The proposed use is compatible in the context of existing land uses.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Community Retail	CP-2, Planned Community Commercial	Undeveloped
<b>North</b>	Community Retail	CP-2, Planned Community Commercial	Undeveloped / Self-storage
<b>South</b>	Community Retail	CP-2, Planned Community Commercial	Retail
<b>East</b>	Community Retail	CP-2, Planned Community Commercial	Retail
<b>West</b>	I-435 Right-of-Way	N/A	I-435 Right-of-Way



## FINAL PLAN REVIEW

This is a final plan application for a single-tenant office building in the Lenexa Point commercial center which includes a grocery store, swim school, fitness center, and indoor entertainment. Three drive-thru restaurants are located south of the private access drive along W. 87<sup>th</sup> Street Parkway. The subject site is the last remaining undeveloped lot. The building is situated on the westernmost area of the site along Interstate 435 with parking east of the building. The site is not platted and will also require a preliminary and final plat approval prior to building permit.

The site plan also shows a proposed one-story building with a floor area of 4,542 SF and a planned expansion on the west side of the building with a floor area of approximately 1,250 SF. Elevations for the future expansion must match the proposed building in material in architectural style. Given the anticipated consistency with design, Staff will review the plans for the addition via a building permit in the future and the addition will not require additional review by the Planning Commission unless the addition design and scope changes substantially.

### DIMENSIONAL STANDARDS

The CP-2 Zoning District has a maximum height limitation of 45 feet. The highest point on the proposed building is the building wall that contains the chimney in a portion of it at 29 feet tall. This is compatible with the surrounding context. The site does not comply with setback requirements and the applicant requested deviations, which are further discussed in the *Deviations* section of the Staff Report.

TABLE 2: CP-2 PROPERTY DEVELOPMENT REGULATIONS		
Standard	Required Minimum Setback	Proposed
Maximum Height	45 feet	29 feet
Minimum Open Space	25% of lot area	51% of lot area

### ACCESS, TRAFFIC, AND PARKING

Access to the site is from a private drive that intersects Maurer Road. The private drive also serves the existing commercial center. Maurer Road is classified as a collector roadway. There is no access from I-435. A sidewalk connection extends from the main entrance of the building eastward to link with the existing sidewalk network planned to be built as part of the Smart Storage development. A cross-access drive connects to the dental office. An internal sidewalk connection also links to the south to connect with the sidewalks in the dental office site. Vehicular and bicycle parking are provided to meet or exceed the minimum standard as illustrated in Table 3.

TABLE 3: PARKING ANALYSIS				
Requirement	Parking Formula	Required Parking	Proposed Parking	Difference
<b>Vehicle Parking: Office</b>				
Current Floorplan (4,542 SF)	1 space per 250 SF	18	47	+29
With Future Expansion (5,792 SF)	1 space per 250 SF	23	47	+24
<b>Bicycle Parking</b>	1 space per 10, 000 SF	1	1	0

## STORMWATER

Stormwater improvements have been previously constructed for this site by the overall commercial center, leaving this site only needing to appropriately tie into the existing and/or approved and under construction systems. Staff notes that with the completion of this pad site, the significant portions of parking lot drainage being conveyed via surface flow in an open channel will be generally replaced and handled via below-grade piping systems.

## FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

## LIGHTING

Pole-mounted fixtures are planned for the parking lot. The fixtures will be downcast cutoff fixtures. The planned mounting height is 25 feet. Concrete bases used for pole-mounted fixtures shall not exceed two feet in height.

## LANDSCAPING

Perimeter plantings and internal parking lot landscaping are required in accordance with [Section 4-1-D-2](#) Landscaping, Buffering and Site Design of the UDC. The landscape plan provides sufficient landscaping to meet code requirements for the parking lot and perimeter plantings. Additional landscaping beyond code requirements is provided along the base of the building to the north, east and south, as well as along the north and south perimeters. The landscape plan shows that some of the required shrubs on the west side are located at the base of the building. When the 1,250 S.F. addition is built, these shrubs as well as any other disturbed landscaping will need to be relocated in the same kind and quantity. There are eight trees on the west side of the site between the building and the freeway that Staff inquired about preserving, to which the applicant responded that the existing trees are volunteer trees in poor condition and due to anticipated grading only one tree at the northwestern corner of the site can be preserved.

A black, vinyl-coated chain-link fence matching fencing on adjacent properties will be located on the west side of the property adjacent to Interstate 435.

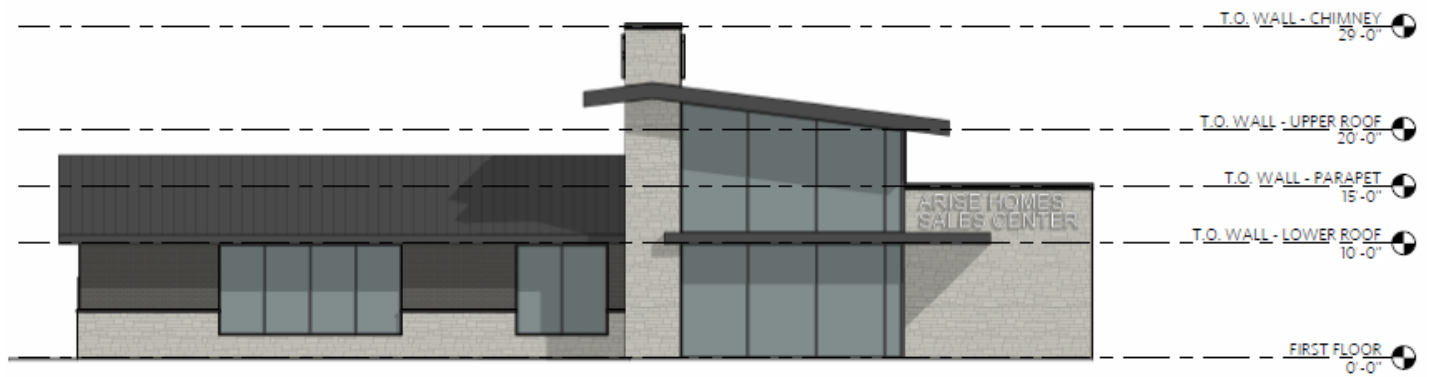
## ARCHITECTURE

The proposed building is one story at ground level with high ceilings and windows in the entryway area. External architectural materials include thin limestone veneer and face brick. Roofing materials include standing seam metal and composite wood soffits. Additional detailing includes a limestone veneer chimney wall and windows that reach 20 feet high on the west elevation, which is interrupted by a lower roof at the 10-foot line. A trash enclosure is planned for the northeast corner of the parking lot. The trash enclosure will match the materials of the building.

The elevations provided also show what the applicant is calling an extended chimney feature that extends above the primary roofline and shows a conceptual sign in the elevations. Staff reviewed the proposed sign location to determine whether it would be considered a roof sign, which is prohibited under [Section 4-1-E-3-F, Prohibited Signs and Attention-Attracting Devices](#) of the UDC. The conclusion of this review was that the element the sign is placed on is considered a wall as it extends the length of the building on the north and south elevations and has a separate eave line. If the design of the building were to change so that the wall becomes a narrow chimney column, the sign would then be considered a roof sign. All signs require individual review and permitting through the sign permitting process and are not approved as part of this final plan.



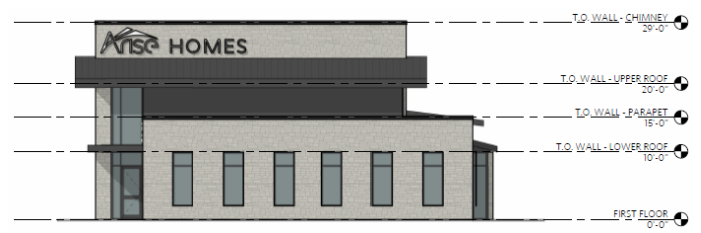
**Exhibit 2. East elevation (main entry).**



**Exhibit 3: West elevation.**



**Exhibit 4. North elevation.**



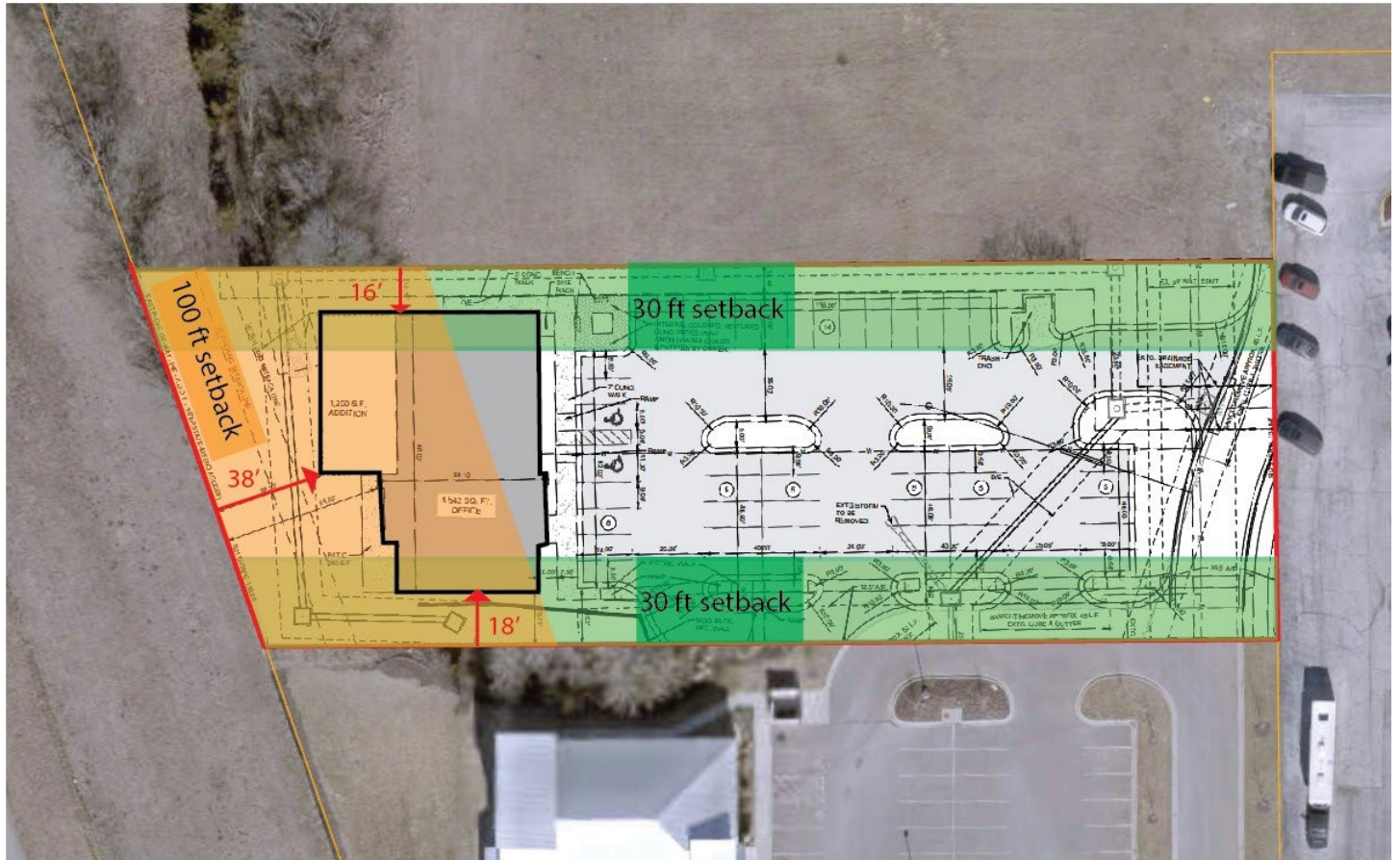
**Exhibit 5. South elevation.**



**Exhibit 6: Trash enclosure elevations.**

## DEVIATIONS

The applicant requested three deviations which are related to freeway setback and setback between the north and south property lines. Staff supports the requested deviations. Further analysis is provided below.



*Exhibit 7: Illustrated Setback Requirements for Requested Deviations.*

### FREEWAY SETBACK

The applicant requests a deviation from the minimum setback from a highway or freeway to allow the building to encroach into the required 100-foot setback (shown in red on Exhibit 7). Staff supports the encroachment of the building into the setback, citing precedent set by the neighboring structures to the north and south which obtained similar deviations to maintain a 30-foot setback. The proposed freeway setback promotes uniformity in the area.

**TABLE 4: FREEWAY SETBACK ANALYSIS**

Yard	Required Minimum Setback	Proposed Setback	Difference
Building Setback from Freeway (West)	100 feet	38 feet	- 61 feet

## NON-STREET PERIMETER SETBACK

The applicant requests a reduction from the building setback requirement along perimeter property lines that are not adjacent to a street for the areas between the north and south property lines. The proposed layout creates a setback of 16 feet to the north and 18 feet to the south of the building. This is not a standalone lot but rather part of the larger Lenexa Point shopping area and these two requested setback deviations are for internal lot lines within the commercial center. The requested deviations are consistent with how commercial centers have been approved in the past and would have no negative impacts on surrounding properties. The reduced setbacks also allow for additional parking that could be beneficial for future uses in the building if the proposed use ever changes. For these reasons, Staff supports the requested setback deviations.

**TABLE 5: PERIMETER SETBACK ANALYSIS**

Yard	Required Minimum Setback	Proposed Setback	Difference
Building Setback (North)	30 feet	16	- 14 feet
Building Setback (South)	30 feet	18	-12 feet

## REVIEW PROCESS

- The Planning Commission is the final authority for approval of this project.
- The addition will not require future Planning Commission approval prior to permitting if it is substantially similar to the footprint shown on this final plan.
- The applicant should inquire about additional City requirements, such as permits and development fees.

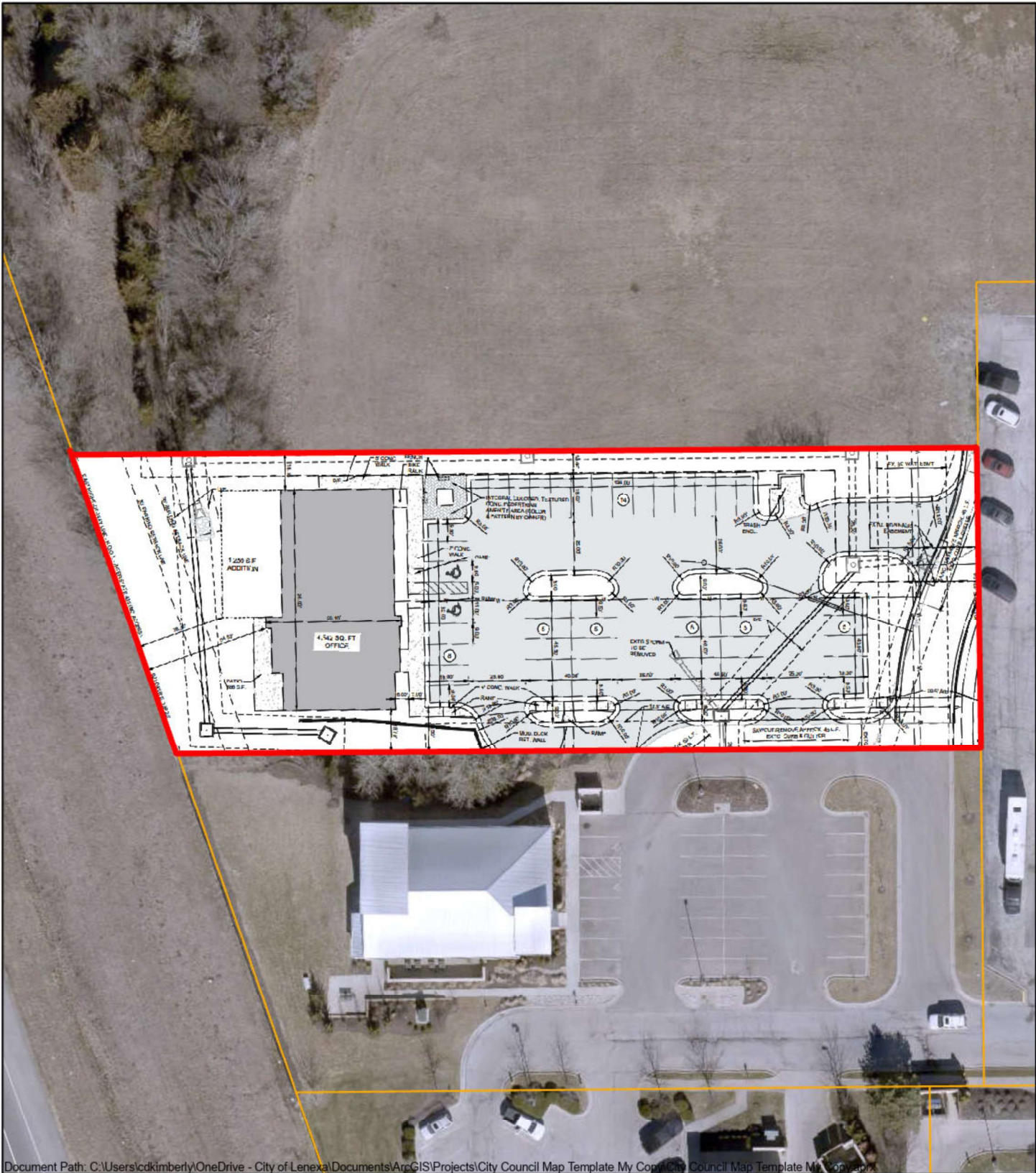
## RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the proposed final plan for Arise Sales Center.**

- The proposed use is an office building.
- A preliminary and final plat of the site is required prior to submittal for building permits.
- The project is consistent with Lenexa’s goals through **Responsible Economic Development** to create **Thriving Economy**.

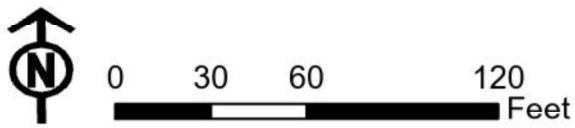
## FINAL PLAN

Staff recommends **approval** of the final plan for PL23-24F – **Arise Sales Center** at near the northwest corner of W. 87<sup>th</sup> Street Parkway and Maurer Road for an office use with three deviations related to required setbacks.

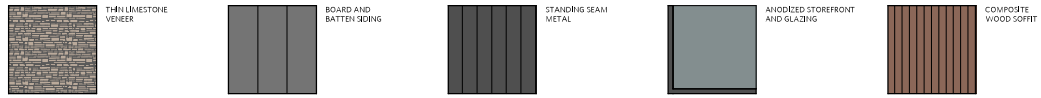


Data Source: City of Lenexa and Johnson County Kansas  
 For further information, please call 913-477-7500

# Arise Sales Center PL23-24F



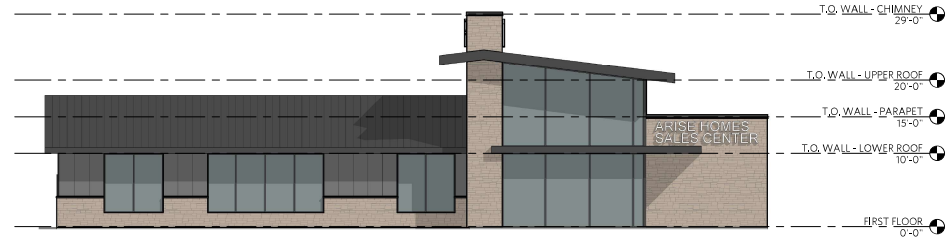
**MATERIALS**



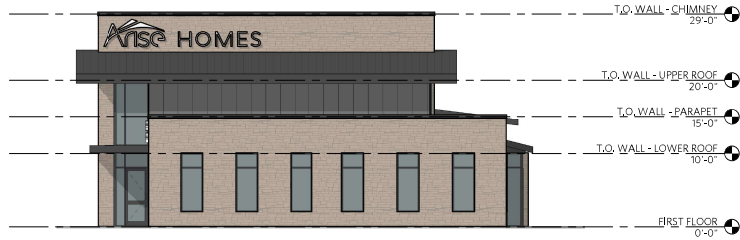
**05 | TRASH ENCLOSURE**  
 1/4" = 1'-0"



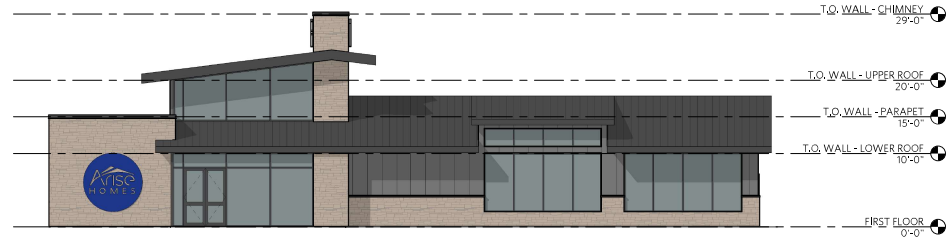
**04 | NORTH ELEVATION**  
 1/8" = 1'-0"



**03 | WEST ELEVATION**  
 1/8" = 1'-0"



**02 | SOUTH ELEVATION**  
 1/8" = 1'-0"



**01 | EAST ELEVATION**  
 1/8" = 1'-0"

**ARISE SALES CENTER**  
 W. BETH ST. @ ARIS  
 LENEXA, KS 66219

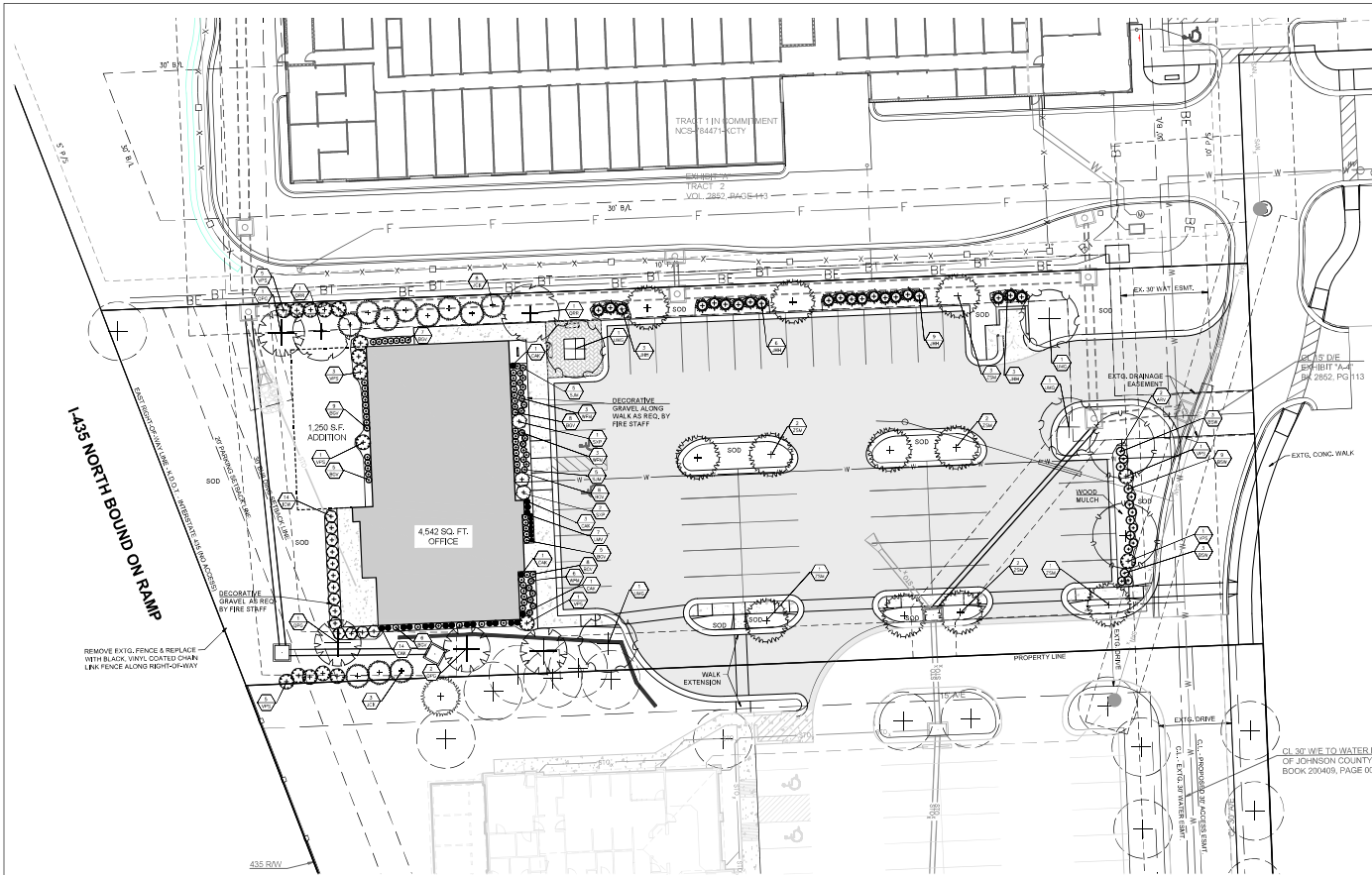


REV	ISSUE	DATE
	PLANNING SUBMITTAL	2023.11.27

EXTERIOR ELEVATIONS  
**A200**







- NOTES**
- UTILITY INFORMATION SHOWN IS DESIGNED LOCATION OR LOCATIONS BASED ON UTILITY LOCATES. AS BUILT LOCATIONS MAY VARY. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO COMMENCING LANDSCAPE INSTALLATION. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS OR OBSTRUCTIONS.
  - QUANTITIES INDICATED ON THE PLAN ARE FOR CONVENIENCE ONLY. CONTRACTOR SHALL VERIFY ALL PLANT QUANTITIES PRIOR TO PLANTING. NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES. THE PLANT QUANTITIES AND NUMBER OF SYMBOLS SHALL SUPERSEDE QUANTITIES IN THE SCHEDULES.
  - ALL PLANT MATERIAL SHALL COMPLY WITH THE CITY OF LENEXA STANDARDS AND AREAS THE AMERICAN STANDARD FOR NURSERY STOCK.
  - ALL TREES SHALL MEET THE SIZE REQUIREMENTS OF THE LENEXA ORDINANCE. ALL TREES SHALL BE CALIPIPED AND UNROOTED TREES SHALL BE REJECTED.
  - ALL SHRUBS TO BE UTILIZED FOR SCREENING SHALL BE 24" HEIGHT AT TIME OF PLANTING.
  - ALL PLANTING BEDS CONTAINING SHRUBS, GROUND COVER, PERENNIALS, ANNUALS SHALL BE IN A PLANTING BED WITH 2" MIN. DEPTH OF MULCH AND A "SCOUT" EDGE.
  - ALL TREES SHALL HAVE A MIN. 1" DB. AREA THAT HAS 2" MIN. DEPTH OF "WOOD MULCH".
  - ALL TURF AREAS SHALL BE SOODED UNLESS INDICATED ON THE PLANS.
  - ANY DEVIATION FROM THE APPROVED LANDSCAPE PLAN SHALL REQUIRE WRITTEN APPROVAL OF THE LANDSCAPE ARCHITECT AND THE CITY OF LENEXA. PRIOR TO INSTALLATION.
  - THE LANDSCAPE ARCHITECT AND OWNER SHALL APPROVE GRADES AND CONDITION OF SITE PRIOR TO SOODING OPERATIONS.
  - INSTALLATION AND MAINTENANCE OF LANDSCAPING SHALL COMPLY WITH THE CITY OF LENEXA STANDARDS.
  - ALL PLANT MATERIAL SHALL BE INSTALLED TO ALLOW A MINIMUM CLEARANCE BETWEEN PLANT AND ADJACENT PAVEMENT OF 1 FT. FOR PERENNIALS AND GROUND COVER AND 1.5 FT. FOR SHRUBS. A 2 FT. CLEARANCE FEET FROM BACK OF CURB TO THE CENTER OF SHOULDER OR CAR DRIVEWAYS IS REQUIRED AT ALL PARKING ISLANDS AND PERIMETERS.
  - AFTER COMPLETE INSTALLATION OF ALL PLANT MATERIAL AND SOO THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT THAT THE WORK IS COMPLETE AND READY FOR REVIEW. THE LANDSCAPE ARCHITECT SHALL REVIEW THE LANDSCAPE INSTALLATION TO DETERMINE COMPLIANCE WITH THE APPROVED PLANS. WHEN THE LANDSCAPE ARCHITECT SHALL PROVIDE A SIGNED AND SEALED LETTER TO THE CITY STATING THAT ALL LANDSCAPE PLANTINGS HAVE BEEN INSTALLED PER THE APPROVED PLAN.
  - AN IRRIGATION SYSTEM OR HOSE BIBS MUST BE WITHIN PROXIMITY OF ANY LANDSCAPE AREA AS REQUIRED BY LENEXA CODES. CONTRACTOR TO PROVIDE PREPARATION/WATERING PLANS AS SEEN BUILT.
  - IN THE EVENT OF WORK IN OR ON THE JOB SANITARY HAZARD, ANY TREES OR PLANTINGS PLACED WITHIN THE SEWER EASEMENT MAY BE REMOVED WITHOUT REPLACEMENT OR COMPENSATION. THESE AND SHALL BE REPLACED BY THE PROPERTY OWNER AS REQUIRED BY THE CITY.

**SCHLAGEL**  
 ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS  
 4625 W. 119TH ST., SUITE 100  
 LENEXA, KS 66245  
 (817) 452-1558 • FAX: (817) 452-0400  
 WWW.SCHLAGELASSOCIATES.COM

PREPARED BY:  
 SCHLAGEL & ASSOCIATES P.A.

ARISE SALES CENTER  
 FINAL DEVELOPMENT PLAN  
 8620 MAURER ROAD LENEXA, KANSAS

LANDSCAPE CALCULATION TABLE			
DESCRIPTION	ORDINANCE REQUIREMENT	LANDSCAPE REQUIRED	LANDSCAPE PROVIDED
PERIMETER PLANTING ALONG WEST SIDE EAST BOUNDARY PRIVATE DRIVE 70' L.F.	2 SHADE TREES PER 100 FT. WEST SIDE 12 SHRUBS PER 100 FT WEST SIDE	2 SHADE TREES (WEST SIDE) 12 SHRUBS (WEST SIDE)	2 SHADE TREES (WEST SIDE) 12 SHRUBS (WEST SIDE ONLY) ADJACENT OWNER RESPONSIBLE FOR EXISTING PARKING SCREENING SHRUBS
PERIMETER PLANTING ALONG WEST PROPERTY BOUNDARY - FREETWAY 138.25 L.F.	PER THE APPROVED PRELIMINARY PLAN 2 SHADE TREES PER 50 FT	2 SHADE TREES 5 EVERGREEN TREES 21 SHRUBS	2 SHADE TREES 4 EVERGREEN TREES 21 SHRUBS
INTERNAL PARKING LOT 20,864 S.F. PARKING AREA	LANDSCAPE AREA = 15% OF PARKING AREA 1 TREE PER 500 FEET OF LANDSCAPE AREA WITH AT LEAST 1 TREE PER ISLAND	1,965.30 S.F. (L.S. AREA) 15% 8 TREES (PER ISLAND)	2,706.18 S.F. 8 TREES (PER ISLAND) 1 TREE RELOCATED DUE TO WALK
PERIMETER PLANTING ALONG LOT LINES NORTH BOUNDARY 384.20 L.F. (153' BLDG. & 242' PARKING) SOUTH BOUNDARY 137 L.F. (BLDG.)	1 TREE PER 100 FEET ALONG BUILDING 1 TREE AND 9 SHRUBS PER 100 FEET ALONG PARKING	NORTH BOUNDARY 5 TREES 21 SHRUBS SOUTH BOUNDARY 2 TREES	5 TREES 21 SHRUBS SOUTH BOUNDARY 2 TREES

- SHADE TREES**
- ARV 2 EA. Acer rubrum 'Sun Valley' Sun Valley Red Maple 2.5" Cal. B&B
  - OPG 4 EA. Quercus palustris Green Pillar Green Pillar Oak 2.5" Cal. B&B
  - QRR 2 EA. Quercus robur x bicolor (aka x ware) 'Long' Regal Prince Oak 2.5" Cal. B&B
  - UMG 4 EA. Ulmus 'Morton Glasy' Triumph Elm 2.5" Cal. B&B
  - ZSM 11 EA. Zelkova serata 'Musashino' Musashino Columnar Zelkova 2.5" Cal. B&B
- EVERGREEN TREES**
- JCK 11 EA. Juniperus chinensis 'Keteleeri' Keteleeri Juniper 6' Mt. B&B
- SHRUBS**
- BOV 56 EA. Buxus x 'Green Velvet' Green Velvet Boxwood 5 gal. Cont.
  - BSW 15 EA. Buxus sinica var. insularis 'Wintergreen' fl. a microphylla var. Wintergreen Boxwood 5 gal. Cont.
  - ICW 14 EA. Ilex x meserveae 'Heckenstar' Plant Patent #14,308 Castle Wall® Blue Holly (Cas. Spire pl) 7 gal (4 ht. Cont.
  - JHH 21 EA. Juniperus horizontalis 'Hughes' Hughes Juniper 5 gal. Cont.
  - SJM 12 EA. Spiraea japonica 'Walburna' (AKA x tumalada 'Magic Carpet' Magic Carpet Spiraea 5 gal. Cont.
  - SXP 3 EA. Syringa x 'Penda' Blooming Lilac 5 gal. Cont.
  - VPS 17 EA. Viburnum plicatum tomentosum 'Summer Snowflake' Summer Snowflake Viburnum 5 gal. Cont.
  - WFM 12 EA. Weigela florida 'My Monet' My Monet Weigela 5 gal. Cont.
- GRASSES**
- CAK 20 EA. Calamagrostis x acutifolia 'Karl Foerster' Karl Foerster Grass 2 gal. Cont.
- PERENNIALS**
- LMV 7 EA. Liriope muscari 'Variegata' Variegated Lijurf 1 gal. Cont.

REVISIONS

NO.	DATE	DESCRIPTION
1	11/20/2023	ISSUED LANDSCAPE UPDATE LANDSCAPE PANEL

APPROVED BY: [Signature]

DATE: 11/20/2023

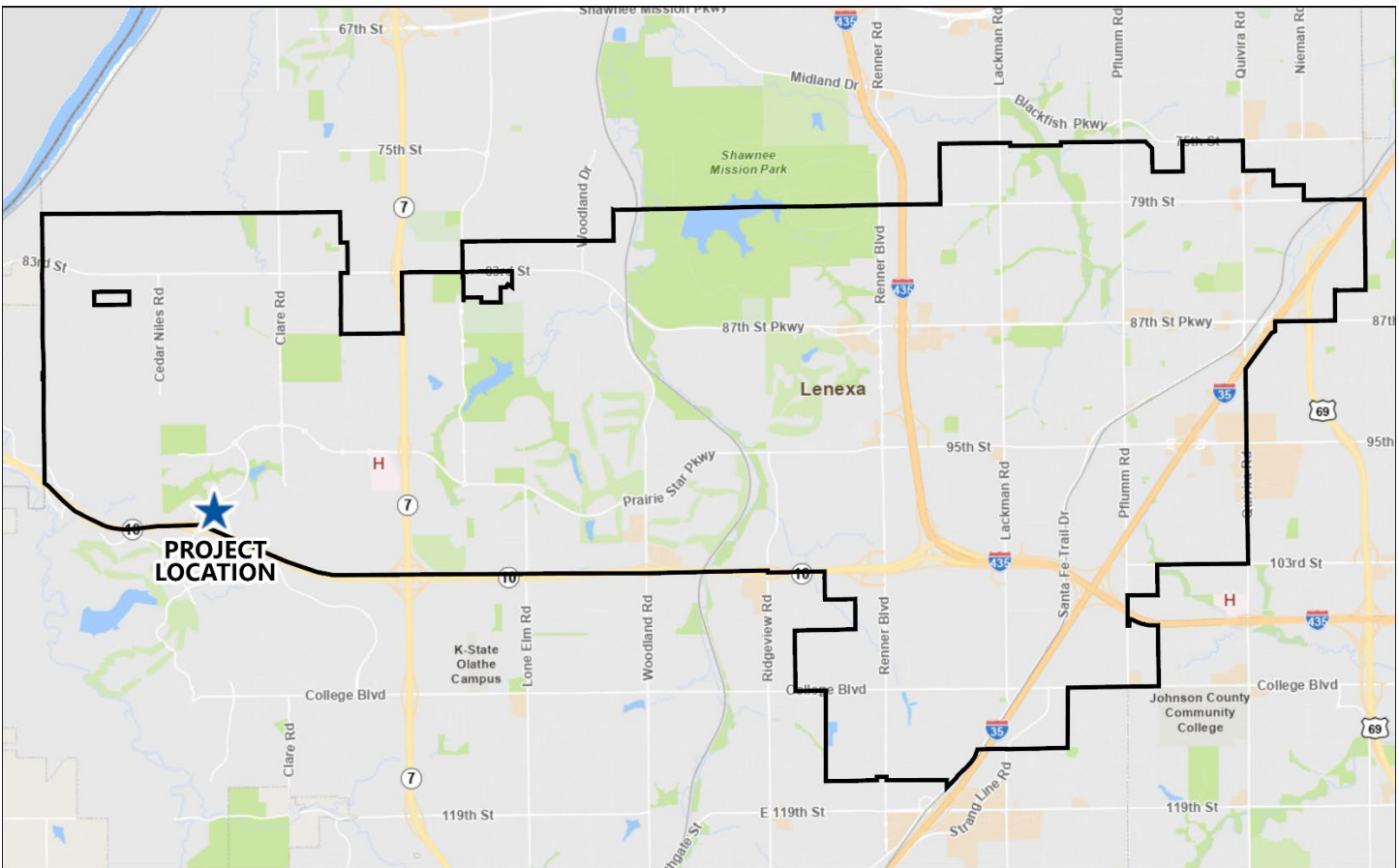
SCALE: 1" = 20'

**L1.1**

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# CEDAR CANYON WEST COMMERCIAL

<b>Project #:</b>	PL23-08P	<b>Location:</b>	Near the northeast corner of Canyon Creek Blvd & K-10 Highway
<b>Applicant:</b>	Dan Foster, Schlagel & Associates	<b>Project Type:</b>	Preliminary Plan/Plat
<b>Staff Planner:</b>	Logan Strasburger	<b>Proposed Use:</b>	Commercial



## PROJECT SUMMARY

The applicant seeks approval of a preliminary plan/plat for Cedar Canyon West Commercial, a commercial development located near the northeast corner of Canyon Creek Boulevard and K-10 Highway, specifically east of the intersection of future 100<sup>th</sup> Street and Canyon Creek Boulevard. The plan includes plans for six lots with one single-story commercial building per lot and one tract. The total building area is 40,550 square-feet. The six proposed buildings include a convenience store with a fueling station, two retail/restaurant buildings with drive-thrus, and three retail/office buildings. The proposed preliminary plan/plat is consistent with the approved concept plan for Cedar Canyon West tracts one and two, which was approved by the Governing Body in February 2023. The applicant requests four deviations from the Unified Development Code (UDC) related to setback and landscape requirements. This project does not require a Public Hearing.

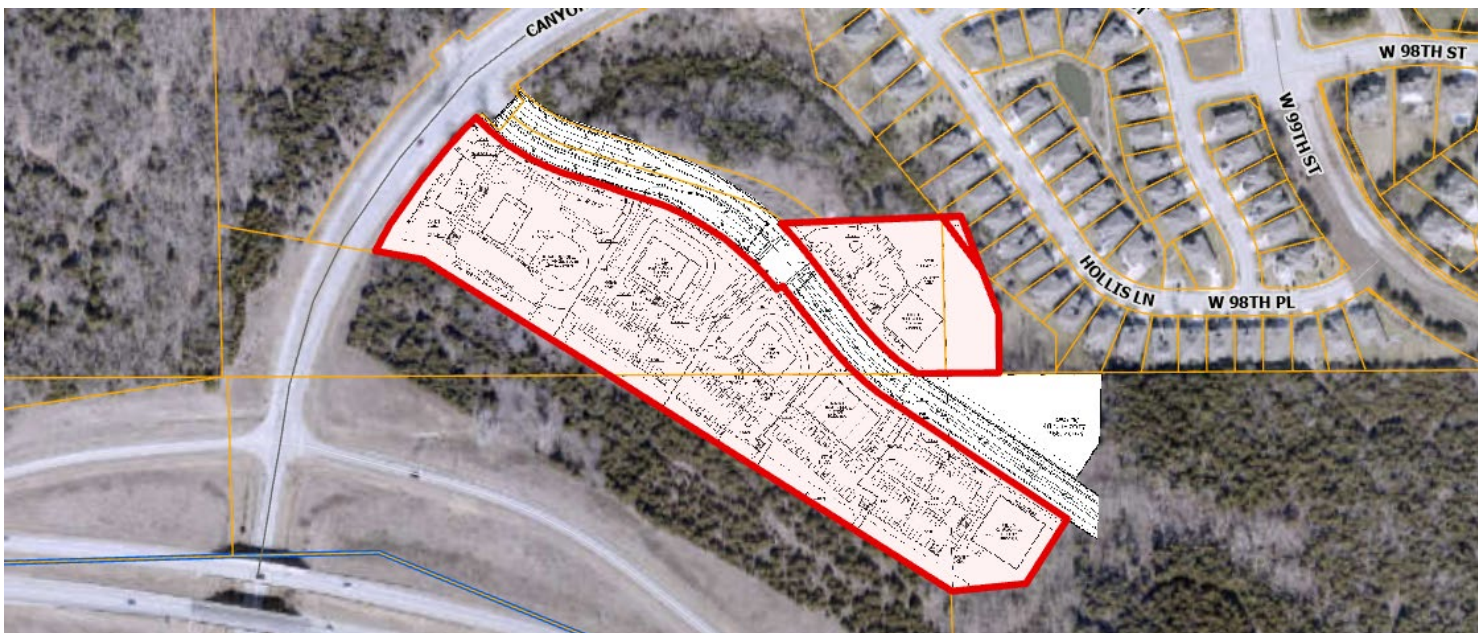
**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

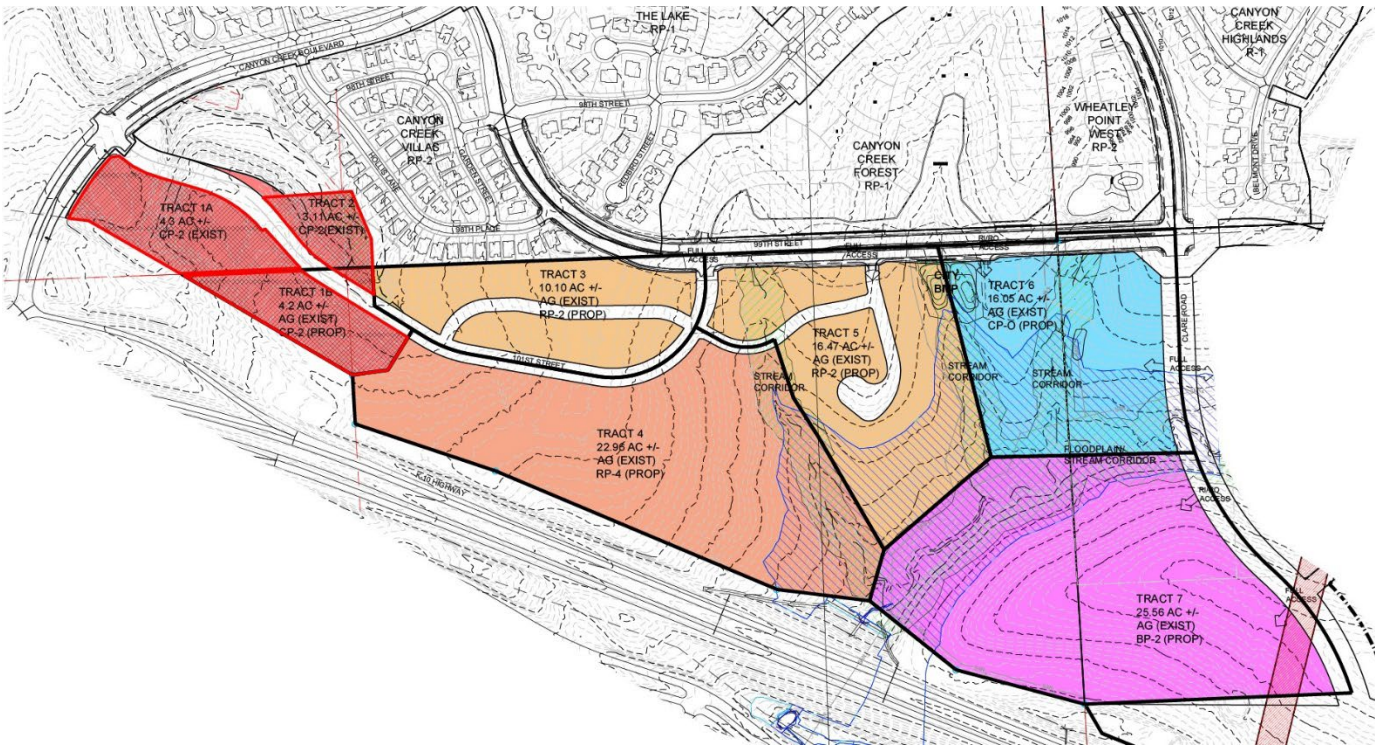
The subject site is 12.82 acres of undeveloped land located near the northeast corner of Canyon Creek Boulevard and K-10 Highway within the CP-2 Zoning District. Relevant planning approvals include:

- February 7, 2023: Governing Body approves a concept plan and rezoning of a 112-acre mixed-use development known as Cedar Canyon West (PL22-04CP and RZ22-09). The subject area, which includes Tract 1 and Tract 2 from the approved concept plan, was rezoned from AG, Agricultural District and CP-2, Planned Community Commercial District to CP-2, Planned Community Commercial District. See Exhibit 2 and 3.
- September 19, 2023: Governing Body approves a preliminary and final plat for a public street (100<sup>th</sup> Street) connecting Canyon Creek Boulevard and West 99<sup>th</sup> Street (PT23-04P & PT23-18F).

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
12.82	40,550	CP-2	Community Commercial Center & Office, Research & Development



**Exhibit 1: Aerial Image of Subject Site**



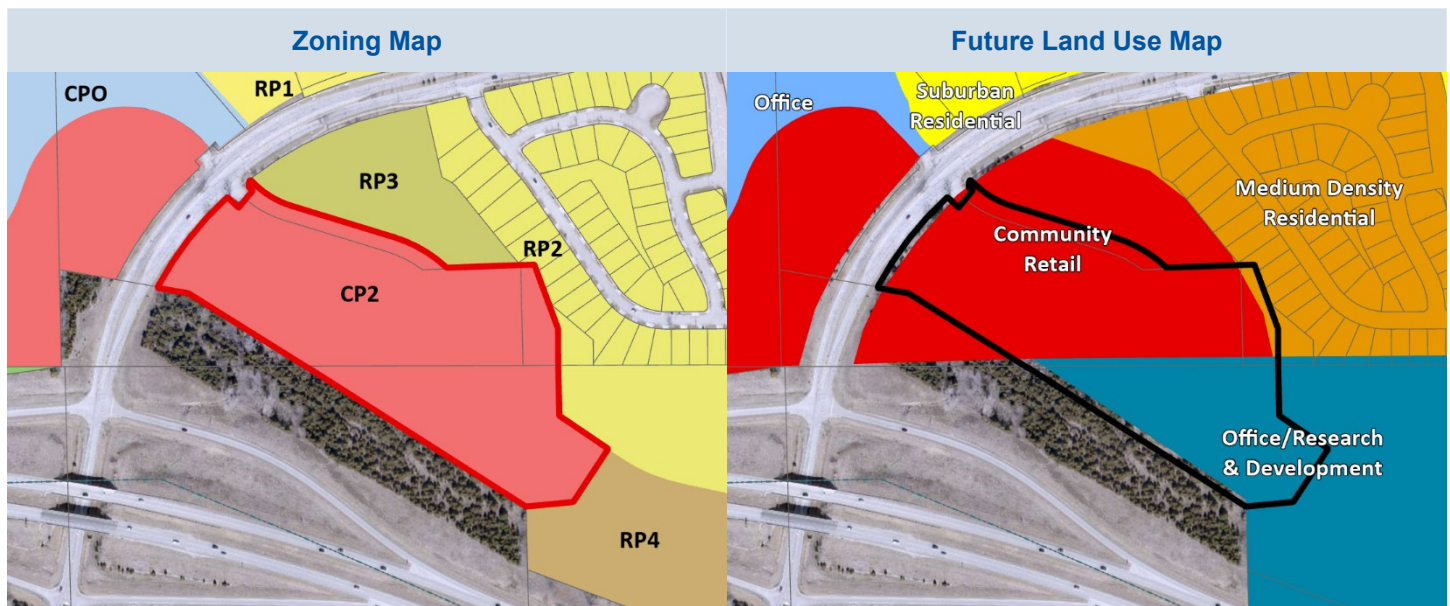
**Exhibit 2: Approved map of tracts within Cedar Canyon West, a 112-acre mixed-use development. The subject area is hatched and outlined in red. (Tract 1a, Tract 1b, and Tract 2).**



**Exhibit 3: Approved Cedar Canyon West Concept Plan from February 2023. Subject site outlined in red.**

## LAND USE REVIEW

The proposed development includes commercial uses such as service station, retail, office, and restaurant uses. The proposed land use was approved as a larger concept plan known as Cedar Canyon West. The Governing Body affirmed the rezoning based on Staff determination that the proposed uses and surrounding uses would be compatible. The current Comprehensive Plan calls for office uses at this location; however, given that the Governing Body recently approved the rezoning and concept plan for Cedar Canyon West, Staff is recommending to the consultant working on the Comprehensive Plan that the Future Land Use designation be updated to align with the approval of Cedar Canyon West, which would change the designation from office to commercial in this area. The new Comprehensive Plan is slated to be completed in early 2024.

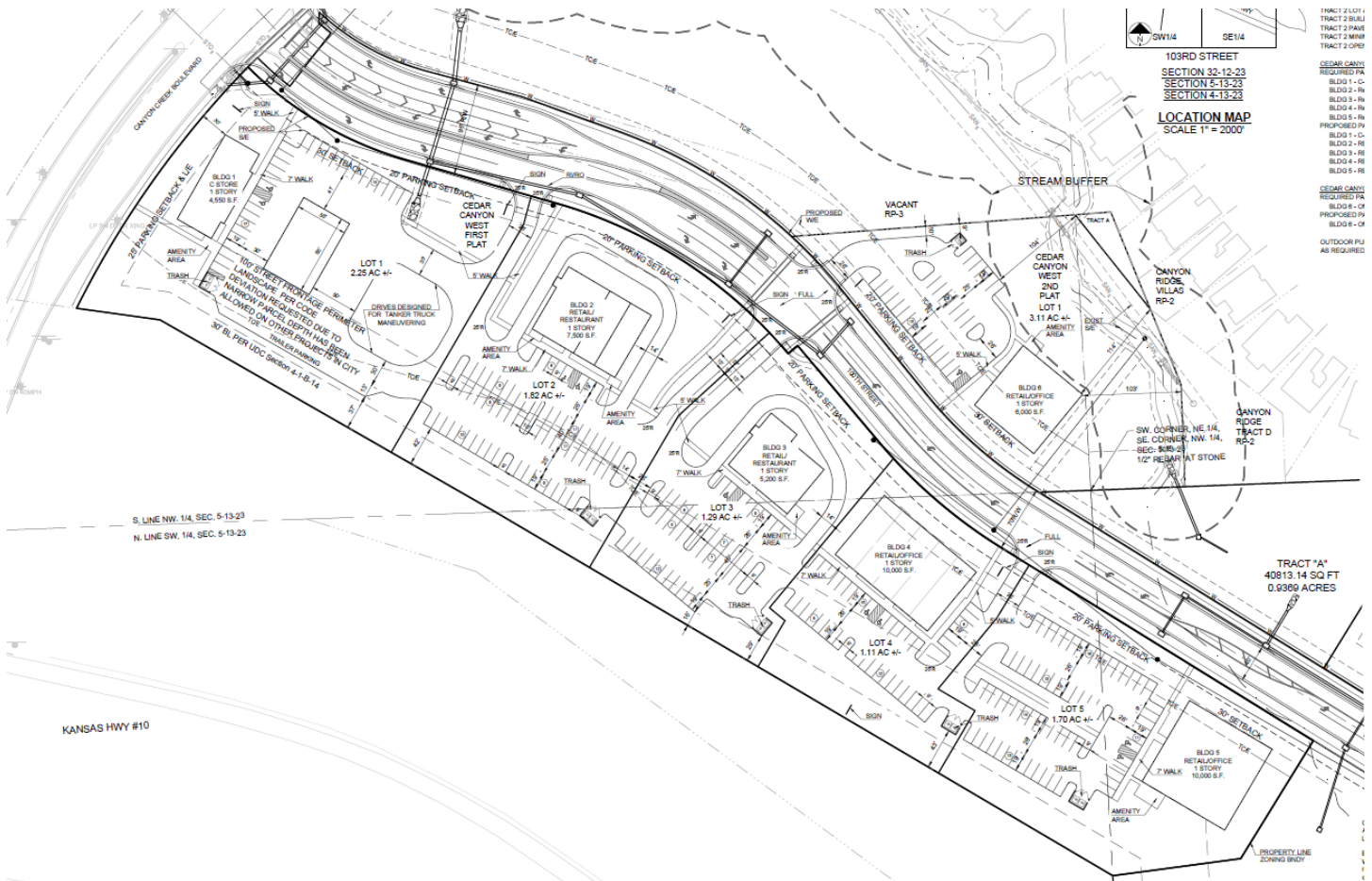


**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Community Retail; Office, Research & Development; Medium-Density Residential	CP-2, Planned Community Commercial District	Undeveloped
<b>North</b>	Medium Density Residential	RP-3, Residential District, Residential Planned (Medium-High Density) District; RP-2, Residential District, Residential Planned (Intermediate-Density) District	Residential, Undeveloped
<b>South</b>	K-10 Highway Right-of-Way	K-10 Highway Right-of-Way	K-10 Highway Right-of-Way
<b>East</b>	Office, Research & Development; Medium-Density Residential	RP-4, Planned Residential (High-Density) District, RP-2, Planned Residential (Intermediate-Density) District	Residential, Undeveloped
<b>West</b>	Community Retail; Office	CP-O, Planned General Office District; CP-2, Planned Community Commercial District	Undeveloped

## PRELIMINARY PLAN REVIEW

The plan features six lots, all with one single-story building per lot. The buildings range in size from 4,500 to 10,000 square-feet. The proposed layout is consistent with the approved concept plan and rezoning (PL22-04CP & RZ22-09) for Tract 1 and Tract 2 of Cedar Canyon West. The proposed uses complement existing single-family and future multifamily residential development while transitioning development southerly toward K-10 Highway.



**Exhibit 4: Excerpt of the Proposed Preliminary Plan.**

The preliminary plan is contingent on the platting for 100<sup>th</sup> Street, which is the access point for the lots. The site contains a stream and stream buffer area. A portion of the stream buffer lies within proposed Tract B, Lot 1 and Tract C which shares property lines with adjacent residential properties. The site meets open space requirements as each lot has dedicated amenity spaces. Landscape requirements are met and further landscaping details are required for all future final plans for each lot.

### DIMENSIONAL STANDARDS

The plans comply with dimensional standards other than the requested deviations related to setbacks for the gas island pump, perimeter plantings, and freeway setback. The approved concept plan contemplated a deviation for the freeway setback requirement. The deviation requests are discussed in greater detail within the *Deviations* section of this Staff Report.

**PUBLIC IMPROVEMENTS**

The project includes the construction of W. 100<sup>th</sup> Street. The street will include associated infrastructure such as sidewalks and lighting. These public improvements will be completed by the developer. W. 100<sup>th</sup> Street must be substantially complete prior to issuance of commercial building site permits and accepted for maintenance prior to occupancy.

**TRAFFIC, ACCESS, AND PARKING**

This site will be accessed from future W. 100<sup>th</sup> Street that connects Canyon Creek Boulevard to W. 99<sup>th</sup> Street. There will be two full access entrances from W. 100<sup>th</sup> Street, one of which will serve Tract B, Lot 1 on the north side of 100<sup>th</sup> Street. There will also be one right-in, right-out access point from eastbound 100<sup>th</sup> Street that provides access closest to the service station and connects to the remainder of the shopping center.

Sidewalks are planned along W. 100<sup>th</sup> Street and along Canyon Creek Boulevard. An internal sidewalk will be provided between Canyon Creek Boulevard and Tract A, Lot 1 (service station). The public sidewalk network along 100<sup>th</sup> Street will connect to internal sidewalks with direct connections to all buildings, parking areas, and amenity spaces.

Bicycle parking is required for final plan applications for each lot. A traffic study may be required in the future for final plan applications for each lot depending on the proposed use.

A cross-access and shared parking agreement is required at time of final plat approval.

Parking will be reviewed in greater detail at the time when final plans are submitted for each lot. The preliminary plan contemplates service station, retail, restaurant, and office uses. Table 2 provides calculations for parking based on anticipated uses and square footages on the preliminary plan.

<b>TABLE 2: PARKING ANALYSIS</b>				
<b>Requirement</b>	<b>Parking Formula</b>	<b>Required Parking</b>	<b>Proposed Parking</b>	<b>Difference</b>
Shopping Center (Tract A, Lots 1-5)	1 space per 250 SF	189	252	+ 63
Retail/Office (Tract B, Lot 1)	1 space per 250 SF	24	24	0

**STORMWATER**

The applicant submitted a preliminary stormwater management plan/study for the proposed development indicating the intent to meet the City’s stormwater standards and requirements. Generally, the site will provide various best management practices (BMPs) such as detention basins, underground detention, and native vegetation to meet stormwater detention requirements as well as water quality requirements.

The north and northeastern portion of the site, most specifically within Tract B, Lot 1, is generally identified as stream buffer. The applicant set the site’s general infrastructure (parking lots, buildings, etc.) outside of the floodplain and adjacent stream buffer.

Tract C lies completely within the floodplain and streamway buffer and is not to be improved or built upon.

## **FIRE PREVENTION**

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

## **LIGHTING**

A preliminary photometric plan has not been submitted. Details for lighting shall be reviewed at final plan stage.

## **LANDSCAPING**

Section 4-1-D-2 of the UDC requires development to establish trees, landscaping, and buffering to enhance the site for existing and future residents. The intention of landscape requirements is to enhance the value and appearance of the development and to provide a natural buffer between different land uses, and in this case, a buffer between the highway and the subject site and a buffer between the subject site and northerly adjacent residential development.

The applicant intends to preserve as many trees as possible. The applicant proposed additional evergreen trees in lieu of shrubs throughout the site. Additional review of the landscape plan will be conducted during the final plan/plat application.

## **SIGNAGE**

The applicant provided preliminary information related to proposed signage. Staff and the applicant will discuss signage in more detail at final plan stage.

## **ARCHITECTURE**

All six proposed buildings are one-story at ground level and of similar architectural style. Accessory structures, such as the gas canopy, also share similar architectural elements. External architectural materials include grey split face block, brown brick, brown brick veneer, and dark grey stucco. Architectural materials are used in varying degrees on all buildings. Brick and block are used heavily along the base of all buildings and other materials, such as the grey stucco, are used to a lesser degree on the upper portion of the structures. A dark grey accent band is utilized in various ways on all buildings to transition from brick or block to stucco. All windows are aluminum. The gas canopy structure features steel columns with brown brick veneer and the roof trim consists of prefinished grey metal. Rooftop units on all buildings will be screened. Other required functional equipment that is to be placed at ground level, such as utility meters and piping, will be screened from view.

Additional review of proposed architecture will be discussed at time of final plan submission. As required, the applicant provided preliminary architectural elevations and details. The following elevations are more conceptual and therefore, will undergo additional revisions once a final plan is submitted.



GREY SPLIT FACE BLOCK



BROWN BRICK



DARK GREY STUCCO

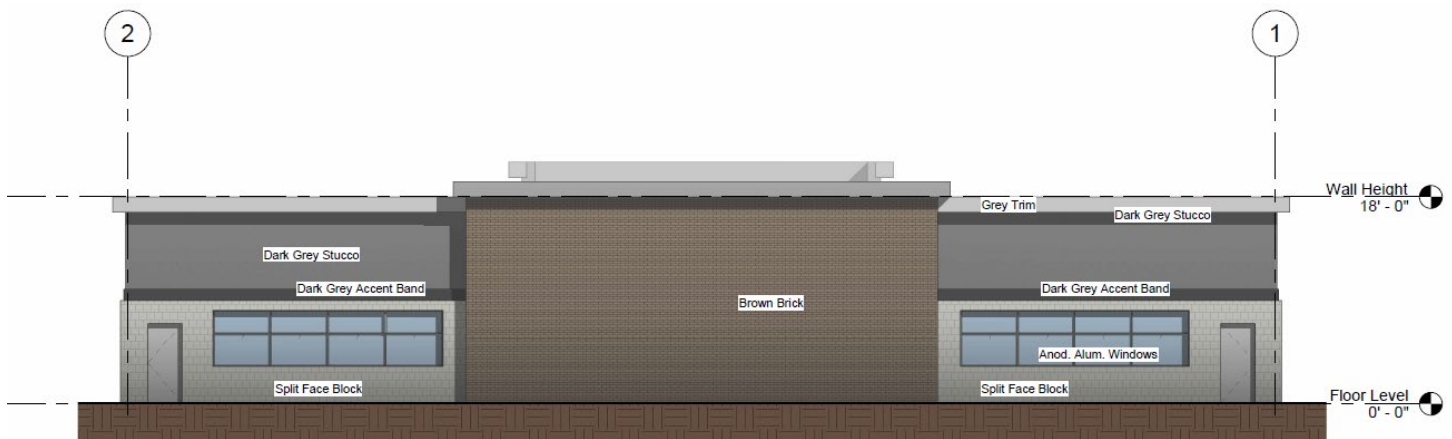
***Exhibit 5: Proposed material palette.***





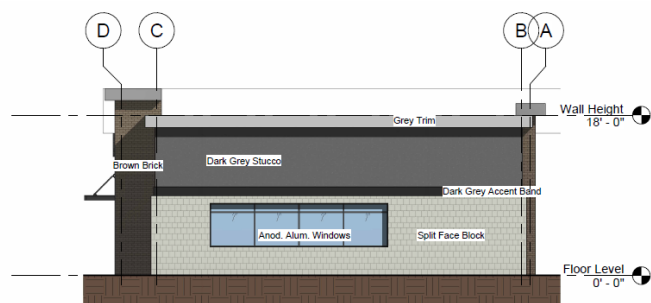
③ Front Color  
1/8" = 1'-0"

**Exhibit 6: Convenience store front elevation.**



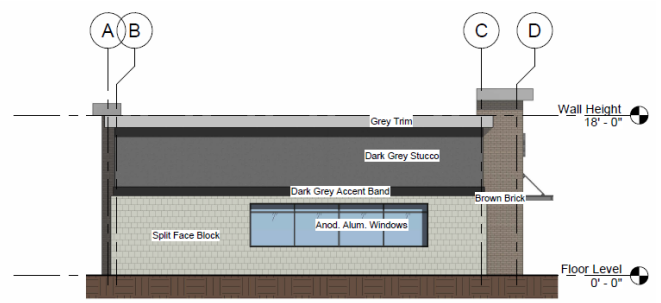
① Rear Color  
1/8" = 1'-0"

**Exhibit 7: Convenience store rear elevation.**



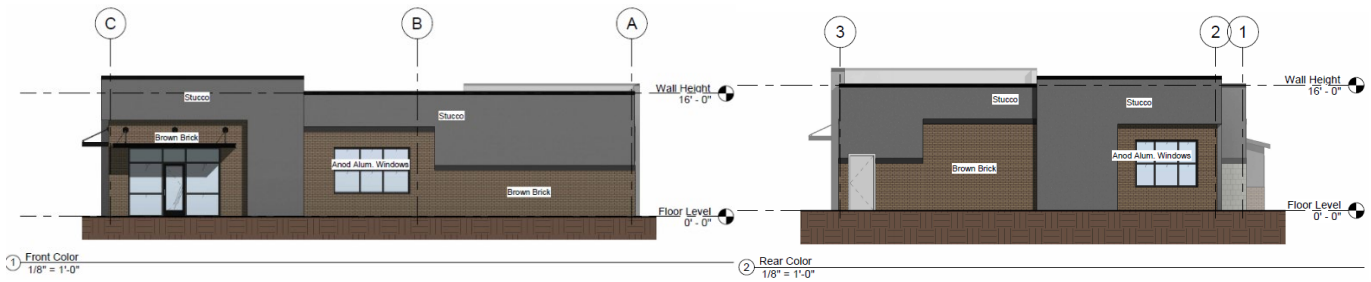
② Side Color  
1/8" = 1'-0"

**Exhibit 8: Convenience store north side elevation.**

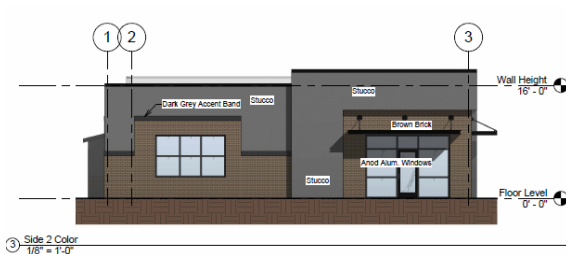


④ Side 2 Color  
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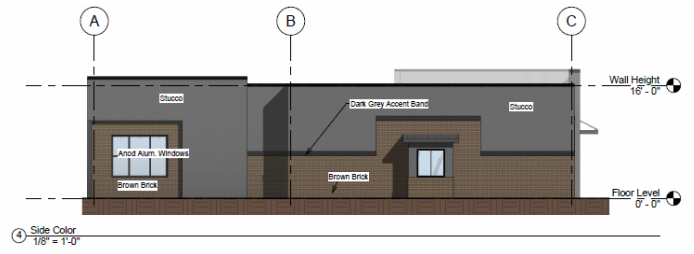
**Exhibit 9: Convenience store south side elevation.**



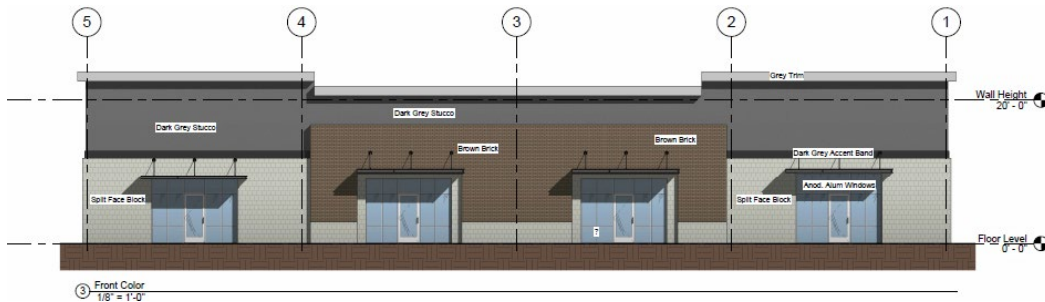
**Exhibit 10: Left: Restaurant/retail with drive-thru front (main entrance) elevation. Right: Restaurant/retail with drive-thru rear elevation.**



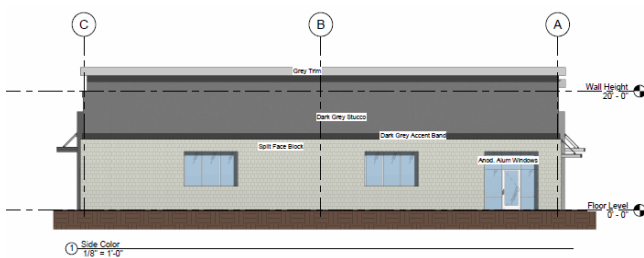
**Exhibit 11: Restaurant/retail with drive-thru side elevation.**



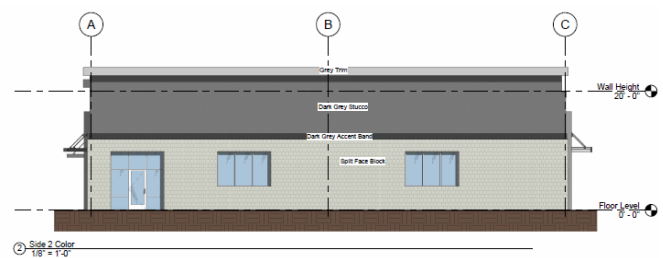
**Exhibit 12: Restaurant/retail with drive-thru side elevation.**



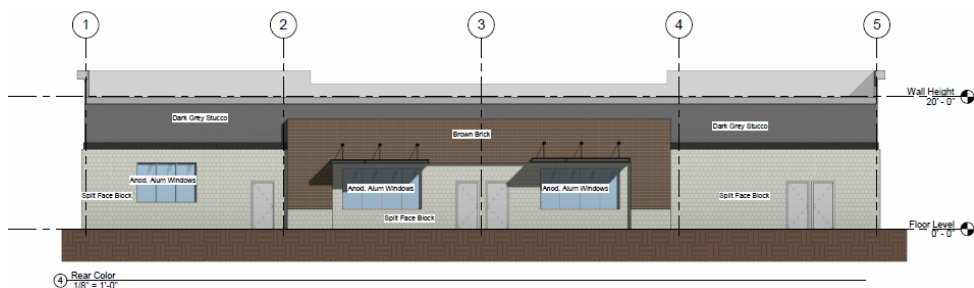
**Exhibit 13: Multitenant retail/office building front (main entrances) elevation.**



**Exhibit 14: Multitenant retail/office building side elevation.**

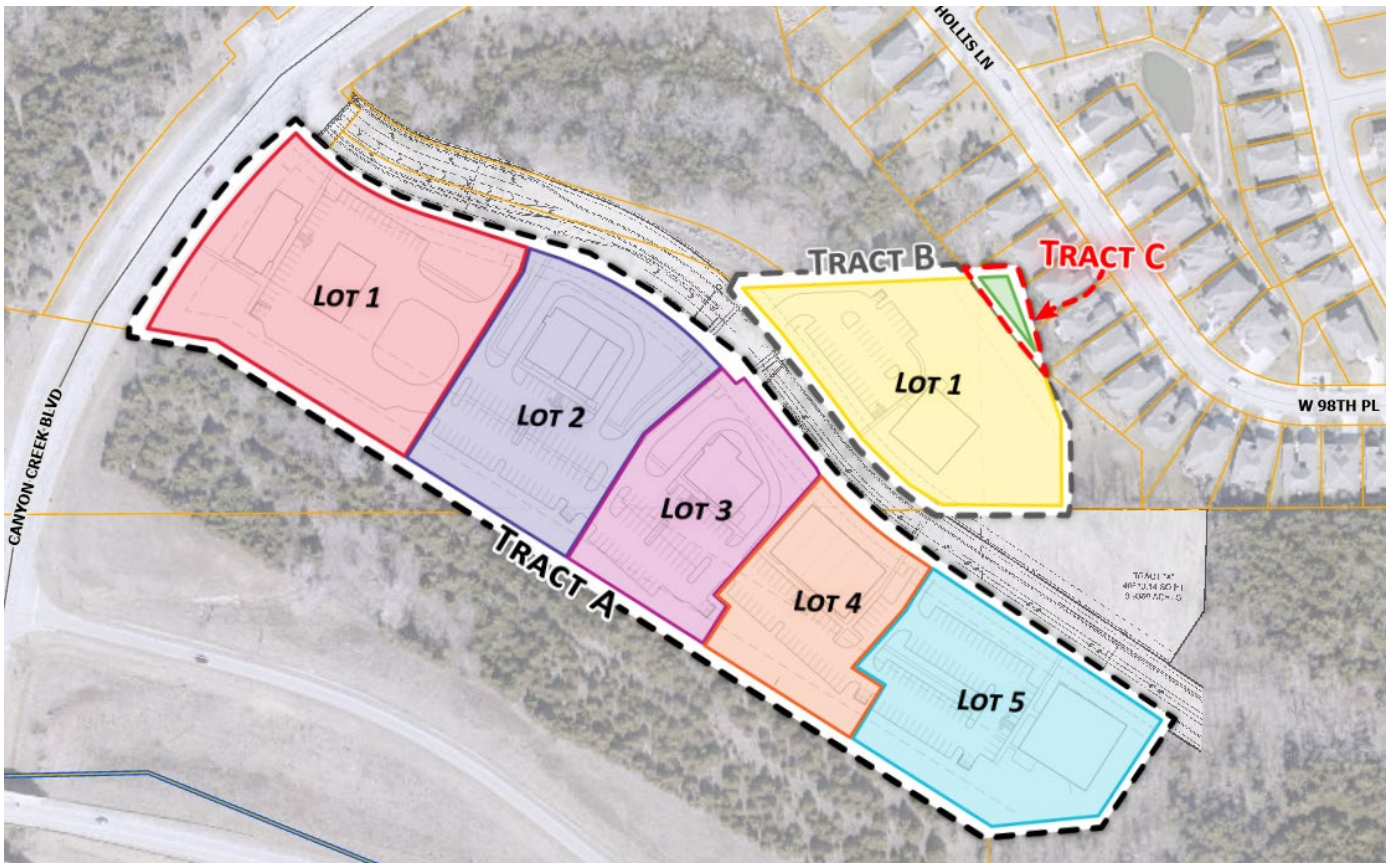


**Exhibit 15: Multitenant retail/office building side elevation.**



**Exhibit 16: Multitenant retail/office building rear elevation.**

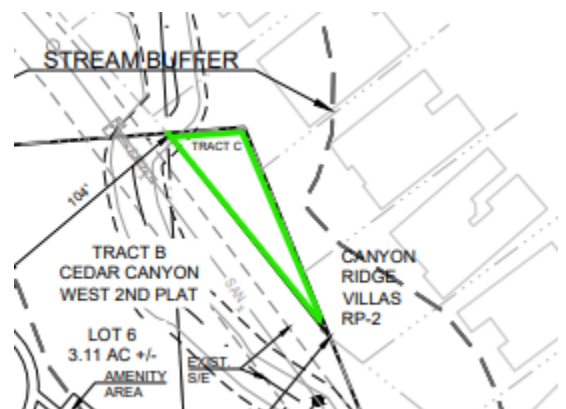




**Exhibit 18: Graphic illustrating tracts, lots, and future 100<sup>th</sup> Street.**

Notable elements of the plat:

- Tract A, Lots 1 through 5 will have two full access points to 100<sup>th</sup> Street and one right-in right-out access onto eastbound 100<sup>th</sup> Street.
- Tract B, Lot 1 will have one full access point to 100<sup>th</sup> Street.
- Tract C is located within the stream buffer area and is not developable. The developer of the subdivision to the north did not acquire the property in Tract C which resulted in smaller, shallow back yards for a few single-family lots in the subdivision (see graphic at right). Tract C is being created to provide the property owner with an opportunity to more easily convey this land to the property owners to the north to provide deeper, squared-off back yards. The City is not requiring the transfer of Tract C and any future real estate transfers will be a private transaction between the property owners.



Dedications and easements on the final plat include:

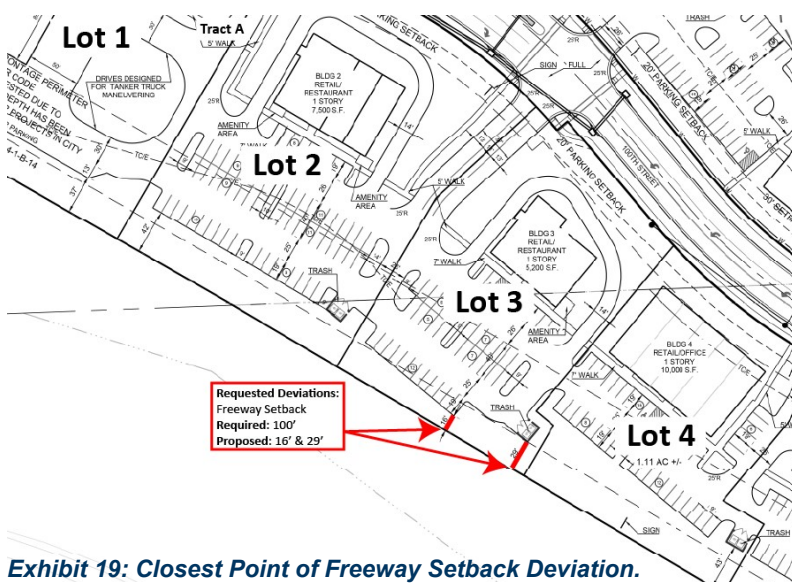
- A utility easement shall be dedicated to the City of Lenexa.
- A drainage easement shall be dedicated to the City of Lenexa.
- A sanitary sewer easement shall be dedicated to Johnson County Wastewater.
- A water easement shall be dedicated to WaterOne.
- A stormwater BMP easement shall be dedicated to the City of Lenexa.

## DEVIATIONS

The applicant requests four deviations related to setback and landscape requirements.

### FREEWAY SETBACK – TRACT A, LOTS 1-5

Section 4-1-B-26-C-1 of the UDC requires a 100' setback from the right-of-way line adjacent to a freeway. The applicant requests a deviation to allow parking and trash enclosures within the required 100' freeway setback along Tract A, Lots 1-5. Specifically, the parking lot pavement and trash enclosures will encroach up to 84' into the required 100' setback at the closest point. The closest trash enclosure is 29' from the south property line and the closest portion of parking lot pavement is 16' from the south property line.



**Exhibit 19: Closest Point of Freeway Setback Deviation.**

sound barriers for the commercial development. The proposed deviation is consistent with a similar deviation granted to a nearby multifamily residential development (Canyon Creek Apartment Homes, PL23-20F) within Cedar Canyon West.

According to the applicant, the developer communicated with Kansas Department of Transportation (KDOT) regarding development of this site. KDOT communicated that there are no plans to expand this section of K-10 Highway. There is a significant distance, roughly 308' at its closest point and 378' at its farthest point, from the subject site's property line and the nearest curb and through lane of K-10.

Staff supports the requested deviation from the 100' freeway setback requirement.

**TABLE 3: FREEWAY SETBACK ANALYSIS**

Buffer	Required Minimum Setback	Proposed Setback	Difference
K-10 (Freeway)	100'	16' at the closest point of the parking lot	- 84'
		29' at the closest point of a trash enclosure	- 71'

**FREEWAY PERIMETER LANDSCAPE BUFFER DEPTH – TRACT A, LOTS 1-5**

Section 4-1-D-2-L of the UDC requires a landscape buffer of a minimum of 100’ depth from the property line when located adjacent to a freeway. The applicant requests to encroach a maximum of 84’ into the 100’ required freeway perimeter landscape buffer along Tract A, Lots 1-5. The proposed plans depict the landscape buffer as narrow as 16’ at the closest point to the south property line; however, the buffer provided is generally at least 40’ wide.

The applicant is mitigating the requested deviation by preserving existing trees and adding trees exceeding the requirements for quantity of trees. Staff supports the requested deviation from the required freeway perimeter landscape buffer depth.

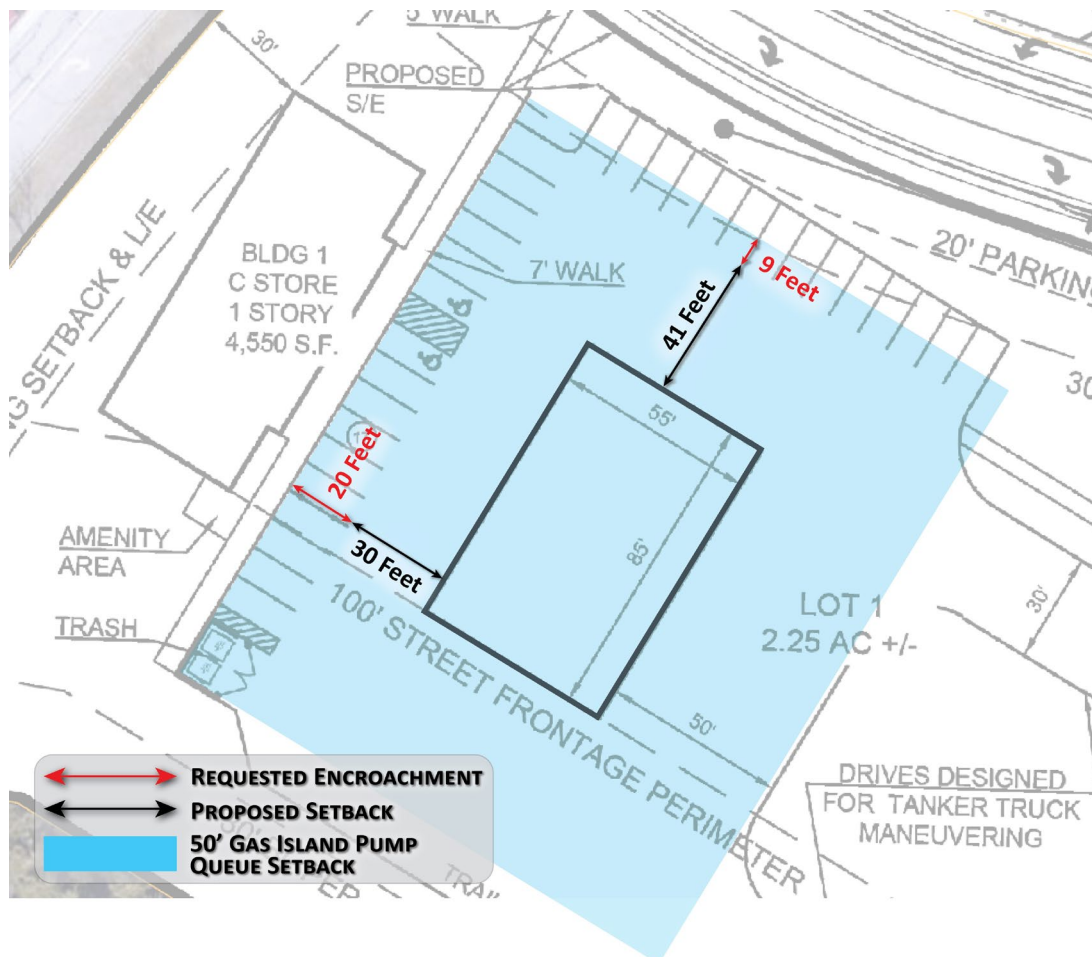


*Exhibit 20: Graphic depicting required 100’ freeway perimeter landscape depth with proposed depth (16’) and the requested encroachment (84’).*

TABLE 4: FREEWAY PERIMETER LANDSCAPE BUFFER ANALYSIS			
Buffer	Required Minimum Depth	Proposed Depth	Difference
Freeway Perimeter	100’	16’ at closest point	- 84’

**GAS PUMP ISLAND SETBACK – TRACT A, LOT 1**

Section 4-1-D-1-N-1 of the UDC requires a 50’ setback from each end of the pump island to accommodate anticipated queue space and circulation around gasoline pumps. On Tract A, Lot 1, the applicant requests to encroach a maximum of 20’ into the required 50’ setback on the west side of the gas canopy, which provides a minimum 30’ setback between the adjacent parking spaces and the gas canopy. Additionally, the applicant requests to encroach a maximum of 9’ into the setback on the north side of the gas canopy, which results in a 41’ setback between the adjacent parking spaces and the gas canopy. The setback is met on the east and south sides of the gas canopy. Staff is aware of other gas stations in Lenexa that do not conform to the 50’ setback requirement and encroach similar distances into the required setback. The site provides additional room for circulation on the east and south sides of the canopy, so Staff believes all gas pump queuing will remain on-site and not interfere with traffic on 100<sup>th</sup> Street. Staff supports the requested deviation from the 50’ setback from each end of the pump island.

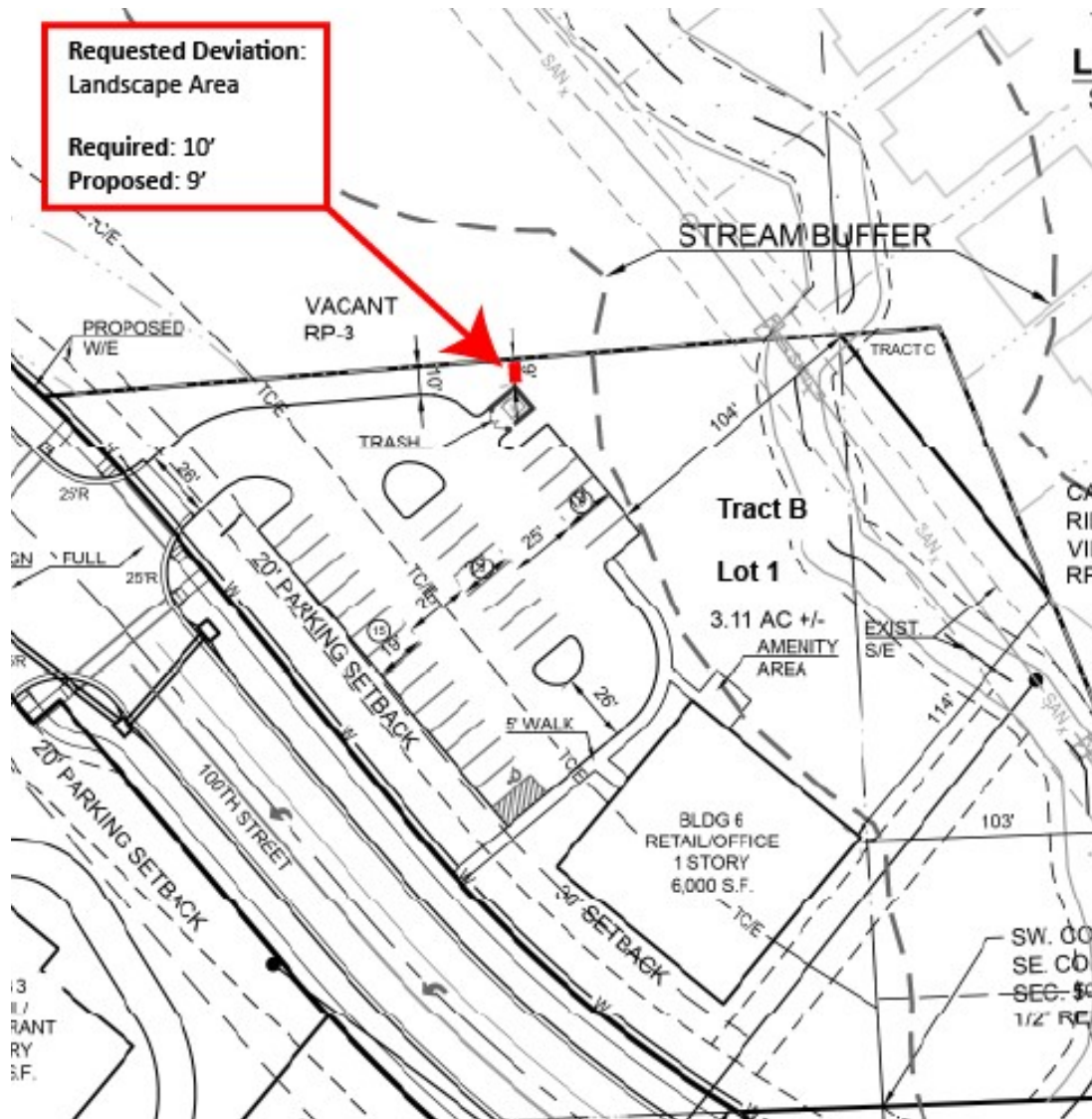


*Exhibit 21: Graphic illustrating requested deviation from the 50’ gas pump queue line setback.*

<b>TABLE 5: GAS PUMP ISLAND SETBACK ANALYSIS</b>			
	<b>Required Minimum Setback</b>	<b>Proposed Setback</b>	<b>Difference</b>
<b>Gas Pump Island Queue Space</b>	50’	30’	<b>- 20’</b>

**PERIMETER LANDSCAPE AREA – TRACT B, LOT 1**

Section 4-1-D-2-L of the UDC requires a minimum depth of 10’ for perimeter landscape areas along interior lot lines. The applicant requests to encroach 1’ into the required 10’ perimeter landscape area along the north property line of Tract B, Lot 1 where the trash enclosure is located. The remainder of the northern perimeter landscape buffer will meet or exceed the 10’ minimum depth. There is limited buildable area of this lot due to the streamway setback. This area will be landscaped with the required quantity of plantings. Staff supports the requested deviation from the required perimeter landscape area.



*Exhibit 22: Graphic showing requested perimeter landscape area setback.*

TABLE 6: PERIMETER LANDSCAPE AREA ANALYSIS			
Perimeter Landscape Area	Required Minimum Depth	Proposed Depth	Difference
Interior Lot Line	10'	9'	-1'



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## REVIEW PROCESS

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- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on December 19, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

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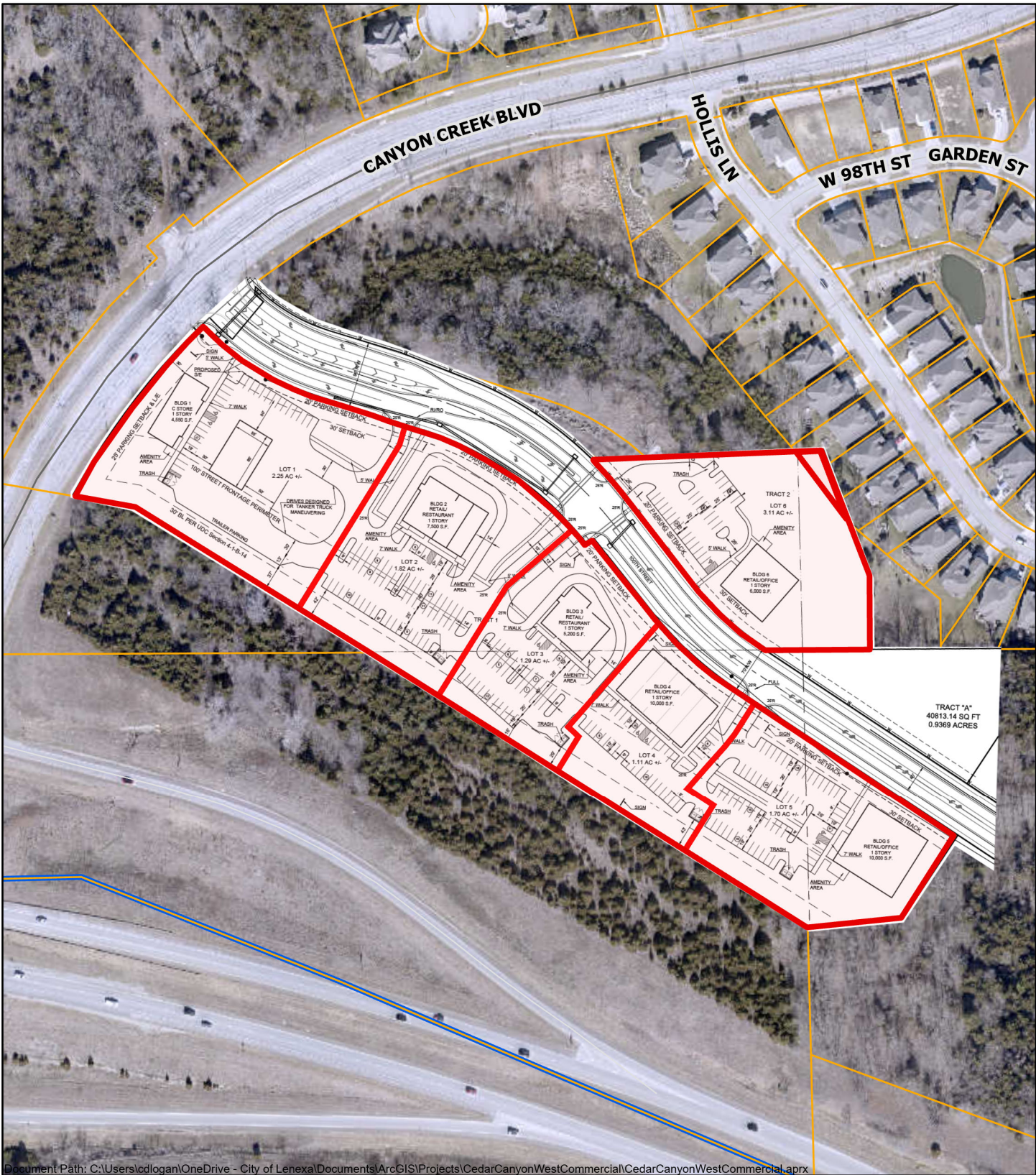
## RECOMMENDATION FROM PROFESSIONAL STAFF

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- ★ **Staff recommends approval of the proposed preliminary plan/plat for Cedar Canyon West Commercial.**
  - This is a preliminary plan, also serving as a preliminary plat, for a multitenant commercial development, including deviations related to setbacks, landscape buffers, and signs.
  - The project is consistent with Lenexa’s goals through ***Responsible Economic Development*** and ***Strategic Community Investment*** to create a ***Thriving Economy*** and ***Inviting Places***.

### PRELIMINARY PLAN/PLAT

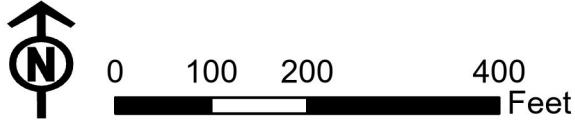
Staff recommends **APPROVAL** of the preliminary plan/plat for PL23-08P – **Cedar Canyon West Commercial** located near the northeast corner of K-10 Highway and Canyon Creek Boulevard for a multitenant commercial development with deviations as outlined in the Staff Report.



Document Path: C:\Users\ldlogan\OneDrive - City of Lenexa\Documents\ArcGIS\Projects\CedarCanyonWestCommercial\CedarCanyonWestCommercial.aprx

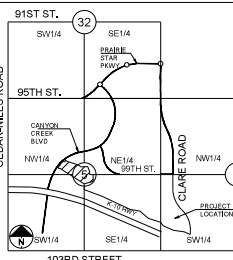
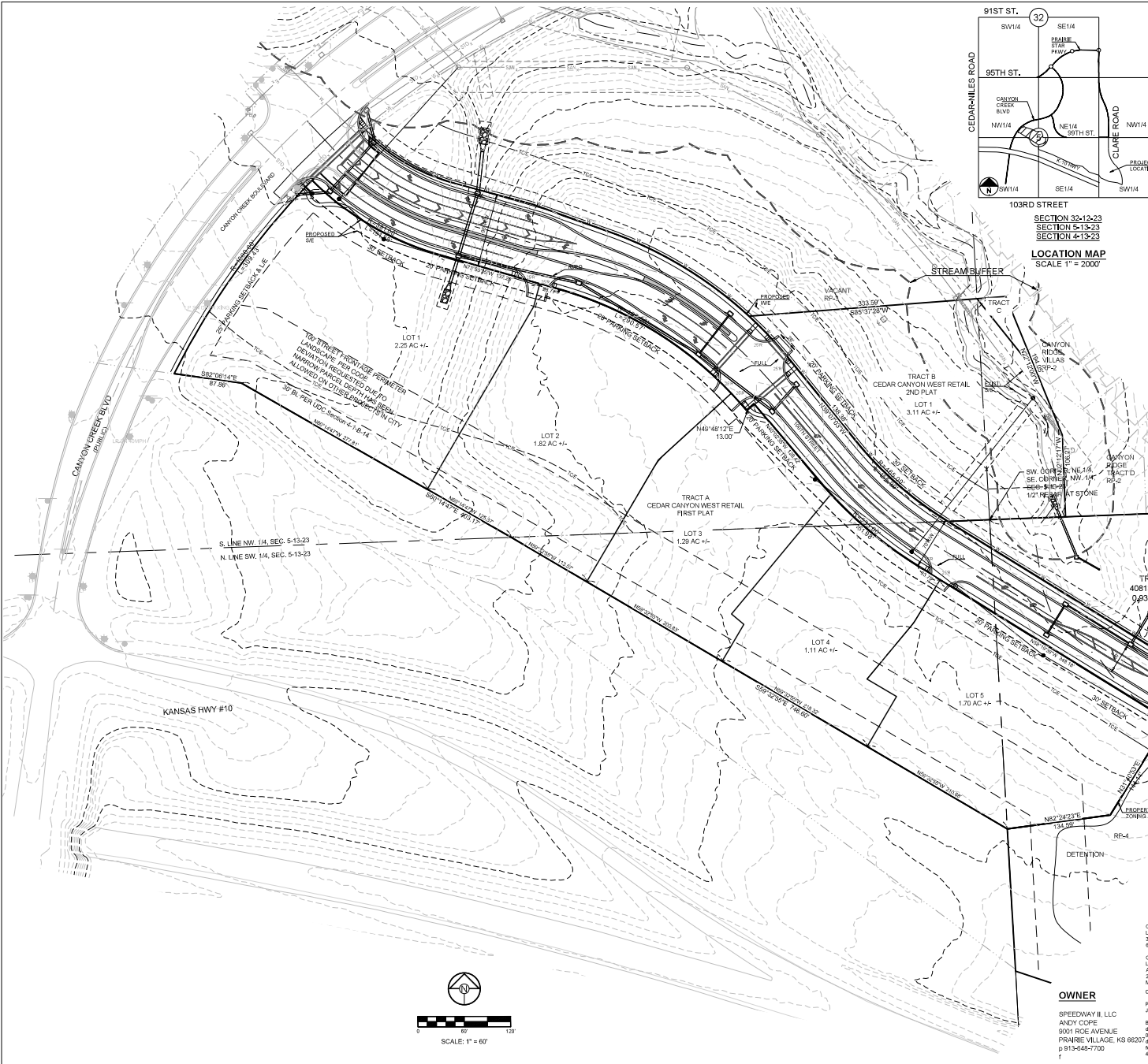
Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Cedar Canyon West Commercial PL23-08P









SECTION 32-13-23  
SECTION 5-13-23  
SECTION 4-13-23  
LOCATION MAP  
SCALE 1" = 2000'

**SITE INFORMATION:**

CURRENT ZONING	CR-2
PROPOSED ZONING	CR-2
CURRENT USE	Vacant-undeveloped
PROPOSED USE	Commercial
GROSS SITE AREA (TRACTS 1 & 2 COMBINED)	12.52 AC (545,371.20 S.F.)
RIGHT OF WAY AREA	2.18 AC (94,860.80 S.F.)

<b>TRACT A CEDAR CANYON WEST FIRST PLAT SITE DATA</b>	
PLAT AREA (EXCL. ROW)	8.17 AC (359,889.20 S.F.)
LOT 1 AREA	2.25 ACRES
LOT 2 AREA	1.82 ACRES
LOT 3 AREA	1.28 ACRES
LOT 4 AREA	1.11 ACRES
LOT 5 AREA	1.70 ACRES

<b>TRACT B CEDAR CANYON WEST 2ND PLAT SITE DATA</b>	
PLAT AREA (EXCL. ROW)	3.67 AC.
TRACT C AREA	0.68 AC.

TRACT C HAS BEEN ADDED TO THIS PRELIMINARY PLAT AS REQUIRED BY THE CITY OF LENEXA.

- GENERAL NOTES:**
- FLOOD NOTE: There is no FEMA identified floodplain located on the proposed property per Flood Insurance Rate Map Panel No. No. 20091C0046 G and 20091C0047 G dated August 3, 2009.
  - Boundary and assessment data is from a survey prepared by Schlagel & Associates, P.A.
  - Topography and adjacent plat and improvements from JOCO AMIS.
  - Existing utilities have been shown to the greatest extent possible based upon field locates by utility companies and information provided to the engineer.
  - Adjacent parcel information is based upon JOCO AMIS and information provided by consultants for the adjacent projects.
  - All parking lot lighting and cable shall be placed underground per the city codes and ordinances.
  - All new on-site wiring and cable shall be placed underground per the city codes and ordinances.
  - All above ground electrical and/or telephone cabinets shall be placed within the interior side or rear building setback yards unless the planning official approves the placement of the cabinets in the front or corner yard as outlined in the city code.
  - All wall and monument signs will require a sign permit from the city codes division.
  - All parking spaces shall meet the requirements in the UDD.
  - All improvements must meet the City of Lenexa design criteria, standards and codes.
  - Retaining wall shall conform to Block Concrete wall or buff colored Keystone modular block retaining wall or approved equal.
  - Approval of this plan does not result in generating a building permit. A separate building permit approval process must be completed.
  - Approval of this plan does not constitute a complete review of the project for compliance with the City building codes. A full code analysis shall be submitted with the building permit. Revisions may be required to make the project building code compliant.
  - Approval of this plan does not constitute a complete review of the project for compliance with the ADA regulations. The project shall comply with all applicable regulations of the ADA.
  - Approval of this plan does not constitute compliance with the Fire Code. Complete fire sprinkler and fire alarm plans are required to obtain a building permit.
  - A sewer permit from Johnson County Wastewater is required prior to obtaining a building permit.
  - All parking spaces will be 9' x 19'. Drive aisles shall be a minimum of 25'. Drive aisles for emergency access shall be 20'.
  - All sidewalks shall be a minimum of 4' width.
  - The architectural style will be determined by the developer of each parcel and will be provided with the preliminary development plan.
  - The project will follow the applicable permit requirements when permit plans are completed.
  - Fire flow requirements for the project shall be established in accordance with Appendix B of the 2018 International Fire Code.
  - Buildings exceeding 30 feet in height shall have proximity to building. At least one of the required special apparatus fire line of 2 1/2 inches in diameter shall be located within a minimum of 15 feet and a maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building. Fire department access roads must comply with the IFC and Appendix D of the IFC.
  - Detention and BMP size and locations indicated on the plan are conceptual. All City of Lenexa stormwater requirements will need to be met at the time of preliminary plan including all stream buffer requirements. A stormwater management report shall be provided with the preliminary development plan.
  - The design for the public streets illustrated on this plan are conceptual based on City staff comments. A traffic study shall be submitted with the preliminary development plan. The final design for the public streets shall be based on the traffic study recommendations.

CR-2 Property ID # 231305-002  
Legal Description: S. 1/4 of NE 1/4 of SW 1/4 of Section 5, Township 13 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, being more particularly described as follows:  
Beginning at the Southeast corner of said Northwest One-Quarter, thence South 87 degrees 54 minutes 09 seconds West along the South line of said Northwest One-Quarter a distance of 617.24 feet to a point on the eastern right-of-way of 1/2 inch higher, thence North 85 degrees 14 minutes 47 seconds West along said right-of-way a distance of 493.02 feet to a point, thence North 72 degrees 50 minutes 14 seconds West along said right-of-way a distance of 87.26 feet to a point on the Eastern right-of-way of Canyon Creek Boulevard, thence North 101 degrees 10 minutes 55 seconds East, a central angle of 18 degrees 54 seconds and an arc length of 100.00 feet, an initial tangent bearing of North 20 degrees 32 minutes 54 seconds East, a central angle of 18 degrees 54 seconds and an arc length of 325.45 feet to a point, thence East along a curve to the left along said right-of-way having a radius of 340.00 feet, an initial tangent bearing of South 42 degrees 05 minutes 05 seconds East, a central angle of 07 degrees 27 minutes 54 seconds and an arc length of 45.26 feet to a point, thence North 37 degrees 45 minutes 18 seconds East along said right-of-way a distance of 35.00 feet to a point of curvature, thence East along a curve to the left having a radius of 305.00 feet, an initial tangent bearing of South 52 degrees 14 minutes 42 seconds East, a central angle of 22 degrees 23 minutes 25 seconds and an arc length of 20.00 feet to a point, thence South 75 degrees 03 minutes 56 seconds East a distance of 311.20 feet to a point of curvature, thence South 62 degrees 03 minutes 19 seconds East a distance of 500.00 feet, a central angle of 18 degrees 51 minutes 15 seconds and an arc length of 118.00 feet to a point, thence North 89 degrees 35 minutes 08 seconds East a distance of 281.68 feet to a point on the East line of said Northwest One-Quarter, thence South 10 degrees 10 minutes 11 seconds East along said East line a distance of 287.24 feet to the POINT OF BEGINNING and containing 7.2155 acres more or less.

CR-2 Property ID # 231305-002  
Legal Description: S. 1/4 of NE 1/4 of SW 1/4 of Section 5, Township 13 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, being more particularly described as follows:  
S. 1/4 AC. 29 AC. W/4

CR-2 Property ID # 231305-002  
Legal Description: S. 1/4 of NE 1/4 of SW 1/4 of Section 5, Township 13 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, being more particularly described as follows:  
S. 1/4 AC. 29 AC. W/4

CR-2 PARCEL PROPERTY ID # 231305-2011A

Parts of the Southeast One-Quarter and Southwest One-Quarter of Section 5, Township 13 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, being more particularly described as follows:  
Beginning at the Northwest corner of the Southeast One-Quarter of Section 5, thence North 87 degrees 49 minutes 18 seconds East, a distance of 66.21 feet, thence South 93 degrees 13 minutes 57 seconds East, a distance of 145.84 feet, thence South 59 degrees 19 minutes 07 seconds East, a distance of 168.86 feet, thence South 73 degrees 05 minutes 53 seconds West, a distance of 173.89 feet, thence South 33 degrees 18 minutes 56 seconds West, a distance of 134.25 feet, thence North 59 degrees 33 minutes 42 seconds West, a distance of 74.46 feet, thence North 87 degrees 51 minutes 17 seconds East, a distance of 62.11 feet, said point being the Point of Beginning, and containing 4.2071 acres, more or less.

**SCHLAGEL**  
ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS  
4650 N. LINCOLN ST., SUITE 100, LENEXA, KS 66150  
(913) 424-6168 FAX: (913) 424-6400  
WWW.SCHLAGELASSOCIATES.COM

PREPARED BY:  
SCHLAGEL & ASSOCIATES P.A.

**CEDAR CANYON WEST  
PRELIMINARY DEVELOPMENT PLAN  
CANYON CREEK BLVD. & 100TH STREET  
LENEXA, KANSAS**

NO.	DATE	BY	DESCRIPTION
1	03/23/24	MM	CITY COMMENTS REPLY (TRACT NAME CHANGES)
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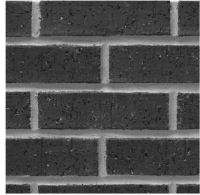
PRELIMINARY PLAT

SHEET  
**PP-1**

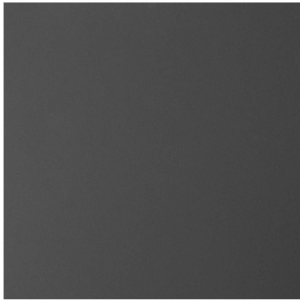
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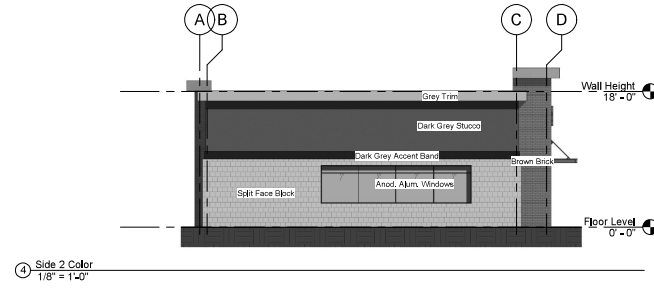
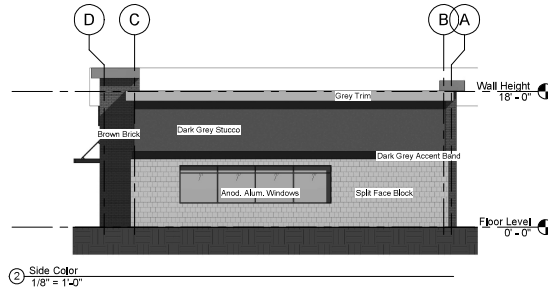
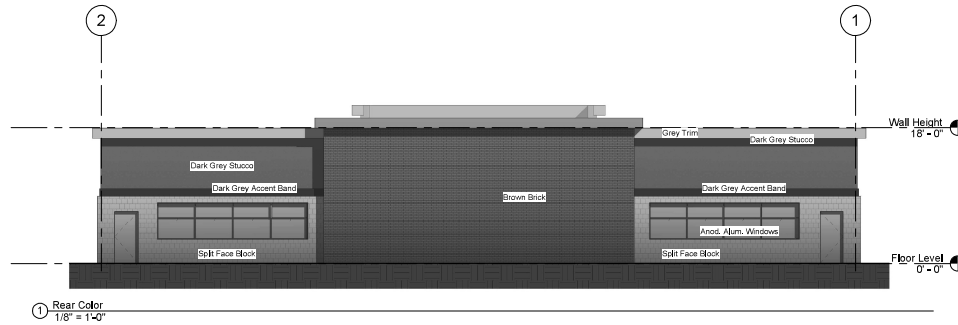
GREY SPLIT FACE BLOCK



BROWN BRICK



DARK GREY STUCCO



ALL ROOFTOP UNITS WILL BE SCREENED BY PARAPET WALLS



HERNIA SCHARHAG COMPANY ARCHITECTS  
5836 Central Kansas City, Mo 64113  
Phone: 816-914-5165 Scharhagarch@gmail.com

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C Store Prototype  
**Cedar Canyon West Retail**  
Speedway 11 Parcel, Lenexa, KS

Jeffrey Schneider P.A. License 2873  
Hernia Scharhag Co., Architects, Inc. (AIA/Architect)

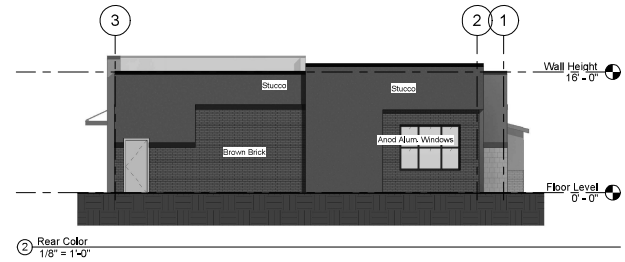
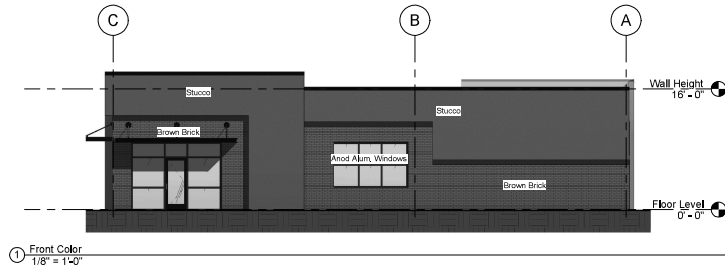
No.	Description	Date
Revision Schedule		

Colored Elevations

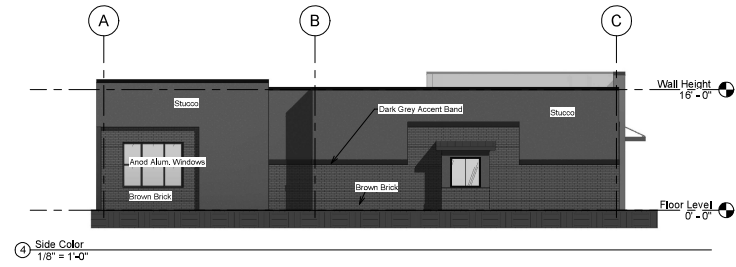
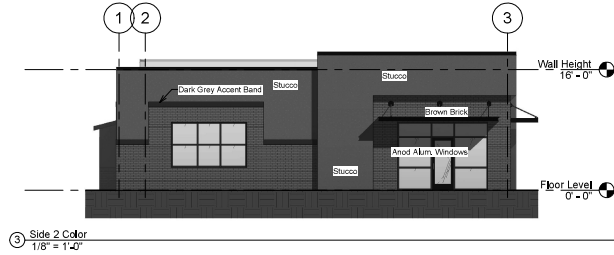
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Date 11.27.2023

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Scale As indicated

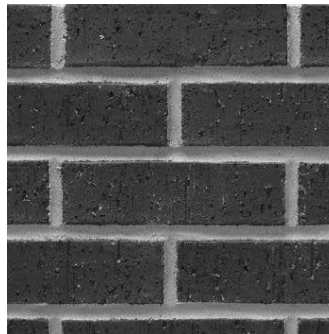
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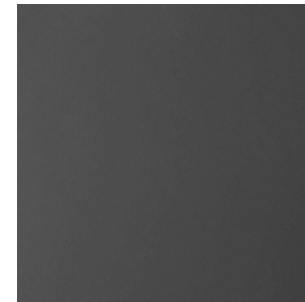
ALL ROOFTOP UNITS WILL BE SCREENED BY PARAPET WALLS



GREY SPLIT FACE BLOCK



BROWN BRICK



DARK GREY STUCCO

**scharhag**  
HERNAN SCHARHAG COMPANY ARCHITECTS

6247 Brookside Blvd. #204 Kansas City, Mo. 64113  
Phone: 816-696-5065 Scharhagarc@gmail.com

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Fast Food Prototype for

**Cedar Canyon West Retail**

Speedway 11 Parcel, Lenexa, KS

Jeffrey Schneider P.A. License 2873  
Hernan Scharhag Co., Architects, P.C. (A/Architect)

No.	Description	Date
Revision Schedule		

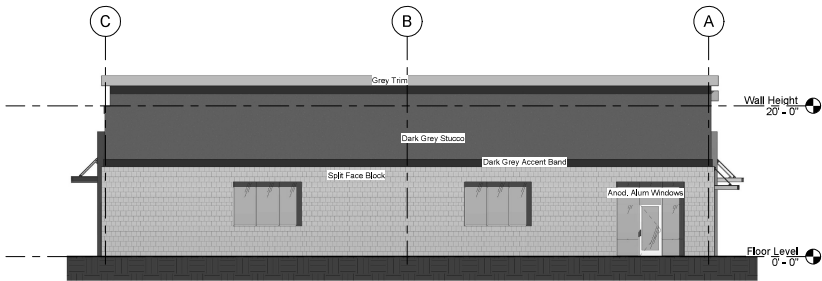
Colored  
Elevations

Project number	2554
Date	11.27.2023

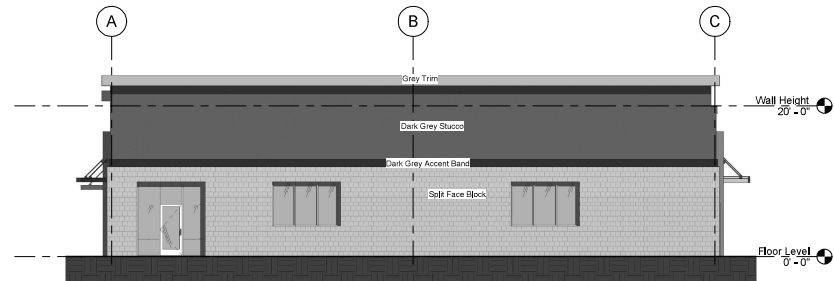
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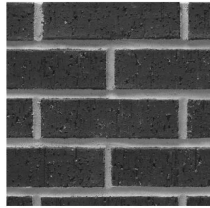
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① Side Color  
1/8" = 1'-0"



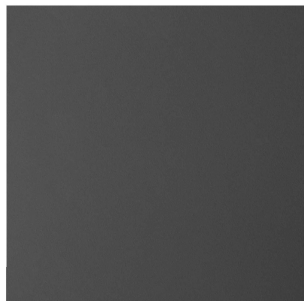
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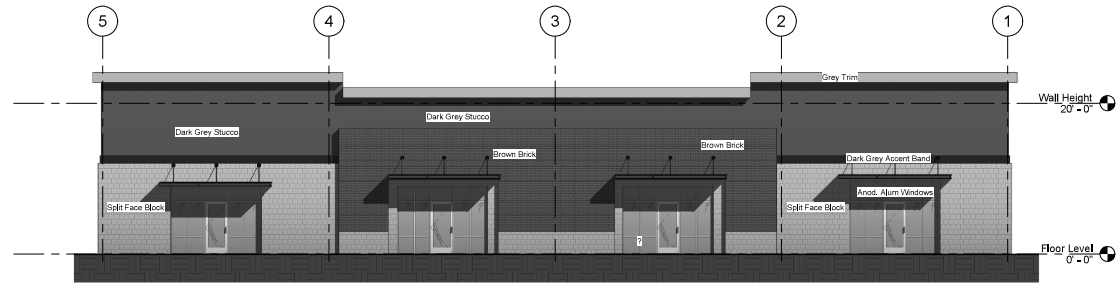
**BROWN BRICK**



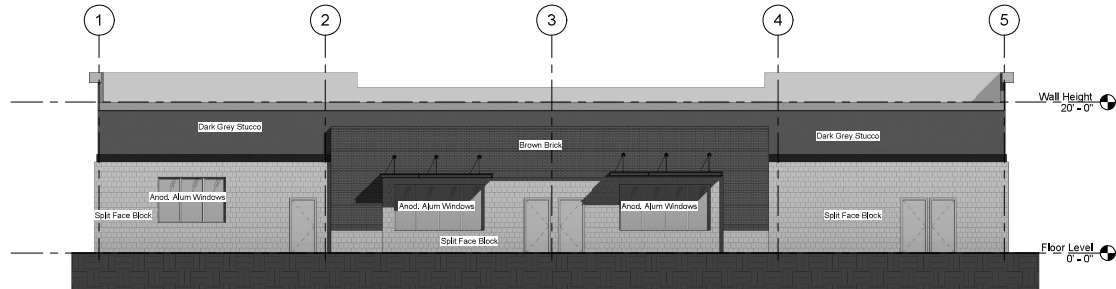
**GREY SPLIT FACE BLOCK**



**DARK GREY STUCCO**



③ Front Color  
1/8" = 1'-0"



④ Rear Color  
1/8" = 1'-0"

**ALL ROOFTOP UNITS WILL BE SCREENED BY PARAPET WALLS**

**scharhag**  
HERNAN SCHARHAG COMPANY ARCHITECTS

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Multi Tenant Prototype for

**Cedar Canyon West Retail**

Speedway 11 Parcel, Lenexa, KS

Jeffrey Schneider, P.A. License 2873  
Hernan Scharhag Co., Architects, C.A. (Architect)

No.	Description	Date
Revision Schedule		

**Colored Elevations**

Project number 2555  
Date 11.27.2023

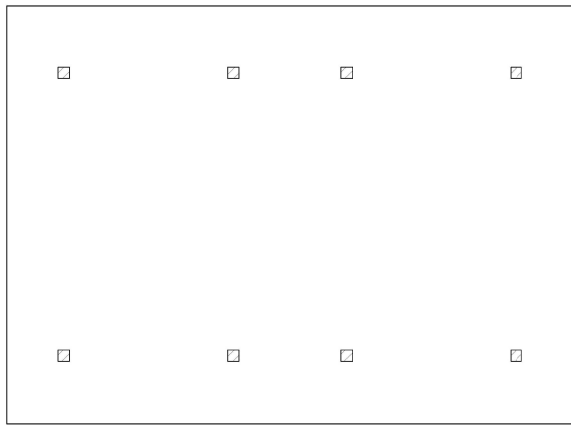
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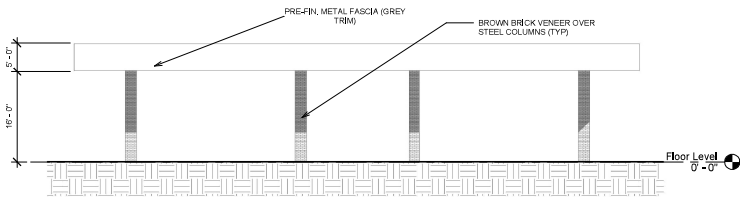






3 Canopy Floor Plan  
3/32" = 1'-0"

EXACT SIZE PER SITE PLAN



1 Canopy Elevation  
3/32" = 1'-0"

TYPICAL ALL FOUR SIDES



2 Canopy Elevation Color  
3/32" = 1'-0"

TYPICAL ALL FOUR SIDES

**scharhag**  
HEMANIA SCHARHAG COMPANY, ARCHITECTS

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C Store Prototype

**Cedar Canyon West Retail**  
Speedway 11 Parcel, Lenexa, KS

JAWY/Schreier MTA, Lenexa #1022  
www.scharhag.com, 11/27/2023, A42

No.	Description	Date
Revision Schedule		

**Canopy Details**

Project number	2553
Date	11.27.2023

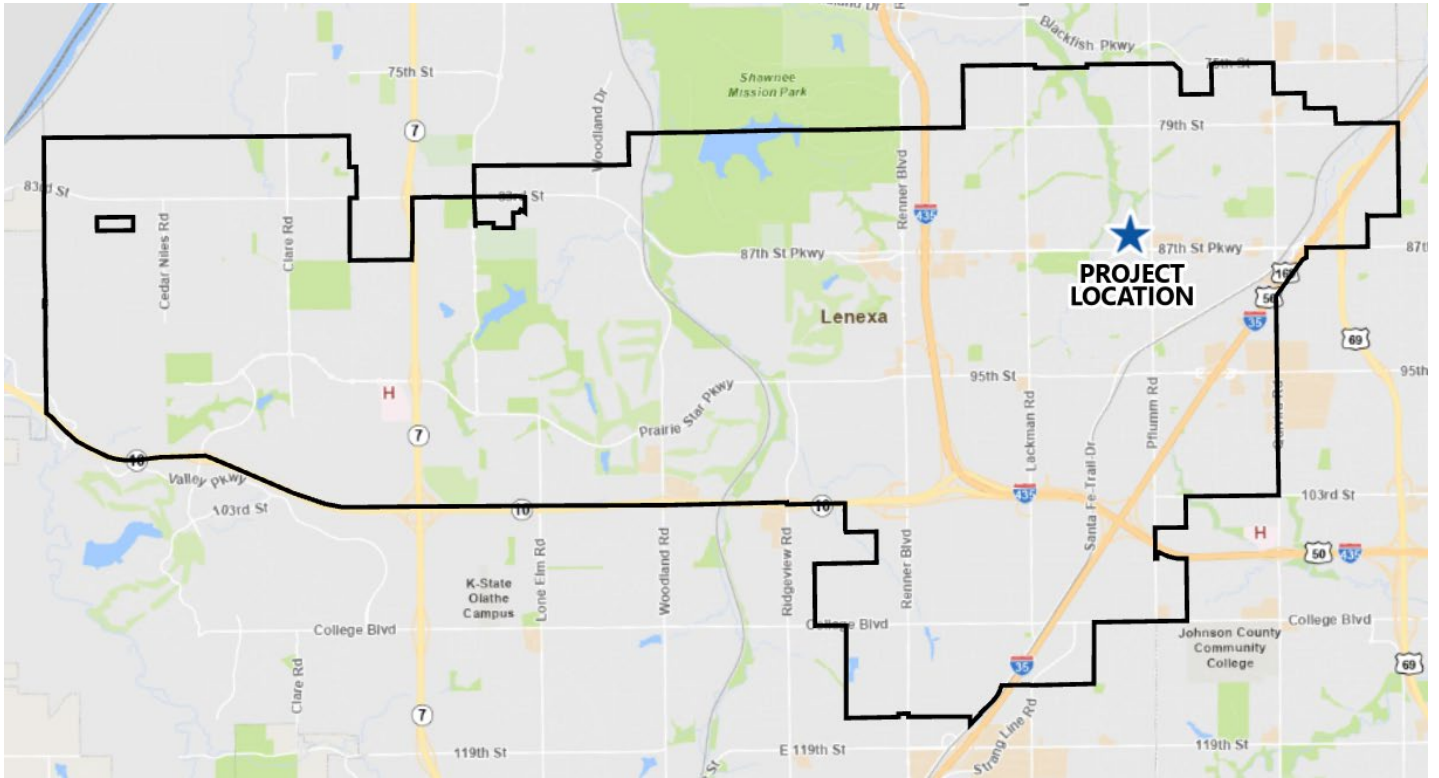
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Scale	3/32" = 1'-0"
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# EXPRESS OIL CHANGE & TIRE ENGINEERS

<b>Project #:</b>	PL23-10P	<b>Location:</b>	8610 Pflumm Road
<b>Applicant:</b>	Ryan Kubert, Olsson	<b>Project Type:</b>	Preliminary Plan
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Auto Service, Limited



## PROJECT SUMMARY

The applicant proposes to construct a new commercial building for an auto service, limited use at 8610 Pflumm Road. The proposed structure is one-story with a floor area of 5,747 square feet. The site is an infill lot and does not require construction of new public streets or infrastructure improvements. The applicant requests approval of a preliminary plan for the development. This project does not require a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

The subject site is an infill lot that was previously occupied by a restaurant use. A Sonic restaurant was located on the site from 1995 until 2019 when it was demolished. The site has remained vacant since.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
1.63	5,747 SF	CP-2	Planned Community Commercial



**Exhibit 1: Aerial Image of Subject Site.**

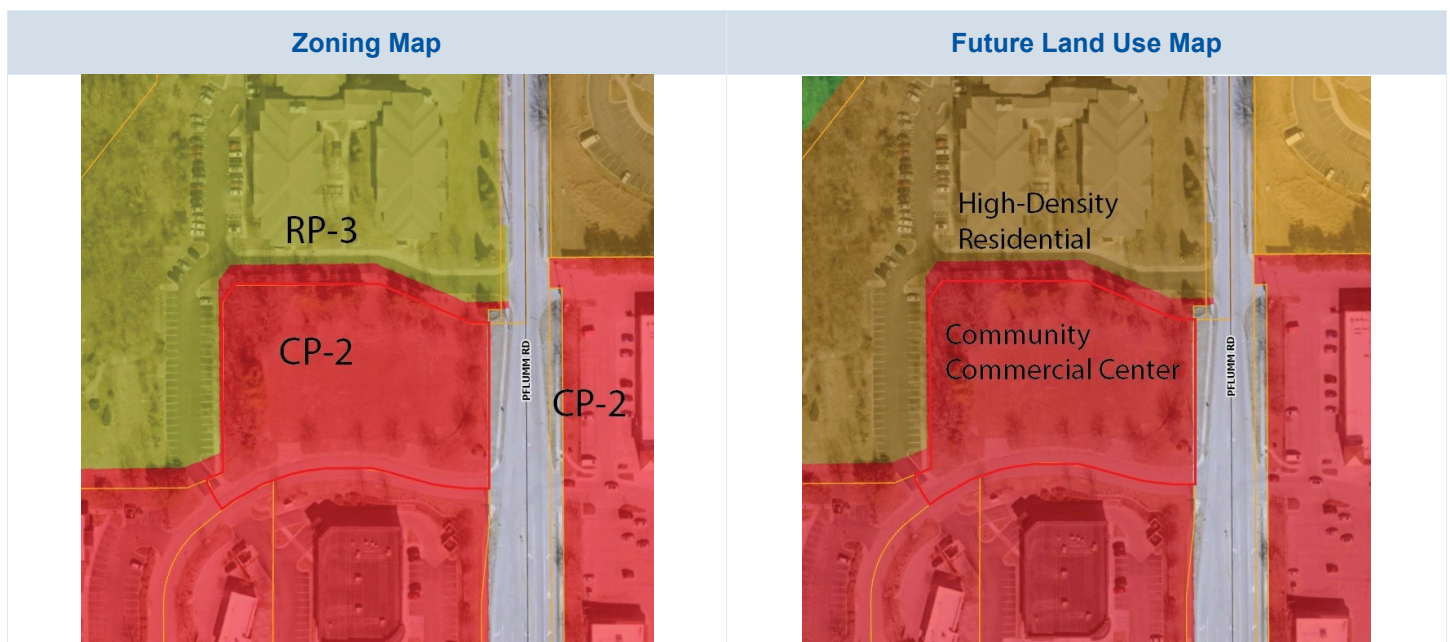


**Exhibit 2: Google Street View of Site from southwest corner looking north.**

## LAND USE REVIEW

The proposed land use is classified as auto service, limited for a business known as Express Oil Change & Tire Engineers. The services provided include oil changes, battery replacement, alignments, tire rotation, and other minor auto services. Services are limited to short-term appointments and walk-ins and do not include keeping vehicles overnight. The anticipated volume of cars is 46 per day. All services are to be provided within the fully enclosed bays. The proposed hours of operation are Monday through Saturday from 8:00 AM to 6:00 PM.

The proposed use of auto service, limited is allowed by-right in the CP-2, Planned Community Commercial Zoning District. Surrounding uses include retailers such as Hy-Vee to the east and CVS Pharmacy to the south. Properties to the north and west of the subject site are zoned and developed as high-density residential. To the west of the site is a parking lot for the multifamily residential buildings located north of the site across a private drive. Due to the proximity of the multifamily residential development, Staff required the proposed development to include additional mitigation measures for visual impact, light, and noise to ensure compatible development.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

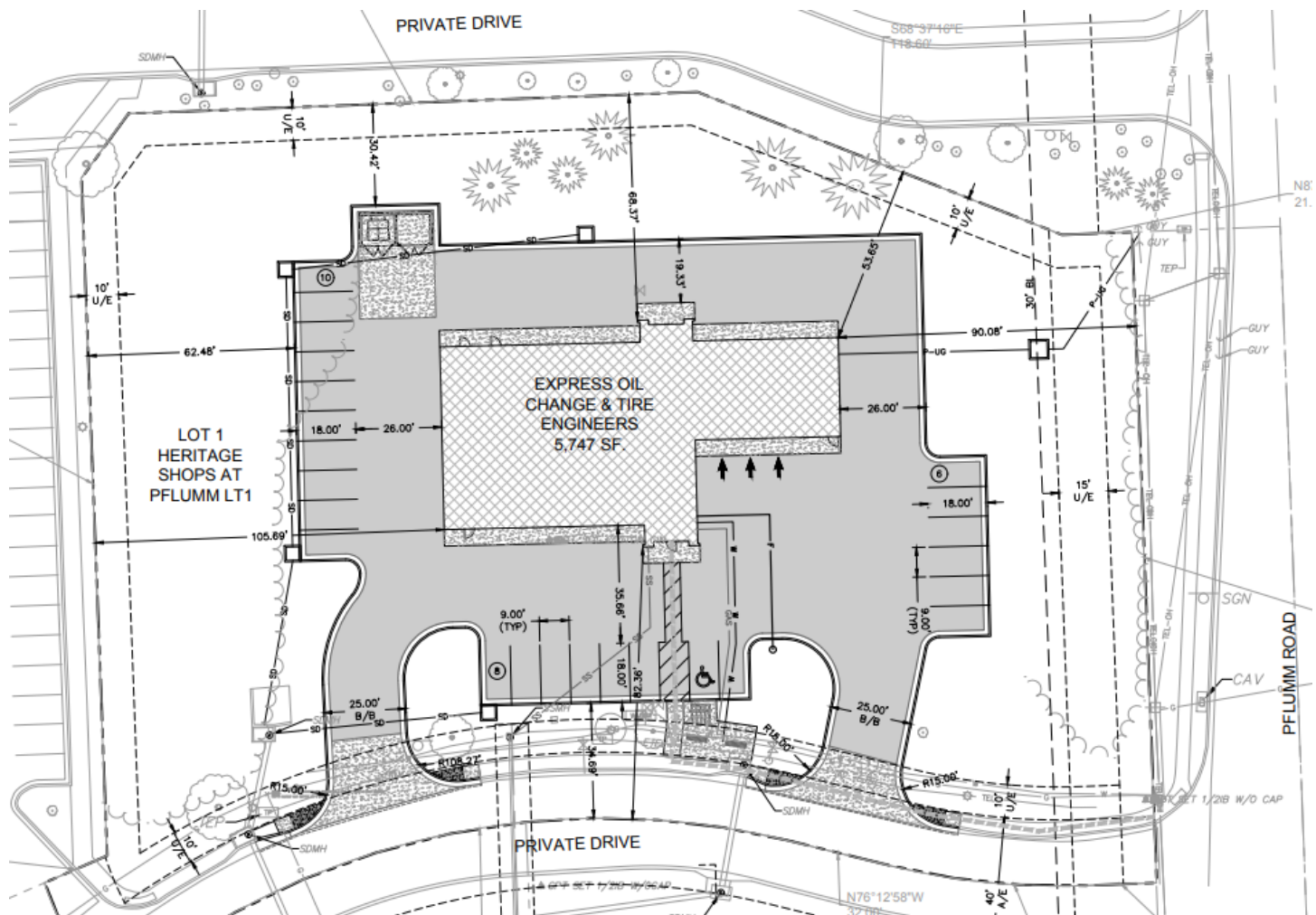
Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Community Commercial Center	BP-2, Planned Manufacturing District	Vacant
<b>North</b>	High-Density Residential	RP-3, Residential Planned (Medium High-Density) District	High-Density Residential
<b>South</b>	Community Commercial Center	CP-2, Planned Community Commercial	Retail
<b>East</b>	Community Commercial Center	CP-2, Planned Community Commercial	Retail
<b>West</b>	Community Commercial Center	CP-2, Planned Community Commercial	High-Density Residential (Parking)

## PRELIMINARY PLAN REVIEW

The preliminary plan is for development of a new commercial building that will be used as a light automotive shop known as Express Oil Change & Tire Engineers.

The site is located along the west side of Pflumm Road approximately 350 feet north of W. 87<sup>th</sup> Street Parkway, with frontage along two private drives to the north and south.

The site is platted as Lot 1 of Heritage Shops and has a 10' utility easement along the perimeter of the property, a 25' landscape easement along Pflumm Road, and a 30' build line along Pflumm Road.



**Exhibit 3: Site Plan.**

## DIMENSIONAL STANDARDS

The proposed development is compliant with the dimensional standards and property development regulations of the CP-2 Zoning District as outlined in Table 2.

TABLE 2: CP-2 PROPERTY DEVELOPMENT REGULATIONS		
Yard	Regulation	Proposed
Minimum Setback from Streets	30 feet	90 feet
Minimum Setback from Other	30 feet	>30 feet
Maximum Height	45 feet	35 feet
Minimum Open Space	25% of lot area	50% of lot area

## PUBLIC IMPROVEMENTS

No public improvements are proposed with this project.

## ACCESS, TRAFFIC, AND PARKING

The site is located between two private drives to the north and south with two access points along the southern private drive. The southern private drive connects from Pflumm Road to W. 87<sup>th</sup> Street Parkway and also provides access to CVS Pharmacy, Jiffy Lube, and a multi-tenant retail strip building. The project site has frontage along Pflumm Road along the east property line, but no access is allowed directly to Pflumm Road, an arterial street.

There is a sidewalk along Pflumm Road and along the private drive to the south. The internal sidewalk network will connect to the private drive and provide access to the main entrance of the building.

Vehicle and bicycle parking are provided to meet or exceed code requirements as shown in Table 3.

TABLE 3: PARKING ANALYSIS				
Requirement	Parking Formula	Required Parking	Proposed Parking	Difference
Vehicle Parking: Auto Service	2 spaces per service bay	18	24	+ 6
Bicycle Parking	1 space per 10,000 SF	1	1	0

## STORMWATER

Staff notes that with the use requested (oil change/tire store) this site will be considered a water quality “hot-spot” which requires additional water quality measures. The applicant is proposing these additional measures be in the form of water quality inserts into all of the site’s curb inlets, which will appropriately address the hot-spot requirements.

**FIRE PREVENTION**

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

**LIGHTING**

Lighting will be reviewed for full compliance at the time of final plan submittal.

**LANDSCAPING**

Pedestrian scale public space is required to be provided for commercial developments. The proposed site layout includes a 250 square foot pedestrian area with benches and landscaping adjacent to the south sidewalk to meet this requirement.

The site has a 25-foot landscape easement along Pflumm Road with existing vegetation to remain.

Perimeter buffer requirements are met, with additional landscaping required to buffer the north property line where bay doors must be screened from view of the adjacent multi-family residential use. In this area, a combination of additional trees, existing berming, and additional separation by the intervening drive will be used to buffer light, noise, and visual impacts.



**Exhibit 4: Landscape Plan.**



**ARCHITECTURE**

The proposed structure is a 5,747 SF, one-story building with a maximum height of 35 feet. Materials include clay-faced masonry with brick and stone appearance in a tan color palette and a grey standing seam roof. Features of design enhancement include double rowlock accent banding along facades and faux window design on the east façade. With the final plan, Staff would also like to see the faux window improvements on the west façade, in keeping with Lenexa’s standards for four-sided architecture. The trash enclosure uses the same material palette as the building. Mechanical equipment will be located on the ground at the northwest corner of the building and will be screened by a masonry wall matching the building.

**EXTERIOR FINISH MATERIAL LEGEND**

								
<b>CLAY FACED MASONRY</b> Accent Color: Sandstone Dark Manuf: Endicott Clay Products Keyholes: 17, 19 & 50	<b>CLAY FACED MASONRY</b> Field Color: Copper Sands Manuf: Endicott Clay Products Keyhole: 18	<b>PAINTED LOUVERS</b> Color: TBD Manuf: Sherwin Williams	<b>ROOF</b> Color: Charcoal Manuf: Berridge Keyholes: 9 & 13	<b>HM DOORS</b> Color: 7669 Summit Gray Manuf: Sherwin Williams	<b>STOREFRONT DOORS</b> Color: Clear Anodized Aluminum Manuf: YKK	<b>SECTIONAL DOORS</b> Color: White Manuf: Raynor Doors	<b>TINTED GLAZING</b> Color: Solarban 90 Optgray Manuf: Vitro Glass	<b>SPANDEL GLAZING</b> Color: Solarban 90 Optgray with Opac-coat- 300 Charcoal Manuf: Vitro Glass

**Exhibit 5: Material Legend.**



**Exhibit 6: Architectural Rendering South.**



**Exhibit 7: Architectural Rendering North.**



**Exhibit 8: Trash Enclosure.**

## DEVIATIONS

The applicant is not requesting any deviations.

## REVIEW PROCESS

- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on December 19, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

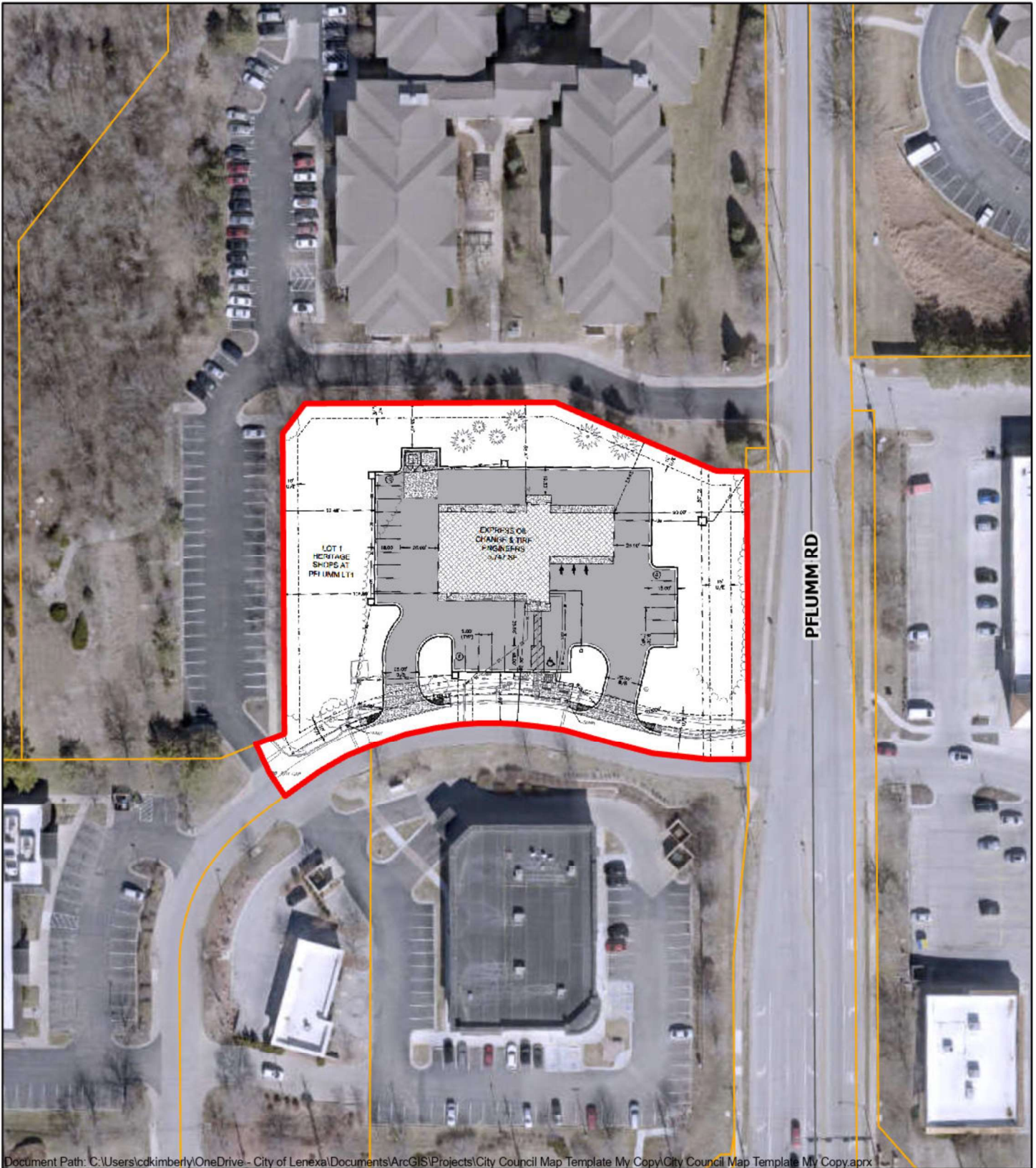
## RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the proposed preliminary plan for Express Oil Change & Tire Engineers.**

- This is a preliminary plan for construction of a new building with an auto service, limited use.
- The project is consistent with Lenexa’s goals through **Responsible Economic Development** to create a **Thriving Economy**.

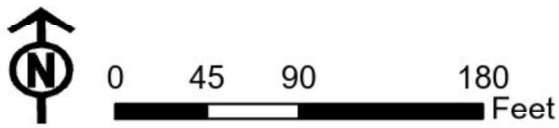
### PRELIMINARY PLAN

Staff recommends **approval** of the preliminary plan for PL23-10P – **Express Oil Change & Tire Engineers** at 8610 Pflumm Road, for an auto service, limited use.



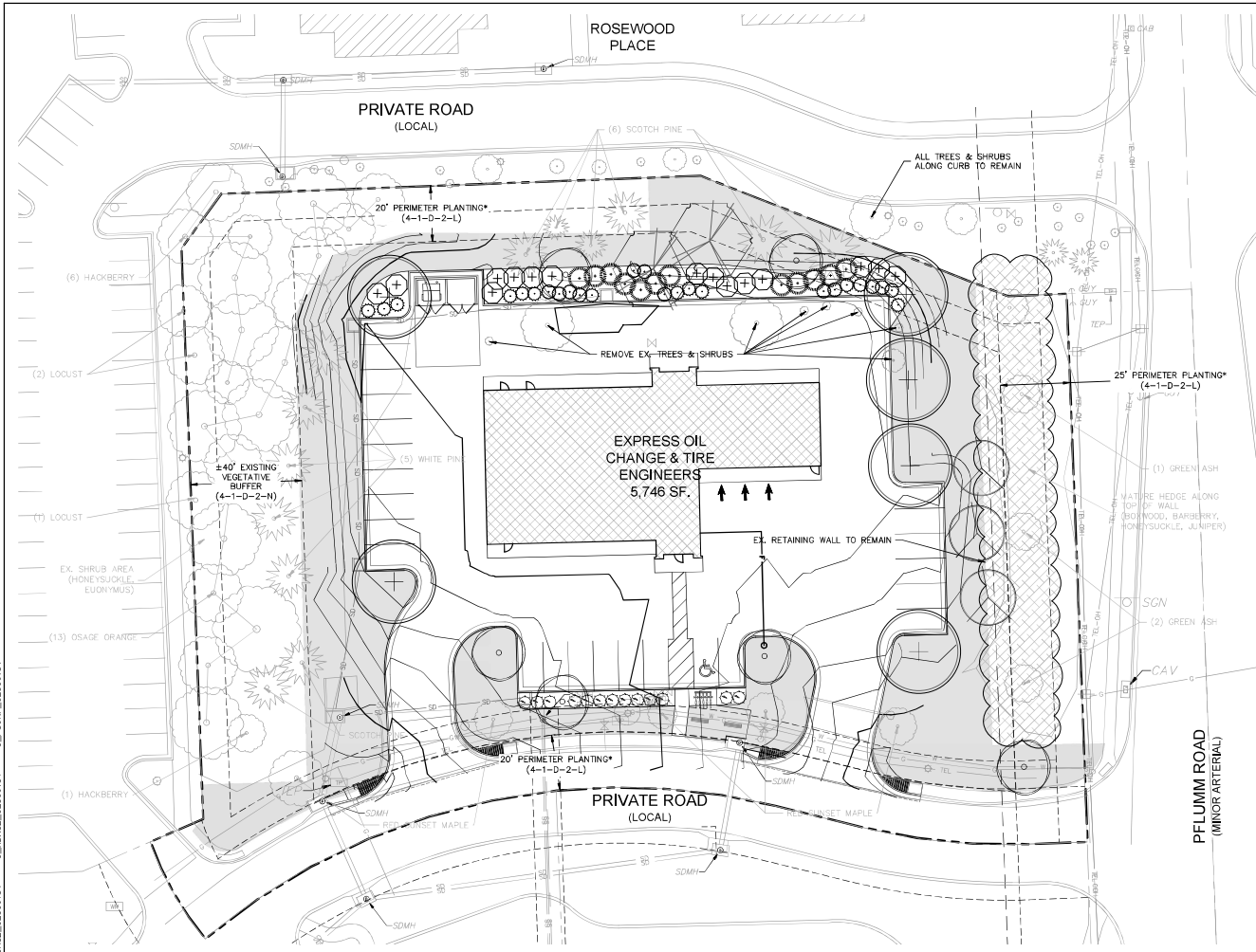
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 For further information, please call 913-477-7500

# Express Oil Change & Tire Engineers PL23-10P





DWG: P:\2023\06001-06500\032-06184\1-Design\Misc\CAD\Vegetation\Planting\Site\Development\_Plan\_V\_C\_03201-02200184.dwg  
 DATE: NOV 20, 2023 11:52am USER: rubert  
 PLOT: C:\BID\23030184\03201-02200184.dwg



**LEGEND**

	PROPERTY LINE
	RIGHT-OF-WAY LINE
	PROPOSED CONTOUR
	EXISTING CONTOUR
	500

**TREE SCHEDULE**

DECIDUOUS TREES	QTY	BOTANICAL / COMMON NAME	SIZE	CAL.
	6	<i>Acer truncatum</i> x <i>platanoides</i> 'JFS-KW202' CRIMSON SUNSET MAPLE	B&B	2.5"
	3	<i>Betula nigra</i> 'Cully' HERITAGE RIVER BIRCH	B&B	2"
	2	<i>Celtis occidentalis</i> HACKBERRY	B&B	2"
	6	<i>Quercus coccinea</i> SCARLET OAK	B&B	2"

**PLANT SCHEDULE**

EVERGREEN SHRUBS	QTY	BOTANICAL / COMMON NAME	SIZE	HEIGHT (MIN.)
	13	<i>Buxus</i> x <i>Yucca</i> Velvet' GREEN VELVET BOXWOOD	5 GAL	24-30"
	12	<i>Juniperus chinensis</i> 'Mini Julep' MINT JULEP JUNIPER	5 GAL	24-30"
	13	<i>Juniperus virginiana</i> 'Grey Owl' GREY OWL JUNIPER	5 GAL	24-30"
DECIDUOUS SHRUBS		BOTANICAL / COMMON NAME		
	12	<i>Foraythia</i> x <i>intermedia</i> 'Spring Glory' SPRING GLORY FORSYTHIA	5 GAL	30-36"
	15	<i>Viburnum burkwoodii</i> 'Muhawk' MOHAWK VIBURNUM	5 GAL	30-36"

**SEEDING / SOD / PLUGS**

SOD/SEED	BOTANICAL / COMMON NAME	TYPE
	FESCUE TURF	SOD

**PERIMETER PLANTING (4-1-D-2-L)**

LOCATION	DEPTH (MIN.)	BUFFER LENGTH	PLANT REQUIREMENTS (PER 100 LF)			
			SHADE TREES (2)	ORNAMENTAL TREES (0)	SHRUBS (12)	
NORTH PRIVATE ROAD (RESIDENTIAL)	20 FEET	312 FEET	REQUIRED	7	0	38
			EXISTING	6	0	38*
			PROVIDED	4	0	0
WEST PRIVATE ROAD (RESIDENTIAL)	20 FEET (60' PROVIDED)	290 FEET	REQUIRED	6	0	35
			EXISTING	32	0	35*
			PROVIDED	0	0	0
SOUTH PRIVATE ROAD (COMMERCIAL)	20 FEET	332 FEET	REQUIRED	7	0	40
			EXISTING	5*	0	0
			PROVIDED	2	0	40
EAST PFLUMM ROAD	25 FEET	197 FEET	REQUIRED	4	2	24
			EXISTING	4	0	24
			PROVIDED	0	0	0

**INTERNAL LANDSCAPING REQUIREMENTS- PARKING LOT**

1 Tree/300 S.F. of Overall Landscape Area  
 Overall Landscape Area = 1843 S.F.  
 7 Trees Required  
 12 Trees Provided  
 7.5% of Parking Lot Area must be Landscaped  
 23,145 S.F. x .075 = 1744 S.F.  
 1744 S.F. Required  
 1843 S.F. Provided

\* EXISTING VEGETATION TO REMAIN TO EXTENT POSSIBLE TO MEET REQUIRED PLANTINGS. IF EXISTING VEGETATION IS REMOVED DURING GRADING OR CONSTRUCTION OPERATIONS, VEGETATION SHALL BE REPLACED WITH LIKE MATERIALS.

**olsson**  
 7701 Main Street, Suite 200  
 Overland Park, KS 66211-1170  
 TEL: 813.381.1170  
 www.olson.com

BY: \_\_\_\_\_  
 REVISIONS DESCRIPTION  
 DATE  
 REV. NO.

LANDSCAPE PLAN  
 PRELIMINARY DEVELOPMENT PLAN  
 EXPRESS OIL CHANGE & TIRE ENGINEERS  
 9610 PFLUMM ROAD  
 LENEXA, KS  
 2023

drawn by: \_\_\_\_\_  
 checked by: \_\_\_\_\_  
 approved by: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 project no.: \_\_\_\_\_  
 drawing no.: 03201-02200184.dwg  
 date: 2023.11.20

**SHEET**  
**L1**

Exterior Keynotes	
Tag	Text
9	Pre-finished standing seam metal roof system
10	Pre-finished metal gutter system
11	Pre-finished metal awning. See details on Sheet A303.
12	Pre-finished metal downspout and downspout boot. Pipe to Storm, see Civil.
13	Pre-finished hip and ridge cap. Color to match roof.
15	1x pressure treated painted fascia board continuous
17	Double brick rowlock accent over 8" cmu. Provide 8" grout filled "u" block bond beam at locations of overhead doors. See Structural.
18	Clay faced field brick over 8" cmu.
19	Clay faced accent brick over 8" cmu.
20	Painted concrete-filled steel bollard
22	Signage by others. Provide blocking as required. See Electrical for power.
23	Wall sconce by others. See electrical for power. Provide blocking as required.
24	Lightbar by others. See electrical for power. Provide blocking as required.
25	Control joint. For control joints in concrete floor slabs, coordinate location with equipment layout by others. Max. distance between control joints in slabs not to exceed 12'-0". Control joints in walls shall be 4'-0" from wall intersection or corner
26	Key box (Locate as directed by the Local Fire Marshal or AHJ)

Exterior Keynotes	
Tag	Text
27	Pre-finished metal coping
35	Foundation sump lift station. Verify location with Civil and tie into Civil storm drain system. Model: Zoeller M86.
37	Lightbar by others. See electrical for power. Provide blocking as required.
44	Concrete apron. Coordinate with Civil.
47	Provide address identification as directed by the Local Fire Marshal or AHJ)
50	Double clay faced brick rowlock accent band over cmu.
52	Sign to be centered on wall horizontally. Align top of sign vertically where wall beigns to pitch unless otherwise indicated. Junction box for sign shall be located in the center of the sign. Verify with sign company prior to rough-in
53	Conduit to be centered horizontally for lights in awning. Verify with sign company prior to rough-in.
54	Locate junction box for sconces 5'-0" a.f.f. vertically and 4" from center horizontally (Typical). Verify with sign company prior to rough-in
56	Metal louver or vent, see Mechanical. Paint to match adjacent surface.
66	1x pressure treated painted frieze board.

Note: Align top of exterior wall packs with bottom of banding at 12'-0" a.f.f. Do not locate exterior wall packs on side of building that contains illuminated lightbars or sconces by others, unless at ext doors as indicated on the Electrical Drawings.



1 01- False Front Elevation (North)  
3/16" = 1'-0"

### EXTERIOR FINISH MATERIAL LEGEND



**CLAY FACED MASONRY**  
Accent Color: Sandstone Dark  
Manuf: Endicott Clay Products  
Keynotes: 17, 18 & 50



**CLAY FACED MASONRY**  
Field Color: Copper Sands  
Manuf: Endicott Clay Products  
Keynote: 18



**PAINTED LOUVERS**  
Color: TBD  
Manuf: Sherwin Williams



**ROOI**  
Color: Cheroke  
Manuf: Berkshire  
Keynotes: 9 & 13



**LIM DOORS**  
Color: 7669 Summit Gray  
Manuf: Sherwin Williams



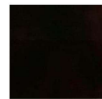
**STONEFRONT DOORS**  
Color: Clear Anodized Aluminum  
Manuf: YKK



**SECTIONAL DOORS**  
Color: White  
Manuf: Raynor Doors



**TINTED GLAZING**  
Color: Solarban 90 Optigray  
Manuf: Vitro Glass



**SPANDREL GLAZING**  
Color: Solarban 90 Optigray  
with Opac-coat 300 Charcoal  
Manuf: Vitro Glass



NOT FOR  
CONSTRUCTION

Express Oil Change & Tire Engineers  
Single Building - Right Hand Oil Change / Rear Entry with Side Tire Storage  
Lenexa, Kansas

CONCEPTUAL

No.	Description	Date

2023  
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Exterior Elevation -  
False Front (North)

Project number 23053  
Date 11/20/2023  
Drawn by ARC  
Checked by TAA

A200

Scale: As indicated

Tag	Text
9	Pre-finished standing seam metal roof system
10	Prefinished metal gutter system
11	Prefinished metal awning. See details on Sheet A303.
12	Prefinished metal downspout and downspout boot. Pipe to Storm, see Civil.
13	Pre-finished hip and ridge cap. Color to match roof.
15	1x pressure treated painted fascia board continuous
17	Double brick rowlock accent over 8" cmu. Provide 8" grout filled "u" block bond beam at locations of overhead doors. See Structural.
18	Clay faced field brick over 8" cmu.
19	Clay faced accent brick over 8" cmu
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22	Signage by others. Provide blocking as required. See Electrical for power.
23	Wall sconce by others. See electrical for power. Provide blocking as required.
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25	Control joint. For control joints in concrete floor slabs, coordinate location with equipment layout by others. Max. distance between control joints in slabs not to exceed 12'-0". Control joints in walls shall be 4'-0" from wall intersection or corner
26	Key box (Locate as directed by the Local Fire Marshal or AHJ)

Tag	Text
27	Prefinished metal coping
35	Foundation sump lift station. Verify location with Civil and tie into Civil storm drain system. Model: Zoeller M98.
37	Lightbar by others. See electrical for power. Provide blocking as required.
44	Concrete apron. Coordinate with Civil.
47	Provide address identification as directed by the Local Fire Marshal or AHJ
50	Double clay faced brick rowlock accent band over cmu.
52	Sign to be centered on wall horizontally. Align top of sign vertically where wall begins to pitch unless otherwise indicated. Junction box for sign shall be located in the center of the sign. Verify with sign company prior to rough-in
53	Conduit to be centered horizontally for lights in awning. Verify with sign company prior to rough-in.
54	Locate junction box for sconces 5'-0" a.f.f. vertically and 4" from center horizontally (Typical). Verify with sign company prior to rough-in.
56	Metal louver or vent, see Mechanical. Paint to match adjacent surface.
66	1x pressure treated painted frieze board.

Note: Align top of exterior wall packs with bottom of banding at 12'-0" a.f.f. Do not locate exterior wall packs on side of building that contains illuminated lightbars or sconces by others, unless at exit doors as indicated on the Electrical Drawings.



1 02- Rear Entry Elevation (South)  
3/16" = 1'-0"

EXTERIOR FINISH MATERIAL LEGEND

								
<b>CLAY FACED MASONRY</b> Accent Color: Sandstone Dark Manuf: Endicott Clay Products Keynotes: 17, 19 & 54	<b>CLAY FACED MASONRY</b> Field Color: Copper Sands Manuf: Endicott Clay Products Keynote: 18	<b>PAINTED LOUVERS</b> Color: T6D Manuf: Sherwin Williams	<b>ROOF</b> Color: Charcoal Manuf: Bertrige Keynotes: 9 & 13	<b>H.M. DOORS</b> Color: 7669 Summit Gray Manuf: Sherwin Williams	<b>STOREFRONT DOORS</b> Color: Clear Anodized Aluminum Manuf: YKK	<b>SECTIONAL DOORS</b> Color: White Manuf: Raynor Doors	<b>TINTED GLAZING</b> Color: Solartan 90 Optigray Manuf: Vitro Glass	<b>SPANDEL GLAZING</b> Color: Solartan 90 Optigray with Opac-coat-300 Charcoal Manuf: Vitro Glass



NOT FOR CONSTRUCTION

00/00/0000

Express Oil Change & Tire Engineers  
Single Building - Right Hand Oil Change / Rear Entry with Side Tire Storage  
Lenexa, Kansas

CONCEPTUAL

No.	Description	Date

2023  
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Exterior Elevation - Rear Entry (South)

Project number 23053  
Date 11/20/2023  
Drawn by ARC  
Checked by TAA

A201

Scale: As indicated

11/20/2023 11:20:30 AM

Exterior Keynotes	
Tag	Text
9	Pre-finished standing seam metal roof system
10	Prefinished metal gutter system
11	Prefinished metal awning. See details on Sheet A303.
12	Prefinished metal downspout and downspout boot. Pipe to Storm, see Civil.
13	Pre-finished hip and ridge cap. Color to match roof.
15	1x pressure treated painted fascia board continuous
17	Double brick rowlock accent over 8" cmu. Provide 8" grout filled "u" block bond beam at locations of overhead doors. See Structural.
18	Clay faced field brick over 8" cmu.
19	Clay faced accent brick over 8" cmu
20	Painted concrete-filled steel bollard
22	Signage by others. Provide blocking as required. See Electrical for power.
23	Wall sconce by others. See electrical for power. Provide blocking as required.
24	Lightbar by others. See electrical for power. Provide blocking as required.
25	Control joint. For control joints in concrete floor slabs, coordinate location with equipment layout by others. Max. distance between control joints in slabs not to exceed 12'-0". Control joints in walls shall be 4'-0" from wall intersection or corner
26	Key box (Locate as directed by the Local Fire Marshal or AHJ)






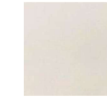

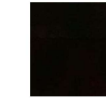
Exterior Keynotes	
Tag	Text
27	Prefinished metal coping
35	Foundation sump lift station. Verify location with Civil and tie into Civil storm drain system. Model: Zoeller M98.
37	Lightbar by others. See electrical for power. Provide blocking as required.
44	Concrete apron. Coordinate with Civil.
47	Provide address identification as directed by the Local Fire Marshal or AHJ)
50	Double clay faced brick rowlock accent band over cmu.
52	Sign to be centered on wall horizontally. Align top of sign vertically where wall begins to pitch unless otherwise indicated. Junction box for sign shall be located in the center of the sign. Verify with sign company prior to rough-in
53	Conduit to be centered horizontally for lights in awning. Verify with sign company prior to rough-in.
54	Locate junction box for sconces 5'-0" a.f.f. vertically and 4" from center horizontally (Typical). Verify with sign company prior to rough-in.
56	Metal louver or vent, see Mechanical. Paint to match adjacent surface.
66	1x pressure treated painted frieze board.

Note: Align top of exterior wall packs with bottom of banding at 12'-0" ±. Do not locate exterior wall packs on side of building that contains illuminated lightbars or sconces by others, unless at exit doors as indicated on the Electrical Drawings.



1 04- Left Elevation (West)  
3/16" = 1'-0"

### EXTERIOR FINISH MATERIAL LEGEND

								
<b>CLAY FACED MASONRY</b> Accent Color: Sandstone Dark Manuf: Endicott Clay Products Keynotes: 17, 18 & 50	<b>CLAY FACED MASONRY</b> Field Color: Copper Sands Manuf: Endicott Clay Products Keynote: 18	<b>PAINTED LOUVERS</b> Color: TBD Manuf: Sherwin Williams	<b>COIL</b> Color: Charcoal Manuf: Berridge Keynotes: 9 & 13	<b>HM DOORS</b> Color: 1659 Summit Gray Manuf: Sherwin Williams	<b>STOREFRONT DOORS</b> Color: Clear Anodized Aluminum Manuf: YKK	<b>SECTIONAL DOORS</b> Color: White Manuf: Raynor Doors	<b>TINTED GLAZING</b> Color: Solarban 90 Optigray Manuf: Vitro Glass	<b>SPARTAN GLAZING</b> Color: Solarban 90 Optigray with Opac-Coat-300 Charcoal Manuf: Vitro Glass



**NOT FOR CONSTRUCTION**

00/00/0000

**Express Oil Change & Tire Engineers**  
 Single Building - Right Hand Oil Change / Rear Entry with Side Tire Storage  
 Lenexa, Kansas

CONCEPTUAL

No.	Description	Date

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Exterior Elevation - Left (West)

Project number: 23053  
Date: 11/20/2023  
Drawn by: ARC  
Checked by: TAA

**A202**

Scale: As indicated

11/20/2023 11:24:56 AM



Exterior Keynotes	
Tag	Text
9	Pre-finished standing seam metal roof system
10	Pre-finished metal gutter system
11	Pre-finished metal awning. See details on Sheet A303.
12	Pre-finished metal downspout and downspout boot. Pipe to Storm, see Civil.
13	Pre-finished hip and ridge cap. Color to match roof.
15	1x pressure treated painted fascia board continuous
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Tag	Text
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1-03- Right Elevation (East)  
3/16" = 1'-0"

### EXTERIOR FINISH MATERIAL LEGEND



**CLAY FACED MASONRY**  
Accent Color: Sainsbore Dark  
Manuf.: Enticott Clay Products  
Keynotes: 17, 19 & 50



**CLAY FACED MASONRY**  
Field Color: Copper Sands  
Manuf.: Enticott Clay Products  
Keynote: 18



**PAINTED LOUVERS**  
Color: TBD  
Manuf.: Sherwin Williams



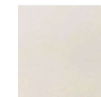
**ROOF**  
Color: Charcoal  
Manuf.: Serrige  
Keynotes: 9 & 13



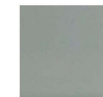
**HM DOORS**  
Color: 7669 Summit Gray  
Manuf.: Sherwin Williams



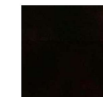
**STOREFRONT DOORS**  
Color: Clear Anodized Aluminum  
Manuf.: YKK



**SECTIONAL DOORS**  
Color: White  
Manuf.: Raynor Doors



**UNITED GLAZING**  
Color: Solarban 90 Optigray  
Manuf.: Vitro Glass



**SPANREL GLAZING**  
Color: Solarban 90 Optigray with Lpcac-coat-300 Charcoal  
Manuf.: Vitro Glass



**NOT FOR CONSTRUCTION**

00/00/0000

Express Oil Change & Tire Engineers  
Single Building - Right Hand Oil Change / Rear Entry with Side Tire Storage  
Lenexa, Kansas

#### CONCEPTUAL

No.	Description	Date

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Exterior Elevation - Right (East)

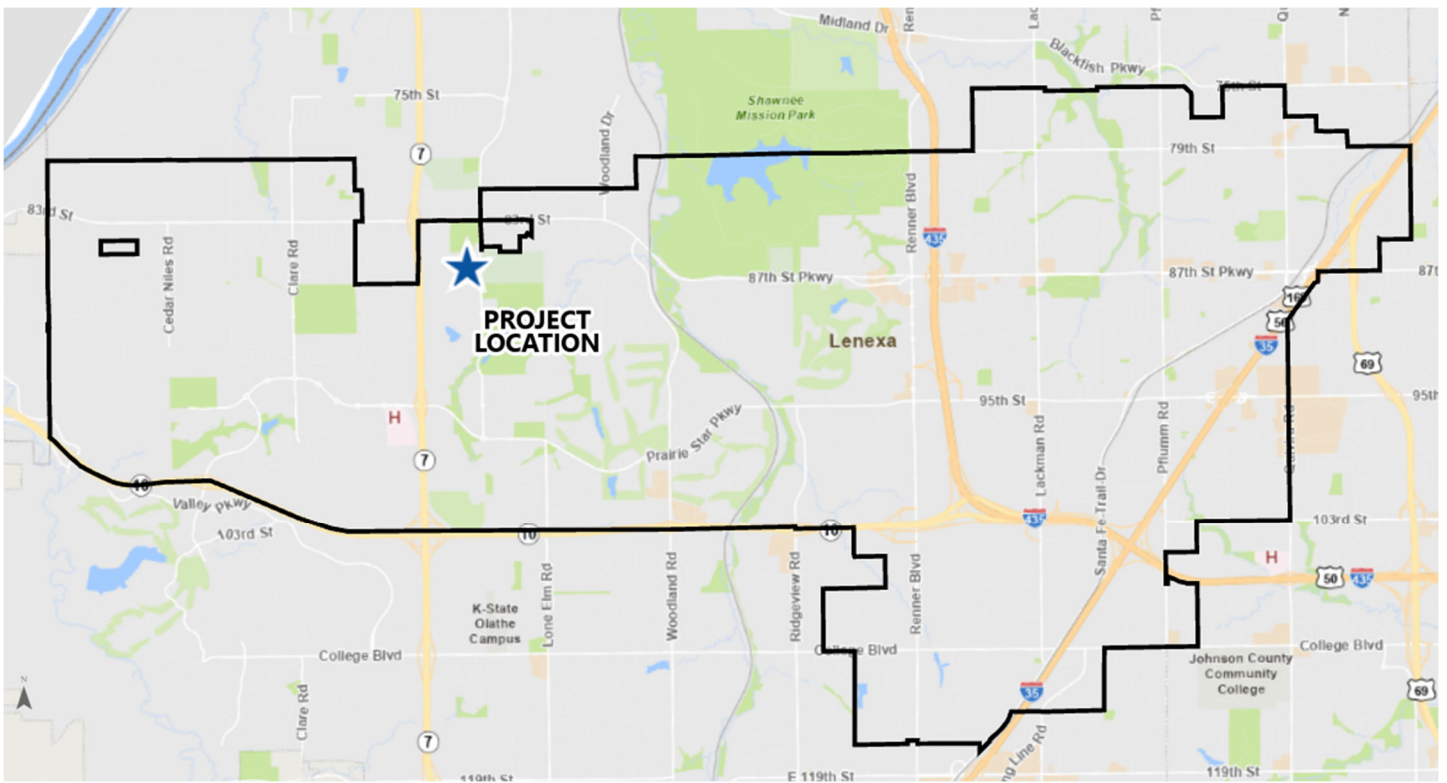
Project number: 23053  
Date: 11/20/2023  
Drawn by: ARC  
Checked by: TAA

**A203**

Scale: As indicated

# THE RESIDENCES ON WOODSONIA (FKA K7 & 83<sup>RD</sup> STREET MULTIFAMILY DEVELOPMENT)

<b>Project #:</b>	RZ23-08 & PL23-11P	<b>Location:</b>	Northeast corner of 86 <sup>th</sup> Street (future) and Woodsonia Drive
<b>Applicant:</b>	Brian Hill, MKEC	<b>Project Type:</b>	Rezoning & Preliminary Plan
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	Multifamily Residential



## PROJECT SUMMARY

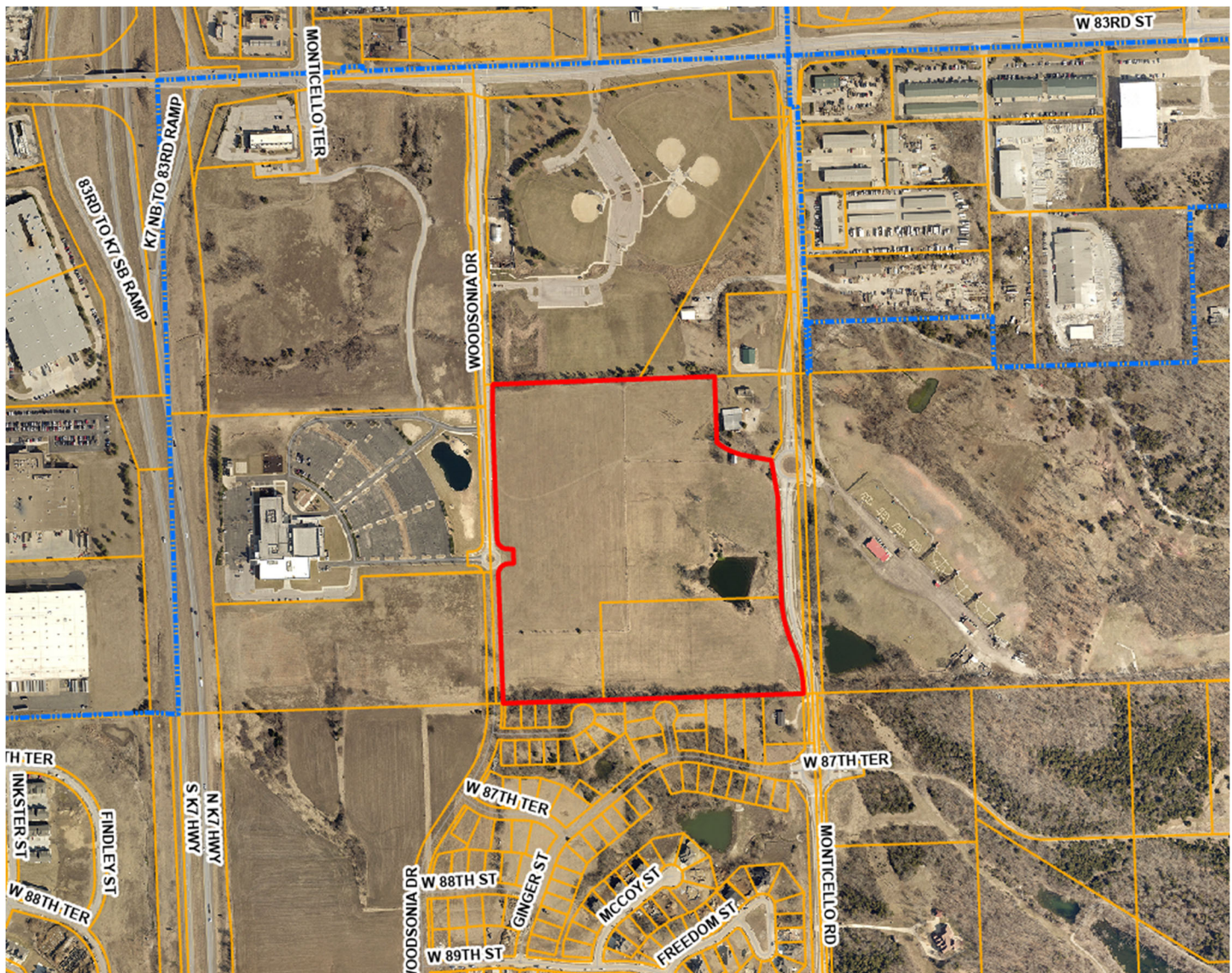
The applicant requests approval to rezone the property at the northeast corner of 86<sup>th</sup> Street (future street) and Woodsonia Drive from the AG, Agricultural District to the RP-2, Planned (Intermediate-Density) District and RP-4, Planned (High-Density) District to develop multifamily residences. The companion preliminary plan shows an apartment component on the northerly part of the site and a townhome component on the southerly part of the site. The north and south components are separated by a new public street that will connect Woodsonia Drive to Monticello Road on the east. A public hearing is required for the rezoning request.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

This site is a 37.97-acre undeveloped tract of land located south of Freedom Fields Park between Woodsonia Drive on the west and Monticello Road on the east. The project will have two components: an apartment building component on the northerly side and a townhome component on the southerly side. A new public street will be constructed between the two components to connect Woodsonia Drive to Monticello Road. All drive entrances into both the apartments and townhomes will connect to the new street. The two components are proposed to be zoned two different residential zoning districts (RP-4 and RP-2) reflective of the type of residential development.

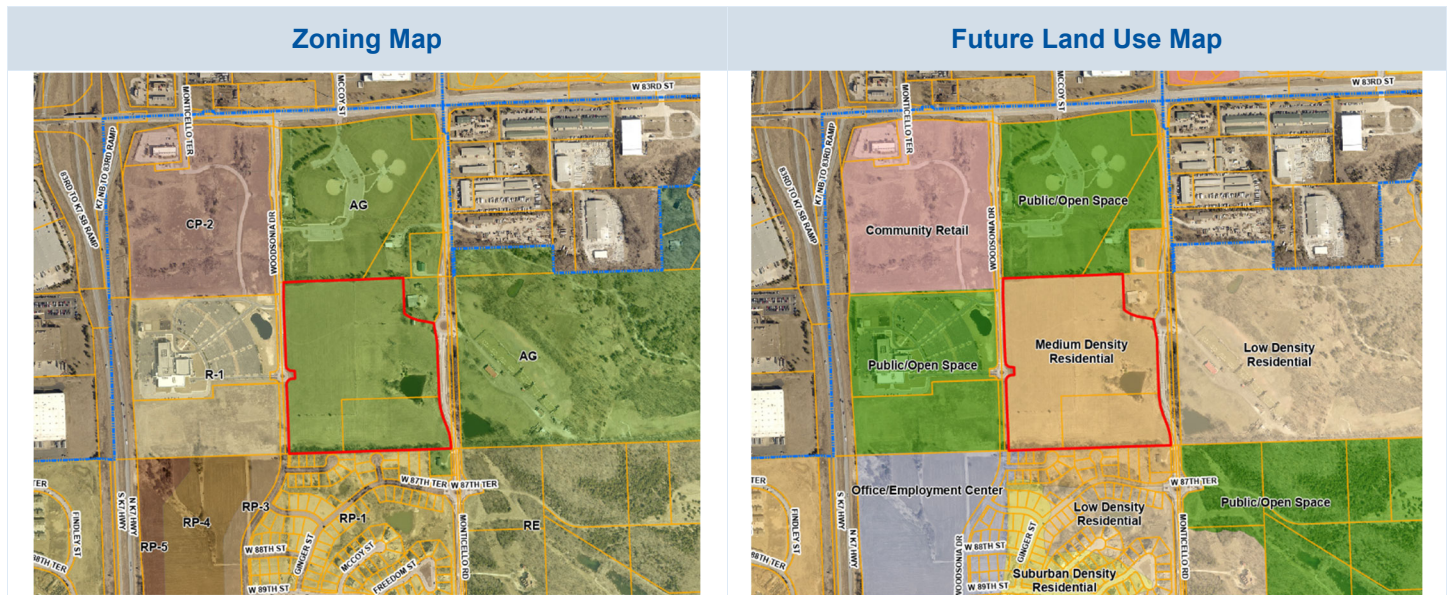
LAND AREA (AC)	DWELLING UNITS (UPA)	CURRENT ZONING	COMP. PLAN
37.97	7.95	AG	Medium Density Residential



**Exhibit 1: Aerial Image of Subject Site.**

## LAND USE REVIEW

The proposed use is multifamily residential containing apartments and townhome dwellings. The applicant is requesting RP-4 and RP-2 zoning which will result in an average density of 7.95 dwelling units per acre. The Future Land Use Map designation for the site is Medium Density Residential. This land use category description is: *Moderate density dwellings, including attached dwellings, not exceeding a gross density of 8 dwellings per acre.* The proposed development of the site is consistent with the Future Land Use Map designation.



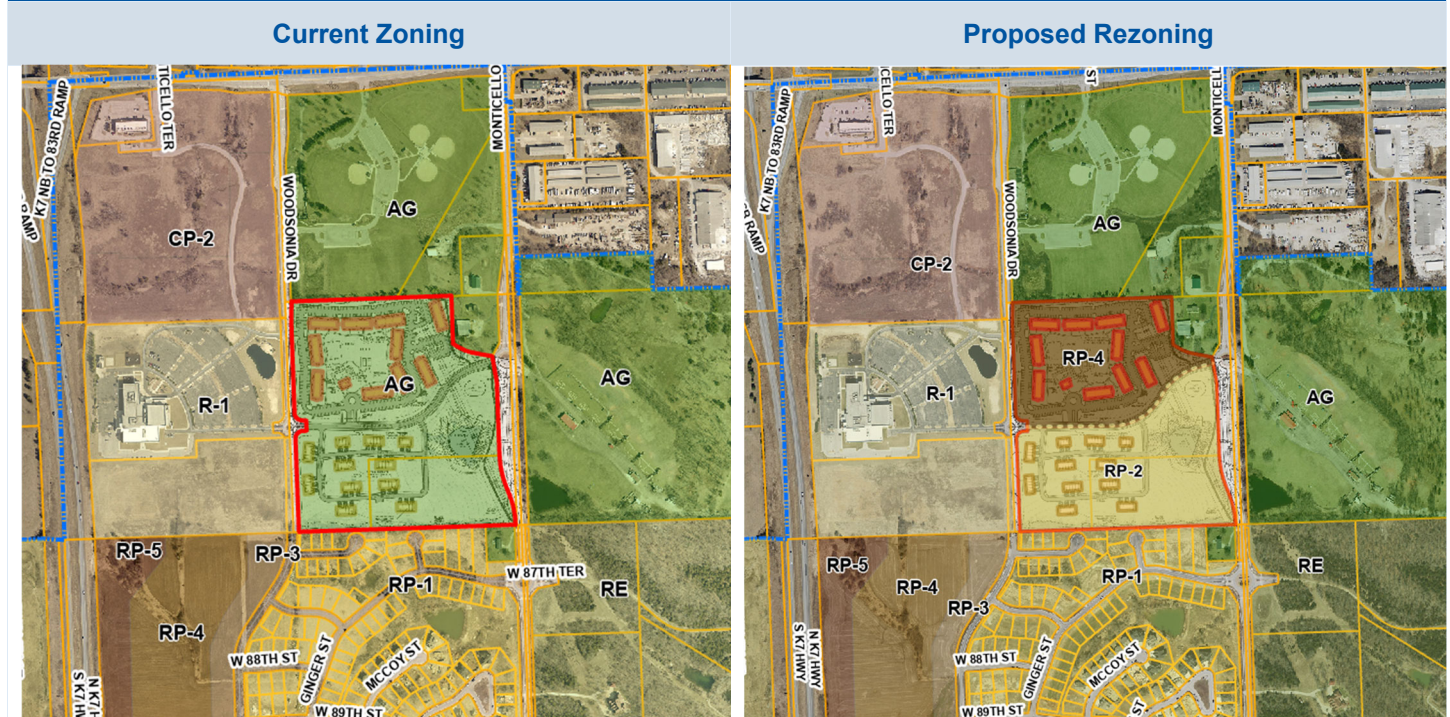
**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Medium Density Residential	AG, Agricultural District	Undeveloped land
<b>North</b>	Public/Open Space	AG, Agricultural District	Public park
<b>South</b>	Medium Density Residential	RP-1, Planned Residential Single-Family (Low-Density) District	Single-family residential
<b>East</b>	Low Density Residential	AG, Agricultural District	Entertainment, outdoor
<b>West</b>	Public/Open Space	R-1, Residential Single-Family (Low-Density) District	Church or place of worship

## REZONING REVIEW

The applicant proposes to rezone the subject site from the AG (Agricultural) District to the RP-2, Planned (Intermediate-Density) and RP-4, Planned (High-Density) Districts.

**TABLE 2: REZONING ANALYSIS**



Staff provides the following analysis for the review criteria within [Section 4-1-G-5](#) of the Unified Development Code (UDC).

### 1. The character of the neighborhood.

The character of the neighborhood is a region that contains different uses of lessening intensities progressing southeasterly from the K-7 Highway and 83<sup>rd</sup> Street intersection. The site at the immediate southeast corner of the K-7 Highway and 83<sup>rd</sup> Street intersection is a 35-acre tract zoned for a Community Commercial size center. A multi-tenant retail building exists within this commercial center. The site is approved for a several more retail buildings including a mid-size box store consistent with the scale of a grocery store. This site was zoned for commercial development in 1999 (RZ99-07). Freedom Fields City Park is immediately north of the subject site. Westside Family Church is to the west along the west side of Woodsonia Drive. Powder Creek, a recreational firing range for target and skeet shooting, exists to the east on the east side of Monticello Road. The Watercrest Landing single-family residential subdivision is south of the subject site. Undeveloped land zoned for multifamily development is nearby to the southwest, and another City park, Blackhoof Park, is located to the southeast.

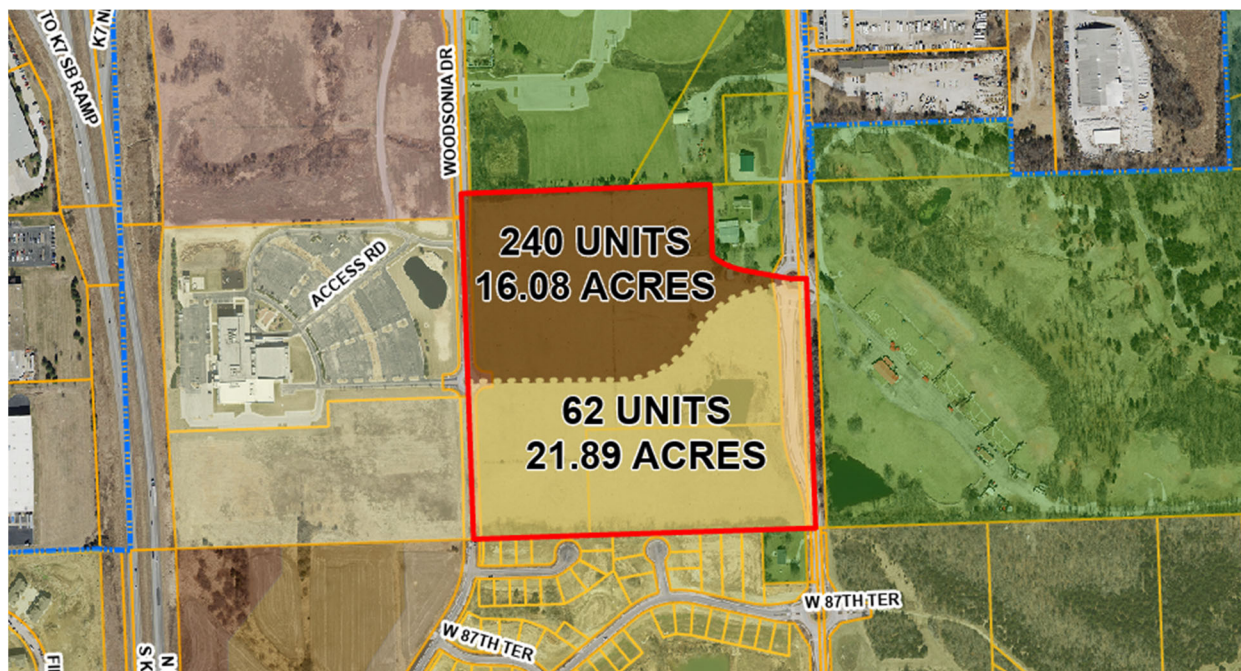
**2. The zoning and use of properties nearby.**

The zoning and uses of the adjacent properties vary. The zoning and land use of adjacent properties is noted in Table 1.

**3. The suitability of the subject property for the uses to which it has been restricted.**

The property is suited for Medium Density Residential development as reflected on the Future Land Use Map within the Comprehensive Plan. Medium Density Residential land use is described as the type of development that contains up to eight dwelling units per acre. The proposed development will have an average density of 7.95 units per acre. Though the RP-4 Zoning District is typically not associated with the medium-density land use classification, shifting density to the north, through the use of the RP-4 Zoning District, allows the south RP-2 Zoning District to be developed with a housing type and density more compatible with the single-family housing to the south of the property. The expectation is that the cumulative density of the two components do not exceed eight units per acre regardless of whichever part develops first or if another developer acquires the site or half the site and desires to increase the density of the south RP-2 zoned area.

<b>TABLE 3: DENSITY ANALYSIS</b>			
<b>Zoning</b>	<b>Units</b>	<b>Acres (gross land area)</b>	<b>Density</b>
<b>RP-4 (apartments)</b>	240	16.08	14.96
<b>RP-2 (townhomes)</b>	62	21.89	2.83
<b>TOTAL</b>	<b>302</b>	<b>37.97</b>	<b>7.95 (average)</b>



**Exhibit 2: Density Distribution.**

**4. The extent to which the proposed use will detrimentally affect nearby property.**

The proposed use will not detrimentally affect nearby property. The proposed use is compatible with the uses of adjacent properties. The two different residential types, apartments on the north and townhomes on the south, provides a transition from commercial development to the northeast to the suburban subdivision to the south.

**5. The length of time the subject property has remained vacant as zoned.**

The property is undeveloped agricultural land and has been zoned AG since being annexed into the City in 1986.

**6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.**

It is Staff's opinion that denial of this rezoning would have no gain to public health, safety, or welfare.

**7. Recommendation of City's permanent professional staff.**

See Staff's recommendation and the end of this report.

**8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.**

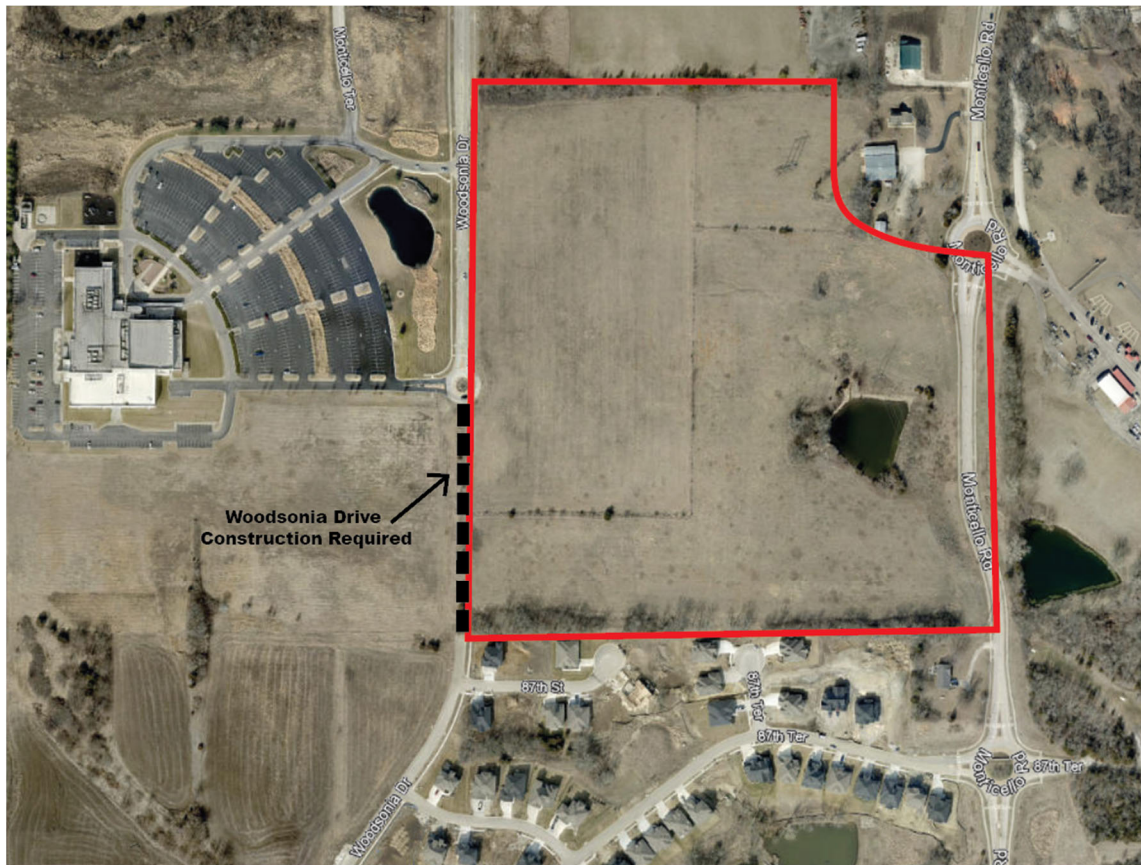
The Future Land Use designation for the site is Medium Density Residential Use. Per the companion description of Medium Density Residential Use, sites that are designated this land use are not to exceed a gross density of eight dwelling units per acre. The proposed zoning districts, when considered with the preliminary plan that concentrates density to the north, are consistent with the Comprehensive Plan limit of eight dwelling units per acre.

**9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.**

The site is in a transitioning area of the City. Several nearby properties are either developed or developing and several properties remain undeveloped. Adequate utilities and services are provided to the subject property. The site is subject to the City's stormwater management requirements applicable to all development in the City. The site is within the Olathe School District.

**10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.**

It is Staff's opinion the proposed use will not adversely impact the capacity or the safety of the street network or present a parking problem in the vicinity of the site. The site abuts Woodsonia Drive, a collector street, to the west, and Monticello Road, a minor arterial street, to the east. Staff notes that one of the requirements for this project is that if it does not already exist at the time this project commences, Woodsonia Drive must be constructed along the entirety of the west property line from its current cul-de-sac termination south to the existing southern stub (roughly 600 to 700 feet south; see Exhibit 3).



**Exhibit 3: Required Construction of Woodsonia Drive.**

A new local public street will be constructed between Woodsonia Drive and Monticello Road. The primary access into the apartment component is from the new local street. A secondary entrance is provided on Woodsonia Drive, across from a drive entrance into Westside Family Church.

The townhome component will have two drive connections onto the new local street. The site will have adequate access for ingress and egress and the points of access are adequately spaced for distribution of vehicles during peak times of circulation.

Off-street parking requirements are met per Table 4.

<b>TABLE 4: PARKING ANALYSIS</b>			
<b>Use</b>	<b>Requirement</b>	<b>Required</b>	<b>Provided</b>
<b>Multifamily (apartments)</b>	1 space per efficiency unit, 1.5 spaces per 1-bedroom unit, 1.75 spaces per 2-bedroom unit, 2 spaces per 3+-bedroom units and 0.25 spaces per unit for visitor parking if parking spaces are located in common parking area	480	481
<b>Multifamily (townhomes)</b>	1 space per efficiency unit, 1.5 spaces per 1-bedroom unit, 1.75 spaces per 2-bedroom unit, 2 spaces per 3+-bedroom units and 0.25 spaces per unit for visitor parking if parking spaces are located in common parking area	140	140
<b>TOTAL</b>		<b>620</b>	<b>621</b>



**11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.**

The proposed multifamily residential development is not anticipated to generate any excessive impacts exceeding the requirements of the UDC.

**12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.**

The site is subject to the UDC requirements for stormwater management and is required to meet the same standards as any new development.

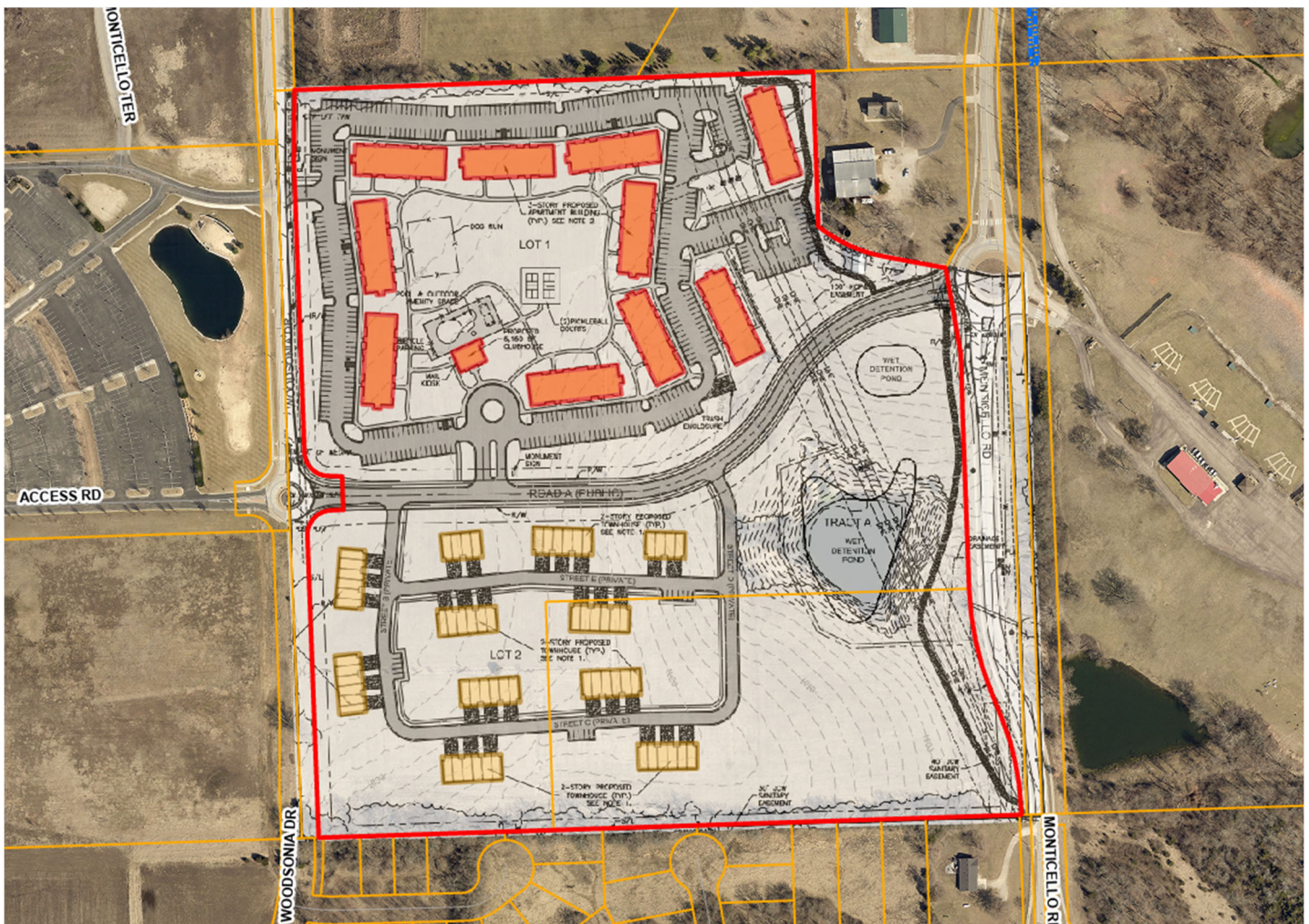
**13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.**

The preliminary plan is in compliance with the zoning requirements of the RP-2 and RP-4 Zoning Districts. The applicant is requesting a building height deviation. [Section 4-1-B-27-G-4-d](#) of the UDC allows for a building height deviation of up to 35 percent. The applicant is requesting a maximum building height of 38 feet, which is 3 feet taller than the 35-foot maximum building height in [Section 4-1-B-9-F](#) of the UDC. A deviation request may be considered using the criteria listed in [Section 4-1-B-27-G-4](#) of the UDC. The deviation request is discussed in more detail within the Deviations section of this report.

## PRELIMINARY PLAN REVIEW

The site is located south of 83<sup>rd</sup> Street between Woodsonia Drive on the west and Monticello Road on the east. A new public street will divide the site into a northerly component for a traditional apartment development and southerly component for in-line townhomes. Primary access into the sites is from the new public street. A secondary access point for the apartments is provided at the northwest corner of the site along Woodsonia Drive. Two access drives are provided off of the new public street for the townhomes. One of the requirements for this project is that if it does not already exist at the time this project commences, Woodsonia Drive construction from its current cul-de-sac termination south to the existing southern stub (roughly 600 to 700 feet south) must be completed (see Exhibit 3).

The applicant submitted a preliminary stormwater management study indicating the intent to meet Lenexa’s stormwater management requirements. This is proposed to be done via both above-ground and below-ground detention facilities.



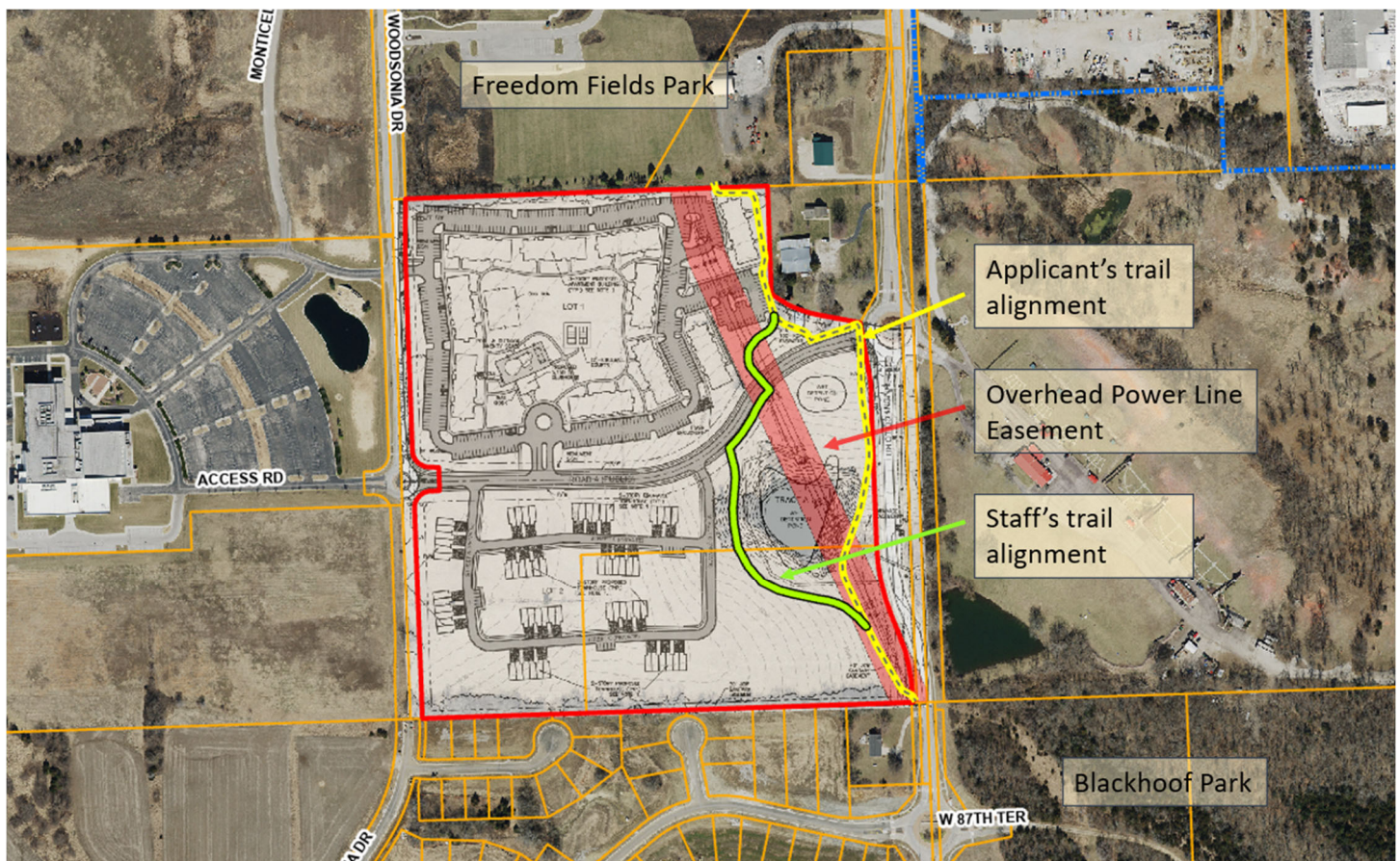
**Exhibit 4: Site Plan.**

The apartment component contains ten apartment buildings and a clubhouse. The buildings are located on the site to encompass an internal court type of space which includes the clubhouse and pool amenity, sport courts, and large lawn space. The parking and drives for circulation through the site establish an external “ring” around the buildings. This configuration is intentional to provide convenient access to the amenities for most of the

residents without crossing parking areas. Two of the apartment buildings are outside of the ring of parking and drives, which will require a longer walk to the amenities. While not as convenient as the closer buildings, the amenities are still accessible by a network of sidewalks from each building.

The townhome component contains eleven buildings of either four- or six-units per building. Access through the townhomes is configured in a simple grid design of two east-west and two north-south drives. Each townhome will have one garage space and one space for a vehicle to park in the driveway. Three additional parking areas along the private drives provide 16 visitor parking spaces.

A public recreation trail link crosses the site along the eastern side. The trail is shown on the Lenexa Trails Alignment Analysis study. The trail is in the vicinity of a utility easement containing overhead power lines. The applicant shows a conceptual alignment of a future trail. The alignment is along the property line at the northeast corner of the site then veers easterly away from the utility easement closer to Monticello Road. Staff prefers the alignment follow the utility easement as close as possible taking an alternate alignment than what is conceptually shown on the plan. The following Exhibit 5 shows Staff's suggestion in the solid green line and the proposed alignment in the yellow dashed line.



**Exhibit 5: Trail Route.**

The applicant's proposed trail alignment closely follows Monticello Road. Staff's suggested alternative alignment will move the trail further from the street. This alternative will require appropriate grading around the detention facility to accommodate the trail. This alignment is closer to the townhomes and can be easily accessed from internal sidewalks by the residents. Staff will continue to coordinate the trail alignment with the applicant prior to defining a trail easement on a preliminary plat. An easement providing for public access/use on the trail will need to be dedicated with the plat for this property.

## DEVIATIONS

The applicant requests a building height deviation for the apartment buildings. The apartment buildings are three-story buildings with a gable style pitched roof. The buildings are 38 feet tall, which is three feet taller than the building height requirement per [Section 4-1-B-9-F](#) of the UDC. Per [Section 4-1-B-27-G-d](#) of the UDC, a request for a building height deviation may be made for up to 35% of the height of the building. The applicant requests a three-foot increase to the building height, which is an 8% increase to the building height. The height of a building with a pitched roof is measured at the midpoint of the ridge of the building and the eave line. This request is for all ten apartment buildings.

TABLE 4: BUILDING HEIGHT DEVIATION			
Deviation	Requirement	Proposed	Difference
Building Height	35 feet	38 feet	3 feet (+8%)

The UDC has different standards for consideration of requests for deviations, depending on what the deviation is for. [Section 4-1-B-27-G](#) of the UDC lists criteria for specific code sections. The criteria are written to protect the rights of adjacent property owners and to not be construed as a privilege awarded to the applicant. It is Staff's opinion the buildings are more proportionally designed and are more aesthetically pleasing with the roof form as proposed. Lowering the overall roofline will result in a building design that appears disproportional and as if the buildings were compressed. The increase of three additional feet of building height will allow for nine-foot-tall floor levels of the top floor of the buildings which is a preferred design for the customer. The requested deviation is the minimum needed to achieve the desired floor height and results in an 8% increase in height where the UDC allows a request of up to 35% of a building height increase. Staff supports the deviation request to allow a three-foot increase in the maximum building height.

## ARCHITECTURE

The buildings, both apartments and townhomes, use a traditional architectural design. The exterior finish materials for the apartments are stone, lap siding, board and batten siding, shake shingle siding and asphalt shingles. The buildings include a projecting beam detail at the gable ends of the roof. Several windows incorporate a sill and lintel detail, and others include shutters. The windows are double-hung style widows with mullion patterns. The inclusion of details adds to the character of the buildings. Staff expects these details to carry through with subsequent plan approvals for the development.



**Exhibit 5: Apartment elevation.**

The townhome buildings are a similar style to the apartments and use the same palette of materials, which creates continuity between the development. The townhomes add standing seam to the roof sections over doorways. The townhomes are expected to have variations in the design from building to building. The variations are to include application of the exterior finish materials and material colors. This detail will be reviewed with final plans for the buildings.



**Exhibit 6: Townhome elevations (6-plex and 4-plex).**

## LANDSCAPING

The landscape plan shows the required number of trees and shrubs required for the site perimeter boundaries, street frontages, and the parking lot. The south property line of the townhome component of the site is adjacent to Watercrest Landing, a single-family subdivision. The townhome component of the development has a density of 2.83 units per acre. This is an equivalent density to the Watercrest Landing subdivision; therefore, does not technically require a Land Use Intensity (LUI) buffer per [Section 4-1-D-2-N](#) of the UDC.

Two six-unit townhome buildings are along the south side of the site. The two townhome buildings are set back 91 feet for one building and 102 feet for the other. The south property line is appropriately landscaped to provide screening between the two neighboring developments.

The landscape plan does not show landscaping around either the apartment or townhome buildings. Landscaping around the buildings is referred to as foundation plantings. The foundation planting design will be provided with the final plan submittal.

## REVIEW PROCESS

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- This project requires a recommendation from the Planning Commission and approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on December 19, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

## RECOMMENDATION FROM PROFESSIONAL STAFF

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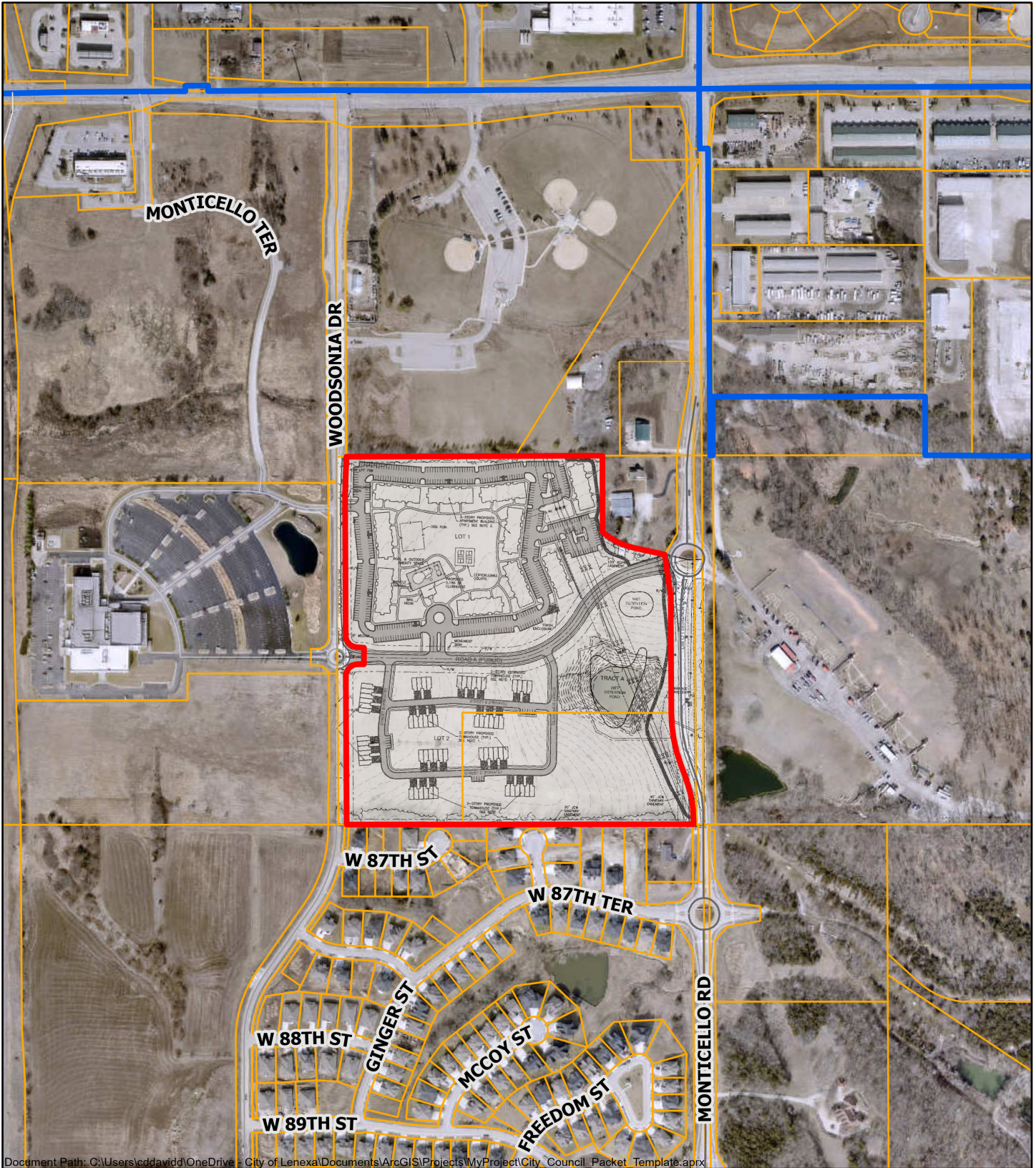
- ★ **Conduct a Public Hearing for the rezoning request.**
- ★ **Staff recommends approval of the proposed rezoning and preliminary plan for The Residences on Woodsonia.**
  - The project is consistent with Lenexa’s goals through *Responsible Economic Development* to create *Vibrant Neighborhoods* and a *Thriving Economy*.

### REZONING

Staff recommends **approval** for rezoning property from AG to RP-4 and RP-2 for **The Residences on Woodsonia** located at the northeast corner of 86<sup>th</sup> Street (future) and Woodsonia Drive.

### PRELIMINARY PLAN

Staff recommends **approval** of the preliminary plan for **The Residences on Woodsonia** located at the northeast corner of 86<sup>th</sup> Street (future) and Woodsonia Drive, for multifamily uses, with a deviation from the UDC as listed in the Staff Report.

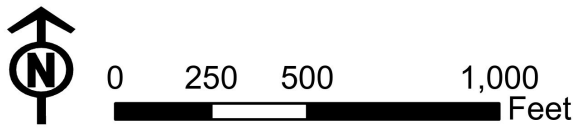


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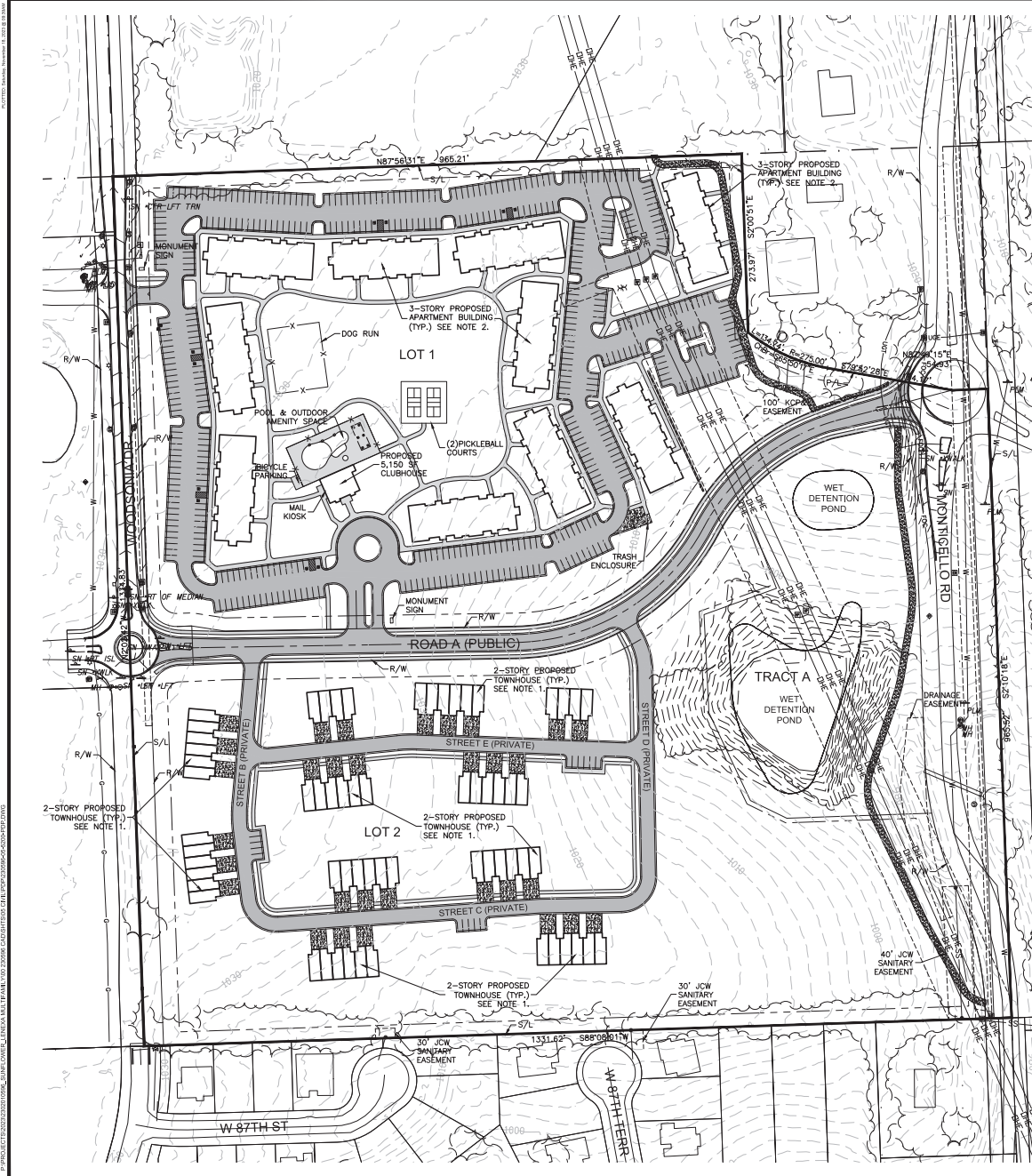
# The Residences on Woodsonia

## RZ23-08 & PL23-11P





REZONING & PRELIMINARY DEVELOPMENT PLANS FOR  
**THE RESIDENCES ON WOODSONIA**  
 LENEXA, KANSAS 66227



City of Lenexa Multi-Family Parking Requirements

Lot 1				
Unit Type	Unit Count	Multi Factor	Total Stalls	
1BR/1BA	60	1.5	90	
2BR/2BA	120	1.75	210	
3BR/3BA	60	2	120	
Visitor Parking	240	0.25	60	
Total Code Required			480	
Total Provided			481	

Lot 2				
Unit Type	Unit Count	Multi Factor	Total Stalls	
Townhomes	62	2	124	
Visitor Parking	62	0.25	16	
Total Code Required			140	
Total Provided			140	

\* EACH TOWNHOME HAS 1 GARAGE SPACE & 1 DRIVEWAY SPACE, FOR A TOTAL OF 2 PARKING SPACES PER TOWNHOME. AN ADDITIONAL 16 VISITOR PARKING SPACES ARE PROVIDED THROUGHOUT LOT 2 FOR A TOTAL OF 140 PARKING SPACES PROVIDED ON LOT 2.

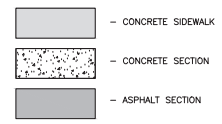
Lot Summary		Building Summary							
Lot Number	Lot Area	Buildings	Use Proposed Zoning	Stories	Total Units	Units Per Acre (UPA)	Total Lot Coverage (Buildings, Drives, and Parking Lots)	Open Space Provided (%)	Open Space Required (%)
Lot 1	18.08 Ac.	10	APARTMENT/IRP-4	3 Story	240	14.96	251,809 Sq. Ft. or 5.78 Ac.	84%	60%
		1	CLUBHOUSE/IRP-4	1 Story	-	-			
Lot 2	16.28 Ac.	62	TOWNHOME/IRP-2	2 Story	62	3.80	116,517 Sq. Ft. or 2.72 Ac.	83%	60%
Tract A	5.61 Ac.	-	STORMWATER/BMP MAINTENANCE TRACT	-	-	-	-	-	-
Total					302	7.96 UPA			
Total Units					302				
Total Density					7.96 UPA				

\* GROSS LAND AREA TO CENTERLINE OF ROAD RIGHT OF WAY

**NOTES:**

- EACH INDIVIDUAL PROPOSED TOWNHOUSE WILL BE A 2-STORY, 1,625 SF UNIT. THE TOWNHOUSES WILL BE COMPOSED OF 4-FLEX, AND 6-FLEX BUILDINGS. SEE ARCHITECTURAL PLANS FOR TOWNHOUSE ELEVATIONS AND ADDITIONAL SQUARE FOOTAGE INFORMATION.
- ALL APARTMENT BUILDINGS WILL BE 3-STORY 26,520 SF BUILDINGS. SEE ARCHITECTURAL PLANS FOR BUILDING ELEVATIONS.

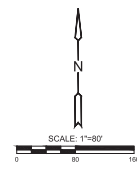
**PAVING LEGEND**



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**OVERALL SITE PLAN**

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SCALE	1"=80'	
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N/A	REVISION	DATE
SHEET NO.		







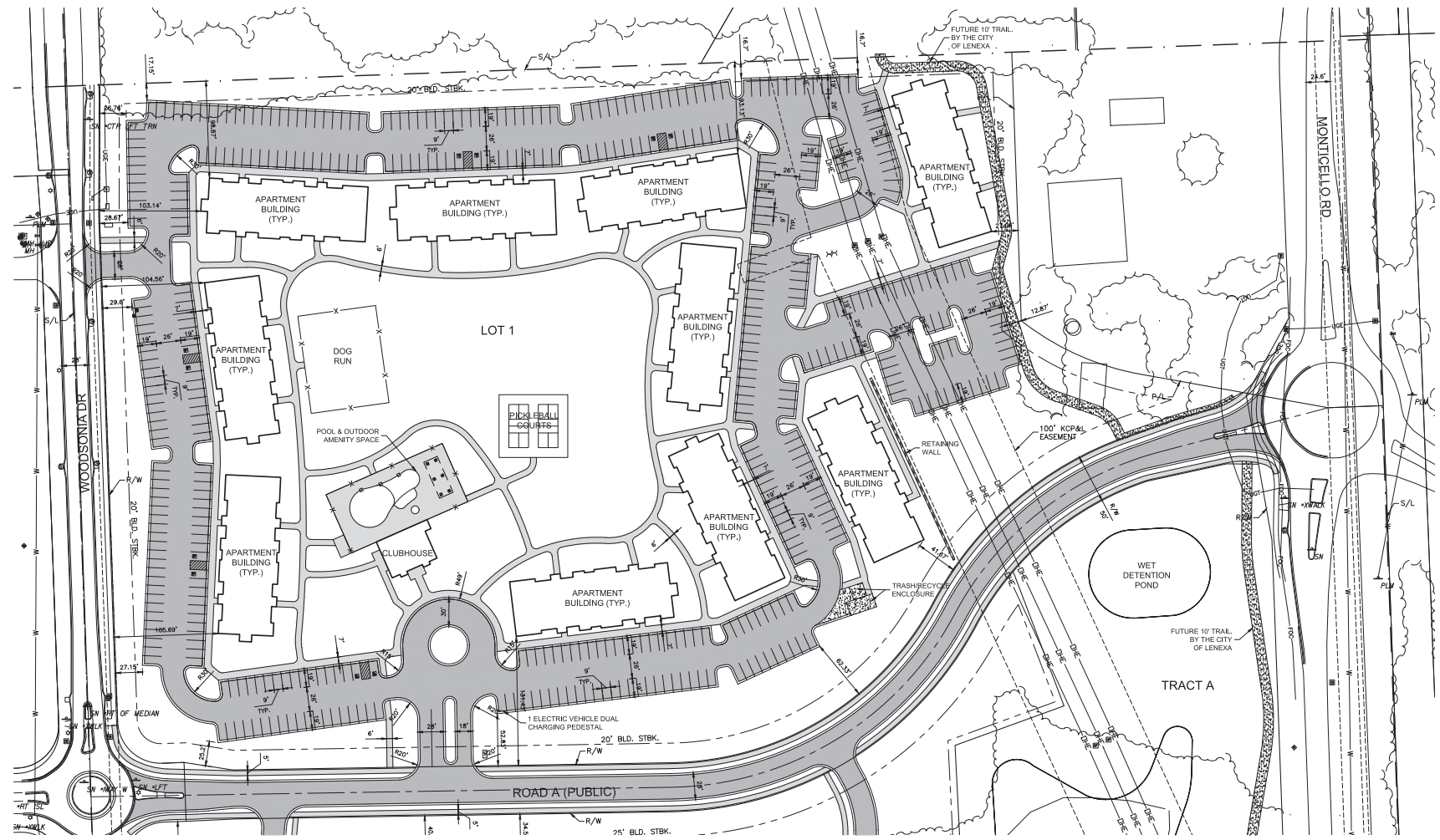
REZONING & PRELIMINARY DEVELOPMENT PLANS FOR  
**THE RESIDENCES ON WOODSONIA**  
 LENEXA, KANSAS 66227

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**PAVING PLAN 1**

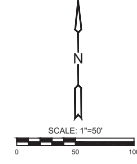
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N/A	REVISION	DATE
SHEET NO.		

C104



**PAVING LEGEND**

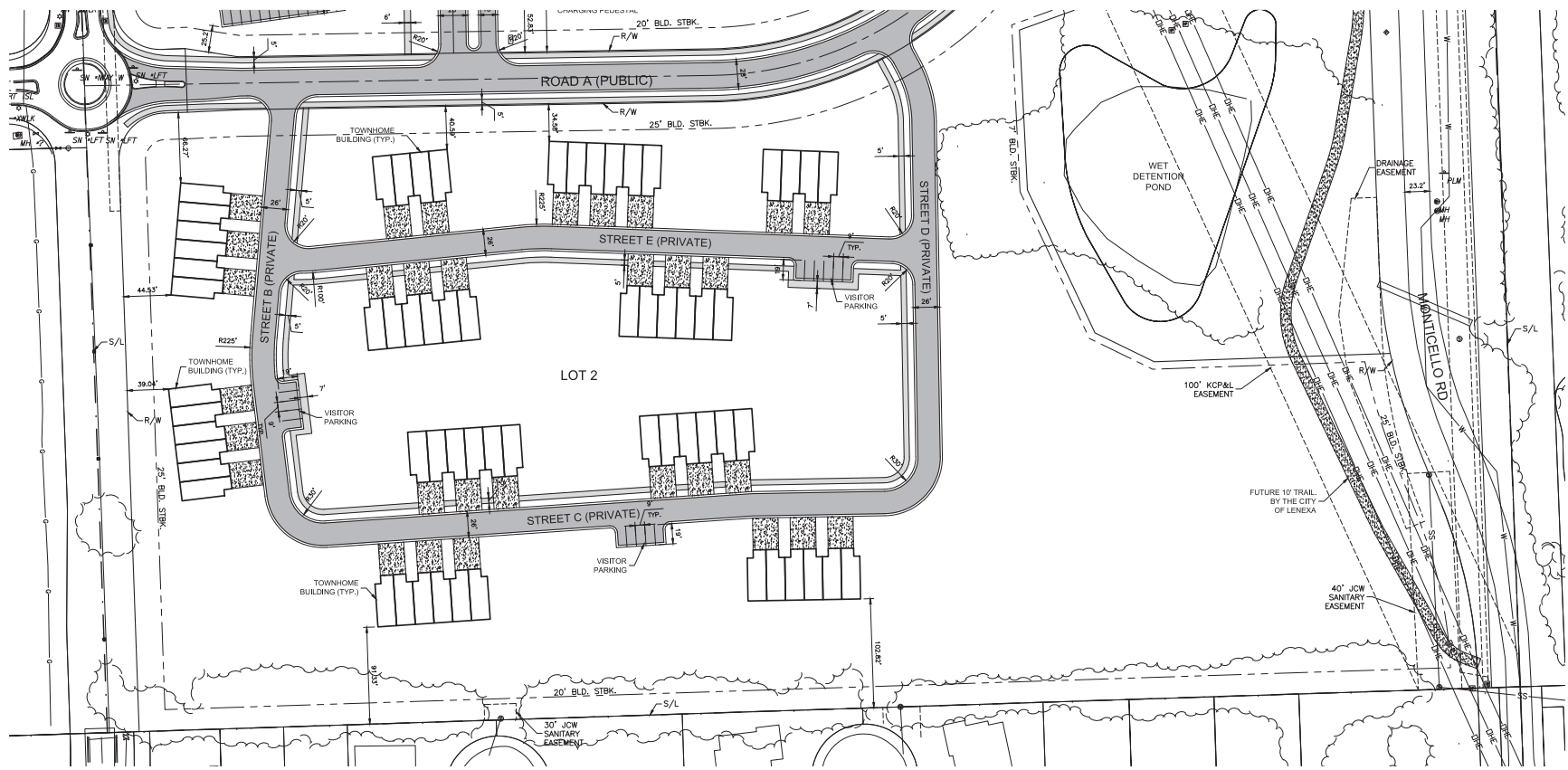
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	- CONCRETE SECTION
	- ASPHALT SECTION



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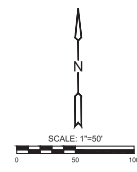


REZONING & PRELIMINARY DEVELOPMENT PLANS FOR  
**THE RESIDENCES ON WOODSONIA**  
 LENEXA, KANSAS 66227



**PAVING LEGEND**

	- CONCRETE SIDEWALK
	- CONCRETE SECTION
	- ASPHALT SECTION

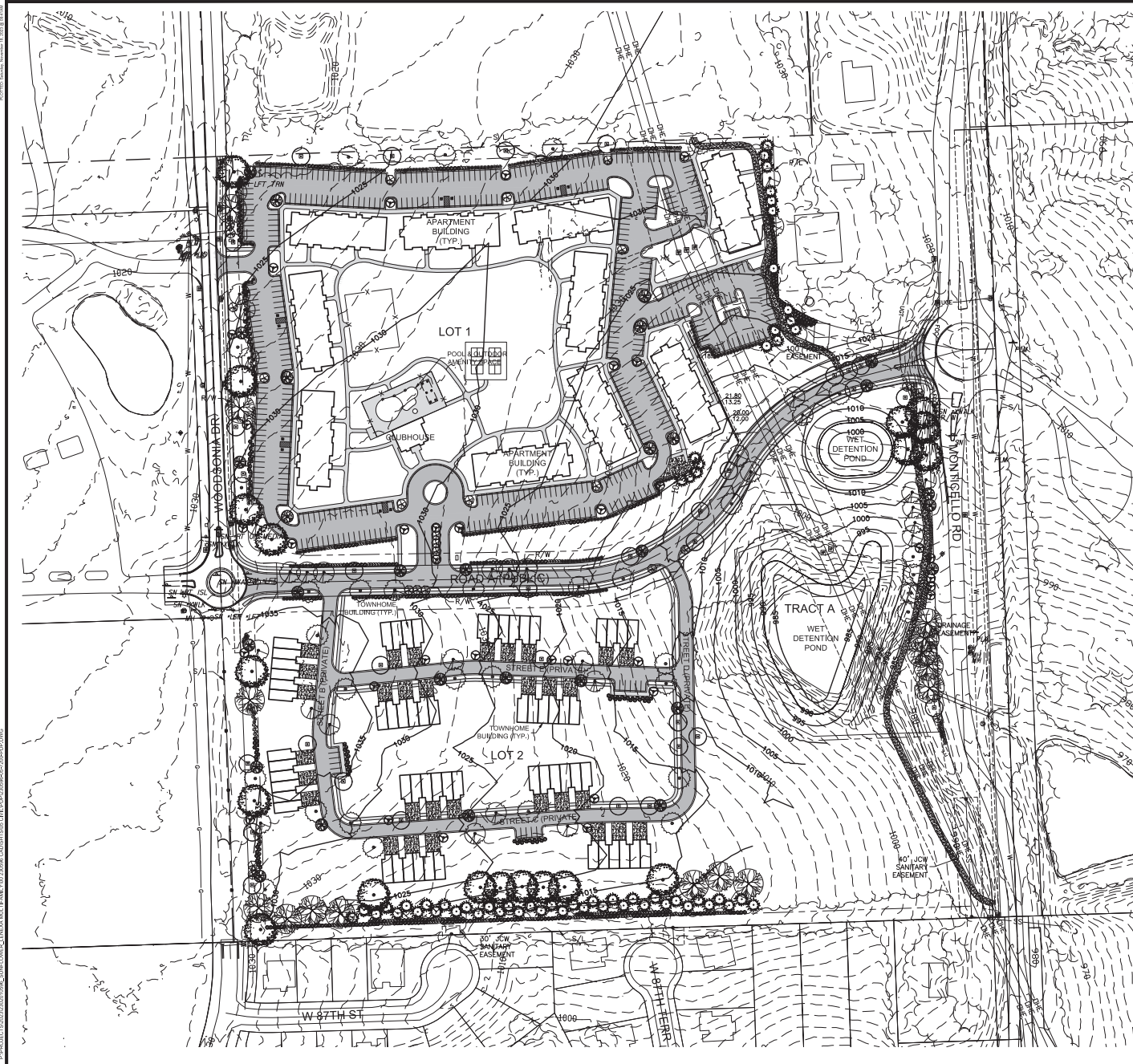


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**PAVING PLAN 2**

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DESIGNED	DRAWN	CHECKED
BLT	JLB	BLT
1	KEY COMMENTS	11.14.23
N/A	REVISION	DATE

SHEET NO.  
**C105**



**PLANT LEGEND**

-  - SWAMP WHITE OAK (QUE)
-  - GREEN VASE ZELKOVA
-  - CADDO MAPLE
-  - SHADEMASTER HONEYLOCUST
-  - GREENSPIRE LITTLE LEAF LINDEN
-  - AUTUMN SERVICEBERRY
-  - EASTERN REDBUD
-  - PRAIRIEFIRE CRABAPPLE
-  - BLACK HILLS SPRUCE
-  - SEA GREEN JUNIPER
-  - DENSIFORMIS YEW
-  - MEYER LILAC
-  - TIGER EYE SUMAC
-  - SNOWMOUND SPIREA

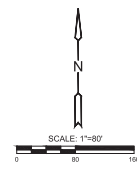


REZONING & PRELIMINARY DEVELOPMENT PLANS FOR  
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




**OVERALL LANDSCAPE PLAN**

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N/A	REVISION	DATE
SHEET NO.		



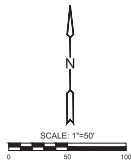


**PLANT LEGEND**

-  - SWAMP WHITE OAK (QUE)
-  - GREEN VASE ZELKOVA
-  - CADDO MAPLE
-  - SHADEMASTER HONEYLOCUST
-  - GREENSPIRE LITTLE LEAF LINDEN
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-  - SEA GREEN JUNIPER
-  - DENSIFORMIS YEW
-  - MEYER LILAC
-  - TIGER EYE SUMAC
-  - SNOWMOUND SPIREA

**NOTES:**

1. SEE LANDSCAPE NOTES SHEET FOR CITY STANDARDS INFORMATION.



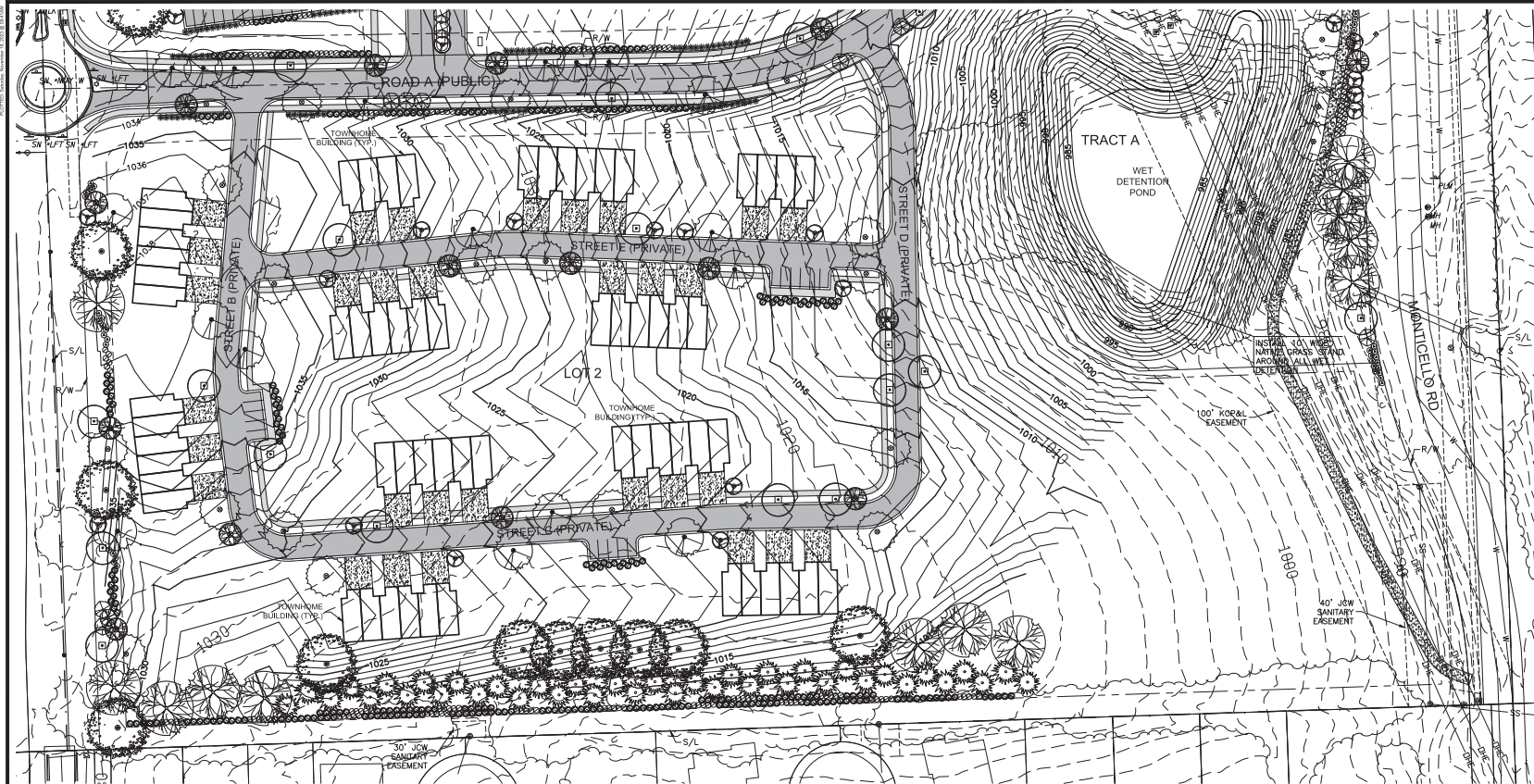
REZONING & PRELIMINARY DEVELOPMENT PLANS FOR  
**THE RESIDENCES ON WOODSONIA**  
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**LANDSCAPE PLAN 1**

PROJECT NO.	2302010596	
DATE	10.30.2023	
SCALE	1"=50'	
DESIGNED	DRAWN	CHECKED
BJH	BJH	BJH
KEY COMMENTS	NO.	DATE

SHEET NO. **L101**



NOTES:  
 1. SEE LANDSCAPE NOTES SHEET FOR CITY STANDARDS INFORMATION.

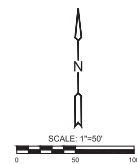
PLANT LEGEND

- SWAMP WHITE OAK
- GREEN VASE ZELKOVA
- CADDO MAPLE
- SHADEMASTER HONEYLOCUST
- GREENSPIRE LITTLE LEAF LINDEN
- AUTUMN SERVICEBERRY
- EASTERN REDBUD
- PRAIRIE FIRE CRABAPPLE
- BLACK HILLS SPRUCE
- SEA GREEN JUNIPER
- DENSIFORMIS YEW
- MEYER LILAC
- TIGER EYE SUMAC
- SNOWMOUND SPIREA

ORIGINAL PLANT SCHEDULE

KEY	QTY	COMMON NAME	BOTANICAL NAME	SIZE & METHOD OF HANDLING
<b>DECIDUOUS SHADE TREES</b>				
ACE	35	CADDO SUGAR MAPLE	ACER SACCHARUM 'JOHN PAIR'	2.5" CAL, 12' MIN. HT.
QUE	12	SWAMP WHITE OAK	QUERCUS BICOLOR	2.5" CAL, 12' MIN. HT.
ZEL	9	GREEN VASE ZELKOVA	ZELKOVA SERRATA 'GREEN VASE'	2.5" CAL, 12' MIN. HT.
SML	30	SHADEMASTER HONEYLOCUST	GLEDITSIA TRICANTHOS INERMIS 'SHADEMASTER'	2.5" CAL, 12' MIN. HT.
LIL	38	GREENSPIRE LITTLE LEAF LINDEN	TILIA CORDATA 'GREENSPIRE'	2.5" CAL, 12' MIN. HT.
<b>DECIDUOUS ORNAMENTAL TREES</b>				
ERB	34	EASTERN REDBUD	CERCIS CANADENSIS	2" CAL, 8' MIN. HT.
PFC	38	PRAIRIE FIRE CRABAPPLE	MALUS SP. 'PRAIRIE FIRE'	2" CAL, 8' MIN. HT.
SER	40	AUTUMN BRILLIANCE SERVICEBERRY	AMELANCHIER X GRANDIFLORA	2" CAL, 12' MIN. HT.
<b>EVERGREEN TREES</b>				
BHS	26	BLACK HILLS SPRUCE	PICEA GLAUCA 'DENSATA'	8' MIN. HEIGHT
<b>SHRUBS</b>				
JUN	132	SEA GREEN JUNIPER	JUNIPERUS CHINENSIS 'SEA GREEN'	3 GAL.
YEW	142	DENSIFORMIS YEW	TAXUS X MEDIA 'DENSIFORMIS'	3 GAL.
LIL	70	MEYER LILAC	SYRINGA MEYERI 'PALIBIN'	3 GAL.
TES	59	TIGER EYE SUMAC	RHUS TYPHINA 'TIGER EYES'	3 GAL.
SNO	9	SNOWMOUND SPIREA	SPIRAEA NIPPONICA 'SNOWMOUND'	3 GAL.
<b>GROUND COVER</b>				
TURF	N/A	FESCUE TURF GRASS	SEE LAWN NOTES	SOD

BMP AND NATIVE GRASS AREAS SHALL BE VEGETATED WITH NATIVE OR ADAPTED PLANT MATERIALS SUITABLE FOR SITE CONDITIONS.



REZONING & PRELIMINARY DEVELOPMENT PLANS FOR  
**THE RESIDENCES ON WOODSONIA**  
 LENEXA, KANSAS 66227

0202  
 MKEC Engineering  
 1414 Riverside  
 Lenexa, MO 64558  
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LANDSCAPE PLAN 2

PROJECT NO.	2302010566	
DATE	10.30.2023	
SCALE	1"=50'	
DESIGNED	DRAWN	CHECKED
BJH	BJH	BJH
1	KEY COMMENTS	11/13/23
N/A	REVISION	DATE

SHEET NO.  
 L102



CLUBHOUSE BUILDING PLAN  
SCALE: 1"=10'-0" C2



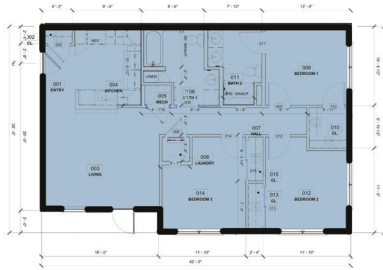
6-PLEX



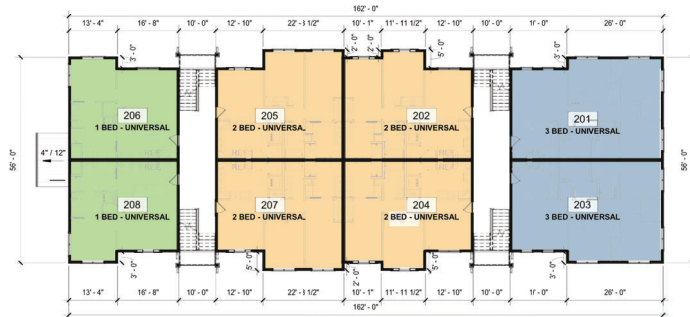
4-PLEX

TOWNHOME BUILDING PLAN  
SCALE: 1"=16'-0" C1

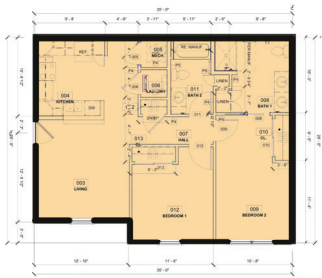
K-7 & 83rd STREET PROJECT DATA:						
3-STORY WALK-UP:		BUILDING DATA:				
UNIT COUNT:	#	TYPE	NET SQ. FT.	PER SQ. FT. PRICE	TOTAL PRICE	
	60	1BD/1BA	705	42,300	790	47,400
	120	2BD/2BA	845	101,400	940	112,800
	60	3BD/2BA	1,005	60,300	1,130	67,800
<b>SUBTOTAL:</b>	<b>240</b>	<b>UNITS</b>	<b>204,000</b>		<b>228,000</b>	
BREEZEWAYS:	60		37,200		600	37,200
<b>TOTAL:</b>	<b>240</b>	<b>UNITS</b>	<b>241,200</b>		<b>265,200</b>	
TOWNHOME:		BUILDING DATA:				
UNIT COUNT:	#	TYPE	NET SQ. FT.	PER SQ. FT. PRICE	TOTAL PRICE	
	62	2BD/2BA	1,225	75,950	1,340	83,080
<b>SUBTOTAL:</b>	<b>62</b>	<b>UNITS</b>	<b>75,950</b>		<b>83,080</b>	
GARAGES:	62		250	15,500	285	17,670
<b>TOTAL:</b>	<b>62</b>	<b>UNITS</b>	<b>91,450</b>		<b>100,750</b>	
BUILDING TOTALS:						
COMMUNITY BUILDING:			5,000		5,150	
<b>TOTAL:</b>	<b>302</b>	<b>UNITS</b>	<b>337,650</b>		<b>371,100</b>	
BUILDING TYPE:		BUILDING DATA:				
#		NET SQ. FT. BLDG.	TOTAL NET SQ. FT. BLDG.	PER SQ. FT. PRICE	TOTAL PRICE	
10	3-STORY WALK-UP	24,120	241,200	26,520	265,200	
2	4-PLEX TOWNHOME	5,900	11,800	6,500	13,000	
9	6-PLEX TOWNHOME	8,850	79,650	8,750	87,750	
1	COMMUNITY BUILDING	5,000			5,150	
<b>TOTAL:</b>	<b>22</b>	<b>BUILDINGS</b>	<b>337,650</b>		<b>371,100</b>	



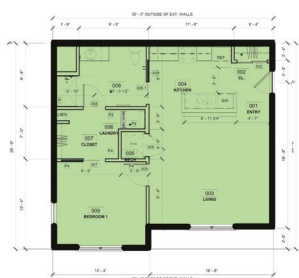
3-STORY WALK-UP  
3-BEDROOM UNIT PLAN  
SCALE: 1"=8'-0" B2



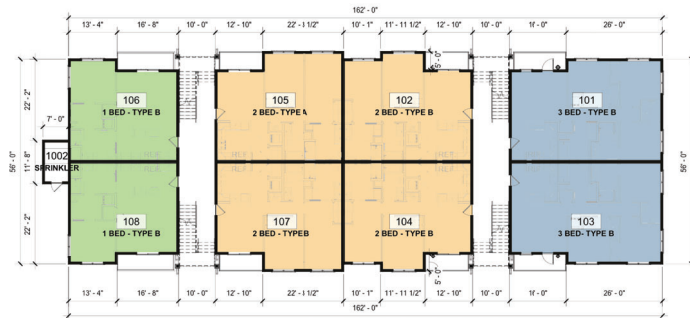
3-STORY WALK-UP - TYP.  
2ND & 3RD FLOOR BUILDING PLAN  
SCALE: 1"=16'-0" B1



3-STORY WALK-UP  
2-BEDROOM UNIT PLAN  
SCALE: 1"=8'-0" A3



3-STORY WALK-UP  
1-BEDROOM UNIT PLAN  
SCALE: 1"=8'-0" A2



3-STORY WALK-UP - TYP.  
1ST FLOOR BUILDING PLAN  
SCALE: 1"=16'-0" A1

PRINTS ISSUED:  
10/30/2023 - PRELIMINARY PLAN  
REVISIONS:

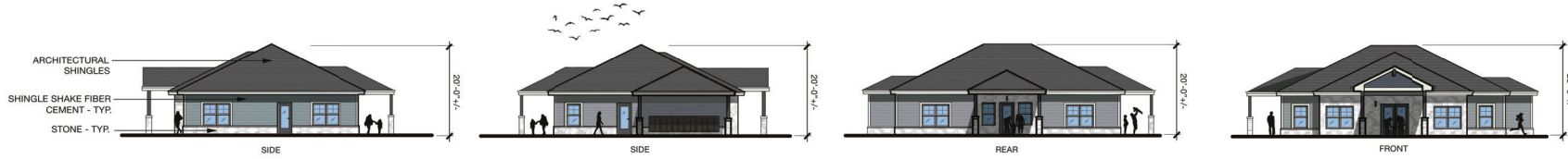
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THE RESIDENCES ON WOODSONIA  
**PRELIMINARY PLAN**  
K-7 & 83rd STREET  
Lenexa, MO

SHEET TITLE:  
UNIT & BUILDING PLANS

PROJECT NUMBER:  
SHEET NUMBER:

**A-100**



CLUBHOUSE ELEVATIONS (C1)  
SCALE: 1"=12'-0"



TOWNHOME ELEVATIONS (B1)  
SCALE: 1"=12'-0"



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## CALL TO ORDER

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Chairman Poss called the regular meeting of the Lenexa Planning Commission to order at 7:34 p.m. on Monday, November 6, 2023. The meeting was held in the Community Forum at Lenexa City Hall at 17101 W. 87<sup>th</sup> Street Parkway, Lenexa, Kansas.

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## ROLL CALL

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### COMMISSIONERS PRESENT

Chairman Chris Poss  
Vice-Chairman Mike Burson  
Commissioner Ben Harber  
Commissioner Don Horine  
Commissioner David Woolf  
Commissioner John Handley  
Commissioner Brenda Macke  
Commissioner Cara Wagner

### COMMISSIONERS ABSENT

Commissioner Curt Katterhenry

### STAFF PRESENT

Stephanie Kisler, Planning Manager  
Tim Green, Deputy Director of Community Development  
Andrew Diekemper, Assistant Chief – Fire Prevention  
Spencer Throssell, Assistant City Attorney II  
Dave Dalecky, Planner II  
Kim Portillo, Planner III  
Logan Strasburger, Planner I  
Will Sharp, Planning Intern  
Colter Stevenson, Management Analyst  
Gloria Lambert, Senior Administrative Assistant  
Mike Nolan, Assistant City Manager

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## APPROVAL OF MINUTES

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The minutes of the October 2, 2023 meeting were presented for approval. Chairman Poss entertained a motion to **APPROVE** the minutes. Moved by Commissioner Burson, seconded by Commissioner Horine, and **APPROVED** by a unanimous voice vote.

Chairman Poss asked staff if there were any changes to the agenda. Stephanie Kisler said there were no changes to the agenda but pointed out the two continued items (Cedar Canyon West Commercial and Towneplace Hotel at Ten Ridge), and the withdrawn application (Watercrest Landing South Apartments), stating there would be no discussion on those items.



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**CONSENT AGENDA**

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1. Resolution adopting the 2024 Planning Commission/Board of Zoning Appeals Schedule of meeting dates and submittal deadlines.
2. Copper Creek North and South Multifamily Development - Consideration of applicant's request to continue this item to the January 8, 2024 Planning Commission meeting. The item consists of two concept plans for a multifamily development located near the northwest corner of W 89th Street and Woodsonia Drive within the RP-3, Planned Residential (Medium-High) Density, RP-4, Planned Residential (High Density), and RP-5, Planned Residential (High Rise, High Density) Districts. PL23-04CP, PL23-05CP
3. Lenexa Baptist Church - Consideration of a revised final plan for construction of a private driveway between property located at 15320 W 87th Street Parkway and 15220 W 87th Street Parkway within the R-1, Planned Residential (Low Density) and CP-1, Planned Neighborhood Commercial Districts. PL23-05FR
4. Canyon Creek Apartment Homes - Consideration of a final plan for a multifamily residential development located east of the northeast corner of K-10 Highway and Canyon Creek Boulevard within the RP-2, Planned Residential (Intermediate Density) and RP-4, Planned Residential (High Density) Districts. PL23-20F
5. KC Bier at Vista Village - Consideration of a final plan for a restaurant with an accessory microbrewery located near the northwest corner of W 98th Street and Ambassador Street within the PUD, Planned Unit Development District. PL23-21F
6. Vista Village Lot 6 (Building 6) - Consideration of a final plan for a restaurant with a drive-thru located near the southeast corner of Prairie Star Parkway and Ridgeview Road within the PUD, Planned Unit Development District. PL23-22F
7. Lenexa Point, 2nd Plat - Consideration of a final plat for a commercial development located near the northwest corner of W 87th Street Parkway and Maurer Road in the CP-2, Planned Community Commercial District. PT23-24F
8. Viscek Estates - Consideration of a final plat for a single-family residential lot located at 25925 W 83rd Street within the RP-1, Planned Residential (Low Density) District. PT23-25F
9. Vista Ridge, 2nd Plat - Consideration of a final plat for a commercial development located near the northeast corner of K-10 Highway and Ridgeview Road in the CP-3, Planned Regional Commercial District. PT23-26F
10. Stoneridge North, Second Plat - Consideration of a final plat for a single-family subdivision located near the northeast corner of W 83rd Street and Cedar Niles Road in the RP-1, Residential Planned Single-Family (Low-Density) and RP-2, Residential Planned (Intermediate-Density) District. PT23-27F

Chairman Poss entertained a motion to **APPROVE** Consent Agenda Items 1-10. Moved by Commissioner Handley, seconded by Commissioner Horine, and carried by a unanimous voice vote.

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## REGULAR AGENDA

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11. **FineLine Solutions - Consideration of a special use permit for medical clinic and personal services uses located at 13100 W 95th Street, Suite 2B within the NP-O, Planned Neighborhood Office District. SU23-10 (Public Hearing)**

### APPLICANT PRESENTATION

Catherine Moss, Susanne Gun, and David Kimani are the prospective tenants and were all present to request a special use permit for medical clinic and personal service uses. Susanne Gun gave a presentation and stated the background, education, and services offered by all three business owners. She gave a breakdown of their potential hours of operation and office usage as it pertained to the type of services received. She displayed images of the building and parking lot, noting there is plenty of parking available. She showed pictures of the interior and how the rooms were set up for clients. The clients will receive services by appointment only. Ms. Gun displayed photos of the building and parking lot.

### STAFF PRESENTATION

Will Sharp presented the Staff Report. He explained that FineLine Solutions was one of three businesses proposing to share the tenant space and the other two businesses are Tivona Naturals and Parkway Direct Primary Care Clinic. He displayed a location map of the applicants' proposed site and provided zoning and Comprehensive Plan information of the property in question. He explained that a special use permit is required because neither a medical clinic nor personal service business is allowed in the NP-O, Planned Neighborhood Office District by right.

Mr. Sharp reminded the commissioners of the previous special use permit recipient, NeuroEdge Chiropractic, noting that this application is for space within the same building. Staff feels it is an appropriate location for low volume medical clinic and personal service businesses. He showed exterior photos the property. He explained as part of the conditions of the previously approved special use permit for NeuroEdge Chiropractic that several maintenance issues needed to be remedied. The property owner has made progress; however, the one remaining issue of screening the dumpster will need to be resolved by December 18, 2023. He reiterated there are no conditions of approval for the current special use permit application.

Mr. Sharp displayed the criteria by which the application was reviewed by Staff and that each of the criteria was discussed in detail within the Staff Report. Staff recommends approval of the special use permit for a period of three years. To clarify terms of the different special use permits he explained that City code has a specific duration for a first-time request of a special use permit. In cases where the code does not specially require a certain duration, it is up to Staff to determine a reasonable time limit. Staff determined that three years is an appropriate duration for the current applicant because of the combination of uses and the context of the building. In addition, the recently approved special use permit at the same location also carries a three-year duration.

### PUBLIC HEARING

Chairman Poss **OPENED** the Public Hearing and asked if anyone wished to speak on this item. No one from the audience came forward.

Chairman Poss entertained a motion to **CLOSE** the Public Hearing. Moved by Commissioner Horine, seconded by Commissioner Handley, and carried by a unanimous voice vote.

## PLANNING COMMISSION DISCUSSION

Commissioner Handley asked why the conditions of screening the receptacle were placed on the previous approved special use permit and not on the current applicant. Stephanie Kisler responded that since there is currently an active condition on the previous special use permit, it would be duplicative to place the condition on both special use permits and it would have to be tracked two ways. Also, the condition is related to the date that City Council approved the last special use permit. It is necessary that Staff go by the days left on the previous active condition. She noted Staff has the ability to review any special use permit if any concerns or code violations occur on the property. If there are any issues with NeuroEdge Chiropractic or the conditions on their special use permit, Staff will address it. Concerning code enforcement, NeuroEdge has shown good faith in making progress on the conditions of approval and Staff does not feel there should be additional conditions placed on the current applicant.

Commissioner Handley said the concern is if the receptacle is not screened by the deadline given to NeuroEdge and the current special use permit is approved. He asked if NeuroEdge would have to stop operating their business. Stephanie Kisler said there is a process to revoke special use permits that could be initiated for NeuroEdge if the conditions of the special use permit are not met by the required deadline.

Commissioner Handley said his intent is to ensure success to both special use permit applicants.

## MOTION

Chairman Poss entertained a motion to recommend **APPROVAL** of a special use permit for medical clinic and personal services uses for **FineLine Solutions, Tivona Naturals, and Parkway Direct Primary Care Clinic** at 13100 W. 95<sup>th</sup> Street, Suite 2B, for a period of three years.

Moved by Commissioner Burson, seconded by Commissioner Macke, and carried by a unanimous voice vote.

12. **Raw Health Company - Consideration of a special use permit for a medical clinic use located at 12760 W 87th Street Parkway, Suite 110 within the NP-O, Planned Neighborhood Office District. SU23-11 (Public Hearing)**

### APPLICANT PRESENTATION

The applicant was not in attendance.

### STAFF PRESENTATION

Stephanie Kisler presented the Staff Report. She displayed an aerial view of the applicant's building location and presented the Zoning Map and Future Land Use Map stating that the current zoning and medical clinic use are compatible with the Comprehensive Plan. She showed several street view angles of the subject site. She noted the criteria for review of the special use permit and reminded the commissioners the criteria were discussed in detail on the Staff Report. Staff recommends approval of the medical clinic use within the NP-O, Planned Neighborhood Office District for a period of five years.

### PUBLIC HEARING

Chairman Poss **OPENED** the Public Hearing and asked if anyone wished to speak on this item. No one from the audience came forward.

Chairman Poss entertained a motion to **CLOSE** the Public Hearing. Moved by Commissioner Horine, seconded by Commissioner Woolf, and carried by a unanimous voice vote.

### PLANNING COMMISSION DISCUSSION

Chairman Poss asked Staff if it was permissible for the commissioners to deliberate and go forward without the applicant being present. Stephanie Kisler asked Spencer Throssell, Assistant City Attorney, to confirm whether the Commission could advance the special use permit without the applicant present. Mr. Throssell responded that under Section 1 of the City code, it was up to the Planning Commission's discretion to proceed without the applicant's presence.

### MOTION

Chairman Poss entertained a motion to recommend **APPROVAL** of a special use permit for a medical clinic known as Raw Health Company at 12760 W. 87<sup>th</sup> Street, Suite 110, for a period of five years.

Moved by Commissioner Harber, seconded by Commissioner Macke, and carried by a unanimous voice vote.

### 13. Ceva Animal Health - Consideration of a preliminary plan/plat for a manufacturing facility at 8940 Long Street in the BP-2, Planned Manufacturing District. PL23-09P

#### APPLICANT PRESENTATION

Jay Healy, George Butler Associates (GBA), and members of his team were present on behalf of the project. Brian Warf, Ceva, said they are requesting approval to move forward with a new manufacturing facility on their property.

#### STAFF PRESENTATION

Kimberly Portillo presented the Staff Report and began by clarifying that the project does not require a public hearing as stated on the Staff Report. The application submitted is for a new manufacturing and office building as part of the Ceva Animal Health campus. She displayed an aerial view of the project location and explained that there are two separate parcels with the intent to combine the two parcels and have one building with two separate parking lots. She showed an image outlining the Ceva campus in the vicinity, highlighting the subject property. She stated the surrounding zoning and Future Land Use designation. She displayed a drawing of the access points available to the site. The overall design of the new building is anticipated to match the materials and colors of the existing buildings on the Ceva campus. She explained the applicant's proposed landscape plan, detailing their requested landscape deviations and site setbacks. Staff will continue to work with the applicant to refine the entire landscape plan as it moves to final plan stage. She displayed another landscape exhibit that showed greater detail with the specific numbers for setback deviation requests. She noted the reason for some of staff's deviation approvals was due to the uniqueness of the site. It was also noted that the proposed sidewalk along Santa Fe Drive is intended to be a trail in the future and the trail design may have an impact on the requested deviation at final plan stage. Staff recommends approval of the preliminary plan/plat for Ceva Animal Health with the applicant's requested deviations.

#### PLANNING COMMISSION DISCUSSION

Commissioner Horine gave the applicant kudos for continuing their expansion in Lenexa.

Commissioner Burson praised Staff for doing a good job of making the record of why they are in support of the deviations and setbacks for Ceva. He stated that because we are improving an area of the City where the lot is not currently in compliance with the setback, he understands and supports approval of the requested deviations for that site.

Commissioner Handley commented that the applicant is dealing with a unique site with a lot of very unique constraints. He also asked about what happens if there are ever different owners of the two parcels. He requested that the applicant make sure there are clear internal crossings to the site because it is a campus with pedestrians and traffic.

Chairman Poss echoed everyone's comments and said he appreciates Ceva's continued investment in the City of Lenexa.

#### MOTION

Chairman Poss entertained a motion to recommend **APPROVAL** of the preliminary plan and deviations for **Ceva Animal Health** at 8940 Long Street, for a manufacturing and office building.

Moved by Commissioner Woolf, seconded by Commissioner Handley, and carried by a unanimous voice vote.

#### 14. Consideration of the recommended 2024-2028 Capital Improvement Program (CIP) for conformance with the Comprehensive Plan.

##### STAFF PRESENTATION

Tim Green, Deputy Director of Community Development/City Engineer, presented the annual Capital Improvement Program proposal. He explained that a capital improvement project was a project that exceeds \$300,000 in cost and must have a useful life of at least five years. The project proposals that are listed below \$300,000 are anticipated to lead to larger projects. There are 48 total projects with expenditures of \$275.2 million, 18 new capital project request and 30 existing. He stated they always plan for the possibility of a recession or downturn by keeping extra finances available and transfer funds if needed. He listed several maintenance projects noting that maintenance projects were highly focused on the funding proposal. The funding sources will come from the Debt Service Fund, General Fund, Stormwater revenue and a General Funds transfer approved by City Council. He went in detail for five of the new projects. The biggest road project is 83<sup>rd</sup> Street from Greason Road to Clare Road because of the significant increase of traffic flow that is projected with the future Panasonic project coming to Lenexa. The project will include widening 83<sup>rd</sup> Street to a four-lane, divided arterial roadway. Construction is slated to begin in 2027 and is budgeted at \$13 million. The Old Town North Parking Lot Reconstruction project will reconfigure and improve the layout of the parking lot. As part of the improvement there will be a new drive connection from the north parking lot to the south parking lot.

Chairman Poss asked Tim Green if the City owned the parking lot north of Old Town. Tim Green responded that the city does own and maintains that parking lot.

Mr. Green said more than anywhere else in the city, 83<sup>rd</sup> Street and Lackman Road is the most requested intersection for a signalization. This traffic signalization project was prompted because of the Ad Astra Pool reconstruction that will increase pedestrian traffic on Lackman Road. There is currently a study being conducted for Old City Hall/Fire Station 6 to determine if it is the best location to house Fire Station 6. Upon completion of the study, funding for the project will be added into the Capital Improvement Program. Four new stormwater infrastructure replacement projects are included in the proposal and work will include replacing and/or lining deteriorated corrugated metal pipe to improve the City's drainage system. The two maintenance projects proposed are Quivira Road bridge over I-35 and sidewalk/trail repair. The Quivira Road bridge over I-35 has not had any work done on it since it was constructed over twenty years ago. Although there are no structural issues, it will receive aesthetic improvements. The City does not have a dedicated fund for sidewalk and trail repairs so this will add \$300,000 annually to the budget for repairs.

Mr. Green summarized the following projects that are actively underway:

- Lenexa Justice Center
- Indian Trails Aquatic Center Improvements
- Cedar Station Park Development
- Pavement Reconstruction Program
- 99th Street & Clare Road Construction
- K-10 & Lone Elm Interchange
- Santa Fe Trail Drive from 95th Street to Park Street
- Stormwater Infrastructure Replacement

Mr. Green gave an update of the CIP Calendar and announced that the Governing Body conducted a work session on October 10<sup>th</sup> and there were no comments or recommendations at that meeting. Tonight, is Planning Commission review and tomorrow, November 7<sup>th</sup>, the Committee of the Whole will meet to

discuss the CIP. Consideration for adoption of the program will be held at the November 21<sup>st</sup> Governing Body meeting. He stated he would answer any questions the Commissioners had. Before starting questions, he noted the status of signalization at Prairie Star Parkway & Lone Elm Road. He said the contractor had not been responsible in getting things done and the City is also waiting for signal equipment. There was also an issue with a retaining wall.

## PLANNING COMMISSION DISCUSSION

Commissioner Horine asked about the medians that were mentioned in the report. Tim Green responded that a lot of people still do not understand how to drive a roundabout. They are looking at modifying the roundabouts to make it easier for drivers.

Commissioner Burson asked why the budget has doubled for Old Town Lenexa Activity Center. Tim Green said there were three reasons. The first reason is due to inflation, the second is that they decided to skin the upper part of the building and get rid of the metal, and the last reason is that they are adding a youth room to the interior. Commissioner Burson asked if that was the reason for the delay. Mr. Green said it was probably due to design development as well as working with the users to get something that worked. Stephanie Kisler noted that Assistant City Manager Mike Nolan was in the audience and said he could provide more information. Mike Nolan responded that the budget increased because the scope of the project increased.

Commissioner Wagner asked the timeline for the 95<sup>th</sup> & Loiret project. Tim Green responded that they are currently in the bid process and the bid opening is scheduled for December 5<sup>th</sup>. Because it is KDOT funded, that process will take a couple of months and the project should begin in the spring. The project should be complete by summer.

Chairman Poss asked what drives tolls. Tim Green replied that it is a question of whether it will generate the funds to pay for itself. The issue with K-10 is the traffic is not bad enough that people will pay for toll. For a toll to be put in place the KTA and each adjacent City must approve it.

Chairman Poss asked if a bike lane or sidewalk will ever be constructed along Prairie Star Parkway over K-7? Tim Green said the plan is to include those improvements with the nearby K-10 project. Improvements for that project will include four lanes and a trail.

Commissioner Horine asked who makes the decision about what goes into the roundabouts. Tim Green responded that the Parks Department makes those decisions but will consult with the Engineering Division of the Community Development Department. Commissioner Horine said he would like to see the Parks Department use some of the funds to improve the interior of the roundabouts along Renner Boulevard. Mr. Green said he would pass that message along to the Parks Department.

Chairman Poss and Tim Green talked about the height of the plantings within roundabouts and ways to make it safer.

## MOTION

Chairman Poss entertained a motion to **CONFIRM** that the recommended 2024-2028 Capital Improvement Program for the City of Lenexa is in conformity with the Comprehensive Plan.

Moved by Commissioner Harber, seconded by Commissioner Horine, and carried by a unanimous voice vote.

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**CONTINUED APPLICATIONS (NO DISCUSSION)**

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- 15. **Cedar Canyon West Commercial - Consideration of a preliminary plan/plat for a commercial development located near the northeast corner of K-10 Highway and Canyon Creek Boulevard in the CP-2, Planned Community Commercial District. PL23-08P**
- 16. **Towneplace Hotel at Ten Ridge - Consideration of a final plan for a hotel near the northeast corner of K-10 Highway and Ridgeview Road in the CP-3, Planned Regional Commercial District. PL23-23F**

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**WITHDRAWN APPLICATIONS (NO DISCUSSION)**

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- 17. **Watercrest Landing South Apartments - Consideration of a preliminary plan for a multifamily residential development located near the northeast corner of K-7 and Prairie Star Parkway within the RP-4, Planned Residential (High Density) District. PL23-07P -- *Withdrawn by Applicant***

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**STAFF REPORT**

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Stephanie Kisler showed a final version of the new public hearing sign and noted that it is currently being used.

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**ADJOURNMENT**

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Chairman Poss ended the regular meeting of the Lenexa Planning Commission at 8:46 p.m. on Monday, November 6, 2023.