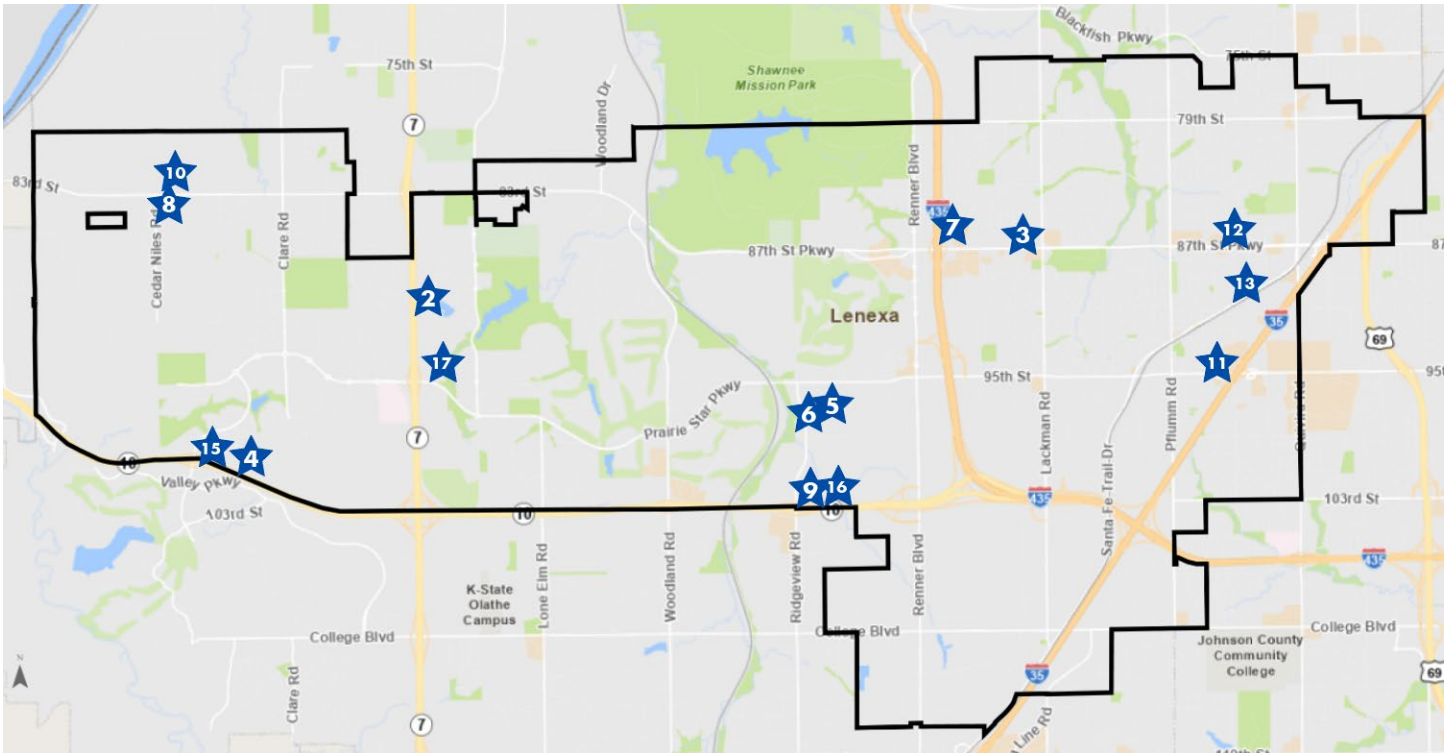


**AGENDA MAP**



**CALL TO ORDER**

**ROLL CALL**

**APPROVE MINUTES FROM THE OCTOBER 2, 2023 MEETING**

**CONSENT AGENDA**

*All matters listed within the consent agenda have been distributed to each member of the Planning Commission for review, are considered to be routine, and will be enacted by one motion with no separate discussion. If a member of the Planning Commission or audience desires separate discussion on an item, that item may be removed from the consent agenda and placed on the regular agenda.*

- 1. Resolution adopting the 2024 Planning Commission/Board of Zoning Appeals Schedule of meeting dates and submittal deadlines.**
- 2. Copper Creek North and South Multifamily Development - Consideration of applicant's request to continue this item to the January 8, 2024 Planning Commission meeting. The item consists of two concept plans for a multifamily development located near the northwest corner of W 89th Street and Woodsonia Drive within the RP-3, Planned Residential (Medium-High) Density, RP-4, Planned Residential (High Density), and RP-5, Planned Residential (High Rise, High Density) Districts. PL23-04CP, PL23-05CP**

3. **Lenexa Baptist Church - Consideration of a revised final plan for construction of a private driveway between property located at 15320 W 87th Street Parkway and 15220 W 87th Street Parkway within the R-1, Planned Residential (Low Density) and CP-1, Planned Neighborhood Commercial Districts. PL23-05FR**
4. **Canyon Creek Apartment Homes - Consideration of a final plan for a multifamily residential development located east of the northeast corner of K-10 Highway and Canyon Creek Boulevard within the RP-2, Planned Residential (Intermediate Density) and RP-4, Planned Residential (High Density) Districts. PL23-20F**
5. **KC Bier at Vista Village - Consideration of a final plan for a restaurant with an accessory microbrewery located near the northwest corner of W 98th Street and Ambassador Street within the PUD, Planned Unit Development District. PL23-21F**
6. **Vista Village Lot 6 (Building 6) - Consideration of a final plan for a restaurant with a drive-thru located near the southeast corner of Prairie Star Parkway and Ridgeview Road within the PUD, Planned Unit Development District. PL23-22F**
7. **Lenexa Point, 2nd Plat - Consideration of a final plat for a commercial development located near the northwest corner of W 87th Street Parkway and Maurer Road in the CP-2, Planned Community Commercial District. PT23-24F**
8. **Viscek Estates - Consideration of a final plat for a single-family residential lot located at 25925 W 83rd Street within the RP-1, Planned Residential (Low Density) District. PT23-25F**
9. **Vista Ridge, 2nd Plat - Consideration of a final plat for a commercial development located near the northeast corner of K-10 Highway and Ridgeview Road in the CP-3, Planned Regional Commercial District. PT23-26F**
10. **Stoneridge North, Second Plat - Consideration of a final plat for a single-family subdivision located near the northeast corner of W 83rd Street and Cedar Niles Road in the RP-1, Residential Planned Single-Family (Low-Density) and RP-2, Residential Planned (Intermediate-Density) District. PT23-27F**

## REGULAR AGENDA

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11. **FineLine Solutions - Consideration of a special use permit for medical clinic and personal services uses located at 13100 W 95th Street, Suite 2B within the NP-O, Planned Neighborhood Office District. SU23-10 (Public Hearing)**
12. **Raw Health Company - Consideration of a special use permit for a medical clinic use located at 12760 W 87th Street Parkway, Suite 110 within the NP-O, Planned Neighborhood Office District. SU23-11 (Public Hearing)**

13. **Ceva Animal Health - Consideration of a preliminary plan/plat for a manufacturing facility at 8940 Long Street in the BP-2, Planned Manufacturing District. PL23-09P**
14. **Consideration of the recommended 2024-2028 Capital Improvement Program (CIP) for conformance with the Comprehensive Plan.**

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### **CONTINUED APPLICATIONS (NO DISCUSSION)**

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15. **Cedar Canyon West Commercial - Consideration of a preliminary plan/plat for a commercial development located near the northeast corner of K-10 Highway and Canyon Creek Boulevard in the CP-2, Planned Community Commercial District. PL23-08P**
16. **Towneplace Hotel at Ten Ridge - Consideration of a final plan for a hotel near the northeast corner of K-10 Highway and Ridgeview Road in the CP-3, Planned Regional Commercial District. PL23-23F**

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### **WITHDRAWN APPLICATIONS**

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17. **Watercrest Landing South Apartments - Consideration of a preliminary plan for a multifamily residential development located near the northeast corner of K-7 and Prairie Star Parkway within the RP-4, Planned Residential (High Density) District. PL23-07P -- *Withdrawn by Applicant***

### **STAFF REPORTS**

### **ADJOURN**

### **APPENDIX**

18. **Draft Minutes - October 2, 2023**

*If you have any questions about this agenda, please contact Stephanie Kisler, Planning Manager, at [skisler@lenexa.com](mailto:skisler@lenexa.com).*

*If you need any accommodations for the meeting, please contact the City ADA Coordinator at 913-477-7550 at least 48 hours prior to the meeting.  
Kansas Relay Service: 800-766-3777*

*Assistive Listening Devices are available for use in the Community Forum by request.*

# **2024 PLANNING COMMISSION & BOARD OF ZONING APPEALS SUBMITTAL SCHEDULE**

## **REQUEST**

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Approve a resolution adopting the 2024 Planning Commission/Board of Zoning Appeals schedule of meeting dates and deadlines.

## **SUMMARY**

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In accordance with the Planning Commission by-laws, regular meetings of the Planning Commission and Board of Zoning Appeals are set annually and adopted by resolution with a current schedule available from the Community Development Department. Unless otherwise noted, the regular meetings shall be at Lenexa City Hall at 7:00 p.m. Meetings of the Board of Zoning Appeals, if scheduled, shall occur first and the Planning Commission meeting shall follow immediately upon conclusion of the Board of Zoning Appeals meeting.

Attached is the 2024 Planning Commission and Board of Zoning Appeals submittal schedule of meeting dates and deadlines.

**STAFF RECOMMENDATION: APPROVAL**

**RESOLUTION NO. 2023- 05**

**BE IT RESOLVED BY THE** Lenexa Planning Commission and pursuant to K.S.A. 12-745 and the approved Planning Commission/Board of Zoning Appeals By-laws;

The regular meetings of the Lenexa Planning Commission/Board of Zoning Appeals for January through December of the 2024 calendar year shall be held at the Lenexa City Hall at 7:00 p.m., or as soon thereafter as may be heard, on the dates reflected on the attached **Exhibit A** incorporated herein by reference.

Special Call Meetings of the Lenexa Planning Commission may be held in accordance with procedures set forth in the Planning Commission/Board of Zoning Appeals By-Laws.

**ADOPTED** by the Lenexa Planning Commission this 6<sup>th</sup> day of November 2023.

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**Chris Poss, Chairman**  
**Lenexa Planning Commission**

**ATTEST:**

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**Scott McCullough, Community Development Director**

**APPROVED AS TO FORM:**

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**Sean McLaughlin**  
**City Attorney**



## 2024 PLANNING COMMISSION & BOARD OF ZONING APPEALS SUBMITTAL SCHEDULE

<b>Submittal Deadline</b> <i>(12:00pm CST)</i>  Submit applications online at <a href="https://permits.lenexa.com">permits.lenexa.com</a>	<b>City Staff Comments Available Online</b> <i>(by end of day)</i>	<b>Applicant Revisions Due</b> <i>(12:00pm CST)</i>	<b>Planning Commission/ Board of Zoning Appeals Meeting*</b>	<b>City Council Meeting*</b> <i>(if applicable)</i>
	<b>Deadline for Applicant to Mail Notices</b> <i>(if applicable)</i>	<b>Deadline for Applicant to Post Sign on Property</b> <i>(if applicable)</i>		
Monday, December 4, 2023	Monday, December 18, 2023	Friday, December 22, 2023	<b>Monday, January 8, 2024</b>	<b>Tuesday, February 6, 2024</b>
Tuesday, January 2, 2024	Friday, January 12, 2024	Monday, January 22, 2024	<b>Monday, February 5, 2024</b>	<b>Tuesday, February 20, 2024</b>
Monday, January 29, 2024	Monday, February 12, 2024	Monday, February 19, 2024	<b>Monday, March 4, 2024</b>	<b>Tuesday, March 19, 2024</b>
Monday, February 26, 2024	Monday, March 11, 2024	Monday, March 18, 2024	<b>Monday, April 1, 2024</b>	<b>Tuesday, April 16, 2024</b>
Monday, April 1, 2024	Monday, April 15, 2024	Monday, April 22, 2024	<b>Monday, May 6, 2024</b>	<b>Tuesday, May 21, 2024</b>
Monday, April 29, 2024	Monday, May 13, 2024	Monday, May 20, 2024	<b>Monday, June 3, 2024</b>	<b>Tuesday, June 18, 2024</b>
Tuesday, May 28, 2024	Monday, June 10, 2024	Monday, June 17, 2024	<b>Monday, July 1, 2024</b>	<b>Tuesday, July 16, 2024</b>
Monday, July 1, 2024	Monday, July 15, 2024	Monday, July 22, 2024	<b>Monday, August 5, 2024</b>	<b>Tuesday, August 20, 2024</b>
Monday, July 22, 2024	Monday, August 5, 2024	Monday, August 12, 2024	<b>Monday, August 26, 2024</b>	<b>Tuesday, September 17, 2024</b>
Monday, August 26, 2024	Monday, September 9, 2024	Monday, September 16, 2024	<b>Monday, September 30, 2024</b>	<b>Tuesday, October 15, 2024</b>
Monday, September 30, 2024	Monday, October 14, 2024	Monday, October 21, 2024	<b>Monday, November 4, 2024</b>	<b>Tuesday, November 19, 2024</b>
Monday, October 28, 2024	Tuesday, November 12, 2024	Monday, November 18, 2024	<b>Monday, December 2, 2024</b>	<b>Tuesday, December 17, 2024</b>
Monday, November 25, 2024	Monday, December 9, 2024	Monday, December 16, 2024	<b>Monday, January 6, 2025</b>	<b>Tuesday, January 21, 2025</b>
Monday, December 30, 2024	Monday, January 13, 2025	Friday, January 17, 2025	<b>Monday, February 3, 2025</b>	<b>Tuesday, February 18, 2025</b>

*\* Unless otherwise noted, all Planning Commission, Board of Zoning Appeals, and City Council meetings are held at 7:00pm in the Community Forum in Lenexa City Hall, which is located at 17101 W. 87<sup>th</sup> Street Parkway, Lenexa, KS 66219.*

Need to know more about code requirements, zoning, the Comprehensive Plan, and what information to submit with your application?

Visit [lenexa.com/planning](https://lenexa.com/planning)

Need help submitting your application via the online portal?

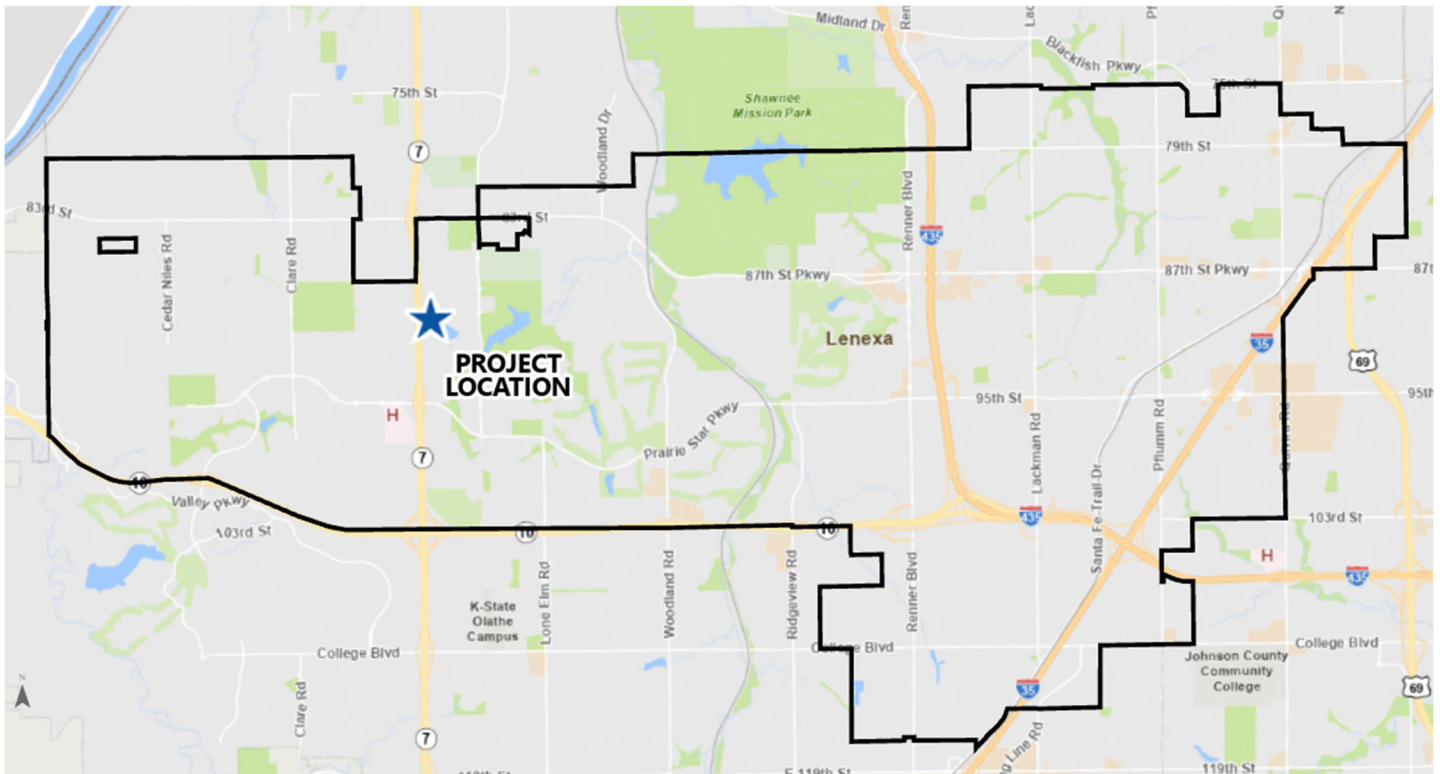
Contact Gloria Lambert at [glambert@lenexa.com](mailto:glambert@lenexa.com) or 913-477-7729

*If you are not already registered as a licensed professional in our system, the City will need to assign a licensed professional number to you.*

*If you have other questions, please contact us at [planning@lenexa.com](mailto:planning@lenexa.com) or 913-477-7500.*

## COPPER CREEK NORTH & SOUTH

<b>Project #:</b>	PL23-04CP & PL23-05CP	<b>Location:</b>	Northwest and southwest quadrants of 89 <sup>th</sup> Street and Woodsonia Drive
<b>Applicant:</b>	Doug Ubben, Phelps Engineering	<b>Project Type:</b>	Concept Plan
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	Multifamily Residential



### REQUEST FOR CONTINUANCE

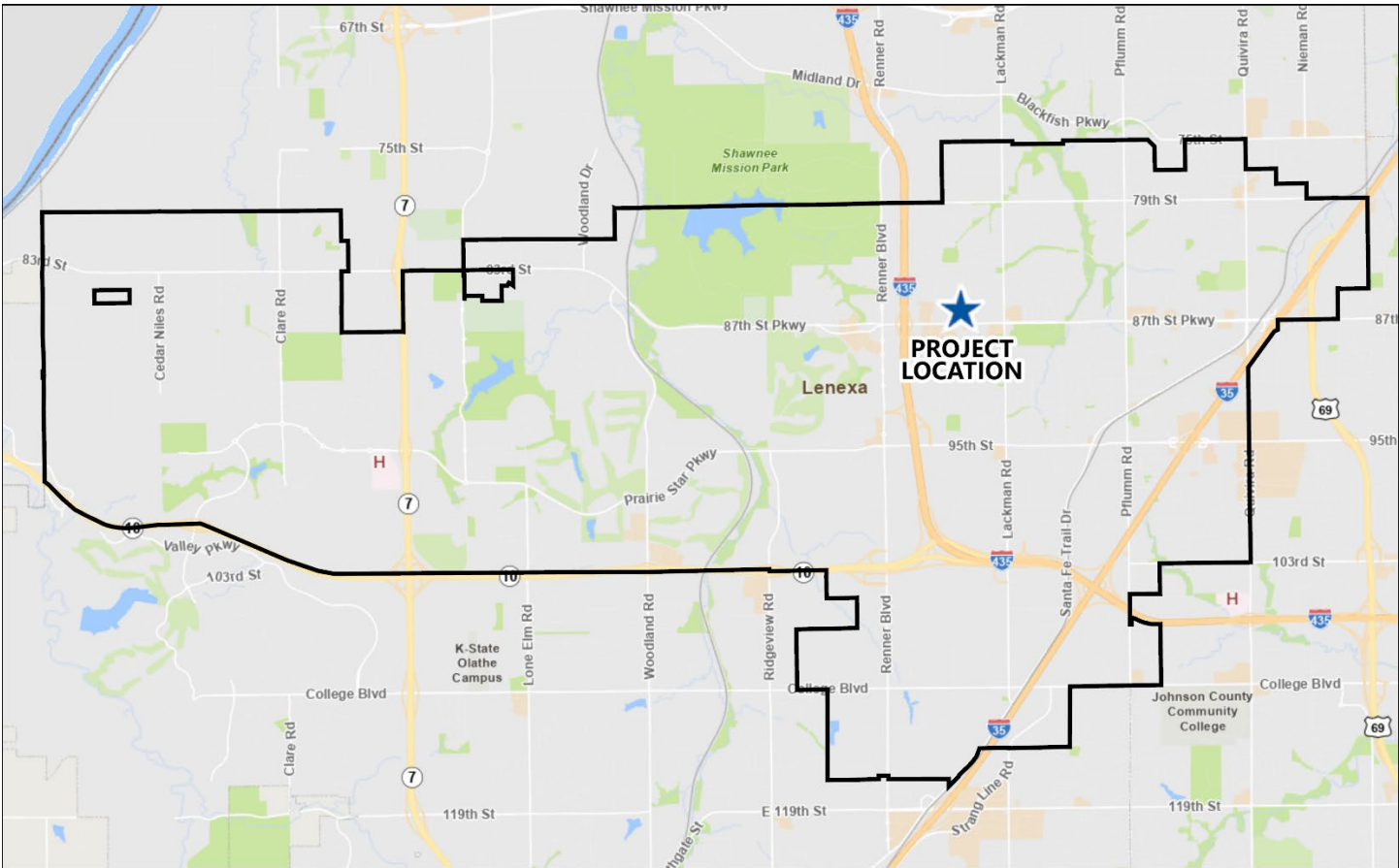
The applicant is requesting to continue the two concept plan applications for Copper Creek to the January 8, 2024 regular Planning Commission meeting. The Planning Commission bylaws allow for two *automatic* continuances, one allowed for Staff and one for the applicant, prior to requiring Planning Commission action of either consideration of the applications or an additional continuance. This is the third continuance of these applications, and the applications now require Planning Commission action.

The purpose of the applications is to clarify the requirement for improving Woodsonia Drive as it relates to the development applications. The requested continuance will allow for the applicant and Staff to discuss the possibility of creating a benefit district for construction of Woodsonia Drive, thus potentially rendering the two applications moot. Should the applications be considered at the January 8, 2024, meeting, a full staff report will be provided to the Planning Commission.

**STAFF RECOMMENDATION: CONTINUE TO JANUARY 8, 2024**

# LENEXA BAPTIST CHURCH EAST PRIVATE DRIVE

<b>Project #:</b>	PL23-05FR	<b>Location:</b>	Between 15320 W. 87 <sup>th</sup> Street Parkway and 15220 W. 87 <sup>th</sup> Street Parkway
<b>Applicant:</b>	Dan Finn, Phelps Engineering	<b>Project Type:</b>	Revised Final Plan
<b>Staff Planner:</b>	Logan Strasburger	<b>Proposed Use:</b>	N/A



## PROJECT SUMMARY

The applicant proposes to construct a private drive that will connect two lots to improve flow of traffic and provide additional parking for Lenexa Baptist Church patrons. The drive will be 34 feet wide and provide additional access between lots, improve traffic flow, and provide additional parking spaces. The proposed private drive also includes a revision to the existing pedestrian sidewalk between the two parcels. The applicant requests approval of a revised final plan for the proposed development. The applicant is not requesting any deviations. This project does not require a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL WITH CONDITIONS**

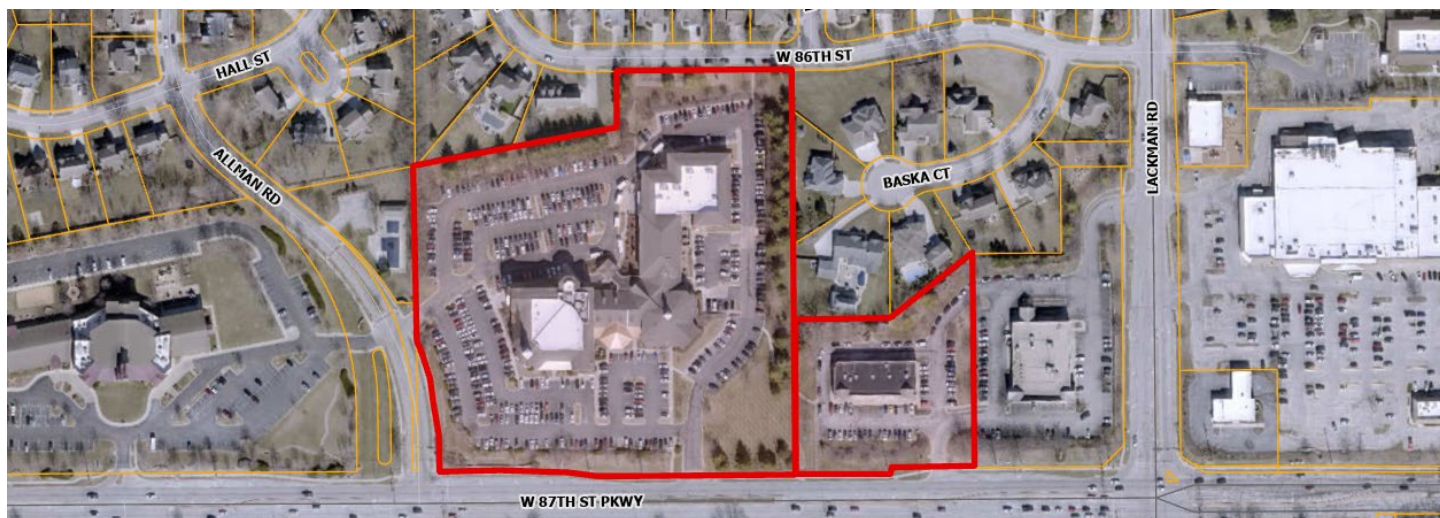


## SITE INFORMATION

- February 1999: The Governing Body approved a final plat (PT98-31F) and final plan (PL98-20F) for phase one of development of the Church.
- December 2001: The Planning Commission approved the final plan (PL01-08F) for part two of phase one of the Church.
- June 2002: The Planning Commission approved a revised final plan (PL02-05FR) for a parking lot expansion to accommodate an additional 68 cars.
- March 2003: The Planning Commission approved a second revised final plan (PL03-04FR) that included a 42,789 SF, two-story plus basement addition, and an additional 60 parking stalls.
- April 2004: The Planning Commission approved a third revised final plan (PL04-10FR) for another parking lot expansion to provide an additional 178 parking stalls.
- July 2004: The Planning Commission approved a fourth revised final plan (PL04-27FR) for additional parking.
- December 2006: The Planning Commission approved a final plan (PL06-43F) for a new Sanctuary addition to the Church.

Lenexa Baptist Church recently acquired the neighboring lot to the east, as outlined in [Exhibit 1](#) below. The lot has a single building with three tenant spaces; two of which are occupied by dentists with long term leases. The lot has one point of ingress and egress on 87<sup>th</sup> Street Parkway but shares an additional point of access with the Walgreens, located on the lot to the east. This additional point of ingress and egress provides access to Lackman Road.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
12.23	+3,100 of Impervious Surface	R-1, CP-1	Public/Open Space, Neighborhood Commercial Center

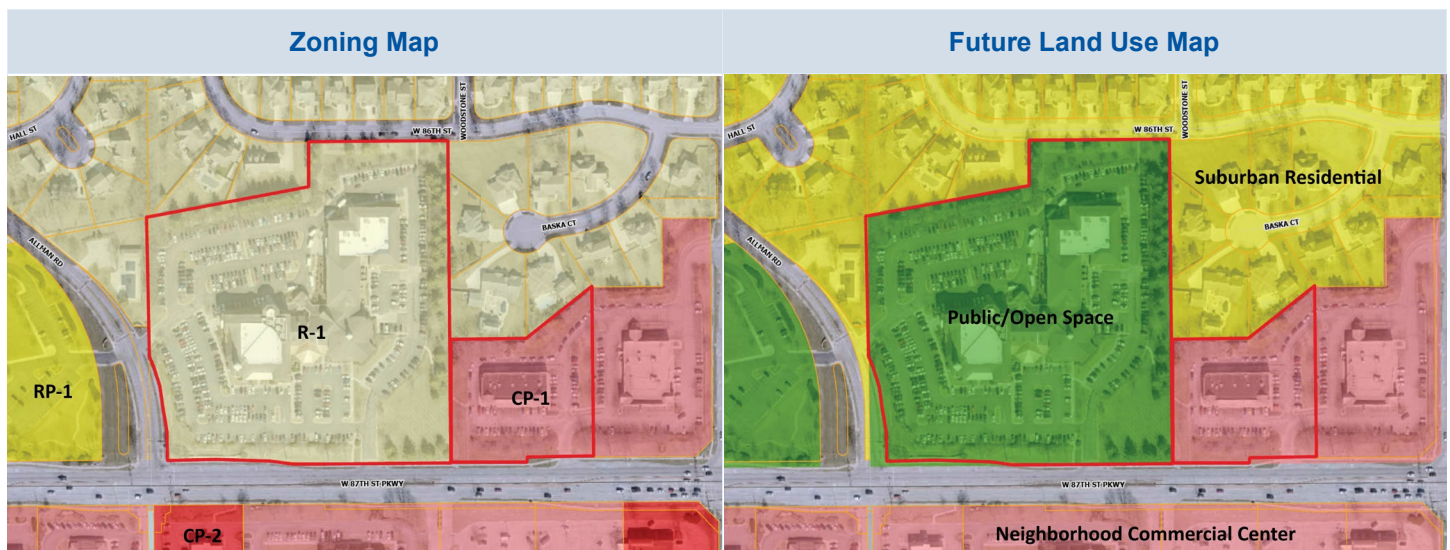


**Exhibit 1: Aerial Image of Subject Site**

## LAND USE REVIEW

The proposed drive connects the Church parcel that lies in the R-1, Single-Family Residential Zoning District, and the east adjacent parcel in the CP-1, Planned Community Commercial Zoning District. The revised final plan adds a drive connection between the two parcels with the intention to alleviate traffic congestion issues for peak times for the Church.

Existing land uses include Church or place of worship and medical or dental clinic. The parcel to the east of the Church has one unoccupied space and two dental offices that have long-term leases. The Church anticipates taking over the unoccupied tenant space for administrative offices soon. An office for administrative purposes is permitted in the CP-1, Planned Community Commercial Zoning District. The addition of the private drive does not change existing uses or zoning designations.

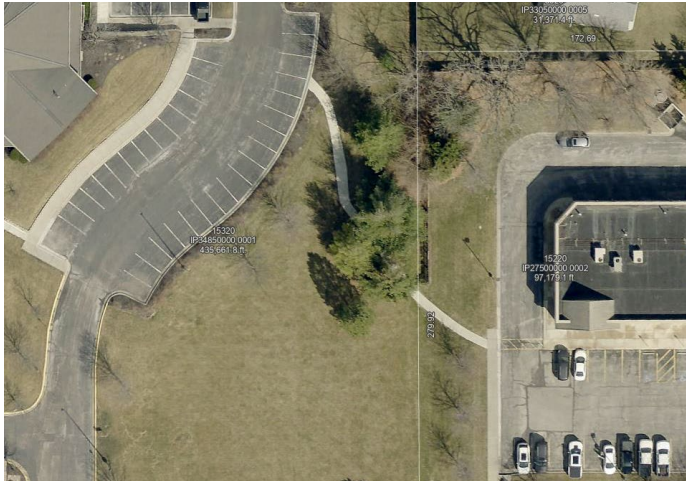


**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Public/Open Space; Neighborhood Commercial Center	R-1, Residential Single-Family; CP-1, Planned Neighborhood Commercial	Church; Medical or Dental Clinic
<b>North</b>	Suburban Residential	R-1, Residential Single-Family	Single-Family Detached
<b>South</b>	Neighborhood Commercial Center	CP-1, Planned Neighborhood Commercial; CP-2, Planned Community Commercial	Retail; Banking Services; Personal Services; Restaurant, general
<b>East</b>	Suburban Residential; Neighborhood Commercial Center	CP-1, Planned Neighborhood Commercial; R-1, Residential Single-Family	Single-Family Detached, Retail
<b>West</b>	Public/Open Space; Suburban Residential	RP-1, Planned Residential Single-Family, R-1, Residential Single-Family	Church; Single-Family Detached

## REVISED FINAL PLAN REVIEW

Lenexa Baptist Church recently acquired the neighboring commercial lot to the east, as outlined in [Exhibit 1](#). The church proposes to link the existing church parking lot to the commercial lot's parking lot with a new drive. The commercial lot has a single building with three tenant spaces; two of which are occupied by dentists with long-term leases.



*Exhibit 2: Existing conditions between the two subject parcels.*



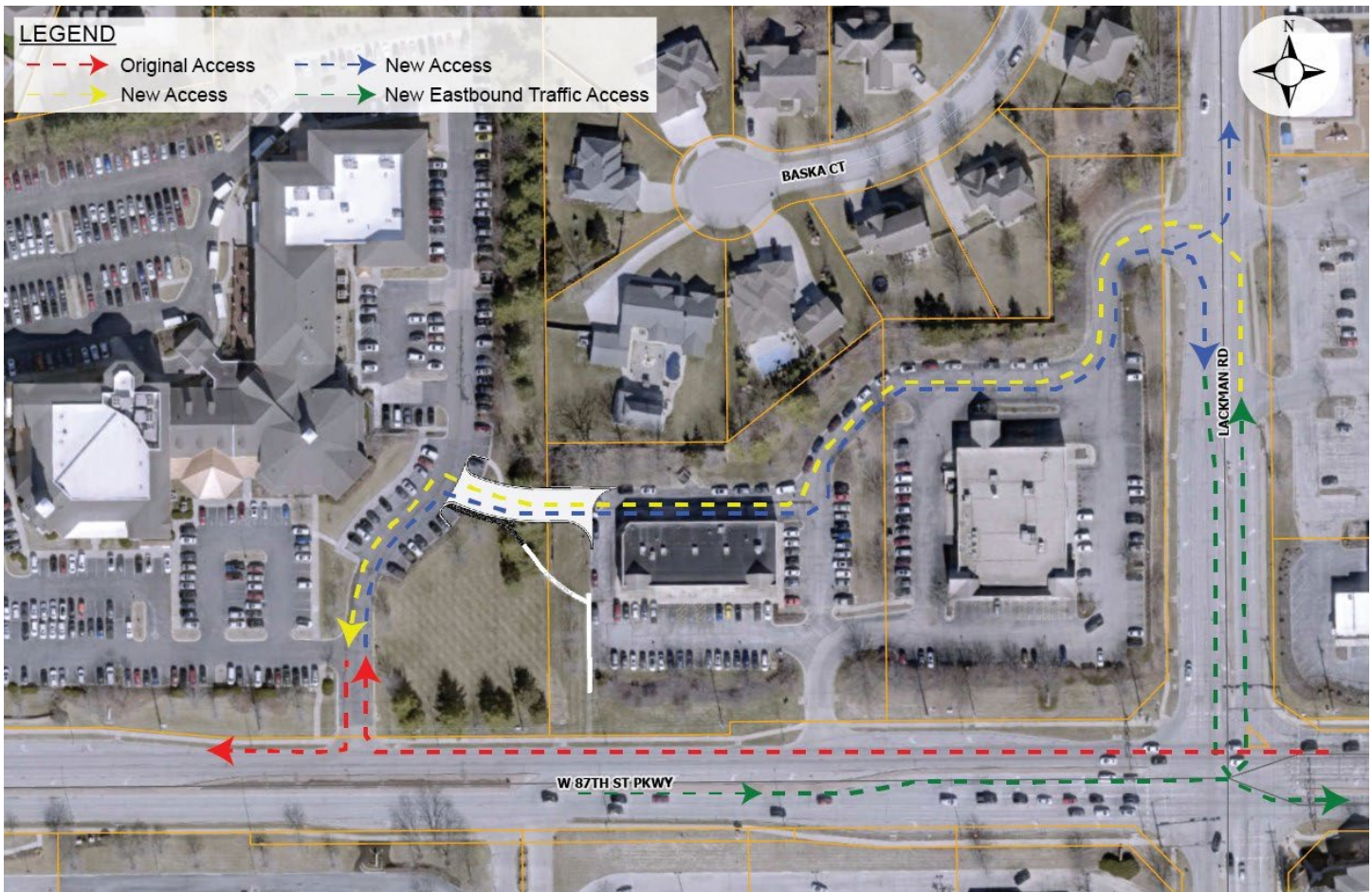
*Exhibit 3: Proposed private drive connecting the Church parcel and the east adjacent parcel.*

### PUBLIC IMPROVEMENTS

No public improvements are required with this revised final plan application.

### TRAFFIC, ACCESS, AND PARKING

The commercial lot has one point of ingress and egress on 87<sup>th</sup> Street Parkway but shares an additional point of access with Walgreens, which is located on the lot to the east of the newly acquired building. This additional point of ingress and egress will provide church patrons with access to Lackman Road.



**Exhibit 4: Aerial of subject site illustrating original access points (in red) to the Church from westbound 87<sup>th</sup> Street Parkway. Green lines illustrate new eastbound traffic access. Blue and yellow lines illustrate new access on Lackman to the proposed private drive.**

There are existing pedestrian connections from the right-of-way along 87<sup>th</sup> Street Parkway. The sidewalk will be slightly modified with the addition of the drive but will still provide connection between 87<sup>th</sup> Street Parkway and both parcels.

Minimum parking requirements were met with previously approved final plans. The addition of the private drive removes seven existing parking spaces. With the addition of the new parcel, there will be additional parking spaces for Church attendees to use.

**TABLE 3: PARKING ANALYSIS**

Land Use	Parking Formula	Required Parking	Existing Parking	Proposed Parking	Difference
Church or place of worship	1 space per 3 seats in main assembly area	384	507	-7	+116

Although the Church exceeds the UDC’s parking requirements, Staff is aware that the Church has private cross-parking agreements with nearby property owners to address Sunday parking demand.

## STORMWATER

Given there are no significant impervious improvements (just an access drive), and that the access drive does not negatively impact the existing detention basin it is crossing just to the north of, there are no additional stormwater requirements for this project.

## FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. If access to 15220 W. 87<sup>th</sup> Street becomes a problem, then additional fire lane markings will be required.

All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

## LANDSCAPING

There will be minimal changes to existing landscaping. Some mature trees must be removed to facilitate the new drive aisle. New trees will be planted nearby to compensate for the tree removal. Mature trees near the proposed development will be outfitted with tree protection measures.

## DEVIATIONS

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The applicant is not requesting any deviations.

## REVIEW PROCESS

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- *The Planning Commission is the final authority for approval of this project.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

## RECOMMENDATION FROM PROFESSIONAL STAFF

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★ **Staff recommends approval of the proposed private drive for Lenexa Baptist Church.**

- The project is consistent with Lenexa's goals through ***Strategic Community Investment*** to create ***Integrated Infrastructure and Transportation***.

## REVISED FINAL PLAN

Staff recommends **APPROVAL** of the revised final plan for PL23-05FR – **Lenexa Baptist Church, East Private Drive** between 15320 W. 87<sup>th</sup> Street Parkway and 15220 W. 87<sup>th</sup> Street Parkway, for a private drive connection, with the following condition:

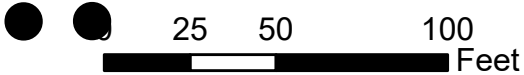
1. Provide landscape details including tree protection plan, tree planting details, and landscape maintenance guarantee at the time of permit issuance.



Document Path: C:\Users\cdlogan\OneDrive - City of Lenexa\Documents\ArcGIS\Projects\Vicinity Map\Vicinity Map.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Lenexa Baptist Church East Private Drive PL23-05FR

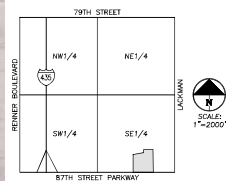
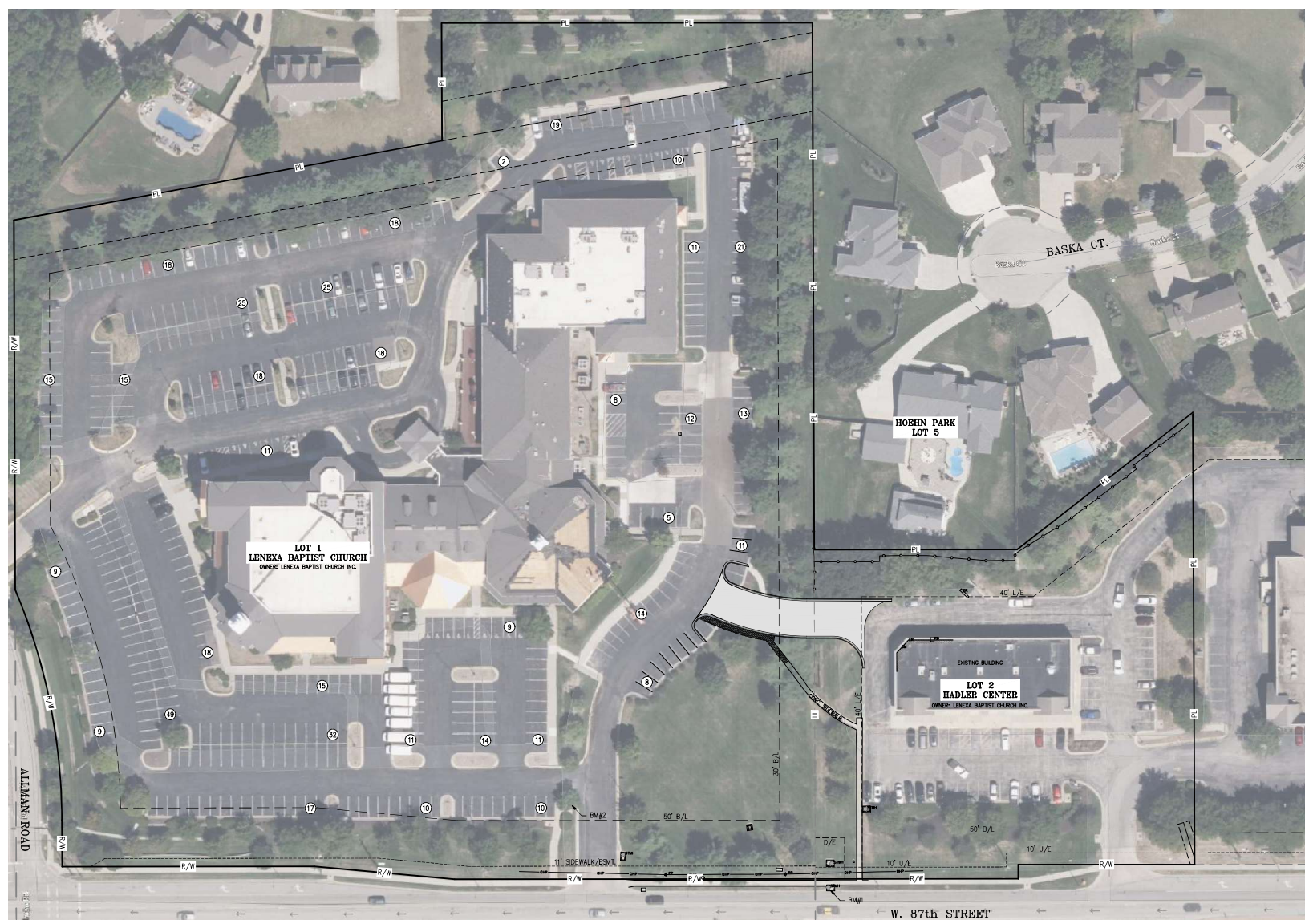




**PHILIP ENGINEERING, INC.**  
 207 N. Woodruff  
 Chiles, Kansas 66219  
 (913) 336-7600  
 www.philipengineering.com

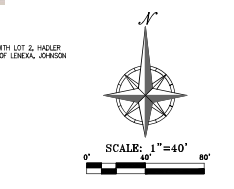


**OVERALL SITE PLAN**  
 LENEXA BAPTIST CHURCH  
 15320 W 87TH STREET  
 LENEXA, KS 66219



VICINITY MAP  
SEC. 29-12-24

- LEGEND**
- PL — PROPERTY LINE
  - - - LL - LOT LINE
  - - - R/W - RIGHT-OF-WAY
  - - - 2' CURB & OUTER
  - ▭ STANDARD DUTY ASPHALT PAVEMENT



**LOT 1, LENEXA BAPTIST CHURCH PROJECT SUMMARY**

SITE AREA	10.50 AC.
EXISTING BUILDING AREA	96,401 S.F.
CHURCH # OF ASSEMBLY SEATS	1,150
ZONING	R1
REQUIRED # OF PARKING STALLS (1 / 3 SEATS)	383
EXISTING PARKING STALLS	510
EXISTING PARKING STALLS TO BE REMOVED	7
PROPOSED PARKING STALLS	503
EXISTING IMPERVIOUS AREA	290,000 S.F.± 67%
EXISTING OPEN SPACE	145,662 S.F.± 33%
PROPOSED IMPERVIOUS AREA	291,440 S.F.± 67%
PROPOSED OPEN SPACE	144,222 S.F.± 33%

**LOT 2, HALDER CENTER PROJECT SUMMARY**

SITE AREA	2.23 AC.
EXISTING BUILDING AREA	9,100 S.F.
ZONING	CP-1
EXISTING PARKING STALLS	55
EXISTING PARKING STALLS TO BE REMOVED	0
PROPOSED PARKING STALLS	55
EXISTING IMPERVIOUS AREA	52,200 S.F.± 54%
EXISTING OPEN SPACE	44,979 S.F.± 46%
PROPOSED IMPERVIOUS AREA	53,860 S.F.± 55%
PROPOSED OPEN SPACE	43,319 S.F.± 45%

**LEGAL DESCRIPTION:**

LOT 1, LENEXA BAPTIST CHURCH TOGETHER WITH LOT 2, HALDER CENTER PLATTED SUBDIVISION IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS.



**UTILITY NOTES:**  
 VISUAL INDICATIONS OF UTILITIES ARE AS SHOWN. UNDERGROUND LOCATIONS SHOWN, AS FURNISHED BY THEIR LESSORS, ARE APPROXIMATE AND SHOULD BE VERIFIED IN THE FIELD AT THE TIME OF CONSTRUCTION. FOR ACTUAL FIELD LOCATIONS OF UNDERGROUND UTILITIES CALL 811.

PROJECT NO.	230477	DATE	11/15/2023
BY	MS	REVISION	REVISED PER SHEET COMMENTS
DATE	11/15/2023	SCALE	1"=40'
PROJECT	LENEXA BAPTIST CHURCH	DATE	11/15/2023
OWNER	LENEXA BAPTIST CHURCH	SCALE	1"=40'
DATE	11/15/2023	SCALE	1"=40'
PROJECT	LENEXA BAPTIST CHURCH	DATE	11/15/2023
OWNER	LENEXA BAPTIST CHURCH	SCALE	1"=40'
DATE	11/15/2023	SCALE	1"=40'
PROJECT	LENEXA BAPTIST CHURCH	DATE	11/15/2023
OWNER	LENEXA BAPTIST CHURCH	SCALE	1"=40'
DATE	11/15/2023	SCALE	1"=40'

SHEET  
**C2**

PROJECT: \\server\projects\15320W87th\15320W87th.dwg, layout: 1, Oct 19, 2023, 2:02:24 PM, David Pim



**LOT 2  
HADLER CENTER**  
OWNER: LENEKA BAPTIST CHURCH INC.

W. 87th STREET

**SITE PLAN NOTES:**

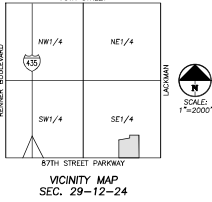
- All construction materials and procedures on this project shall conform to the latest revision of the following governing requirements, incorporated herein by reference:  
a) City Ordinances & C.S.A. Regulations.  
b) The City of Lenexa Technical Specifications and Municipal Code.
- The contractor shall have one (1) signed copy of the plans (approved by the City) and one (1) copy of the appropriate Design and Construction Standards and Specifications at the job site at all times.
- The contractor will be responsible for securing all permits, bonds and insurance required by the contract documents, City of Lenexa, Kansas, and all other governing agencies (including local, county, state and federal authorities) having jurisdiction over the work proposed by these construction documents. The cost for all permits, bonds and insurance shall be the contractor's responsibility and shall be included in the bid for the work.
- The contractor is responsible for coordination of his and his sub-contractor's work. The contractor shall assume all responsibility for protecting and maintaining his work during the construction period and between the various trades/sub-contractors constructing the work.
- The demolition and removal (or relocation) of existing pavement, curbs, structures, utilities, and all other features necessary to construct the proposed improvements, shall be performed by the contractor. All waste material removed during construction shall be disposed off the project site. The contractor shall be responsible for all permits for hauling and disposing of waste material. The disposal of waste material shall be in accordance with all local, state and federal regulations.
- Contractor shall be responsible for all relocations, including but not limited to, all utilities, storm drainage, sanitary sewer services, signs, traffic signals & poles, etc. as required. All work shall be in accordance with governing authorities specifications and shall be approved by each. All cost shall be included in base bid.
- All existing utilities indicated on the drawings are according to the best information available to the Engineer; however, all utilities actually existing may not be shown. The contractor shall be responsible for contacting all utility companies for an exact field location of each utility prior to any construction. All underground utilities shall be protected at the contractor's expense. All utilities, shown and unshown, damaged through the negligence of the contractor shall be repaired or replaced by the contractor at its expense.
- The contractor will be responsible for all damage to existing utilities, pavement, fences, structures and other features not designated for removal. The contractor shall repair all damages at his expense.
- The contractor shall verify the true lines of all existing storm or sanitary sewer connections and utility crossings prior to the start of construction. Notify the engineer of any discrepancies.
- SAFETY NOTICE TO CONTRACTOR:** In accordance with generally accepted construction practices, the contractor shall be solely and completely responsible for conditions of the job site, including safety of all persons and property during performance of the work. This requirement will apply continuously and shall not be limited to normal working hours. Any construction observation by the engineer of the contractor's performance is not intended to relieve the contractor of the responsibility of the contractor's safety measures, in or near the construction site.

**SITE DIMENSION NOTES:**

- BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF PROPOSED WALLS. THE SUBCONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR SPECIFIC DIMENSIONS AND LAYOUT INFORMATION FOR THE BUILDINGS.
- ALL DIMENSIONS SHOWN FOR THE PARKING LOT AND CURBS ARE MEASURED FROM BACK OF CURB TO BACK OF CURB.

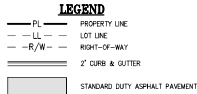
**PAVEMENT MARKING AND SIGNAGE NOTES:**

- PARKING STALL MARKING STRIPES SHALL BE FOUR INCH (4") WIDE WHITE STRIPES. DIRECTIONAL ARROW AND HANDICAP STALL MARKINGS SHALL BE FURNISHED AT LOCATIONS SHOWN ON PLANS.
- HANDICAP PAVEMENT MARKINGS AND SIGNS SHALL CONFORM TO ALL FEDERAL (AMERICANS WITH DISABILITIES ACT) AND STATE LAWS AND REGULATIONS.
- TRAFFIC CONTROL DEVICES AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
- STOP SIGNS SHALL BE PROVIDED AT ALL LOCATIONS AS SHOWN ON PLANS AND SHALL CONFORM TO THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES". SIGNS SHALL BE 18" X 12", 18 GAUGE STEEL AND SHALL BE ENGINEER GRADE REFLECTIVE.
- TRAFFIC CONTROL AND PAVEMENT MARKINGS SHALL BE PAINTED WITH A WHITE SHERWIN WILLIAMS S-W TRAFFIC MARKING SERIES B-3000 OR APPROVED EQUAL. THE PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. APPLY ON A CLEAN, DRY SURFACE AND AT A SURFACE TEMPERATURE OF NOT LESS THAN 50°F AND THE AMBIENT AIR TEMPERATURE SHALL NOT BE LESS THAN 40°F AND RISING. TWO COATS SHALL BE APPLIED.



**SITE KEY NOTES:**

- (A) CONSTRUCT PRIVATE 2' CURB & GUTTER (TYPICAL).
- (B) CONSTRUCT FULL DEPTH PRIVATE ASPHALT PAVEMENT.
- (C) CONSTRUCT PRIVATE CONCRETE SIDEWALK (TYPICAL).
- (D) CONSTRUCT PRIVATE ACCESSIBLE SIDEWALK CURB RAMP (OMIT DETECTABLE WARNING).



**UTILITY NOTES:**  
VISUAL INDICATIONS OF UTILITIES ARE AS SHOWN. UNDERGROUND LOCATIONS SHOWN, AS FURNISHED BY THEIR LESSORS, ARE APPROXIMATE AND SHOULD BE VERIFIED IN THE FIELD AT THE TIME OF CONSTRUCTION. FOR ACTUAL FIELD LOCATIONS OF UNDERGROUND UTILITIES CALL 811.

Know what's below.  
Call before you dig.



**PHILIP ENGINEERING, INC.**  
1070 N. Woodbury  
Chick, Kansas 66204  
(913) 251-1100  
www.philipengineering.com



**SITE PLAN**  
LENEKA BAPTIST CHURCH  
15320 W 87TH STREET  
LENEKA, KS 66219

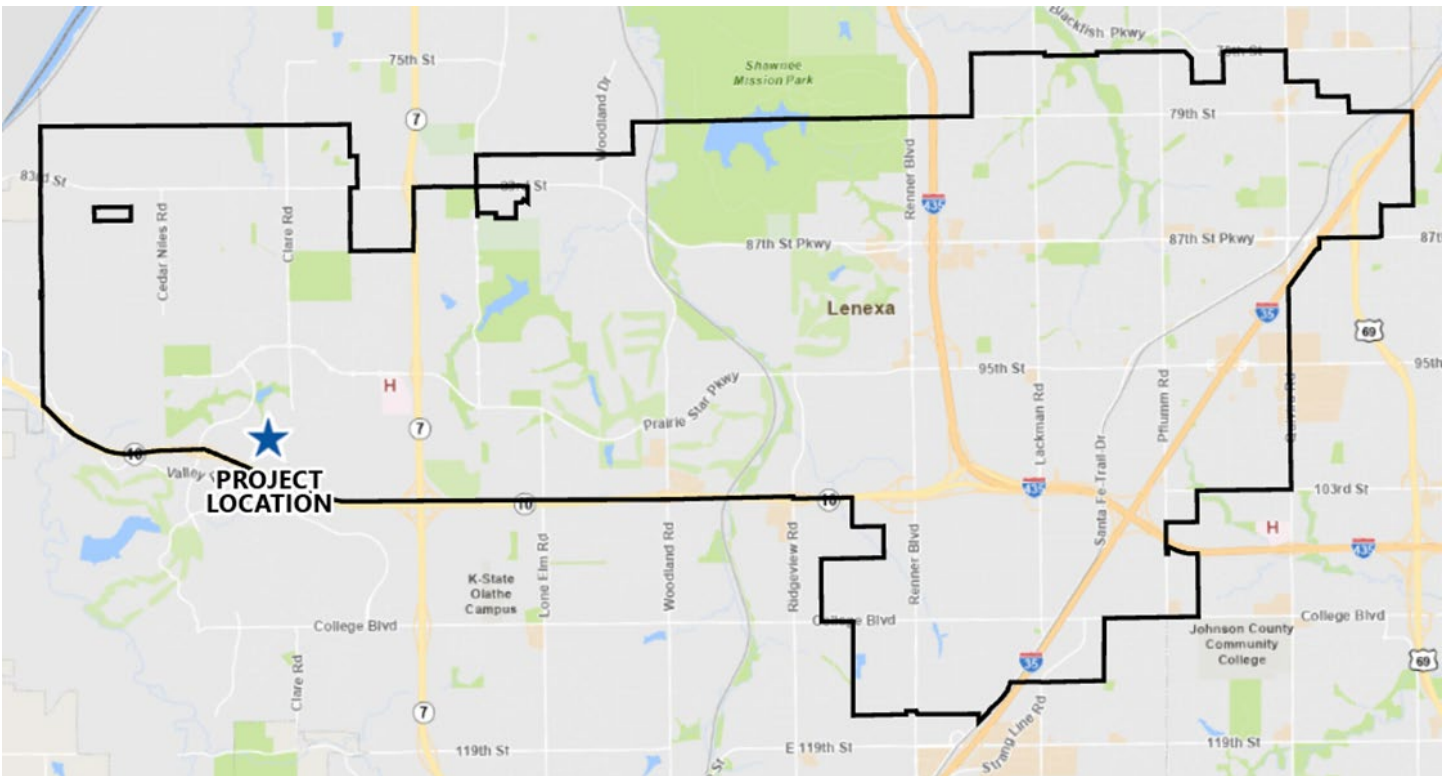
PROJECT NO.	DATE	NO.	DESCRIPTION	BY	DATE
255477	11/17/2023	1	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	2	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	3	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	4	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	5	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	6	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	7	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	8	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	9	RECEIVED PER STREET COMMENTS	MM	
255477	11/17/2023	10	RECEIVED PER STREET COMMENTS	MM	

SHEET  
**C2.1**



# CANYON CREEK APARTMENT HOMES

<b>Project #:</b>	PL23-20F	<b>Location:</b>	NEC of Canyon Creek Blvd & K-10 Hwy
<b>Applicant:</b>	Erin Merrill, Eskie + Associates	<b>Project Type:</b>	Final Plan
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Multifamily Residential



## PROJECT SUMMARY

The applicant seeks approval of a final plan for Canyon Creek Apartment Homes, a multifamily residential development located approximately .3 miles east of the northeast corner of Canyon Creek Boulevard and K-10 Highway. The plan will include five multifamily buildings with a total of 212 dwelling units and building area of 205,000 square feet. The proposed density of the development is 12.82 dwelling units per acre. Of the five buildings, one is proposed to be three stories while the remaining four will be three/four story split-type buildings due to grade changes. The plan includes private amenities (dog park, exercise area, pavilions, hammocks), landscaping, parking, and related infrastructure. The proposed final plan is consistent with the approved preliminary plan/plat (P23-06P), which included deviations for building height and setback from K-10 Highway and was approved by the Governing Body on September 19, 2023. This project does not require a Public Hearing.

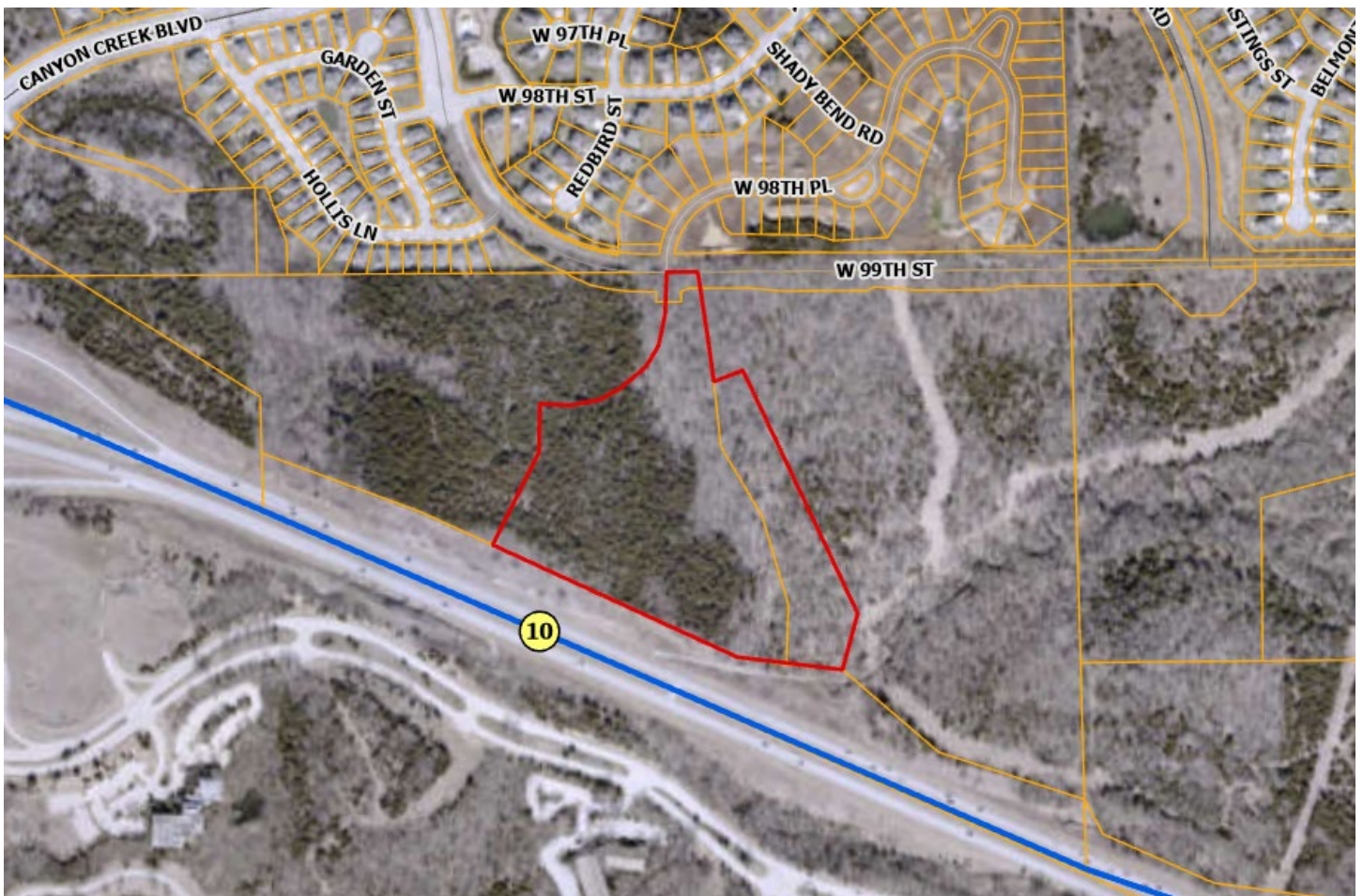
**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

This site has the following development application history:

- Approval by the Governing Body of a concept plan and rezoning of a 112-acre mixed use development known as Cedar Canyon West (PL22-04CP and RZ22-09), on February 7, 2023. The subject area was rezoned from AG, Agricultural District to RP-2, Planned Residential (Intermediate-Density) District and RP-4, Residential Planned (High Density) District.
- Approval by the Governing Body of a preliminary plan/plat for a multifamily residential development and amenities (PL23-06P), on September 19, 2023.
- Approval by the Governing Body of a final plat for a multifamily residential development (PT23-23F), on October 17, 2023.

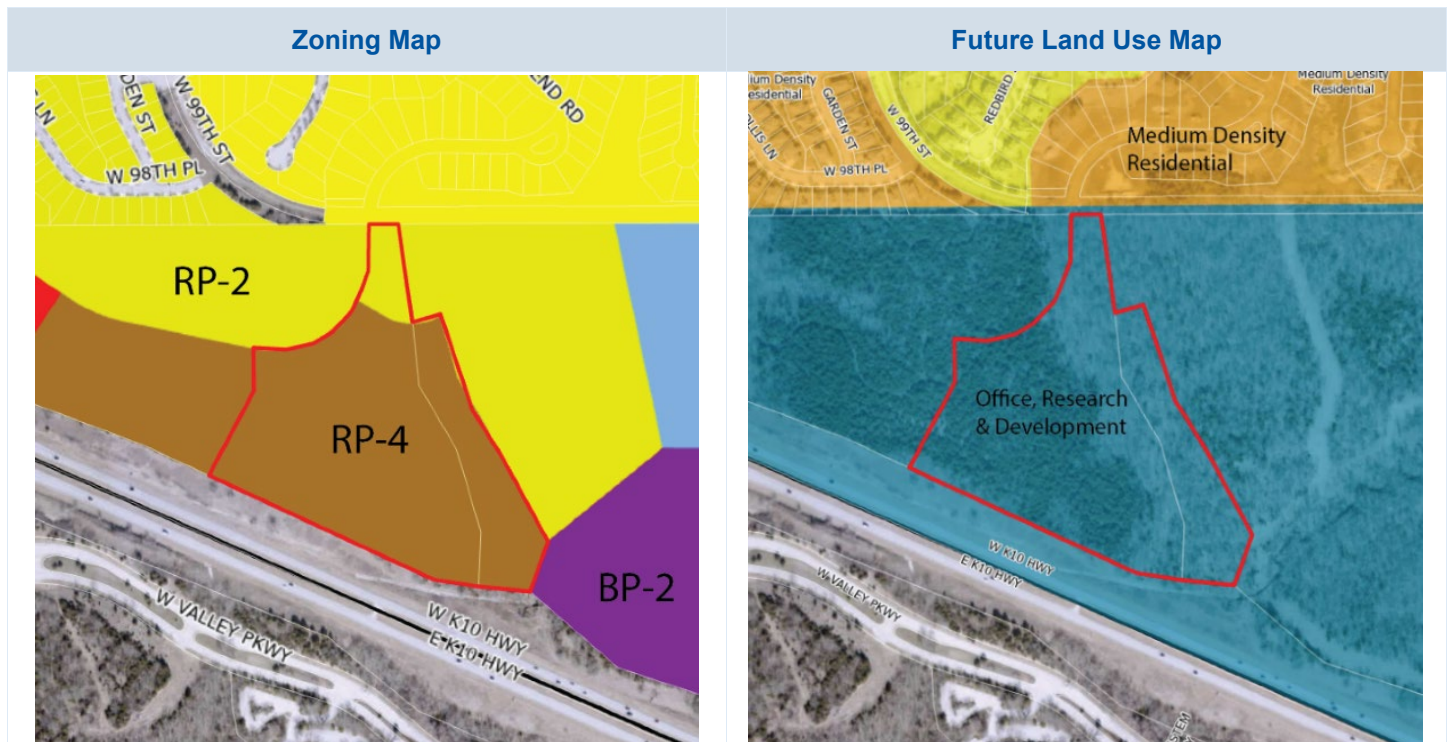
LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
17.62	205,000	RP-4, RP-2	Office, Research & Development



**Exhibit 1: Aerial Image of Subject Site**

## LAND USE REVIEW

The proposed use is multifamily residential, and the plans include five buildings totaling 212 dwelling units with a mixture of one-bedroom, two-bedroom, and three-bedroom apartments. The proposed density is 12.82 dwelling units per acre. The current Comprehensive Plan calls for office uses at this location; however, given that the Governing Body recently approved a rezoning and concept plan for this mixed-use development, Staff is recommending to the consultant working on the Comprehensive Plan that the Future Land Use designation be updated to align with these approvals, which would change the designation from office to high-density residential in this area. The new Comprehensive Plan is anticipated to be completed this winter. Amenities include a clubhouse, hammock areas, trail system, playground, and dog park. The dog park will be located on the RP-2 portion of the site. The proposed use was approved as part of a larger concept plan, approving the multifamily designation and surrounding uses simultaneously. The Governing Body affirmed the rezoning based on Staff determination that the proposed use and surrounding uses would be compatible.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Office, Research and Development	RP-4, Residential Planned (High-Density) District and RP-2 Residential Planned (Intermediate-Density) District	Agricultural
<b>North</b>	Medium Density Residential	RP-1, Residential District, Residential Planned (Low-Density) District	Vacant Residential
<b>South</b>	City of Olathe, across K-10 Highway	Agricultural and Business Commercial (Olathe, across K-10 Highway)	Office and Agricultural (Olathe, across K-10 Highway)
<b>East</b>	Office, Research and Development	RP-2, Residential Planned (Intermediate-Density) District and BP-2, Planned Manufacturing District	Agricultural
<b>West</b>	Office, Research and Development	RP-4, Residential Planned (High-Density) District and RP-2, Residential Planned (Intermediate-Density) District	Agricultural

## FINAL PLAN REVIEW

This final plan application follows approval of a preliminary plan/plat and final plat for Canyon Creek Apartment homes. The final plan includes development of a multifamily residential complex with five buildings, three tracts and amenities.

## DIMENSIONAL STANDARDS

The site is in compliance with dimensional standards other than the approved deviations for height and freeway setback. Compensatory design has been provided for the requested deviations, to include increased landscaping and open space.

**TABLE 2: DIMENSIONAL STANDARDS**

	Required	Proposed	Difference
Maximum Height	35 feet	48 feet	+13 feet
Minimum Open Space	60% of lot area	62% of site area	+2 %
Maximum Density	16 dwelling units per acre	12.82 dwelling units per acre	-3.18 dwelling units per acre

## PUBLIC IMPROVEMENTS

The developer of this project is also constructing W. 100<sup>th</sup> Street under a separate permit. This final plan includes construction of a portion of W. 99<sup>th</sup> Terrace and associated infrastructure, to include sidewalks and lighting. These public improvements will be paid for by the developer.

## TRAFFIC, ACCESS, AND PARKING

Lot 1, containing the five residential buildings, has one full access entrance from W. 100<sup>th</sup> Street at the northwest corner of the site and a second access point from W. 99<sup>th</sup> Place at the north. Tract A, which contains the dog park, has access to W. 99<sup>th</sup> Place directly across from the apartment entrance. Sidewalks are planned along W. 100<sup>th</sup> Street as part of the construction of the street and along W. 99<sup>th</sup> Place as part of this application. The public sidewalk network will connect to the internal sidewalks, with direct connections to all buildings, parking areas and amenity spaces.

A 10-foot-wide trail will connect to W. 99<sup>th</sup> Place and run along a portion of the eastern property line adjacent to the common open space area and is intended to connect to future development to the east through the adjacent property to the larger public trail network along future Clare Road. This will be a public trail with a dedicated easement shown on the final plat.

**TABLE 3: PARKING ANALYSIS**

Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Multifamily 74 1-bedroom units 74 2-bedroom units 64 3-bedroom units 212 total units	1 space per efficiency unit 1.5 space per 1-bedroom unit 1.75 spaces per 2-bedroom unit 2 spaces per 3+ bedroom unit .25 spaces per unit for visitors	422	422	0
Bicycle 212 total units	1 space per 5 units	42	45	+ 3

## STORMWATER

The applicant submitted a final stormwater management plan/study for the proposed property that indicates the intent to meet the City’s stormwater standards and requirements.

Generally, the site will provide various best management practices (BMPs) to meet stormwater detention requirements as well as water quality requirements. The BMPs that will be utilized to achieve these requirements consist of preserved or established native vegetation, a hydrodynamic separator, as well as an extended dry detention basin.

The eastern portion of the site is generally identified as stream buffer. The applicant set the site’s general infrastructure (retaining walls, parking lots, buildings, etc.) outside of the floodplain and subsequent stream buffer, except for some sidewalk and trails, which are allowable components within the stream buffer. There is some grading shown on the back slope of the extended dry detention basin within the floodplain and buffer, with Lenexa code allowing for this minor grading outside of the floodway. These areas will need to be reestablished with native vegetation upon completion of grading, as they are within the area identified as preserved or established native vegetation.

## FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

**LIGHTING**

Design feature lighting is proposed at building corners with an up-down outdoor accent wall light. Parking lot and amenity spaces will have pole-mounted lighted with full-cutoff fixtures. Pole-mounted lighting shall not exceed 27 feet in height (including the base) and the base shall not exceed 2 feet in height.

**LANDSCAPING**

The landscape plan shows preservation of existing vegetation on the eastern portion of the site which serves as stormwater BMPs, recreation and also houses the stream and stream buffer. Additional areas of vegetation to be preserved include shade trees and evergreens along the southern perimeter of the site. Other open space areas that will be disturbed shall be re-established with native seeding. Perimeter plantings are required along street frontages dependent on the street type. W. 100<sup>th</sup> Street and W. 99<sup>th</sup> Street have perimeter plantings in accordance with the requirements for collector streets. Landscaping along the south perimeter must meet the standards for a highway or freeway. Parking lot landscaping is provided in compliance with code requirements. Overall, the plans include landscaping that meets or exceeds the code requirements.



**Exhibit 2: Landscape Plan**

## ARCHITECTURE

The proposal includes five multifamily residential buildings of similar architectural style and related accessory structures such as play areas and pavilions. Four of the five residential buildings have received height deviations to allow a maximum building height of 45 feet. The height deviation request is due to the sloping grades of the site.

There are three proposed building designs:

- Type A is a three-story design and includes the clubhouse/ leasing office. Building 1 is type A.
- Type B is a three/four split-story design with four stories only on the southeast corner of the building. Building 2 is type B.
- Type C is a three/four split-story design with an equal split of three-story on the west side and four-story on the east side. Buildings 3,4, and 5 are type C.

The difference between building types B and C was intentional so that building 2 would present as three-story from the single-family and two-family zoning districts to the north and west.

TABLE 4: ARCHITECTURAL ANALYSIS			
Building Type	Stories	Maximum Height	Units
A (Building 1)	3	35'	46
B (Building 2)	3/4	44' 9 1/8"	40
C (Building 3,4,5)	3/4	44' 9 1/8"	42

All three building types will use the same material palette. Stone and block veneer are used varyingly throughout the first, second and third floors to distinguish floor levels among the facades. Upper levels use shingle-shake siding, vertical board and batten, and horizontal fiber cement siding in shades of dark wood and taupe. Column accents and details use wood with a dark brown finish. Parapet cap, fascia, window and exterior door frames will be aluminum with dark brown finish. Balcony railings will be matte black galvanized steel.

Fencing around amenity areas will be black aluminum with stone columns.

MATERIAL LEGEND	
1	BRICK - WHITE GLAZED - 4"X8"X8" - 1/2" THICK
2	BRICK - RED GLAZED - 4"X8"X8" - 1/2" THICK
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PRELIMINARY SET FOR CONSTRUCTION, RECORDING PURPOSES OR SUPPLEMENTAL DATA

**Exhibit 3: Material Legend**



**Exhibit 4: Building Type A**





**BUILDING TYPE "B" FRONT ELEVATION**

SCALE: 3/32" = 1'-0"



**BUILDING TYPE "B" REAR ELEVATION**

SCALE: 3/32" = 1'-0"

**Exhibit 5: Building Type B**



**BUILDING TYPE "C" FRONT ELEVATION**

SCALE: 3/32" = 1'-0"



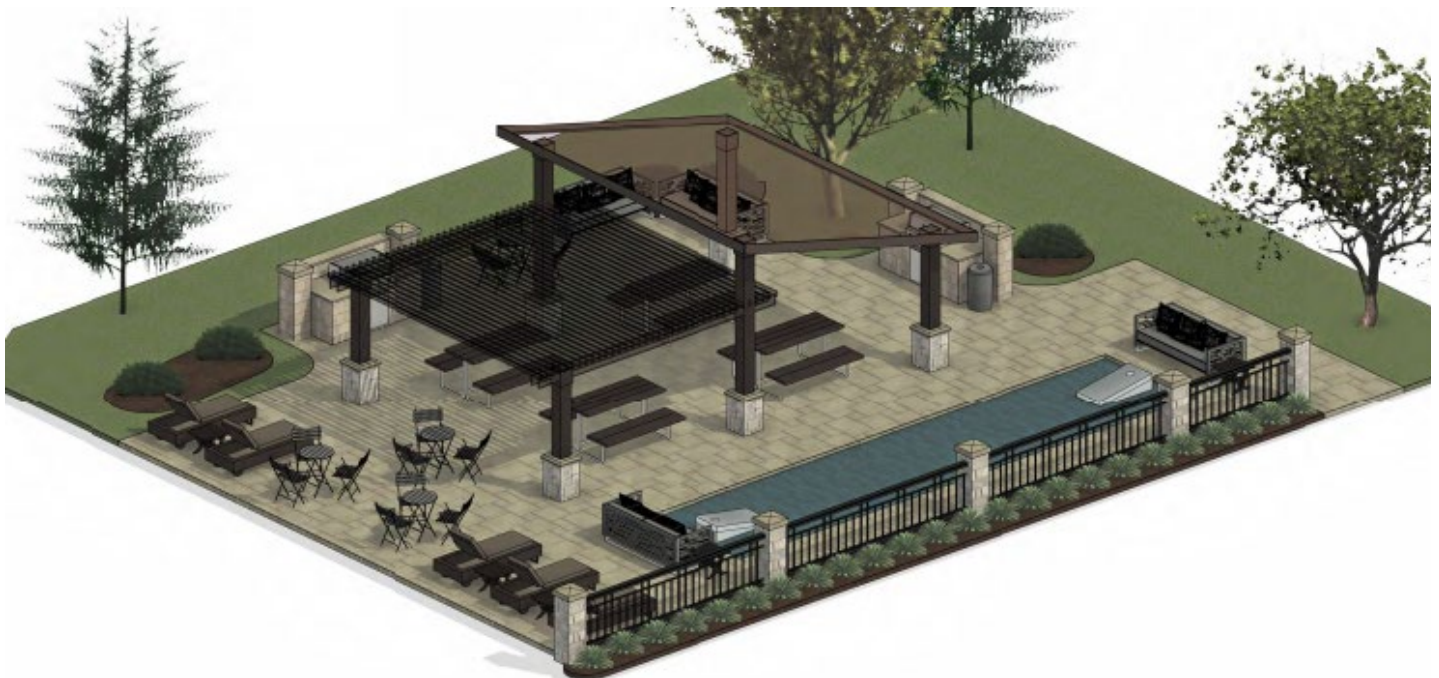
**BUILDING TYPE "C" REAR ELEVATION**

SCALE: 3/32" = 1'-0"

**Exhibit 6: Building Type C**



*Exhibit 7: Trash Enclosure*



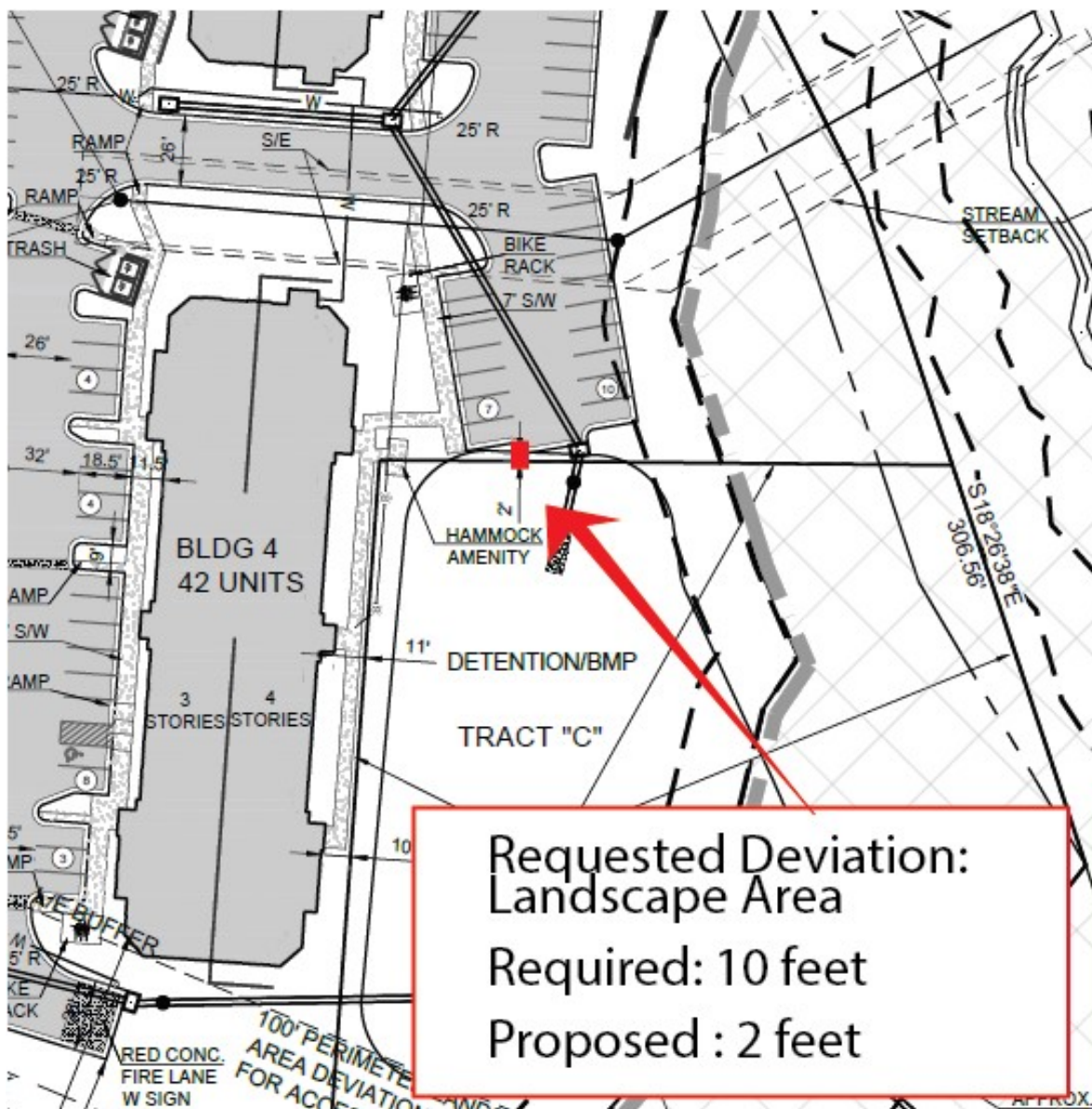
**PAVILION PERSPECTIVE**

*Exhibit 8: Sample of Amenity Area*

## DEVIATIONS

The applicant received approval for two deviations with the approval of the preliminary plan/plat and said deviations are reflected on the final plan. The deviations relate to building height and freeway setback.

The applicant is requesting one additional deviation with the final plan, which is for landscape area between the edge of a parking lot and the lot line. A perimeter landscape area of 10 feet is required between parking lots and the lot line, the applicant is requesting two feet. The parking lot abuts Tract C, which is a stormwater detention basin. Due to the required slopes for the detention area, the boundary of the tract can not be moved to accommodate the required setback. For future maintenance, City Staff prefers all detention areas be located within a separate tract. Considering that the lot line is simply separation between the tract and the lot, with significant vegetation of the tract, and is located internally to the overall site plan, Staff does not have an issue with this request.



**Exhibit 9: Deviation Request**

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## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

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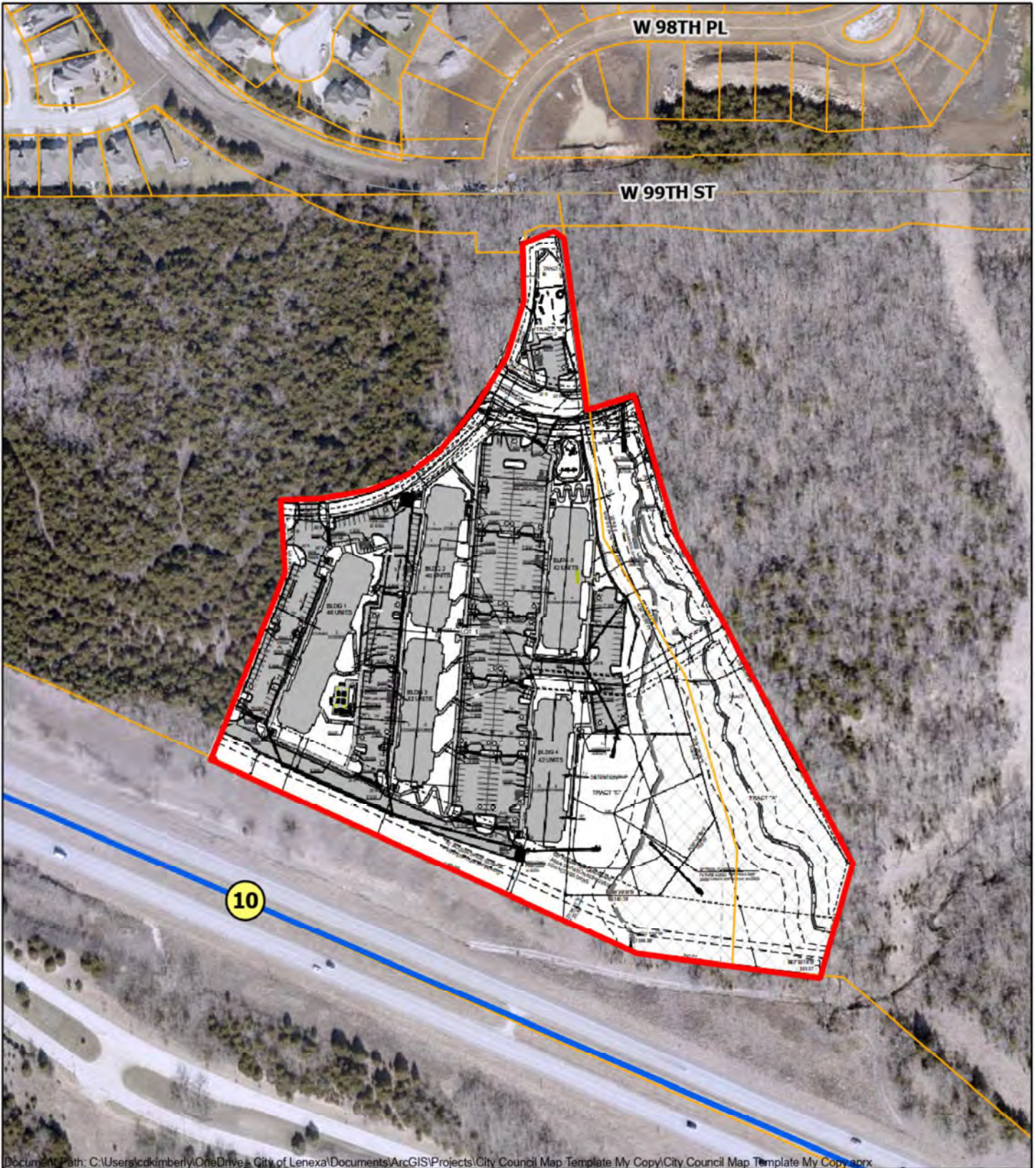
## RECOMMENDATION FROM PROFESSIONAL STAFF

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- ★ **Staff recommends approval of the proposed Final Plan for Canyon Creek Apartment Homes.**
  - This is a final plan for a multifamily residential development consisting of five multifamily buildings.
  - The project is consistent with Lenexa’s goals through ***Inclusive Community Building*** and ***Responsible Economic Development*** to create a ***Thriving Economy***.

### FINAL PLAN

Staff recommends **approval** of the final plan and deviation for PL23-20F – **Canyon Creek Apartment Homes** located near the northeast corner of Canyon Creek Boulevard and K-10 Highway, for a multifamily residential use with one deviation related to perimeter landscaping.

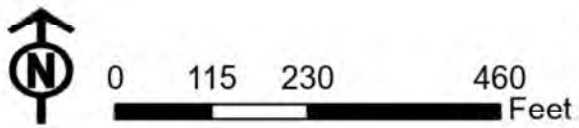


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Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Canyon Creek Apartment Homes

## PL23-20F





PRELIMINARY NOT FOR  
CONSTRUCTION, RECORDING  
PURPOSES OR IMPLEMENTATION

**MATERIAL LEGEND**

1	SHINGLE SHAKE SIDING - JAMES HARDIE CORN ESTONE
2	VERTICAL BOARD & BATTEN SIDING - JAMES HARDIE MONTERAY TAUPÉ
3	HORIZONTAL SIDING - JAMES HARDIE TIMBER BARK
4	ALL SIDING TRIM - JAMES HARDIE TIMBER BARK
5	STONE VENEER - SEMCO STONE MOUNTAIN RIDGE #M191
6	BLOCK VENEER
7	COLUMNS & ACCENT DETAILS - WOOD W/ DARK BROWN WAX OR STAIN
8	STANDING BEAM METAL CANOPY
9	SIGNAGE - MATTE FINISH METAL
10	ENERGY STAR RATED ALUMINUM WINDOWS STOREFRONTS/EXTERIOR DOORS - MATTE DARK BROWN FINISH
11	RAILING - MATTE BLACK GALVANIZED STEEL
12	RETAINING WALL - SEMCO STONE CASTLEWOOD LEDGEROCK (8"-16")
13	FENCE - AMERISTAR ALUMINUM MATTE BLACK
14	PARAPET CAP & FASCIA - ALUMINUM MATTE BRONZE/BROWN
15	STONE VENEER - DARK BROWN
16	STONE SILL - SEMCO STONE RUSTIC CEDAR SILL

**ELECTRICAL & SERVICE EQUIPMENT  
SCREENING & MATERIALS**

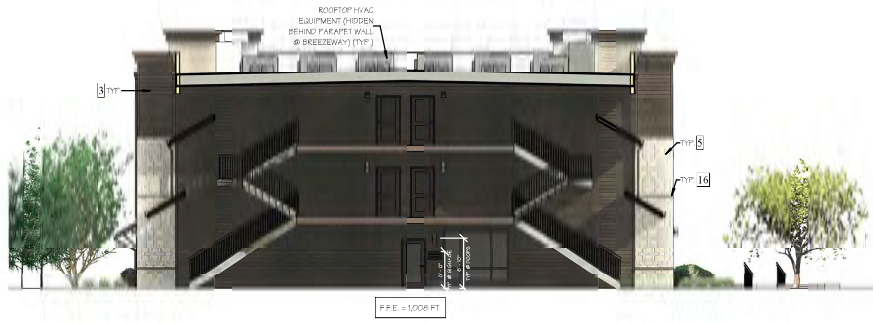
- WALL MOUNTED METERS TO HAVE BASE SCREENED BY LANDSCAPING/PANT TO MATCH EXTERIOR WALL MATERIAL ON MOUNTING WALL
- HVAC UNITS INSTALLED ON ROOF SHALL BE HIDDEN BEHIND PARAPET WALL - OUTLINE INDICATED IN ELEVATION



1  
VP18 SCALE: 1/8" = 1'-0"  
**CLUBHOUSE FRONT ELEVATION**



2  
VP18 SCALE: 1/8" = 1'-0"  
**CLUBHOUSE RIGHT SIDE ELEVATION**



3  
VP18 SCALE: 1/8" = 1'-0"  
**BLDG. TYPE "A" SECTION - CLUBHOUSE REAR ELEVATION**



4  
VP18 SCALE: 1/8" = 1'-0"  
**CLUBHOUSE LEFT SIDE ELEVATION**

PRELIMINARY NOT FOR  
CONSTRUCTION, RECORDING  
PURPOSES OR IMPLEMENTATION

**MATERIAL LEGEND**

1	SHINGLE SHAKE SIDING - JAMES HARDIE, CORN ESTONE
2	VERTICAL BOARD & BATTEN SIDING - JAMES HARDIE, MONTEREY TAUPÉ
3	HORIZONTAL SIDING - JAMES HARDIE, TIMBER BARK
4	ALL SIDING TRIM - JAMES HARDIE, TIMBER BARK
5	STONE VENEER - SEMCO STONE, MOUNTAIN RIDGE #M191
6	BLOCK VENEER
7	COLUMNS & ACCENT DETAILS - WOOD W/ DARK BROWN WAX OR STAIN
8	STANDING BEAM METAL CANOPY
9	SIGNAGE - MATTE FINISH METAL
10	ENERGY STAR RATED ALUMINUM WINDOWS/STOREFRONTS/EXTERIOR DOORS - MATTE DARK BROWN FINISH
11	RAILING - MATTE BLACK GALVANIZED STEEL
12	SEPARATING WALL - SEMCO STONE, CASTLEWOOD LEDGEROCK (18"-18")
13	FENCE - AMERISTAR ALUMINUM, MATTE BLACK
14	PARAPET CAP & FASCIA - ALUMINUM, MATTE BRONZE/BROWN
15	STONE VENEER - DARK BROWN
16	STONE SILL - SEMCO STONE, RUSTIC CEDAR SILL

**ELECTRICAL & SERVICE EQUIPMENT SCREENING & HIGHLIGHTS**

- WALL MOUNTED METERS TO HAVE BASE SCREENED BY LANDSCAPING/PANT TO MATCH EXTERIOR WALL MATERIAL ON MOUNTING WALL
- HVAC UNITS INSTALLED ON ROOF SHALL BE HIDDEN BEHIND PARAPET WALL - OUTLINE INDICATED IN ELEVATION



F.F.E. = 1,009 FT

1 AP14 SCALE: 3/32" = 1'-0"



F.F.E. = 1,009 FT

2 AP14 SCALE: 3/32" = 1'-0"



F.F.E. = 1,009 FT

F.F.E. = 1,009 FT

3 AP14 SCALE: 3/32" = 1'-0"



F.F.E. = 1,009 FT

F.F.E. = 1,009 FT

4 AP14 SCALE: 3/32" = 1'-0"



PRELIMINARY NOT FOR CONSTRUCTION, RECORDING PURPOSES OR IMPLEMENTATION

**MATERIAL LEGEND**

1	SHINGLE SHAKE SIDING - JAMES HARDIE CORN ESTONE
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4	ALL SIDING TRIM - JAMES HARDIE TIMBER BARK
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14	PARAPET CAP & FASCIA - ALUMINUM MATTE BRONZE/BROWN
15	STONE VENEER - DARK BROWN
16	STONE BILL - SEMCO STONE RUSTIC CEDAR BILL

**ELECTRICAL & SERVICE EQUIPMENT SCHEDULING & MATERIALS**

- WALL MOUNTED METERS TO HAVE BASE SCREENED BY LANDSCAPING/PANT TO MATCH EXTERIOR WALL MATERIAL ON MOUNTING WALL
- HVAC UNITS INSTALLED ON ROOF SHALL BE HIDDEN BEHIND PARAPET WALL - OUTLINE INDICATED IN ELEVATION



1 BUILDING TYPE "B" LEFT SIDE ELEVATION  
SCALE: 3/8" = 1'-0"



2 BUILDING TYPE "B" RIGHT SIDE ELEVATION  
SCALE: 3/8" = 1'-0"



3 BUILDING TYPE "B" FRONT ELEVATION  
SCALE: 3/8" = 1'-0"



4 BUILDING TYPE "B" REAR ELEVATION  
SCALE: 3/8" = 1'-0"

PRELIMINARY NOT FOR CONSTRUCTION, RECORDING PURPOSES OR IMPLEMENTATION

**MATERIAL LEGEND**

1	SHINGLE SHAKE SIDING - JAMES HARDIE, CORN ESTONE
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15	STONE VENEER - DARK BROWN
16	STONE SILL - SEMCO STONE, RUSTIC CEDAR SILL

**ELECTRICAL & SERVICE EQUIPMENT SCREENING MATERIALS**

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- HVAC UNITS INSTALLED ON ROOF SHALL BE HIDDEN BEHIND PARAPET WALL - OUTLINE INDICATED IN ELEVATION



1 AP16 SCALE: 3/32" = 1'-0"

**BUILDING TYPE "C" LEFT SIDE ELEVATION**



2 AP16 SCALE: 3/32" = 1'-0"

**BUILDING TYPE "C" RIGHT SIDE ELEVATION**



3 AP16 SCALE: 3/32" = 1'-0"

**BUILDING TYPE "C" FRONT ELEVATION**

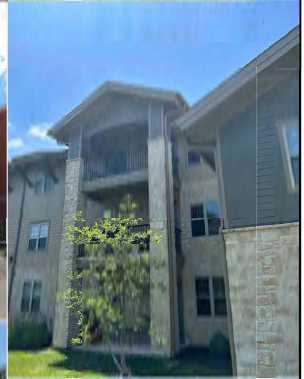
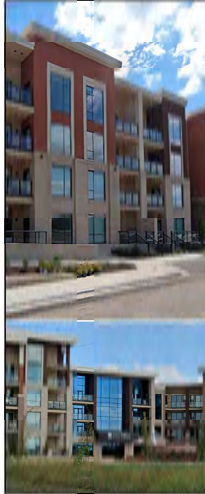
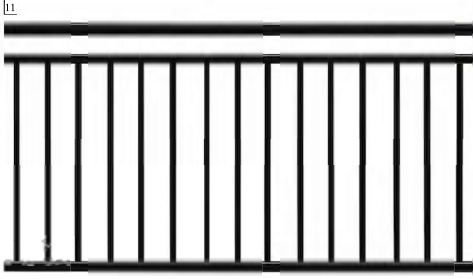
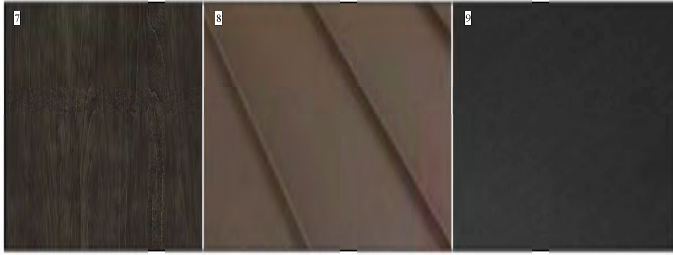


4 AP16 SCALE: 3/32" = 1'-0"

**BUILDING TYPE "C" REAR ELEVATION**

**MATERIAL LEGEND**

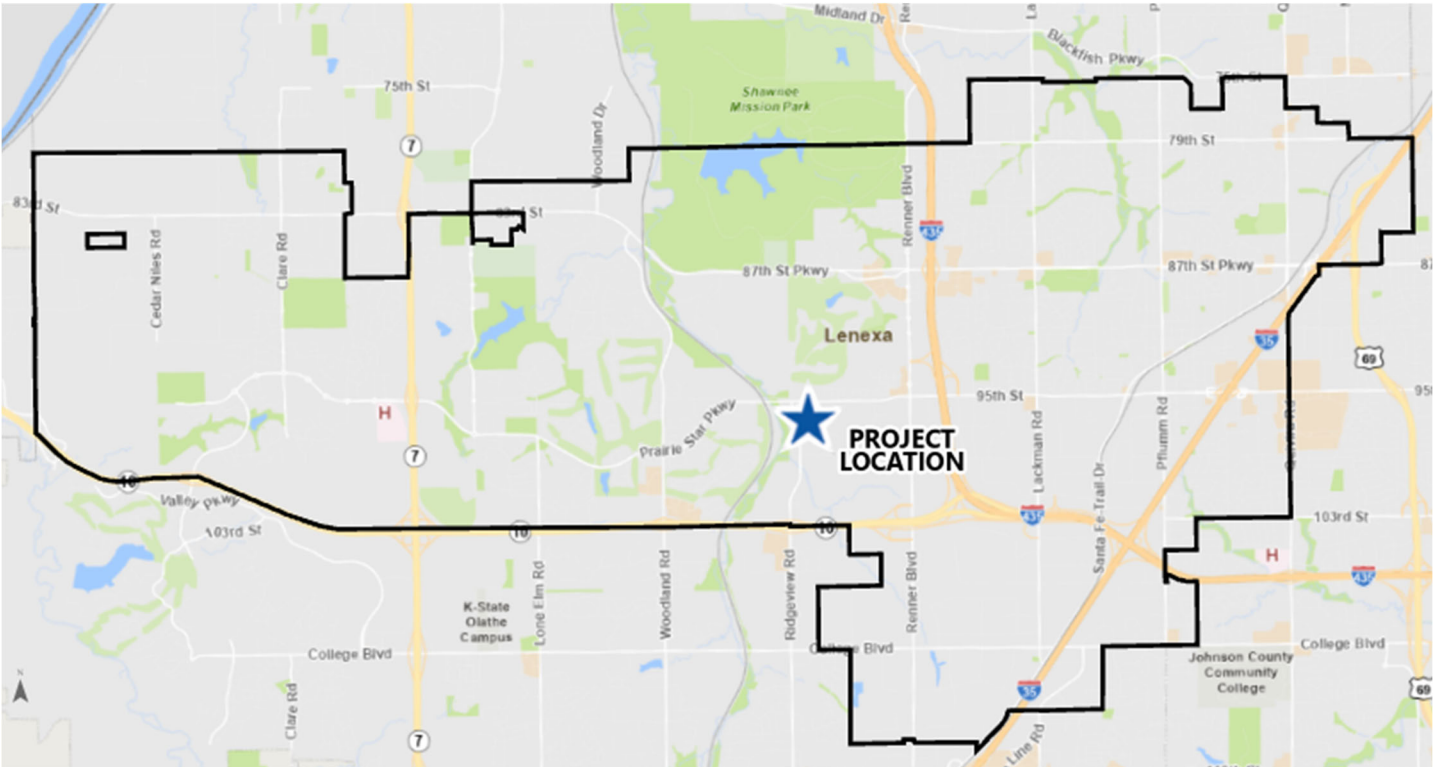
1	SHINBLE SHALE SIDING - JAMES HARDIE, CORBLESTONE
2	VERTICAL BOARD & BATTEN SIDING - JAMES HARDIE, MONTEREY TAUPE
5	HORIZONTAL SIDING - JAMES HARDIE, TIMBER BARK
4	ALL SIDING TRIM - JAMES HARDIE, TIMBER BARK
5	STONE VENEER - BEMOD STONE, MOUNTAIN RIDGE #V1R1
6	BLOCK VENEER
7	COLUMNS & ACCENT DETAILS - WOOD W/ DARK BROWN WAX OR STAIN
8	RETAINING BEAM METAL CANOPY
9	SIGNAGE - MATTE FINISH METAL
10	ENERGY STAR RATED ALUMINUM WINDOWS/STOREFRONTS/ENTRY DOORS - MATTE DARK BROWN FINISH
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15	STONE VENEER - DARK BROWN
16	STONE SILL - BEMOD STONE, RUSTIC CEDAR SILL



PRELIMINARY NOT FOR CONSTRUCTION, RECORDING PURPOSES OR IMPLEMENTATION

## KC BIER AT VISTA VILLAGE

<b>Project #:</b>	PL23-21F	<b>Location:</b>	Near the northeast corner of 98 <sup>th</sup> Street and Ridgeview Road
<b>Applicant:</b>	Chad Porter, Payne & Brockway	<b>Project Type:</b>	Final Plan
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	Restaurant with an accessory use for production for a microbrewery



### PROJECT SUMMARY

The applicant proposes to construct a new 14,800 SF building for a restaurant use, with an accessory microbrewery use, within the Vista Village development at the southeast corner of Prairie Star Parkway and Ridgeview Road. Vista Village is a Planned Unit Development (PUD) that includes retail, restaurants, multifamily apartments, and townhomes. The building is located at the southwesterly corner of the site.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

This site is an undeveloped lot in Vista Village, a Planned Unit Development (PUD) at the southeast corner of Prairie Star Parkway and Ridgeview Road. Vista Village was initially zoned PUD in 2015 (RZ15-06) with a preliminary plan (PL15-08P). The project has had several revised preliminary plans since the time of rezoning. The most recent was approved in August of 2022 (PL22-02PR). The subject site is the lot at the southwesterly corner of the development.

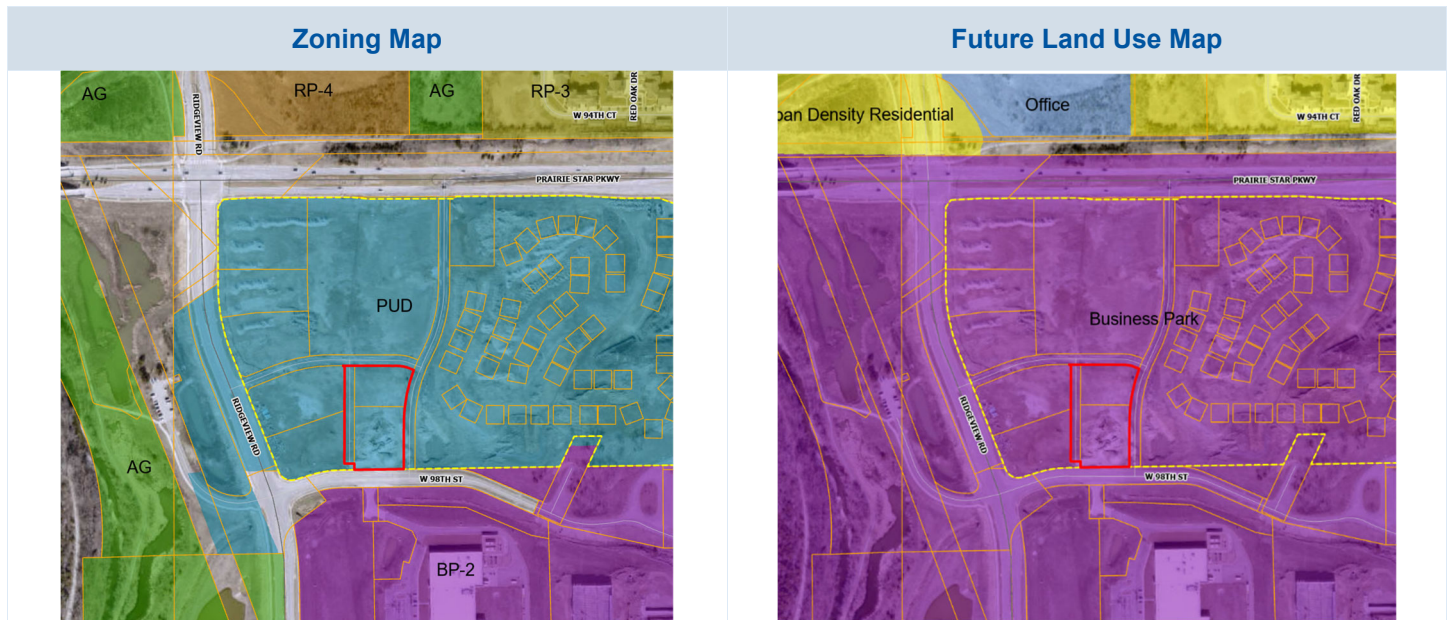
LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
1.38	14,800	PUD	Business Park



**Exhibit 1: Aerial Image of Subject Site outlined in red - Vista Village PUD outlined in yellow.**

## LAND USE REVIEW

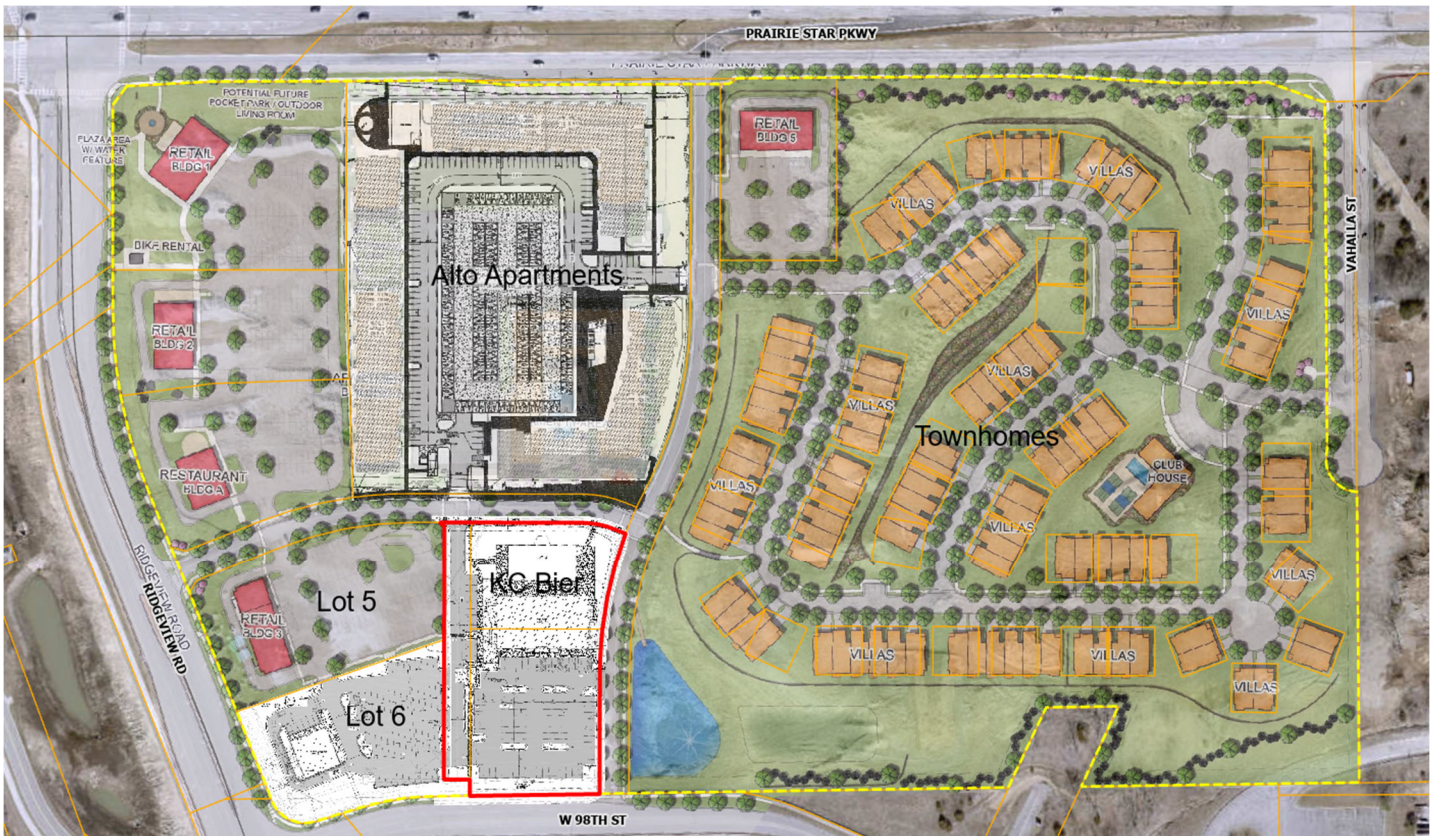
The site is within Vista Village, a Planned Unit Development (PUD) containing commercial and residential uses. The final plan is for a restaurant with an accessory microbrewery. The Future Land Use designation for this location is Community Commercial Center. Retail and restaurant uses are anticipated for this site. The proposed uses are consistent with the zoning designation. The Future Land Use Map of the Comprehensive Plan designates the site as Business Park.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Business Park	PUD	Undeveloped land
<b>North</b>	Office/Employment Center, Suburban Density Residential	RP-4, AG	Undeveloped land
<b>South</b>	Business Park	BP-2	Manufacturing and Undeveloped land
<b>East</b>	Office/Research & Development	CP-O	Public safety services and Undeveloped land
<b>West</b>	Business Park	AG	Public Park

The proposed use is compatible with the existing land uses and development on adjacent sites. The preliminary plan for Vista Village shows a series of buildings along the westerly part of the development that face out to Ridgeview Road. The buildings are expected to be retail buildings and restaurants. Vista Village is to include two primary amenity features, one at the immediate intersection of Prairie Star Parkway and Ridgeview Road and one is to be centrally located on the site at the intersection of the internal drives. These two features are on different lots. A condition of the latest preliminary plan requires that the amenity internal to the site shall be completed concurrently with any final plan. The KC Bier development will meet this condition by providing an outdoor space.



**Exhibit 2: Vista Village PUD Overall Plan**

The plan now shows an outdoor patio space and an integrated performance stage within the KC Bier site. The type of amenity is similar to the amphitheater and is an acceptable alternative, though there are a few key differences between the plans. The former plan presented more of an open space amenity for the development and much more integrated with the multifamily development to the north. The proposed space will be more internal to the restaurant and less integrated with the surroundings, but a visual connection will remain with the Alto Apartment plaza space and a direct pedestrian link is provided from the sidewalk into this space. The stage and outdoor area will also have regular programming with activities and events associated with the operation of KC Bier.



**Exhibit 3: Approved Plan**



**Proposed Plan**

## FINAL PLAN REVIEW

The site is in the westerly part of the Vista Village PUD. The site will front onto 98<sup>th</sup> Street to the south and onto Ambassador Drive, a private north-south street within the development. The site contains a two-story building, an outdoor patio and entertainment space, and parking area.

KC Bier is a restaurant use which will have an accessory microbrewery space. The microbrewery will produce beer that is consumed on site and packaged for distribution to other restaurants and retail stores. The restaurant is designed to replicate a traditional *German Bier Haus* keeping with the character of the KC Bier brand. The dining space is a large room with long tables where multiple groups sit.

The outdoor space replaces the amphitheater amenity concept from the preliminary plan. This space will offer the same, if not more, activity and community interaction than the amphitheater. The KC Bier business will provide programming and functions for patrons of the restaurant. This space is slightly lower in grade than the drives to the north and east of the site. The outdoor space will be a multipurpose and multifunctional space. A stage will be integrated into the space for live performances. A fenced dog park area is on the westerly side of the outdoor space and a children’s play area is on the easterly side.

The space will be landscaped around the perimeter. Landscaping will help mitigate noise generated from the activities in the space. [Section 4-1-C-4-E-2](#) of the Unified Development Code (UDC) establishes a maximum noise level that a use may generate during both daytime and evening hours. The UDC considers a PUD that contains residential uses to be subject to the residential tier of noise level requirement. The requirement is a maximum of 70dB(A) from 7 AM to 7 PM and 55dB(A) from 7 PM to 7 AM. This use, as any use in the City, is subject to this requirement. The applicant states that performances in the outdoor space will comply with the noise regulations.



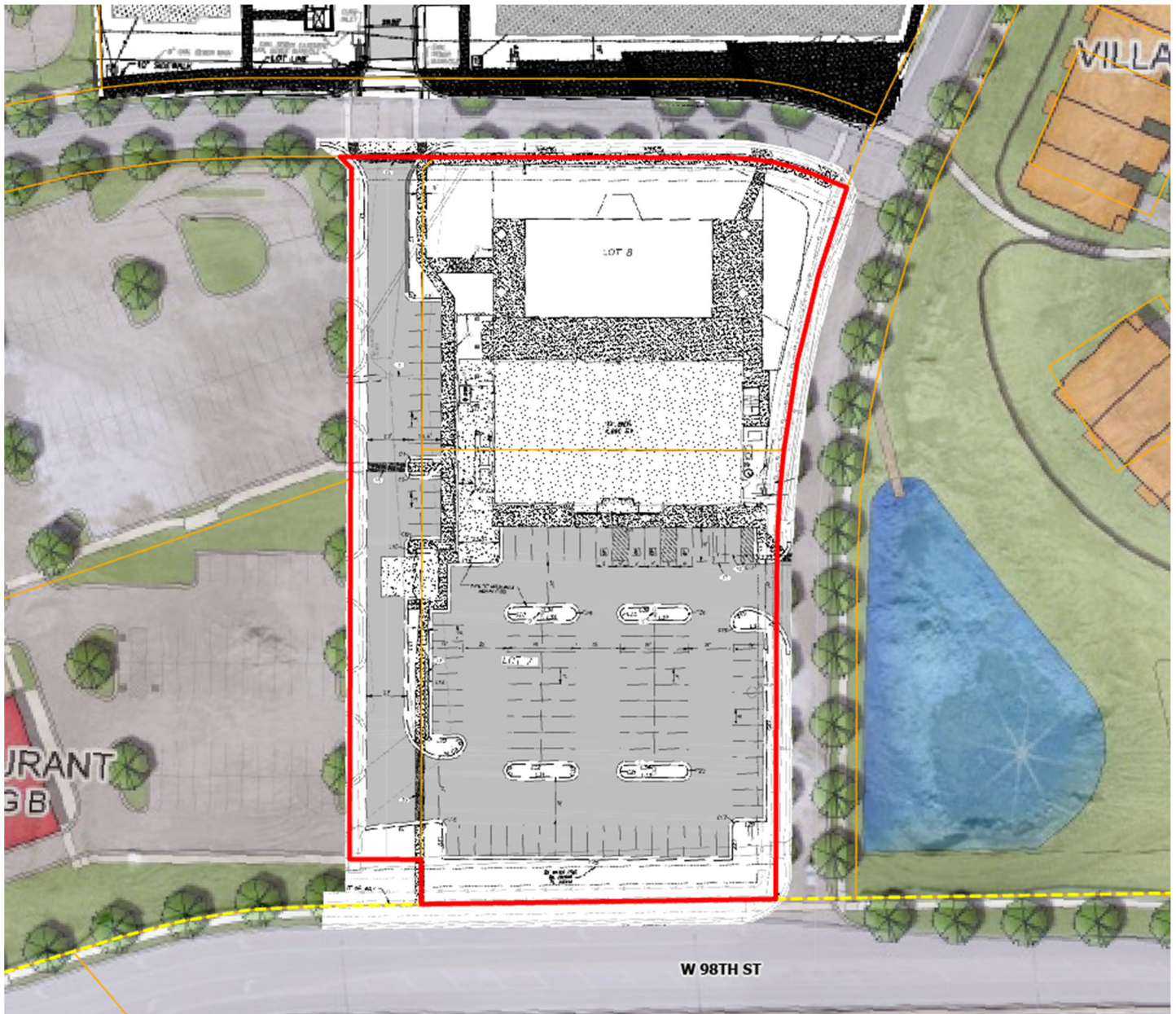


Exhibit 4: Site Plan.

## STORMWATER

This site is consistent with the overall Vista Village stormwater management design, which includes already constructed stormwater facilities on the west side of Ridgeview Road. Based on this consistency, the site will meet the City's stormwater standards and requirements.

## TRAFFIC, ACCESS, & PARKING

Access into the site is from private drives on the east and west sides of the site. The north-south private drive on the east side is now constructed and will provide access to the overall development. This drive is now named Ambassador Drive. The drive on the westerly side of the site will be constructed with this project, or the with the

Lot 6 project if it precedes this development. KC Bier will occasionally have large trucks enter the site to transport packaged beer to distribute to other retailers. The drive entrances are designed so a larger truck can circulate through the site without driving over a curb.

Per [Section 4-1-B-20-C-14](#) of the UDC, a PUD shall comply with the parking requirements for the designated use of the site, unless a deviation is granted for reduced parking is granted. A deviation was not specifically granted to allow fewer parking stalls than required, but shared parking among the lots was approved as part of the overall Vista Village development. The latest preliminary plan for Vista Village shows an excess of 198 parking stalls are provided for the retail lots along Ridgeview Road. Per the plan 1,088 stalls are required for the entire development and 1,282 stalls are provided. The KC Bier site is a restaurant use and is in close proximity to the other retail lots of the development where excess parking is provided. Staff concludes that a deviation for a reduced number of parking stalls is not necessary because sufficient parking is provided among the like uses among the lots of the development.

TABLE 2: PARKING ANALYSIS			
Land Use	Parking Formula	Parking Required	Parking Proposed
Restaurant	1 stall for every 75 square feet	197	101

The reduced number of parking stalls is acceptable because the overall Vista Village development provides more parking than required.

The plan does not show bicycle racks. The site will require at least two bicycle racks per the bicycle parking requirement in [Section 4-1-D-1-R](#) of the UDC.

## ARCHITECTURE

The building is designed to resemble a traditional *German Bier Haus* and use a materials palette consistent with the materials of this type of building. Those materials consist of stone, stucco (or plaster), and wood trim. The proposed building also uses metal siding in various applications throughout the building façade. The metal siding is to be of an architectural style with a geometric profile, patterning, and seams. The building is two stories with a pitched roof form.

The entrance of the building is on the south facing façade. The entrance is identified with a gable roof element and a stone colonnade with four archways. The entranceway is a glass storefront behind the arches. The ground level of the front façade uses stucco and stone pilasters. This façade contains an overhead door using wood-grain metal panels and a window to appear less conspicuous than a roll-up style door. The ground level continues to use stucco and stone as the primary field materials with introductions of metal panel materials installed with a horizontal orientation. The metal panels are applied to projected portions of the wall plane. The projected features use a shed-style roof with a wood shake-looking metal roofing material. The north façade will face out to the outdoor space. The façade has a row of wood and stone columns with an angular wooden bracket detail. The columns support a shed awning feature over a part of the outdoor space. A part of the north façade uses stone materials with two glass overhead doors that allow for a large passageway between the indoor and outdoor spaces.



**Exhibit 5: South façade.**

The second floor of the building uses an insulated metal panel system as the primary field material. The metal panel system is intended to replicate the pattern and rhythm of vertical boards. The second floor will include windows on the east and west side of the building with a second type of wood shake shingle material. The horizontal oriented metal panel is used on the second floor to provide interruptions of the insulated metal panel siding. Another wood grain metal panel system is used as a detail element at specific positions around the building and is used to delineate the ground floor from the second floor.



**Exhibit 6: North façade.**

The building is an eclectic design and uses several different types of building materials. Staff is supportive a distinctive building that emulates the character of a traditional *German Bier Haus*. Staff still has concerns regarding the composition of the building meets the intent of the Vista village Design Guidelines and the actual types of materials once installed. Staff is including a condition to the final plan regarding the continued review of the design and the selection of materials prior to approval of a building permit for the building.

The elevations show multiple signs on all four façades of the building. Vista Village has a sign criteria and the sign criteria is also subject to the requirements of the sign regulations in [Section 4-1-E](#) of the UDC. All signs will require a separate sign permit and signs which are not allowed by the UDC will require submittal of a sign deviation request.

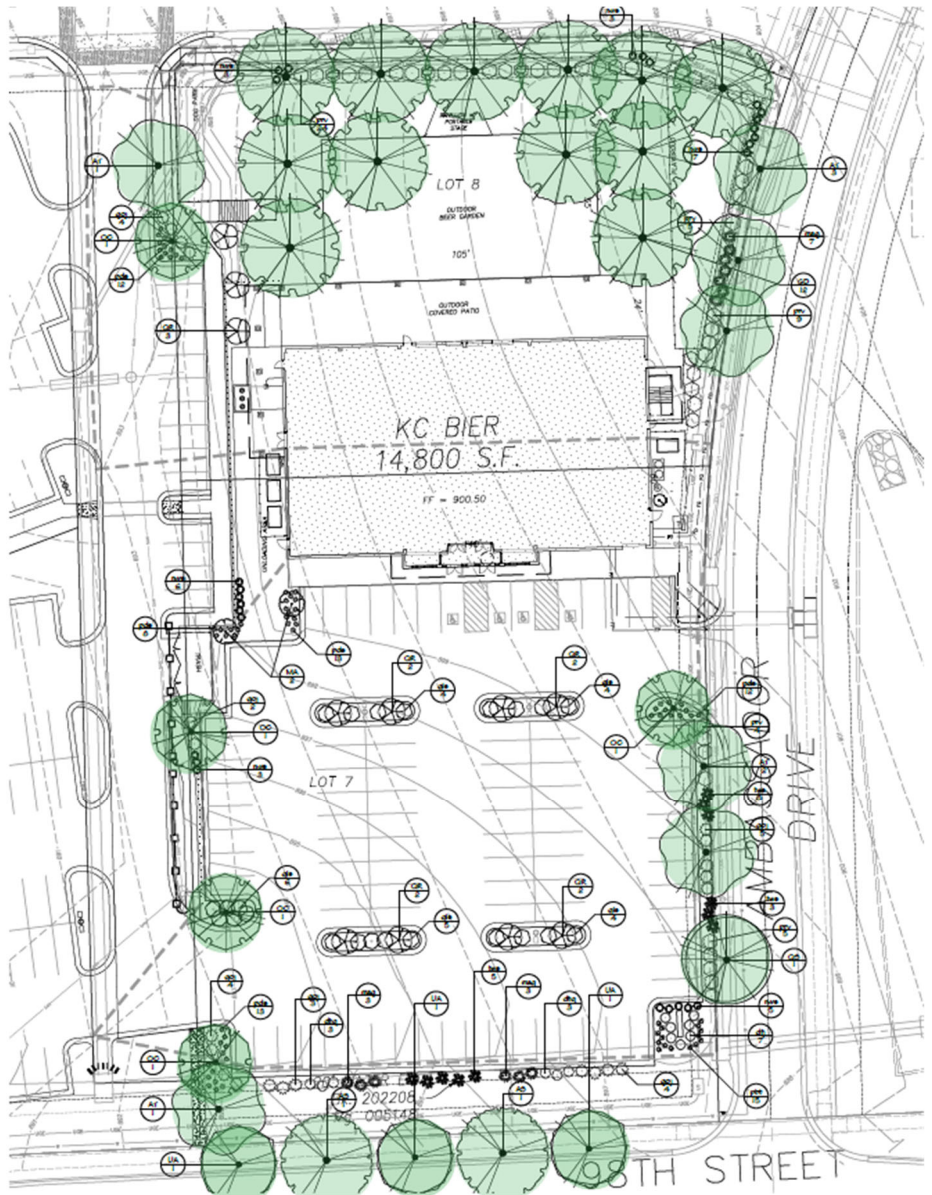
## LANDSCAPING

The proposed landscaping for the site is consistent with the landscape requirements for the Vista Village design guidelines and the landscape requirements per [Section 4-1-D-20-C-12](#) of the UDC. Perimeter landscaping will include trees and shrubs planted along 98<sup>th</sup> Street and Ridgeview Road. The plan shows trees and shrubs are planted in a linear pattern in areas along the internal drives and edges of the parking lot. This may be necessary in some areas because the space is relatively narrow for clustering the groups of plants. Other areas may have larger spaces to cluster the plant materials and group plantings in an offset pattern to create a more visually appealing landscape.

***At Right: Exhibit 7: Landscape plan.***

Landscaping is shown in the parking islands, except for one island which will be constructed to allow for large trucks to drive over the island. This surface of this island will use a contrasting material and a roll-over style curb. The trees shown along 98<sup>th</sup> Street are between the curb of the street and the sidewalk. 98<sup>th</sup> Street is a route where semi-trucks drive will frequently drive along to access the Meritex storage facility. The trees should be positioned so that they are not going to be impacted by the large vehicles that drive on this street. The site has adequate space to move the trees to other locations of the site and still meet the UDC requirements. Staff will continue to coordinate the design of the landscape plan with the applicant.

A monument sign is shown on the plan at the Ambassador Drive entrance onto 98<sup>th</sup> Street. A sign is permissible at this location, but the sign shall be for the Vista Village PUD and consistent with the sign criteria.



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## REVIEW PROCESS

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- *The Planning Commission is the final authority for approval of this project.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

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## RECOMMENDATION FROM PROFESSIONAL STAFF

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★ **Staff recommends approval of the proposed final plan for KC Bier.**

- This is a final plan for development of a restaurant and accessory microbrewery building.
- The project is consistent with Lenexa's goals as an ***Inviting Places*** and promotes a ***Thriving Economy***

### FINAL PLAN

Staff recommends **approval** of the final plan for PL23-21F – **KC Bier at Vista Village** located near the northeast corner of 98<sup>th</sup> Street and Ridgeview Road, for a restaurant and production for a microbrewery use with the following condition:

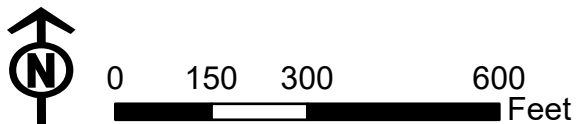
1. Prior to the issuance of a building permit, the building elevations and exterior finish materials are subject to additional review by Staff.



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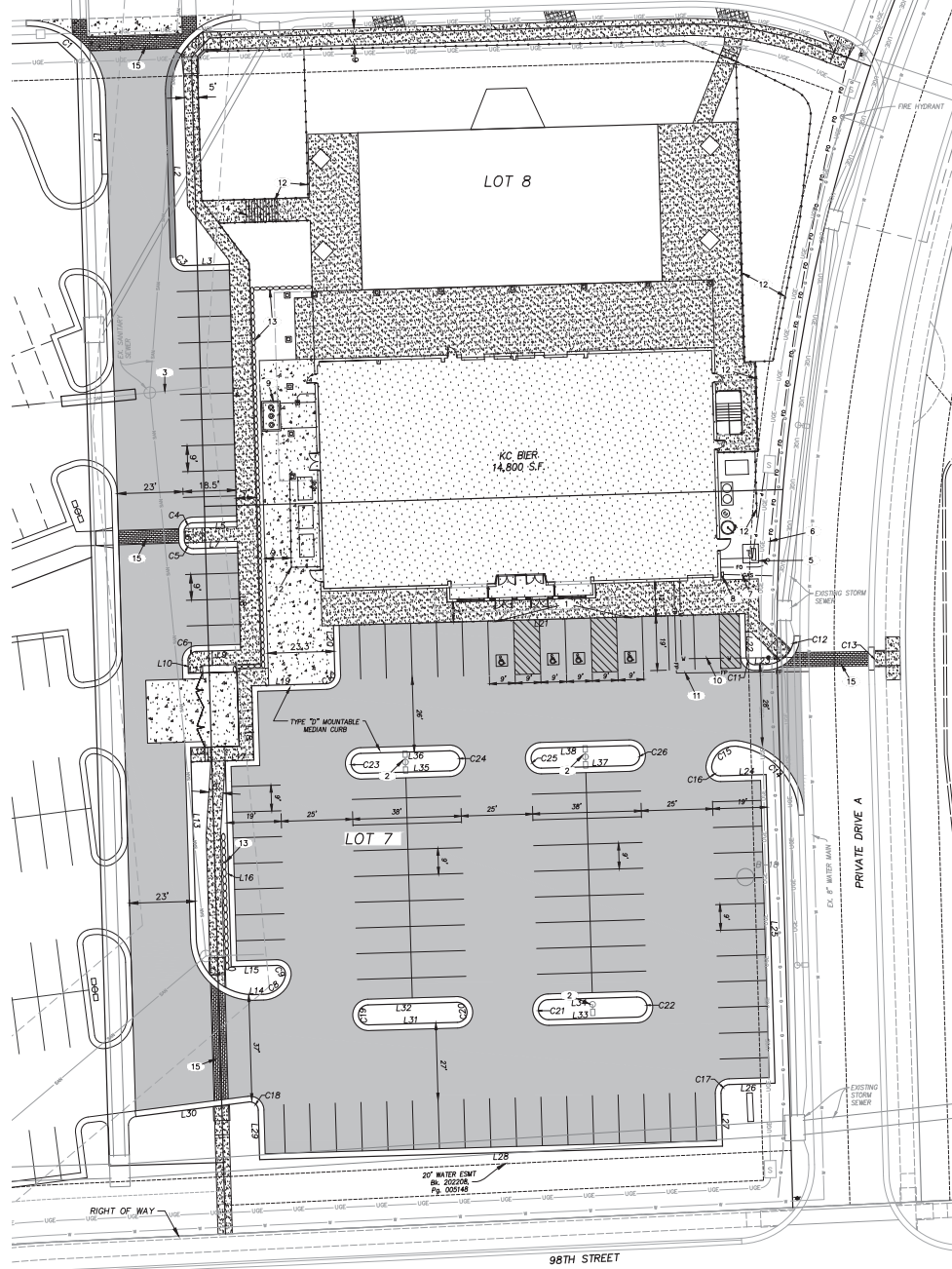
Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# KC Bier at Vista Village PL23-21F



LINE TABLE		
LINE #	LENGTH	DIRECTION
L1	27.58'	N1° 42' 06.09"W
L2	57.36'	S1° 42' 06.09"E
L3	15.50'	S88° 17' 53.91"W
L4	90.00'	S1° 42' 06.09"E
L5	15.50'	S88° 17' 53.91"W
L6	3.00'	S1° 42' 06.09"E
L7	15.50'	S88° 17' 53.91"W
L8	36.00'	S1° 42' 06.09"E
L9	15.50'	S88° 17' 53.91"W
L10	4.57'	S1° 42' 06.09"E
L11	5.00'	S88° 17' 53.91"W
L12	5.00'	S88° 17' 53.91"W
L13	67.01'	S1° 42' 06.09"E
L14	2.91'	S88° 20' 14.89"W
L15	15.90'	S88° 20' 14.89"W
L16	72.00'	S1° 39' 45.11"E
L17	10.02'	S88° 20' 14.89"W
L18	25.99'	S1° 39' 45.11"E
L19	26.20'	N88° 20' 14.89"E
L20	17.26'	S1° 39' 45.11"E
L21	144.00'	S88° 20' 14.89"W
L22	15.00'	S1° 39' 45.11"E
L23	3.99'	N88° 20' 14.89"E
L24	15.94'	N88° 20' 14.89"E
L25	108.00'	S1° 39' 45.11"E
L26	16.00'	N88° 20' 14.89"E
L27	18.50'	S1° 39' 45.11"E
L28	162.00'	N88° 20' 14.89"E
L29	17.28'	S1° 39' 45.11"E
L30	39.73'	S82° 37' 18.40"W
L31	31.00'	N88° 20' 14.89"E
L32	31.00'	N88° 20' 14.89"E
L33	31.00'	N88° 20' 14.89"E
L34	31.00'	N88° 20' 14.89"E
L35	31.00'	N88° 20' 14.89"E
L36	31.00'	N88° 20' 14.89"E
L37	31.00'	N88° 20' 14.89"E
L38	31.00'	N88° 20' 14.89"E

CURVE TABLE		
CURVE #	LENGTH	RADIUS
C1	39.25'	25.00'
C2	39.29'	25.00'
C3	4.71'	3.00'
C4	4.71'	3.00'
C5	4.71'	3.00'
C6	4.71'	3.00'
C7	31.40'	20.00'
C8	10.96'	8.00'
C9	5.32'	3.00'
C10	4.71'	3.00'
C11	4.71'	3.00'
C12	15.42'	10.00'
C13	8.00'	586.00'
C14	34.77'	25.00'
C15	12.76'	8.00'
C16	5.18'	3.00'
C17	4.71'	3.00'
C18	5.01'	3.00'
C19	11.00'	3.50'
C20	11.00'	3.50'
C21	11.00'	3.50'
C22	11.00'	3.50'
C23	11.00'	3.50'
C24	11.00'	3.50'
C25	11.00'	3.50'
C26	11.00'	3.50'



**LEGAL DESCRIPTION:**  
 ALL OF LOTS 7 AND 8, VISTA VILLAGE, A SUBDIVISION OF LAND IN  
 LENEXA, JOHNSON COUNTY, KANSAS.

**DEVELOPMENT NOTES:**

1. SIDEWALKS SHALL BE CONCRETE.
2. CROSSWALKS SHALL BE PAINTED WITH A LADDER TYPE PATTERN.
3. PRIVATE DRIVES SHOWN HEREON WILL PROVIDE SHARED ACCESS FOR EACH LOT WITHIN THE DEVELOPMENT.
4. PAVEMENT MARKINGS SHALL BE PAINTED IN ACCORDANCE WITH THE ADA PARKING SPACE DETAIL.

**CONSTRUCTION NOTES:**

1. ADA PARKING SIGN.
2. PARKING LOT LIGHTS. SEE LIGHTING PLAN.
3. SANITARY SEWER SERVICE LINE.
4. ROOF DRAIN DOWNSPOUT.
5. ENERGY TRANSFORMER. SEE UTILITY PLAN.
6. 4" CONDUIT FOR TELEPHONE SERVICE. SEE UTILITY PLAN.
7. GAS SERVICE. SEE UTILITY PLAN.
8. GAS METER. SEE UTILITY PLAN.
9. GREASE INTERCEPTOR.
10. WATER SERVICE. SEE UTILITY PLAN.
11. FIRE PROTECTION LINE. SEE UTILITY PLAN.
12. 6" BLACK ALUMINUM PICKET FENCE.
13. LIMESTONE BLOCK RETAINING WALL.
14. CONCRETE STAIRS.
15. CROSSWALK PER DETAIL.

**LEGEND**

- [Hatched Box] DENOTES PROPOSED BUILDING
- [Dotted Box] DENOTES PROPOSED 8" REINFORCED CONCRETE
- [Cross-hatched Box] DENOTES PROPOSED 6" ASPHALT PAVEMENT
- [Horizontal Lines Box] DENOTES PROPOSED CONCRETE SIDEWALK
- [Vertical Lines Box] DENOTES PROPOSED TYPE B CURB
- [Diagonal Lines Box] DENOTES PROPOSED TYPE B CURB (DRY)

PARKING CALCULATIONS				
USE	RATIO	SQUARE FOOTAGE	REQUIRED	PROVIDED
RESTAURANT	1 PER 75 S.F.	14,800	197	101
<b>PARKING TOTALS</b>			<b>197</b>	<b>101</b>

**PARKING NOTES:**

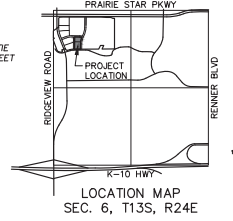
1. ALL REGULAR PARKING SPACES SHOWN HEREON ARE 9'x19'.
2. THE ANGLE OF ALL PARKING SPACES SHOWN HEREON IS 90°.

SITE DEVELOPMENT DATA	
SITE AREA	1.38 AC. +/-
TOTAL BUILDING AREA	14,800 S.F.
TOTAL LOT COVERAGE	43,241 S.F. (72%)
OPEN SPACE	16,775 S.F. (28%)
ZONING	PUD

**VICINITY ZONING PATTERN:**

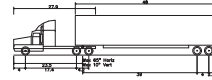
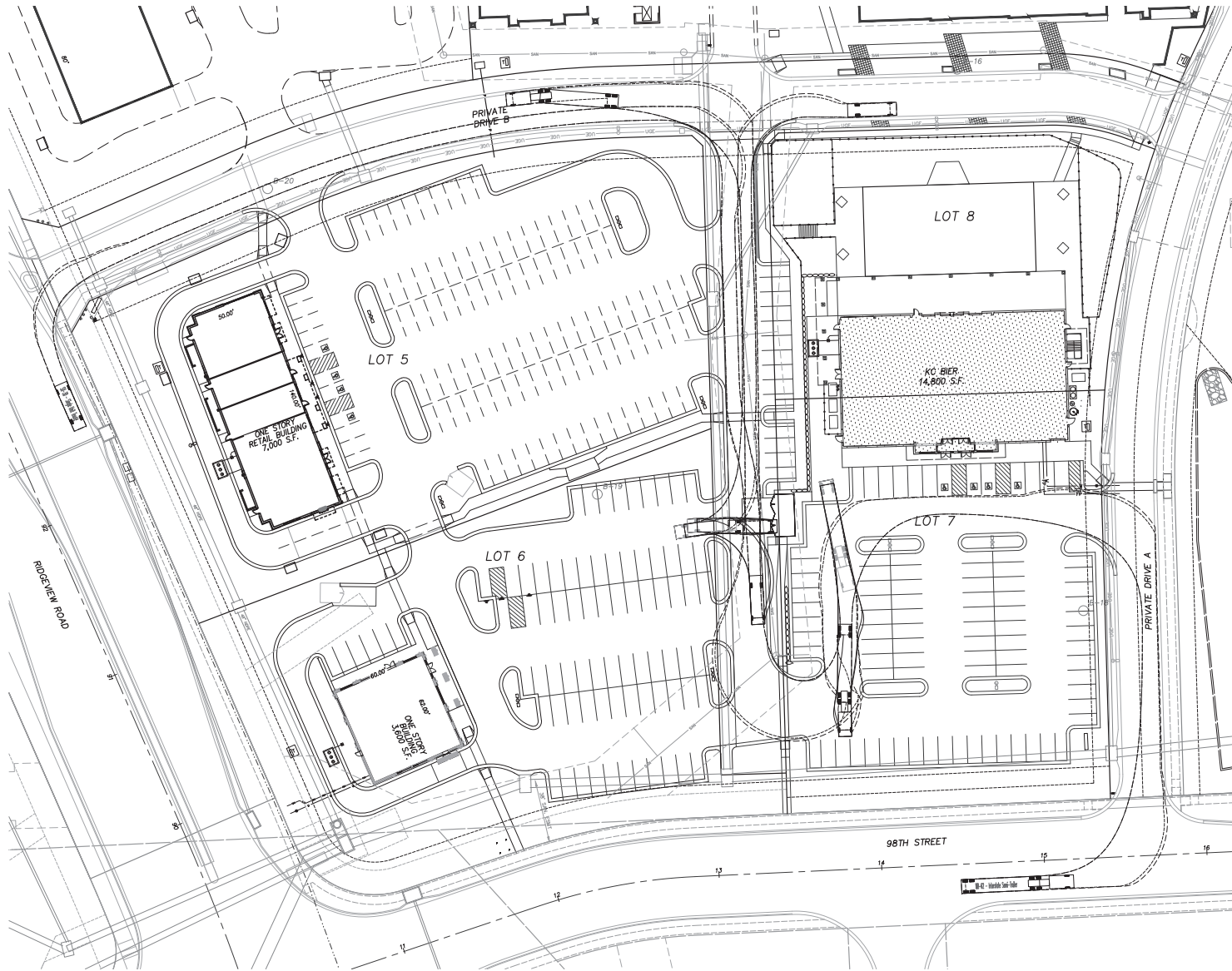
NORTH: AG, AGRICULTURAL, NPO, PLANNED NEIGHBORHOOD OFFICE & RP-3, PLANNED RESIDENTIAL MEDIUM HIGH DENSITY ZONING DISTRICTS  
 SOUTH: BP-2, PLANNED MANUFACTURING ZONING DISTRICT  
 EAST: AG, AGRICULTURAL ZONING DISTRICT  
 WEST: AG, AGRICULTURAL ZONING DISTRICT (PARK)

**OWNERS NAME:**  
 VISTA #7, LLC  
 c/o MICHAEL A. CHRISTIE  
 10550 S. WARWICK STREET  
 OLATHE, KS 66061



NO.	DATE	DESCRIPTION	BY	
			DESIGNER	CHKD
1	10-23-23	PER CITY COMMENTS	PJS	
2				
3				
4				
5				
6				
7				

PLOT DATE: 10/23/2023  
 18-0041 KC BIER SITE.dwg - C102



MB-62 - Interstate Semi-Trailer  
 Overall Length: 65.000ft  
 Overall Body Height: 13.500ft  
 Min. Body Height Clearance: 10.000ft  
 Max. Trailer Width: 8.500ft  
 Lock-to-Lock Time: 28.40s  
 Max. Steering Angle (Virtual): 28.40°



SU-30 - Single Unit Truck  
 Overall Length: 30.000ft  
 Overall Body Height: 10.000ft  
 Min. Body Height Clearance: 7.500ft  
 Lock-to-Lock Time: 10.00s  
 Max. Steering Angle (Virtual): 30.00°



1" = 30'  
 0 15 30 60

18-0041

**Payne & Brockway P.A.**

CIVIL ENGINEERS & LAND SURVEYORS  
 430 W. 11th St., Suite 100  
 Lenexa, KS 66150  
 Phone: 785.261.0601  
 Fax: 785.261.0601  
 www.Payne-Brockway.com



VISTA VILLAGE  
 LENEXA, KANSAS

TRUCK MANEUVER PLAN

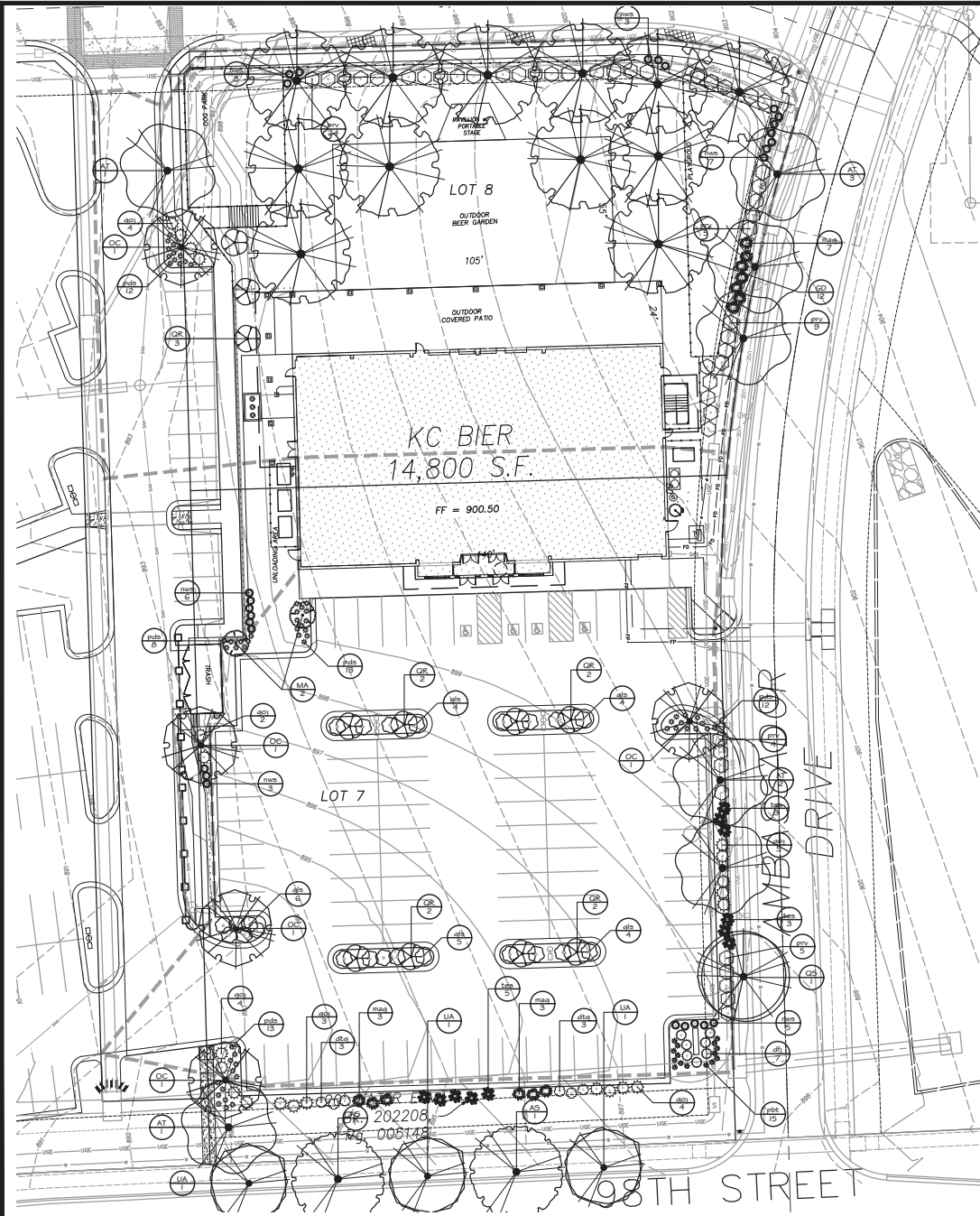
NO.	DATE	DESCRIPTION	BY	
			DESIGNER	PLT
1	10-23-23	PER CITY COMMENTS		
2				
3				
4				
5				
6				
7				

10-2-23

C102



PLOT DATE: 10/22/2023  
 PLOT NO: 18-0041 KC BIER SITE STD LSP REV: 02.23.24.dwg - L100



**PLANT SCHEDULE**

ORNAMENTAL TREE	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	MA	2	Malus x 'Adirondack'	Adirondack Crabapple	6' Ht.	B&B / Cont.
	OR	11	Quercus robur x bicolor 'Nadler'	Kindred Spirit Oak	6' Ht.	B&B / Cont.
SHADE TREE						
CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	
	AS	2	Acer saccharum 'Autumn Splendor'	Autumn Splendor Caddo Sugar Maple	2" Cal.	B&B / Cont.
	AT	7	Acer truncatum 'Pacific Sunset' TM	Pacific Sunset Maple	2" Cal.	B&B / Cont.
	GD	12	Gymnocladus dioica 'Espresso'	Espresso Coffeetree	2" Cal.	B&B / Cont.
	OC	5	Ostrya carpinifolia	European Hophornbeam	2" Cal.	B&B / Cont.
	QS	1	Quercus shumardi	Shumard Red Oak	2" Cal.	B&B / Cont.
	UA	3	Ulmus americana 'Jefferson'	Jefferson American Elm	2" Cal.	B&B / Cont.
SHRUBS						
CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	
	dtq	6	Chaenomeles x 'Double Take' TM	Double Take Quince	18" Ht.	Pot
	dfj	7	Juniperus chinensis 'Daub's Frosted'	Daub's Frosted Juniper	18" Ht.	Pot
	gri	22	Juniperus virginiana 'Grey Owl'	Grey Owl Juniper	18" Ht.	Pot
	gls	23	Rhus aromatica 'Gro-Low'	Gro-Low Sumac	18" Ht.	Pot
	tes	11	Rhus typhina 'Tiger Eyes'	Tiger Eyes Sumac	18" Ht.	Pot
	prv	47	Viburnum x pragensis	Prague Viburnum	18" Ht.	Pot
PERENNIALS						
CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	
	pbt	15	Penstemon digitalis 'Pocahontas'	Pocahontas Beardtongue	18" Ht.	Pot
GRASSES						
CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	
	mag	13	Miscanthus sinensis 'Gracillimus'	Maiden Grass	18" Ht.	Pot
	nws	27	Panicum virgatum 'North Wind'	Northwind Switch Grass	18" Ht.	Pot
	pds	58	Sporobolus heterolepis	Prairie Dropseed	18" Ht.	Pot

**LANDSCAPE CALCULATIONS:**

PER SECTION 4-1-D-2 OF THE LENEXA UDC. THE FOLLOWING IS REQUIRED:

**SECTION 4-1-D-2-J MINIMUM STREET TREE REQUIREMENTS:**

- STREET TREES - ONE TREE PER 40' OF PUBLIC OR PRIVATE STREET FRONTAGE
- North - 97th St - 233.49 LF / 40 = 6 Trees Req'd. - 6 Tree Provided
  - East - Ambassador Dr - 402.83 LF / 40 = 11 Trees Req'd. - 11 Tree Provided
  - South - 98th St - 198.69 LF / 40 = 5 Trees Req'd. - 5 Tree Provided
  - West - Internal drive - 411.73 LF / 40 = 11 Trees Req'd. - 9 Tree Provided

**SECTION 4-1-D-2-L MINIMUM PERIMETER LANDSCAPE REQUIREMENTS:**

- ALONG STREET FRONTAGE 2 SHADE TREES AND 12 SHRUBS REQ. PER 100 LF
- North - 97th St - 233.49 LF = 5 Trees + 28 Shrubs Req'd. - 6 Trees + 30 Shrubs Provided
  - East - Ambassador Dr - 402.83 LF = 9 Trees + 49 Shrubs Req'd. - 11 Trees + 52 Shrubs Provided
  - South - 98th St - 198.69 LF = 4 Trees + 24 Shrubs Req'd. - 5 Trees + 24 Shrubs Provided
  - West - Internal drive - 411.73 LF = 9 Trees + 50 Shrubs Req'd. - 9 Tree + 21 Shrubs Provided

**SECTION 4-1-D-2-M INTERNAL PARKING LOT LANDSCAPE AREAS:**

- A MINIMUM OF 5% OF TOTAL PARKING LOT AREA TO BE LANDSCAPE ISLANDS.
- ONE TREE PER ISLAND / PROVIDED

1 LANDSCAPE PLAN  
 1" = 20'



**Chad D. Weinand, PL.A, ASLA**  
 Landscape Architect  
 15173 W. 157th Terrace, Olathe, Kansas 66062  
 913.484.9738 cweinand74@gmail.com  
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18-0041

**Payne & Brockway P.A.**  
 Civil Engineers & Land Surveyors  
 426 N. Broadway, Suite 200  
 Lenexa, KS 66150  
 Ph: 913.282.4800 Fax: 913.282.0907  
 www.Payne-Brockway.com



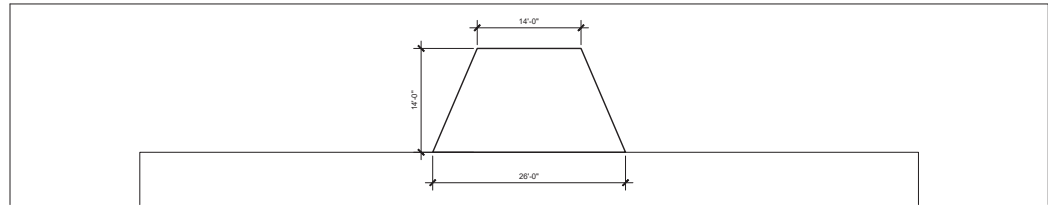
**VISTA VILLAGE LOTS 7 & 8**  
 LENEXA, KANSAS  
 LANDSCAPE PLAN

REVISIONS		BY	DATE	DESCRIPTION
1	10-23-23	CSW		PER CITY COMMENTS
2				
3				
4				
5				
6				
7				

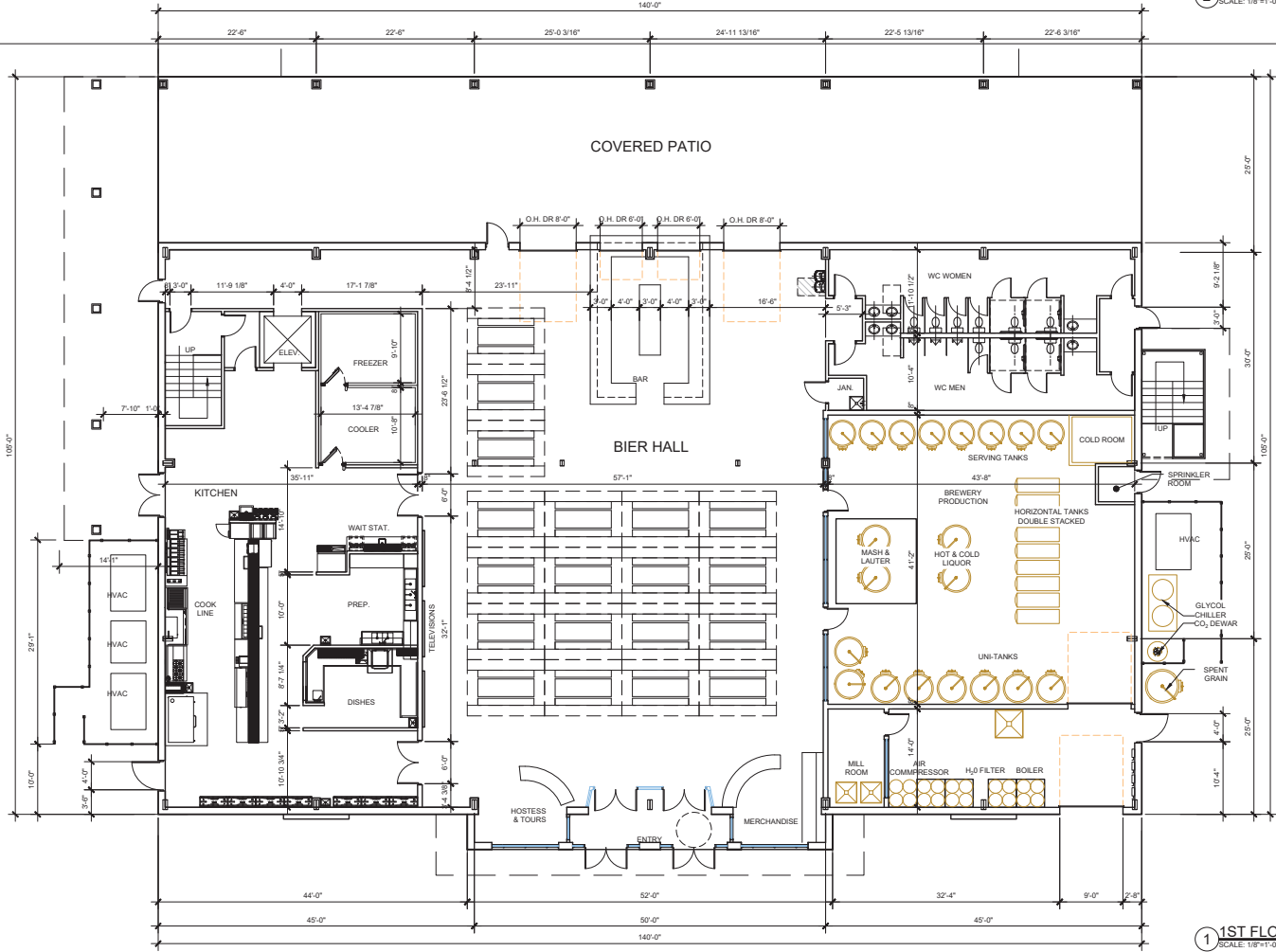


10-2-23

**L100**



2 STAGE FLOOR PLAN  
SCALE: 1/8"=1'-0"



1 1ST FLOOR PLAN  
SCALE: 1/8"=1'-0"



NOLTE & ASSOCIATES P.A.  
ARCHITECTS / PLANNERS  
1505 WESTER PARK, SUITE 200  
OVERLAND PARK, KANSAS 66207  
TEL: (913) 222-2444  
FAX: (913) 222-0277



NEW HALL AND OUTDOOR THEATER FOR:  
**KC BIER**  
at VISTA VILLAGE

DATE PRINTED
DATE ISSUED
REVISIONS

ARCHITECTURAL PROJECT NUMBER  
2022033

1ST FLOOR  
PLAN  
AREA

SHEET NUMBER  
**A-101**

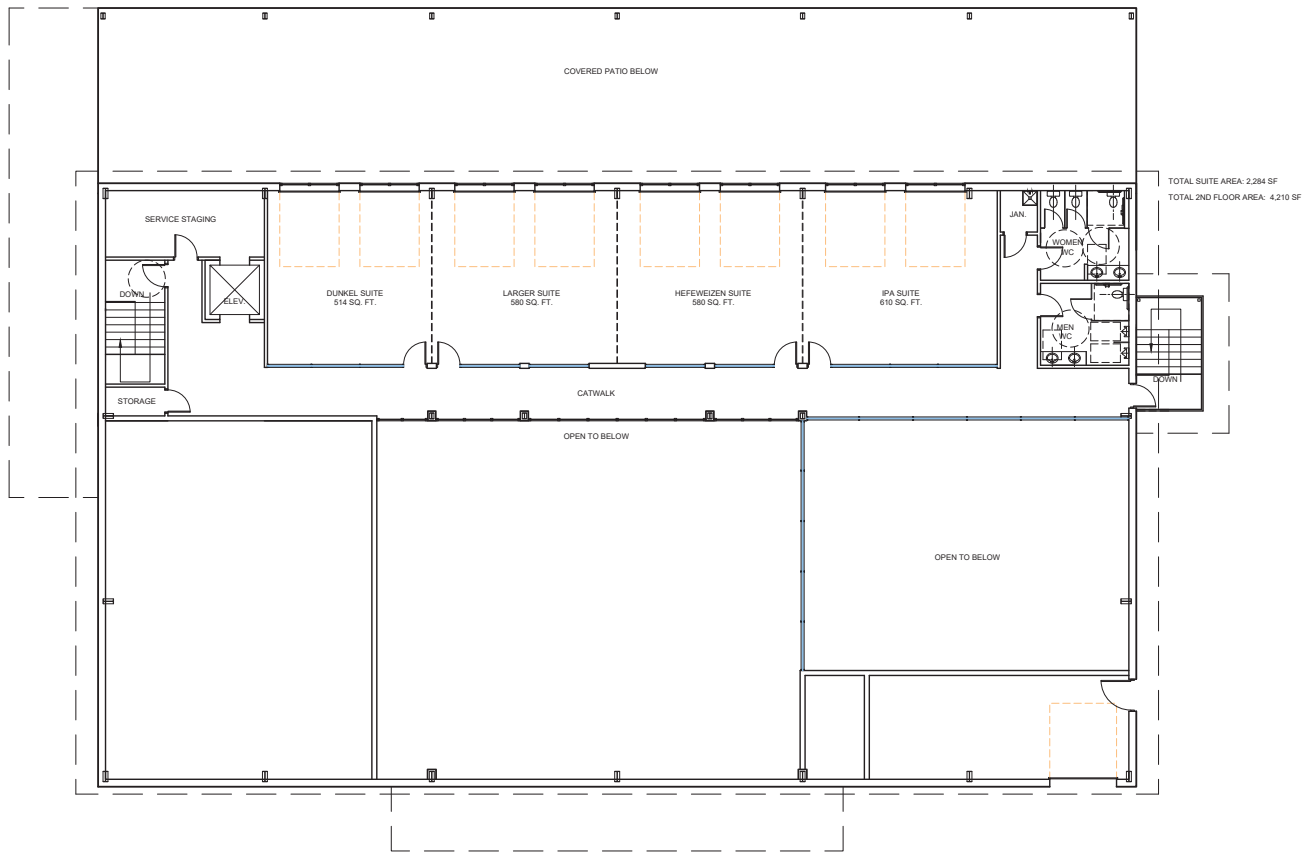
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4 TRASH ENCLOSURE  
EAST ELEVATION  
SCALE: 1/4"=1'-0"

3 TRASH ENCLOSURE  
WEST ELEVATION  
SCALE: 1/4"=1'-0"

2 TRASH ENCLOSURE  
NORTH/SOUTH ELEVATION  
SCALE: 1/4"=1'-0"



1 2ND FLOOR PLAN  
SCALE: 1/8"=1'-0"



NOLTE & ASSOCIATES P.A.  
ARCHITECTS / PLANNERS  
1505 WESTERS BLVD., SUITE 200  
OVERLAND PARK, KANSAS 66207  
TEL: (913) 222-2444  
FAX: (913) 222-0277



NEW HALL AND OUTDOOR THEATER FOR:  
**KC BIER**  
at VISTA VILLAGE

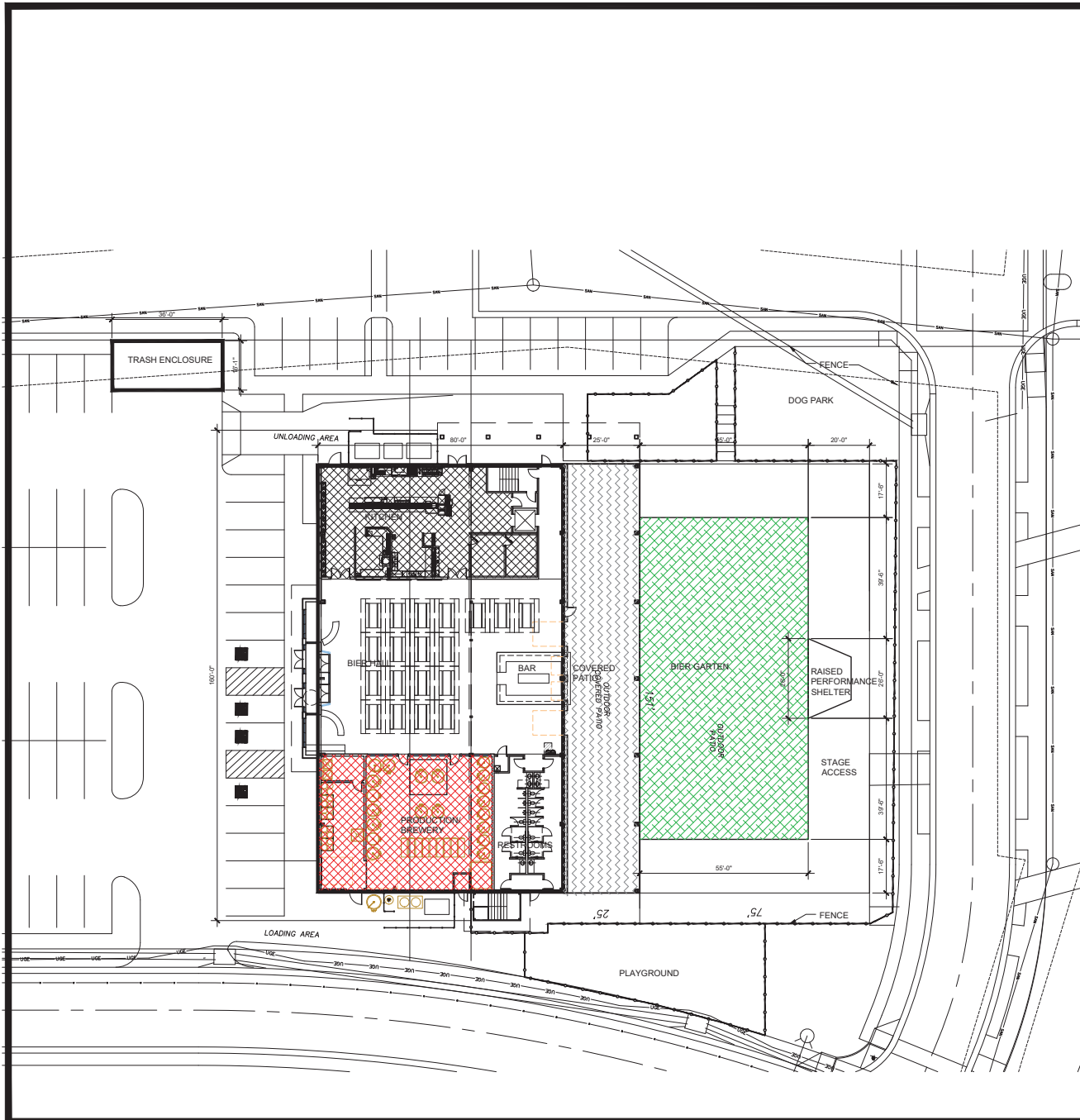
DATE PRINTED
DATE ISSUED
REVISIONS

ARCHITECTURAL PROJECT NUMBER  
2022033

2ND FLOOR  
PLAN  
AREA

SHEET NUMBER  
**A-102**

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OCCUPANCIES			
A-2 OCCUPANCY	15 SQ FT PER PERSON		
LOCATION	SQUARE FOOTAGE	OCCUPANCY	ROUND UP OCCUPANCY
BIER HALL	4,151	276.733333	277
COVER PATIO	3,512	234.133333	235
BIER GARDEN	5,775	385.000000	385
2ND FLOOR PARTY RM	2,484	165.600000	166
<b>SUBTOTAL</b>			<b>1063</b>
<b>ASSEMBLY</b>			
FABRICATION & MANUFACTURING OCCUPANCY 200 SQ FT PER PERSON			
LOCATION	SQUARE FOOTAGE	OCCUPANCY	OCCUPANCY
KITCHEN	2,352	11.760000	12
PRODUCTION	2,502	12.510000	13
<b>SUBTOTAL FABRICATION/MANUFACTURING</b>			<b>25</b>
<b>TOTAL OCCUPANCY</b>			<b>1088</b>

J) PLUMBING FIXTURE CALCULATIONS  
(ISC TABLE 2002.1)

A-2 ASSEMBLY			
WATER CLOSETS			
WATER CLOSETS REQUIRED	ROUND UP TO NEAREST WHOLE NUMBER		WATER CLOSETS PROVIDED
MEN: [1088 X 0.5+544] / 75 = 7.25	8	MEN:	4 + 4 URINALS
WOMEN: [1088 X 0.5+544] / 75 = 7.25	8	WOMEN:	9
LAVATORIES			
LAVATORIES REQUIRED	ROUND UP TO NEAREST WHOLE NUMBER		LAVATORIES PROVIDED
MEN: [1088 X 0.5+544] / 200 = 2.72	3	MEN:	5
WOMEN: [1088 X 0.5+544] / 200 = 2.72	3	WOMEN:	5
DRINKING FOUNTAINS			
DRINKING FOUNTAINS REQUIRED	ROUND UP TO NEAREST WHOLE NUMBER		DRINKING FOUNTAINS PROVIDED
1088/500 = 2.176	3		2*
SERVICE SINK REQUIRED			
SERVICE SINK REQUIRED	REQUIRED		SERVICE SINK PROVIDED
1 SERVICE SINK REQUIRED	1		1

\*2 DRINKING FOUNTAINS PROVIDED. LOCATION WILL HAVE FREE WATER UPON REQUEST IN AREAS WHERE THE DRINKING FOUNTAINS ARENT PRESENT.

**ARCHITECTURAL SITE PLAN**  
1 SCALE: 1/16"=1'-0"

NOLTE & ASSOCIATES P.A.  
ARCHITECTS / PLANNERS  
1800 WESTERS BLVD., SUITE 200  
OVERLAND PARK, KANSAS 66207  
TEL: (913) 222-2444  
FAX: (913) 222-0277



NEW HALL AND OUTDOOR THEATER FOR:

**KC BIER**  
at VISTA VILLAGE

DATE PRINTED	
DATE ISSUED	
REVISION	
▲	
▲	
▲	
▲	

ARCHITECTURAL PROJECT NUMBER  
**2022033**

**PRELIMINARY SITE PLAN**

SHEET NUMBER  
**AS101**



METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN

STANDING SEAM METAL ROOF  
COLOR: TERRACOTTA

EDCO STEEL SIDING  
ARROWLINE SHAKE  
SIDEWALL  
COLOR: CLASSIC RED

INSULATED METAL PANEL  
COLOR: SURREY BEIGE

STANDING SEAM METAL ROOF  
COLOR: TERRACOTTA

METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN

STANDING SEAM METAL ROOF  
COLOR: TERRACOTTA

CONORADO STONE OSAGE  
COLOR: CAPE  
CODE GREY

INSULATED METAL PANELS  
STUCCO FINISH  
COLOR: LIGHT STONE

METAL 7.2 RIBBED PERFORATED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE

EDCO STEEL SIDING  
ARROWLINE SHADE SIDEWALL  
COLOR: CEDAR

METAL 7.2 RIBBED PERFORATED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE

EDCO STEEL SIDING  
ARROWLINE SHADE SIDEWALL  
COLOR: CEDAR

CONORADO STONE OSAGE  
COLOR: CAPE  
CODE GREY

CONTINENTAL CAST STONE  
COLOR: 1101

**2** EAST ELEVATION  
SCALE: 1/8"=1'-0"



STANDING SEAM METAL ROOF  
COLOR: TERRACOTTA

INSULATED METAL PANEL  
COLOR: SURREY BEIGE

METAL 7.2 RIBBED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE

METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN

EDCO STEEL SIDING  
ARROWLINE SHADE SIDEWALL  
COLOR: CEDAR

METAL 7.2 RIBBED PERFORATED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE

CONORADO STONE OSAGE  
COLOR: CAPE  
CODE GREY

INSULATED METAL PANELS  
TUFF WALL STUCCO FINISH  
COLOR: LIGHT STONE

METAL 7.2 RIBBED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE

METAL 7.2 RIBBED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE

CONORADO STONE OSAGE  
COLOR: CAPE  
CODE GREY

CONTINENTAL CAST STONE  
COLOR: 1101

INSULATED METAL PANELS  
TUFF WALL STUCCO FINISH  
COLOR: LIGHT STONE

CONORADO STONE OSAGE  
COLOR: CAPE  
CODE GREY

**1** NORTH ELEVATION  
SCALE: 1/8"=1'-0"

NEW HALL AND OUTDOOR THEATER FOR:

**KC BIER**  
at VISTA VILLAGE

NOLTE & ASSOCIATES P.A.  
ARCHITECTS / PLANNERS  
1500 WESTERS BLVD. SUITE 200  
OVERLAND PARK, KANSAS 66207  
TEL: (913) 232-2444  
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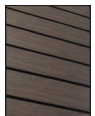
**NORTH & EAST PROPOSED ELEVATIONS**

SHEET NUMBER  
**A-202**

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STANDING SEAM METAL ROOF  
COLOR: TERRACOTTA



METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN



INSULATED METAL PANEL  
COLOR: SURREY BEIGE



EDCO STEEL SIDING ARROWLINE SHAKE  
COLOR: CLASSIC RED



STANDING SEAM METAL ROOF  
COLOR: TERRACOTTA



METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN

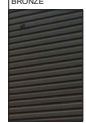


CONORADO STONE OSAGE  
COLOR: CAPE CODE GREY

CONTINENTAL CAST STONE  
COLOR: 1101



INSULATED METAL PANELS STUCCO FINISH  
COLOR: LIGHT STONE



METAL 7.2 RIBBED PERFORATED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE



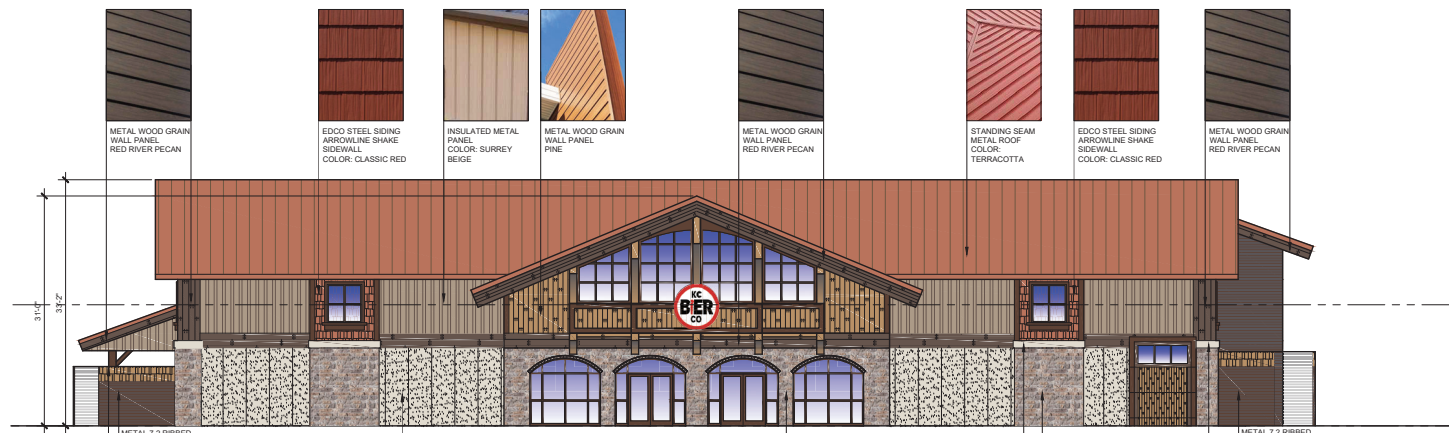
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COLOR: CEDAR

CONTINENTAL CAST STONE  
COLOR: 1101

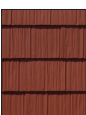


CONORADO STONE OSAGE  
COLOR: CAPE CODE GREY

WEST ELEVATION  
SCALE: 1/8"=1'-0"



METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN



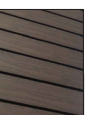
EDCO STEEL SIDING ARROWLINE SHAKE  
COLOR: CLASSIC RED



INSULATED METAL PANEL  
COLOR: SURREY BEIGE



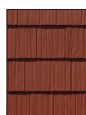
METAL WOOD GRAIN WALL PANEL  
PINE



METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN



STANDING SEAM METAL ROOF  
COLOR: TERRACOTTA



EDCO STEEL SIDING ARROWLINE SHAKE  
COLOR: CLASSIC RED



METAL WOOD GRAIN WALL PANEL  
RED RIVER PECAN



EDCO STEEL SIDING ARROWLINE SHAKE  
COLOR: CEDAR

METAL 7.2 RIBBED PERFORATED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE



INSULATED METAL PANELS TUFF WALL STUCCO FINISH  
COLOR: LIGHT STONE



CONORADO STONE OSAGE  
COLOR: CAPE CODE GREY

CONTINENTAL CAST STONE  
COLOR: 1101



CONORADO STONE OSAGE  
COLOR: CAPE CODE GREY

CONTINENTAL CAST STONE  
COLOR: 1101

METAL 7.2 RIBBED PERFORATED WALL PANEL  
TURNED HORIZONTAL  
COLOR: MEDIUM BRONZE



FRONT SOUTH ELEVATION  
SCALE: 1/8"=1'-0"

NEW HALL AND OUTDOOR THEATER FOR:

KC BIER  
at VISTA VILLAGE

NOLTE & ASSOCIATES P.A.  
ARCHITECTS / PLANNERS  
1505 WESTER PARK, SUITE 200  
OVERLAND PARK, KANSAS 66207  
TEL: (913) 222-2444  
FAX: (913) 222-0277



DATE PRINTED	
DATE ISSUED	
REVISIONS	
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▲	
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ARCHITECTURAL PROJECT NUMBER  
2022033

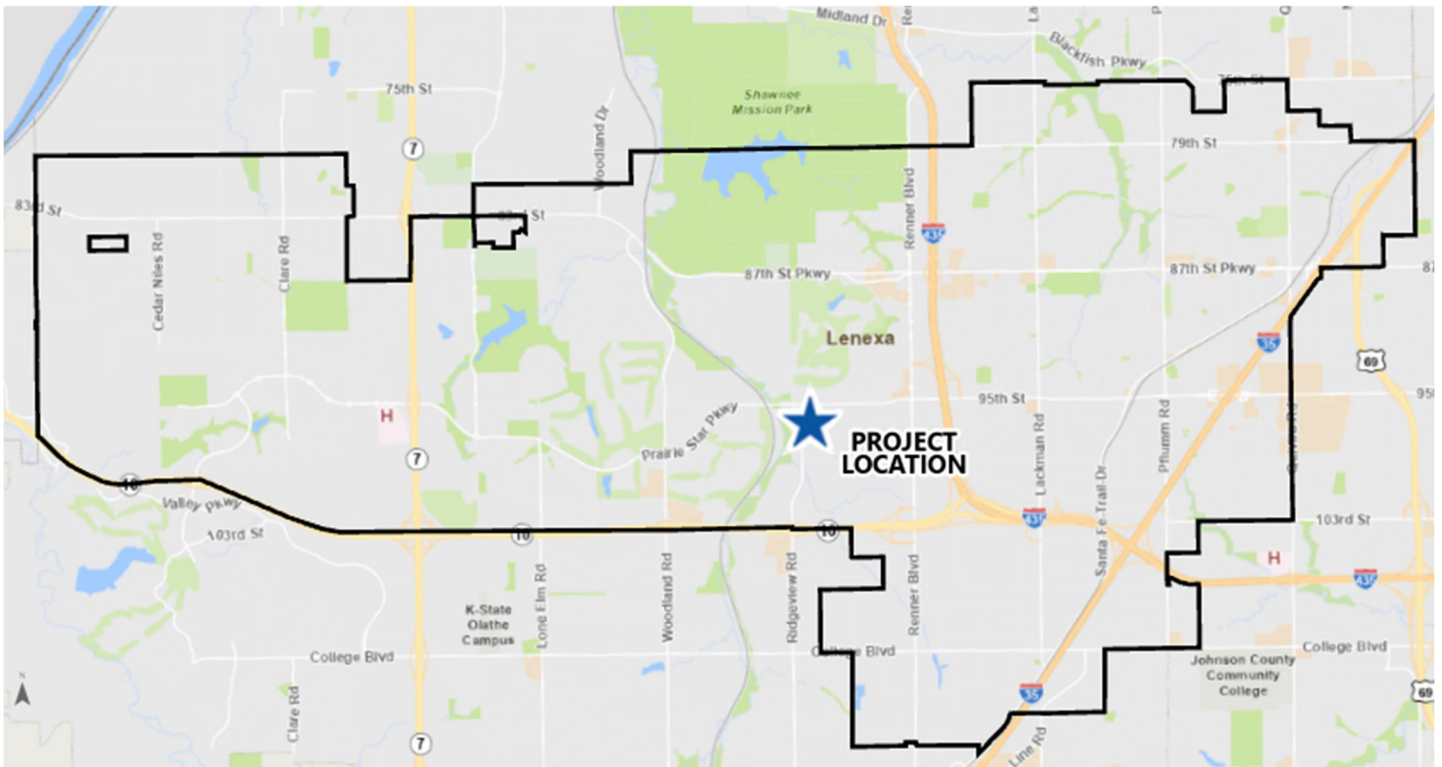
SOUTH &  
WEST  
ELEVATION

SHEET NUMBER  
A-201

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## VISTA VILLAGE LOT 6

<b>Project #:</b>	PL23-22F	<b>Location:</b>	Northeast corner of 98 <sup>th</sup> Street and Ridgeview Road
<b>Applicant:</b>	Chad Porter, Payne & Brockway	<b>Project Type:</b>	Final Plan
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	Restaurant



### PROJECT SUMMARY

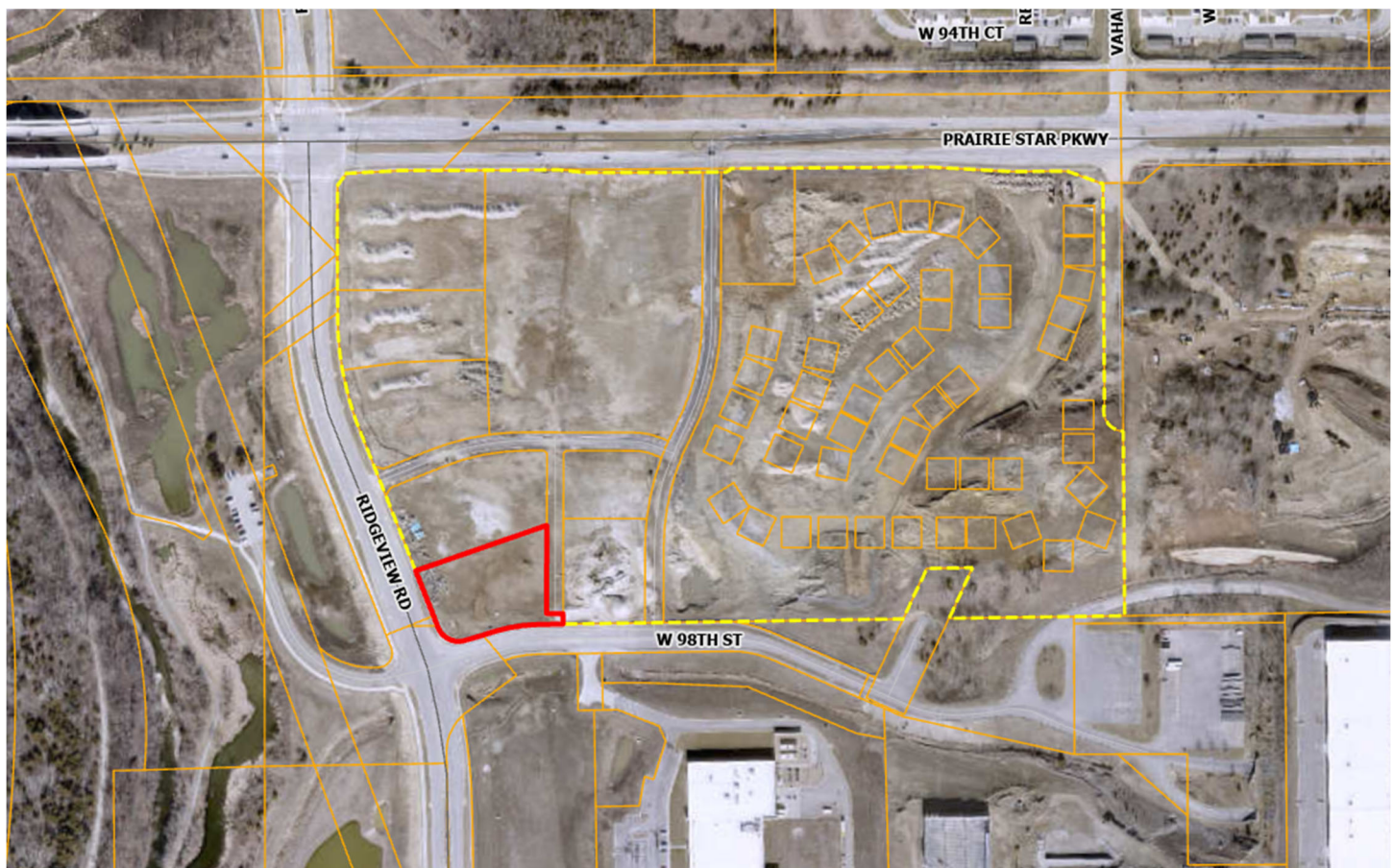
The applicant proposes to construct a 3,600 SF restaurant building on Lot 6 in the Vista Village development at the southeast corner of Prairie Star Parkway and Ridgeview Road. Vista Village is a Planned Unit Development (PUD) that includes retail, restaurants, multifamily apartments, and townhomes. The building is located at the southwesterly corner of the site.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

This site is an undeveloped lot in Vista Village, a Planned Unit Development (PUD) at the southeast corner of Prairie Star Parkway and Ridgeview Road. Vista Village was initially zoned PUD in 2015 (RZ15-06) with a preliminary plan (PL15-08P). The project has had several revised preliminary plans since the time of rezoning. The most recent was approved in August of 2022 (PL22-02PR). The subject site is the lot at the southwesterly corner of the development. The proposed final plan is consistent with the approved preliminary plan.

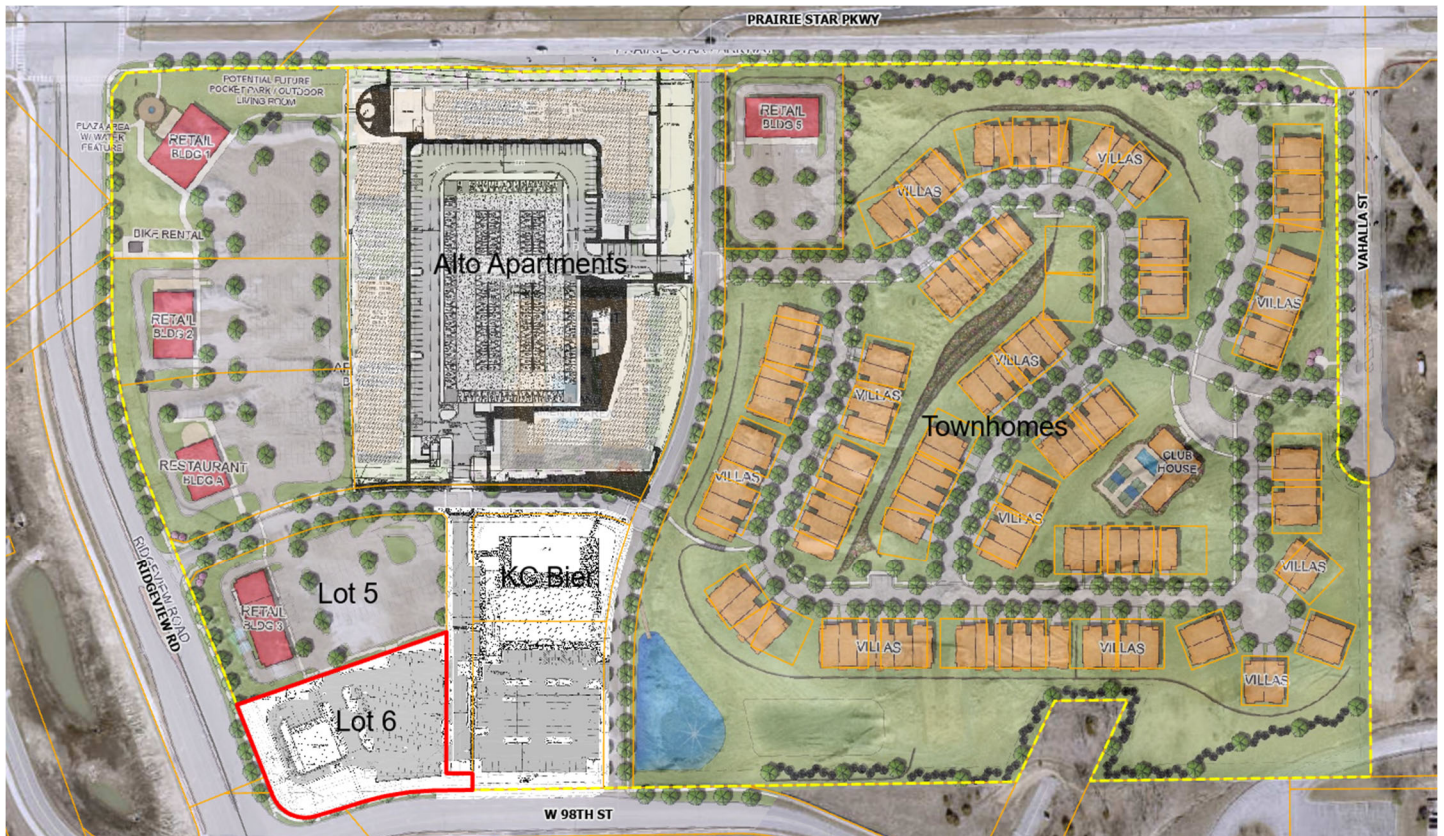
LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
1.38	3,600	PUD	Business Park



**Exhibit 1: Aerial Image of Subject Site outlined in red - Vista Village PUD outlined in yellow.**

The westerly side of Vista Village will have retail development and the Alto apartment buildings. The easterly side will have two-unit townhomes and one retail outparcel. Final plans have been approved for Lot 5 (PL20-07F, *now expired*), the Alto apartment buildings (PL22-16F), and the townhomes (PL22-04F).

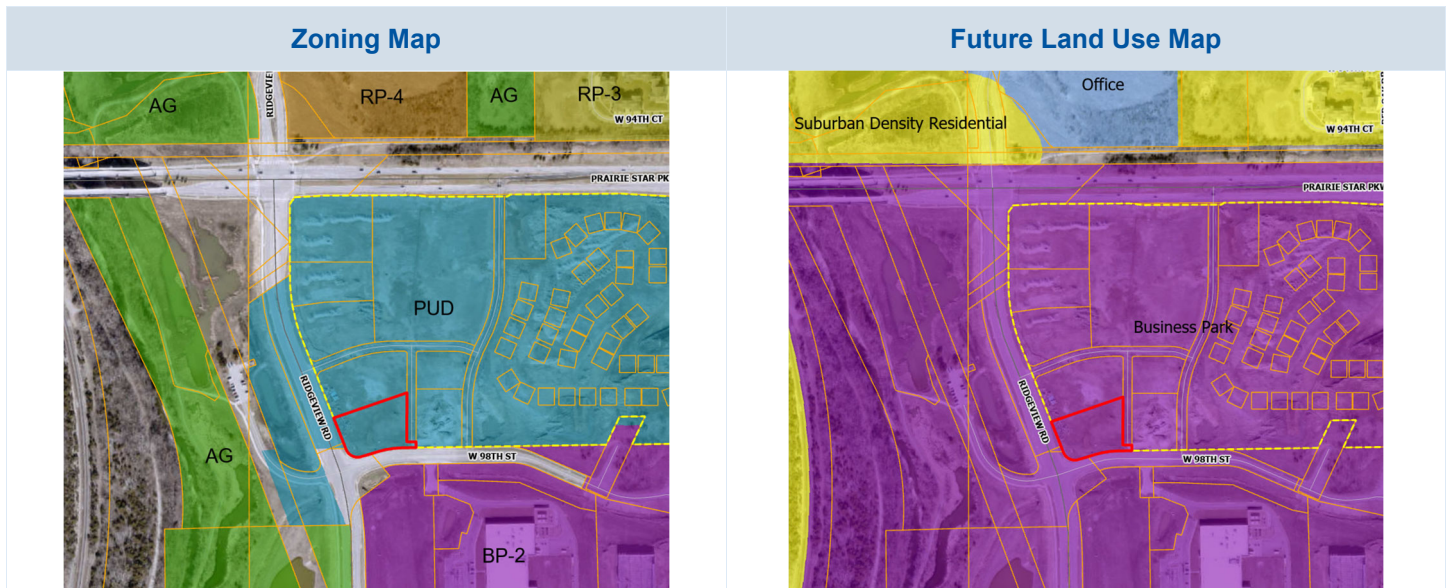




**Exhibit 2: Vista Village PUD Overall Plan**

## LAND USE REVIEW

The site is within Vista Village, a Planned Unit Development (PUD) which contains retail, restaurant, multifamily residential, and townhome residential uses. The final plan is for a restaurant building for retail uses and a restaurant. The Future Land Use designation for this location is Community Commercial Center. A restaurant use is anticipated for this site. The proposed uses are consistent with the zoning designation of PUD. The Future Land Use Plan of the Comprehensive Plan designates this site for Business Park uses.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

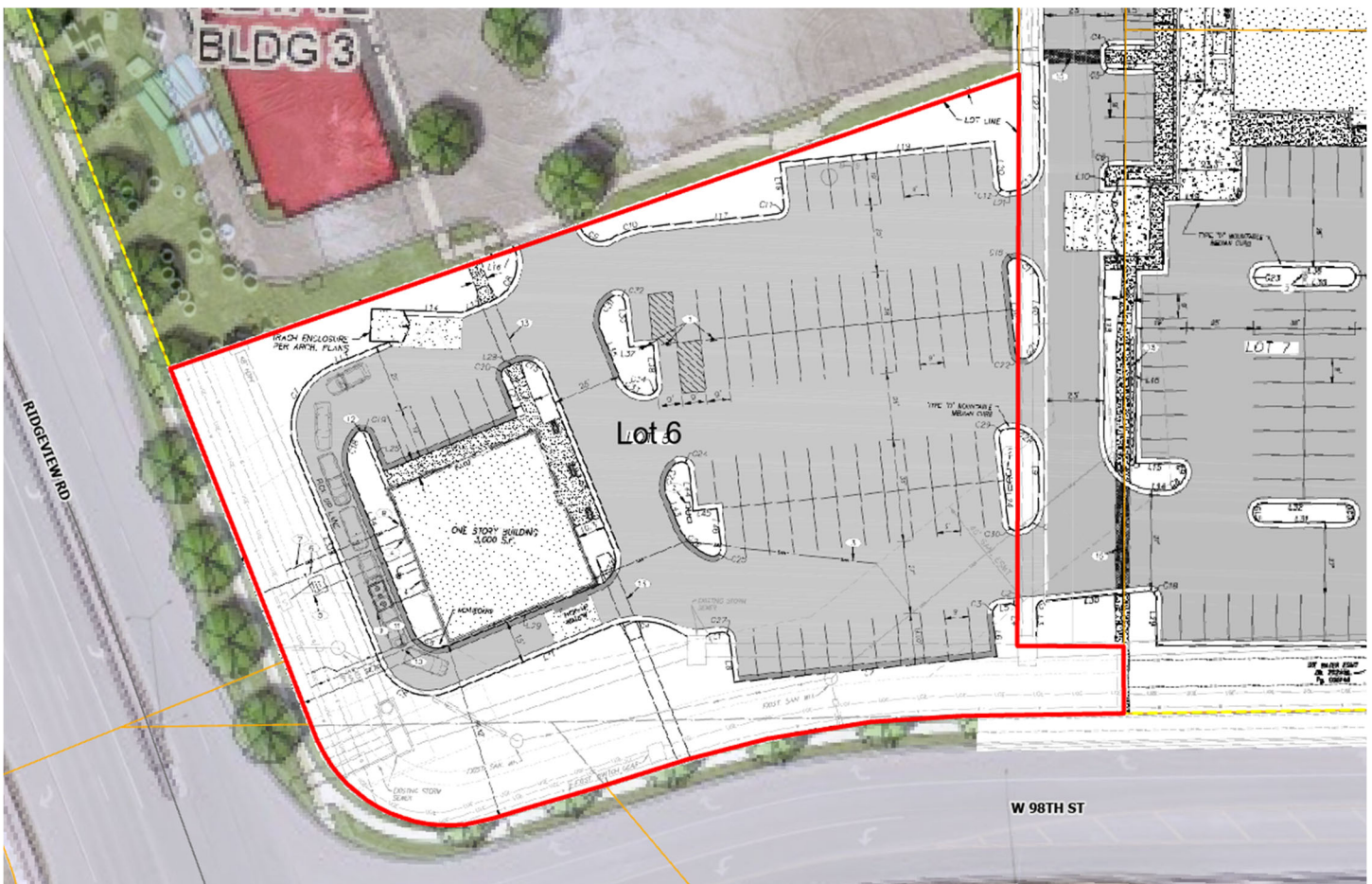
Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Business Park	PUD	Undeveloped land
<b>North</b>	Office/Employment Center, Suburban Density Residential	RP-4, AG	Undeveloped land
<b>South</b>	Business Park	BP-2	Manufacturing and Undeveloped land
<b>East</b>	Office/Research & Development	CP-O	Public safety services and undeveloped land
<b>West</b>	Business Park	AG	Public Park

The proposed use is compatible with the existing land uses and development on adjacent sites. The preliminary plan for Vista Village shows a series of buildings along the westerly part of the development that face out to Ridgeview Road. The buildings are expected to be retail buildings and restaurants. Vista Village is to include two primary amenity features, one at the immediate intersection of Prairie Star Parkway and Ridgeview Road and one is to be centrally located on the site at the intersection of the internal drives. These two features are on different lots. A condition of the latest preliminary plan requires that the amenity internal of the site to be completed concurrently with any final plan. This feature is shown on the final plan for KC Bier (PL23-21F), which is a companion final plan for a lot in Vista Village. Staff concludes the inclusion of the amenity feature with the KC Bier final plan will satisfy the condition of the preliminary plan.

## FINAL PLAN REVIEW

The site is a lot on the commercial portion of the Vista Village PUD. The building is proposed to be a restaurant with drive-thru service. The building is a one-story and contains 3,600 square feet. The front of the building will face east internally to the site and the drive-thru encircles the building with the pick-up window on the south facing façade.

The site will require connections through adjacent lots for access to the public street. An internal drive connects to the lot to the north and the parking area connects to an internal drive aisle to the east. The internal drive aisle to the east is anticipated to be developed prior to, or concurrently with this site, as part of the KC Bier site.



**Exhibit 3: Site Plan.**

### STORMWATER

This site is consistent with the overall Vista Village stormwater management design, which includes already constructed stormwater facilities on the west side of Ridgeview Road. Based on this consistency, the site will meet the City’s stormwater standards and requirements.

### TRAFFIC, ACCESS, & PARKING

Per [Section 4-1-B-20-C-14](#) of the Unified Development Code (UDC), a PUD shall comply with the parking requirements for the designated use of the site. The preliminary plan for Vista Village was approved with the expectation that parking will be shared among the lots and uses in the overall development. This plan shows the

site will provide more parking than is required for a 3,600 square-foot restaurant. Other sites in Vista Village will provide fewer stalls than required. The overall development has a cross-access and shared parking agreement to allow customers to park on any commercial lot and visit any of the other sites. A lot that has more parking than is required by [Section 4-1-D-1-C](#) of the UDC will compensate for a site that may have fewer stalls than required.

TABLE 2: PARKING ANALYSIS			
Land Use	Parking Formula	Parking Required	Parking Proposed
Restaurant	1 stall for every 75 square feet	48	80

A new sidewalk connects to the existing sidewalk on 98<sup>th</sup> Street to the south continuing along the east side of the building and connecting to the lot to the north. A sidewalk connection will continue between the parking area of this lot and the lot to the north then to sidewalks interconnecting other lots in the development. Each lot is to be accessible by sidewalks throughout the development which will provide for customers and visitors to park once and move from lot to lot without driving. The sidewalks also create a sense of continuity.

The plan does not show bicycle racks. Space on the site is available for bicycle racks. Staff will coordinate the placement of bicycle racks on the site with a building permit application. Two bicycle spaces are required by [Section 4-1-D-1-R](#) of the UDC.

## ARCHITECTURE

The building uses a mix of different exterior finish materials for the façades. The materials include brick, stone, and EIFS. The materials are consistent with the Vista Village Design Guidelines. Stone is used for a wainscot around the base of the building with a cast stone detail to provide a transition to brick. A brick soldier course detail is used above the windows and is repeated to reinforce a horizontal line around the parapet of the building.

The building has a clerestory element on the northerly portion of the building. The clerestory feature extends over the covered entry and emphasizes the importance of this part of the building. The roofline of the building incorporates a large ogee style cornice. The cornice uses two tones of color creating more detail to this element. A screen wall that mimics the stone and brick pattern of the building will be used to screen utility service connections on the side of the building that faces out to Ridgeview Road. Decorative light sconces will be placed on the building to further identify the front facing side of the building and the pedestrian areas around the building.

Vista Village has a sign criteria that is part of the development’s design guidelines. The elevations show wall signs for a tenant within a space on the building façade designed to accept a sign. This is consistent with the sign criteria.

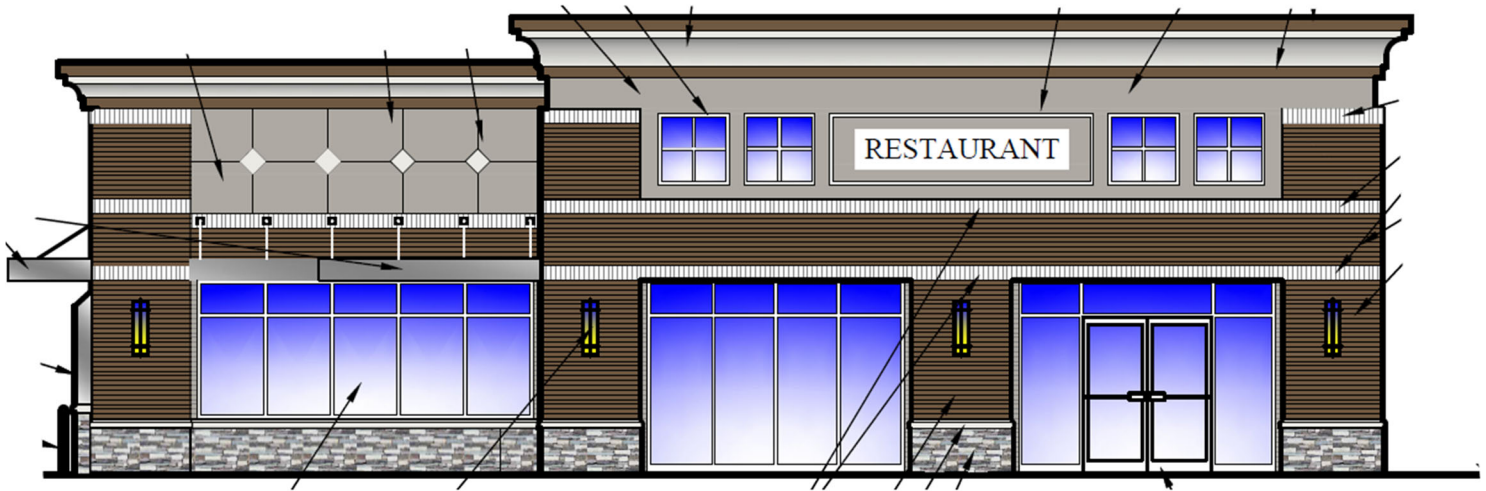


Exhibit 4: East Elevation.

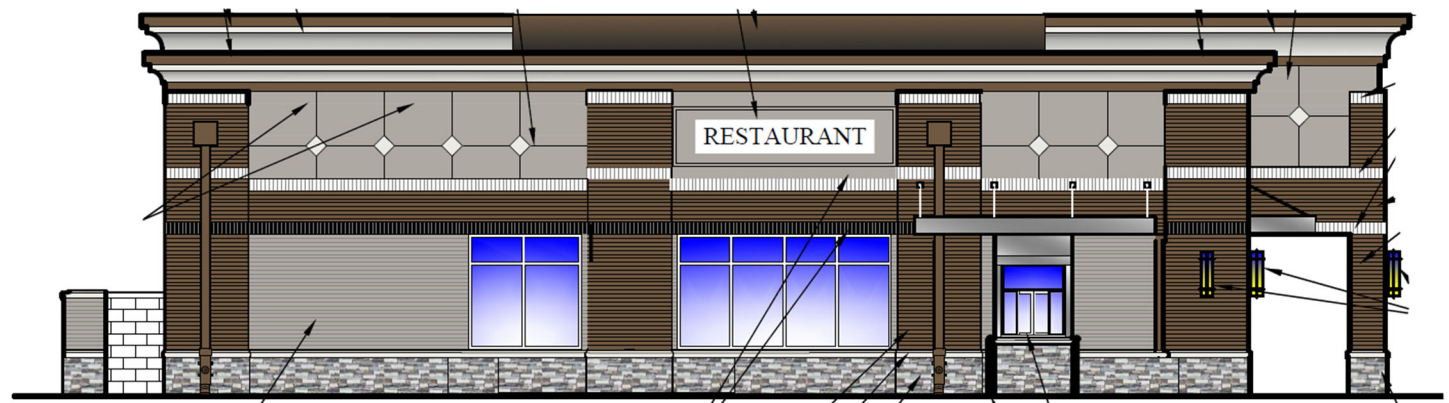


Exhibit 5: South Elevation.

## LANDSCAPING

The proposed landscaping for the site is consistent with the landscape requirements for the Vista Village design guidelines and the landscape requirements per [Section 4-1-D-20-C-12](#) of the UDC. Perimeter landscaping will include trees and shrubs planted along 98<sup>th</sup> Street and Ridgeview Road. The plans show the trees and shrubs are planted in a linear pattern along the drive-thru lane. These groups of plant materials can be arranged in clusters and spaced in an offset pattern to be more strategically positioned for screening and creating views into and out of the development.

Landscaping is shown in the parking islands, except for one island which will be constructed to allow for large trucks to drive over the island. This surface of this island will use a contrasting material and a roll-over style curb. The trees shown along 98<sup>th</sup> Street are between the curb of the street and the sidewalk. 98<sup>th</sup> Street is a route where semi-trucks will frequently drive along to access the Meritex storage facility. The trees should be positioned so that they are not going to be impacted by the large vehicles that drive on this street. The site has adequate space to move the trees to other locations of the site and still meet the UDC requirements. Staff will continue to coordinate the design of the landscape plan with the applicant.

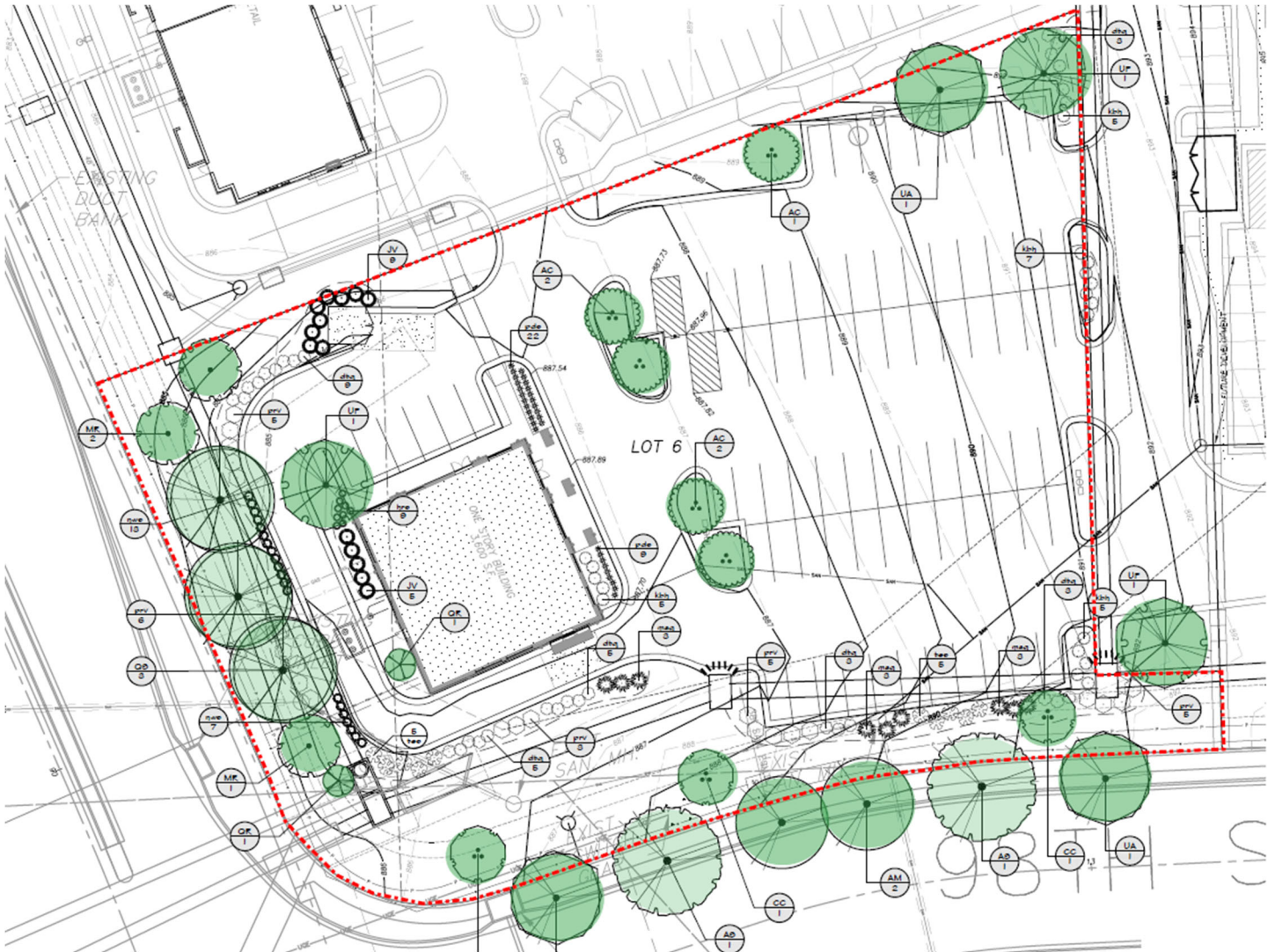


Exhibit 6: Landscape plan.

## REVIEW PROCESS

- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

## RECOMMENDATION FROM PROFESSIONAL STAFF

### ★ Staff recommends approval of the proposed final plan for Vista Village Lot 6.

- This is a final plan for development of a restaurant building.
- The project is consistent with Lenexa’s goals as an *Inviting Places* and promotes a *Thriving Economy*

## FINAL PLAN

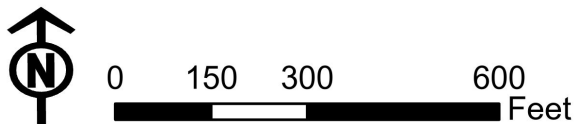
Staff recommends **approval** of the final plan for PL23-22F – **Vista Village Lot 6** located near the northeast corner of 98<sup>th</sup> Street and Ridgeview Road, for a restaurant use.

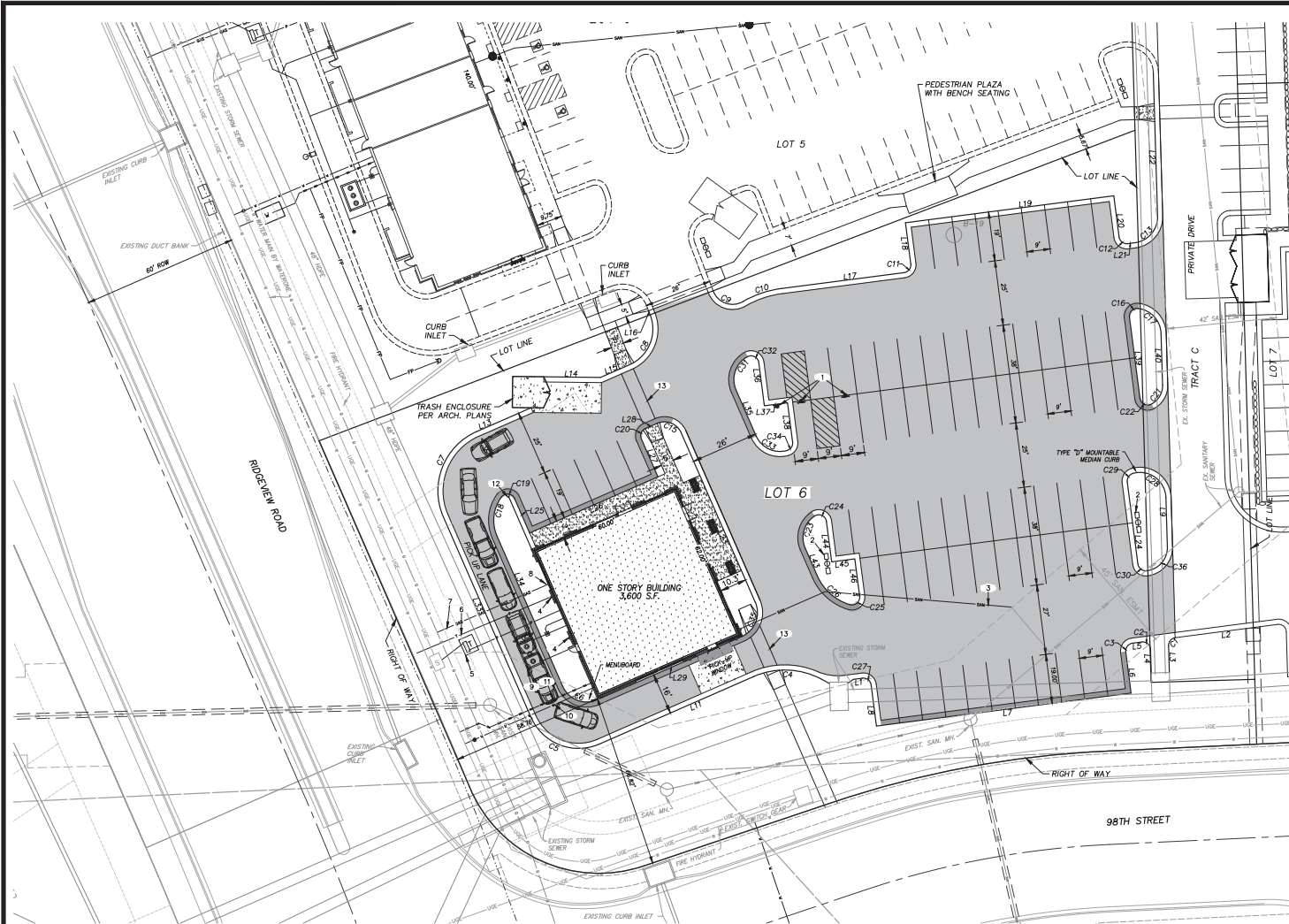


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Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Vista Village Lot 6 PL23-22F





**LEGAL DESCRIPTION:**

ALL OF LOT 6, VISTA VILLAGE, A SUBDIVISION OF LAND IN LENEXA, JOHNSON COUNTY, KANSAS.

**DEVELOPMENT NOTES:**

1. SIDEWALKS SHALL BE CONCRETE.
2. CROSSWALKS SHALL BE PAINTED WITH A LADDER TYPE PATTERN.
3. PRIVATE DRIVES SHOWN HEREON WILL PROVIDE SHARED ACCESS FOR EACH LOT WITHIN THE DEVELOPMENT.
4. PAVEMENT MARKINGS SHALL BE PAINTED IN ACCORDANCE WITH THE ADA PARKING SPACE DETAIL.

**CONSTRUCTION NOTES:**

1. ADA PARKING SIGN.
2. PARKING LOT LIGHTS. SEE LIGHTING PLAN.
3. SANITARY SEWER SERVICE LINE.
4. ROOF DRAIN DOWNSPOUT.
5. EVERY TRANSFORMER. SEE UTILITY PLAN.
6. 4" CONDUIT FOR TELEPHONE SERVICE. SEE UTILITY PLAN.
7. GAS SERVICE. SEE UTILITY PLAN.
8. GAS METER. SEE UTILITY PLAN.
9. GREASE INTERCEPTOR.
10. WATER SERVICE. SEE UTILITY PLAN.
11. FIRE PROTECTION LINE. SEE UTILITY PLAN.
12. REMOTE FIRE DEPARTMENT CONNECTION.
13. CROSSWALK PER DETAIL.

**LEGEND**

- [Hatched Box] DENOTES PROPOSED BUILDING
- [Dotted Box] DENOTES PROPOSED 8" REINFORCED CONCRETE
- [Cross-hatched Box] DENOTES PROPOSED 6" ASPHALT PAVEMENT
- [Horizontal Lines Box] DENOTES PROPOSED CONCRETE SIDEWALK
- [Vertical Lines Box] DENOTES PROPOSED TYPE B CURB
- [Thin Line Box] DENOTES PROPOSED TYPE B CURB (DRY)

PARKING CALCULATIONS				
USE	RATIO	SQUARE FOOTAGE	REQUIRED	PROVIDED
RESTAURANT	1 PER 75 S.F.	3,600	48	80
<b>PARKING TOTALS</b>			<b>48</b>	<b>80</b>

**PARKING NOTES:**  
 1. ALL REGULAR PARKING SPACES SHOWN HEREON ARE 9'x19'.  
 2. THE ANGLE OF ALL PARKING SPACES SHOWN HEREON IS 90°.

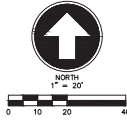
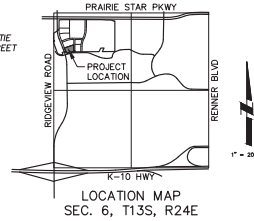
SITE DEVELOPMENT DATA		
SITE AREA	11.38 AC ±/-	
TOTAL BUILDING AREA	3,600 S.F.	
TOTAL LOT COVERAGE	31.241 S.F. (72%)	
OPEN SPACE	18,276 S.F. (28%)	
ZONING	PLU3	

**VICINITY ZONING PATTERN:**

NORTH: AG, AGRICULTURAL, NPO, PLANNED NEIGHBORHOOD OFFICE & RP-3, PLANNED RESIDENTIAL MEDIUM HIGH DENSITY ZONING DISTRICTS  
 SOUTH: BP-2, PLANNED MANUFACTURING ZONING DISTRICT  
 EAST: AG, AGRICULTURAL ZONING DISTRICT  
 WEST: AG, AGRICULTURAL ZONING DISTRICT (PARK)

**OWNERS NAME:**

JAYHAWK RIDGE, LLC  
 C/O MICHAEL A. CHRISTIE  
 10550 S. WATKINS STREET  
 OLAHE, KS 66061



LINE TABLE		
LINE #	LENGTH	DIRECTION
L1	3.46'	S82° 37' 18.40"W
L2	39.73'	S82° 37' 18.40"W
L3	10.35'	S1° 39' 45.11"E
L4	9.05'	S1° 39' 45.11"E
L5	3.61'	S82° 37' 18.40"W
L6	16.00'	S7° 22' 41.60"E
L7	99.00'	S82° 37' 18.40"W
L8	16.00'	S7° 22' 41.60"E
L9	18.99'	N1° 42' 06.09"W
L11	64.94'	N66° 22' 25.90"E
L13	22.85'	S66° 22' 25.90"W
L14	21.86'	S89° 22' 10.43"E
L15	12.21'	S66° 22' 25.90"W

LINE TABLE		
LINE #	LENGTH	DIRECTION
L16	1.81'	S20° 50' 49.50"E
L17	47.63'	S82° 37' 18.40"W
L18	16.00'	S7° 22' 41.60"E
L19	81.00'	S82° 37' 18.40"W
L20	16.00'	S7° 22' 41.60"E
L21	2.78'	S7° 22' 41.60"E
L22	55.54'	S1° 42' 06.09"E
L23	71.33'	S23° 37' 34.10"E
L24	31.92'	S7° 22' 41.60"E
L25	14.39'	S23° 37' 34.10"E
L26	54.00'	S66° 22' 25.90"W
L27	16.00'	S23° 37' 34.10"E
L28	5.00'	S66° 22' 25.90"W

LINE TABLE		
LINE #	LENGTH	DIRECTION
L29	61.00'	S20° 50' 49.50"E
L33	84.33'	N23° 37' 34.10"W
L34	63.33'	N23° 37' 34.10"W
L35	19.36'	N23° 37' 34.10"W
L36	16.78'	S7° 22' 41.60"E
L37	9.00'	N82° 37' 18.40"E
L38	15.24'	S7° 22' 41.60"E
L39	31.50'	S7° 22' 41.60"E
L40	19.90'	S1° 42' 06.09"E
L43	11.67'	N23° 37' 34.10"W
L44	15.67'	S7° 22' 41.60"E
L45	9.00'	N82° 37' 57.42"E
L46	14.49'	S7° 22' 41.60"E

CURVE TABLE		
CURVE #	LENGTH	RADIUS
C1	4.41'	3.00
C2	5.01'	3.00
C3	4.71'	3.00
C4	27.17'	30.00
C5	31.42'	20.00
C6	15.71'	10.00
C7	39.27'	25.00
C8	22.83'	15.00
C9	13.85'	8.00
C10	15.81'	40.00
C11	4.71'	3.00
C12	4.71'	3.00
C13	14.72'	10.00

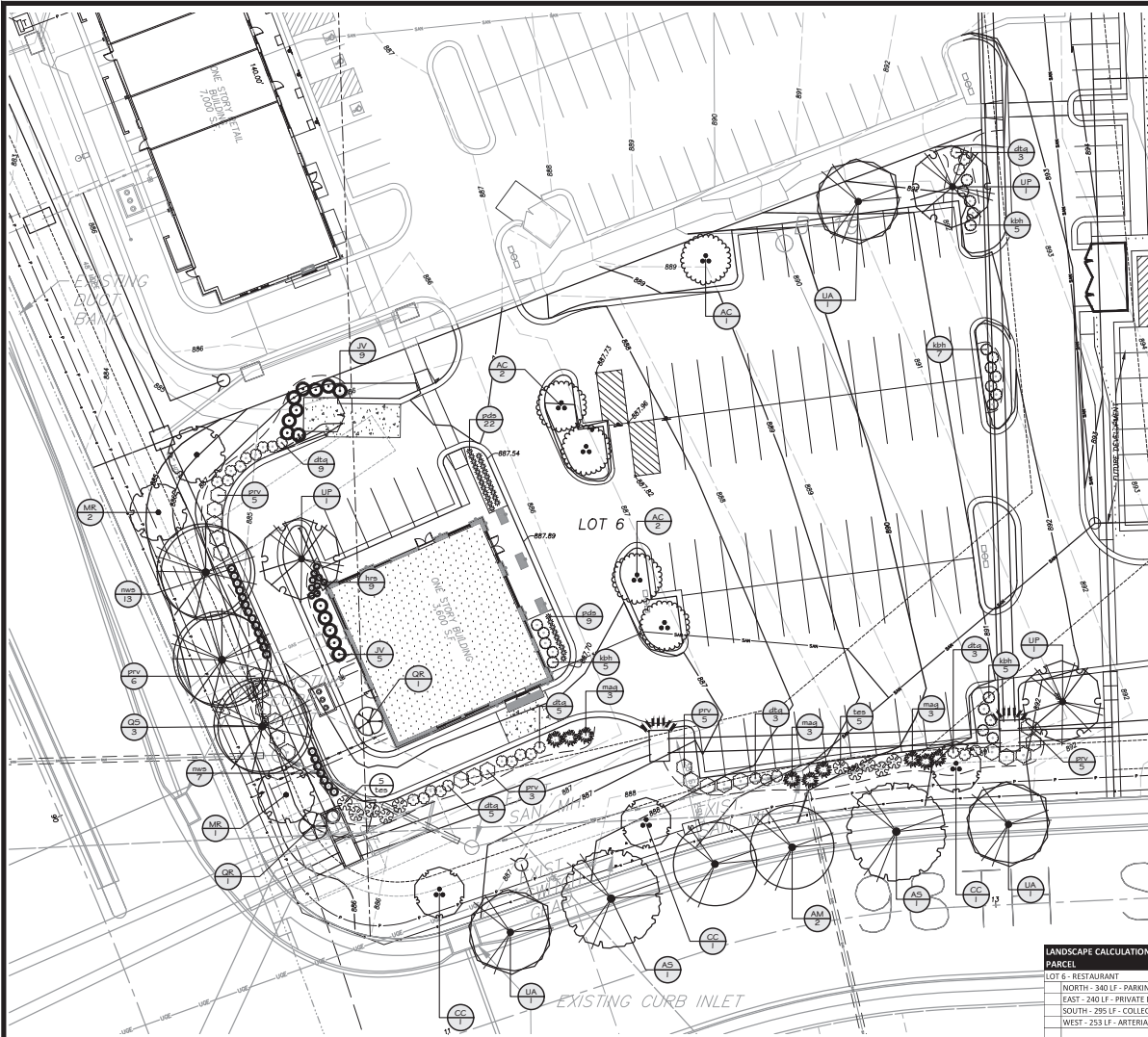
CURVE TABLE		
CURVE #	LENGTH	RADIUS
C15	11.00'	7.00
C16	5.45'	3.00
C17	14.26'	10.00
C18	15.71'	15.00
C19	6.28'	3.00
C20	4.71'	3.00
C21	9.05'	8.00
C22	5.73'	3.00
C23	13.88'	10.00
C24	6.11'	3.00
C25	5.99'	3.00
C26	17.24'	20.00
C27	4.71'	3.00

CURVE TABLE		
CURVE #	LENGTH	RADIUS
C28	18.20'	10.00
C29	4.26'	3.00
C30	4.63'	3.00
C31	14.83'	10.00
C32	5.83'	3.00
C33	14.37'	10.00
C34	4.26'	3.00
C35	15.71'	10.00
C36	13.49'	9.00

PLOT DATE: 10/23/2023  
 DESIGNER: C100 & SITE PLAN - C100

NO.	DATE	DESCRIPTION	BY
			CMP
1	10-23-23	PER CITY COMMENTS	
2			
3			
4			
5			
6			
7			





ORNAMENTAL TREE	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	AC	5	Ametanchier canadensis 'Autumn Brilliance'	Autumn Brilliance Serviceberry Clump	6' HL	B&B / Cont.
	CC	3	Cercis canadensis	Eastern Redbud Clump	6' HL	B&B / Cont.
	MR	3	Malus x 'Royal Raindrops'	Royal Raindrops Crabapple	6' HL	B&B / Cont.
	QR	2	Quercus robur x bicolor 'Nadler'	Kindred Spirit Oak	6' HL	B&B / Cont.
SHADE TREE	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	AM	2	Acer nysebeei 'State Street'	State Street Maple	2" Cal.	B&B / Cont.
	AS	2	Acer saccharum 'Autumn Splendor'	Autumn Splendor Caddo Sugar Maple	2" Cal.	B&B / Cont.
	QS	3	Quercus shumardii	Shumard Red Oak	2" Cal.	B&B / Cont.
	UA	3	Ulmus americana 'Jefferson'	Jefferson American Elm	2" Cal.	B&B / Cont.
	UP	3	Ulmus parvifolia 'Alee'	Alee Lacebark Elm	2" Cal.	B&B / Cont.
EVERGREEN TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	JV	14	Juniperus virginiana 'Taylor'	Taylor Juniper	5' HL	B&B / Cont.
SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	dtq	28	Chaenomeles x 'Double Take' TM	Double Take Quince	18" HL	Pot
	kbh	22	Diervilla rivularis 'SMNDRSF'	Kodak Black Bush-Honeysuckle	18" HL	Pot
	tes	10	Rhus typhina 'Tiger Eyes'	Tiger Eyes Sumac	18" HL	Pot
	prv	24	Viburnum x pragnense	Prague Viburnum	18" HL	Pot
GRASSES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	naq	9	Miscanthus sinensis 'Gracillimus'	Maiden Grass	18" HL	Pot
	hrs	9	Panicum virgatum 'Hot Rod'	Hot Rod Switch Grass	18" HL	Pot
	nes	20	Panicum virgatum 'North Wind'	Northwind Switch Grass	18" HL	Pot
	pds	31	Sporobolus heterolepis	Prairie Dropseed	18" HL	Pot

LANDSCAPE CALCULATIONS		SHADE TREES		ORN / EVGRN TREES		SHRUBS		LANDSCAPE BED (SF)		PARKING LOT ISLAND TREES	
PARCEL	LOT AREA / PARKING AREA	REQ'D	PROP	REQ'D	PROP	REQ'D	PROP	REQ'D	PROP	REQ'D	PROP
LOT 6 - RESTAURANT	62,000 SF / 27,410 (3 ISL)										
NORTH - 340 LF - PARKING		3	5	5	0	24	YES				
EAST - 240 LF - PRIVATE DR		6	8	3	3	36	YES	222	YES		
SOUTH - 295 LF - COLLECTOR		6	6	3	3	31	YES	190	YES		
WEST - 255 LF - ARTERIAL											

1 LANDSCAPE PLAN  
 1" = 20'

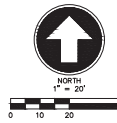
LANDSCAPE PLAN NOTES:

- Existing underground (u/g), overhead (o.h.) utilities and drainage structures have been plotted from available information and therefore, their locations must be considered approximate only. It is the responsibility of the individual contractors to verify existence and location of all utilities before starting any work.
- Prior to commencement of work, the contractor shall give 48 hours advance notice to all those companies/utilities which have facilities in the near vicinity of the construction to be performed.
- Contractor shall verify all landscape material quantities and shall report any discrepancies immediately to the Landscape Architect.
- Contractor shall stake plant locations in the field and have approval by Owner's Representative before proceeding with installation.
- No substitutions for variety or cultivar shall be accepted without first obtaining written approval from the Landscape Architect.
- All plant material shall be of excellent quality, free of disease and

- infestation, and true to type, variety, size specified, and form per American Nursery Association Standards. All trees and shrubs shall be laid out in a uniform and consistent pattern, following the landscape plan accurately.
- All shrub beds in lawn areas shall be edged as shown in the planting details.
- All plants shall receive 3" minimum of shredded hardwood mulch as detailed, unless otherwise noted. In landscape beds, mulch shall be a consistent 3" depth throughout. For trees planted in turf areas, a 3" ring of mulch shall be formed into a saucer in a minimum ring twice the diameter of the rootball from the trunk. Elevation of top of mulch shall be 1/2" below any adjacent pavement.
- Contractor shall thoroughly water in each plant immediately following installation.
- Contractor shall be responsible for calculating all areas of mulch, sod and seed and the amounts of each needed for specified depth or coverage.
- Contractor shall spread two inches of cotton burr compost material

- over entire landscape bed areas and till into existing topsoil to a depth of at least 6" prior to planting.
- Contractor shall install Turf-type tall fescue sod consisting of 90% fescue blend and 10% bluegrass in all areas disturbed during construction not otherwise designated as another material.
- Proposed trees shall not be placed over existing or proposed utility service lines. It is the contractor's responsibility to understand utility locations and have them marked during tree planting operations. If utility is damaged during planting, contractor is responsible for notifying the general contractor and owner of utility and paying for repair of the damaged utility.
- Trees and shrubs located adjacent to parking stalls must be set back a minimum of 4 feet from back of curb to center of the plant to allow for vehicle overwing.
- Landscaping adjacent to the fire department connection shall allow for unobstructed access to the connection, with no shrubs/trees within a 3-foot radius of the connection.

- In the event of work in or on the JCW sanitary main, any trees or plantings placed within the sewer easement may be removed without replacement or compensation thereof and shall be replaced by the property owner as required by the City.
- IRRIGATION: Contractor shall design and install a complete automated irrigation system with direction from the owner or owner's rep and per City requirements. Irrigation system design shall be submitted to the owner or owner's rep for approval prior to installation. Irrigation system shall consist of, at a minimum, the point of connection tap, backflow prevention, smart ET-based controller, all pipe, wire, heads, valves, drip tubing, connectors and appurtenances necessary to deliver water to turf and landscape areas to maintain optimal plant health in all growing seasons. Turf, trees and shrubs beds should be watered on separate zones and at rates necessary to facilitate plant health. Contractor shall supply an as-built drawing electronically and to scale of exact irrigation system component location to the owner at completion of the installation.



Chad D. Weinand, P.A., ASLA  
 Landscape Architecture  
 15173 W. 157th Terrace, Olathe, Kansas 66062  
 913.484.9738 - cweinand74@gmail.com  
 Copyright 2023

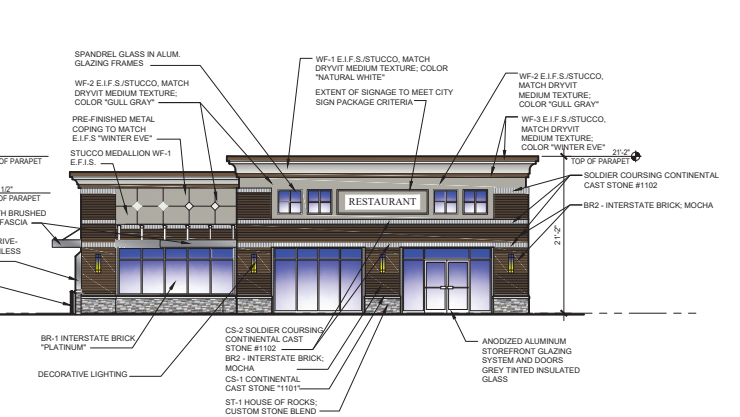
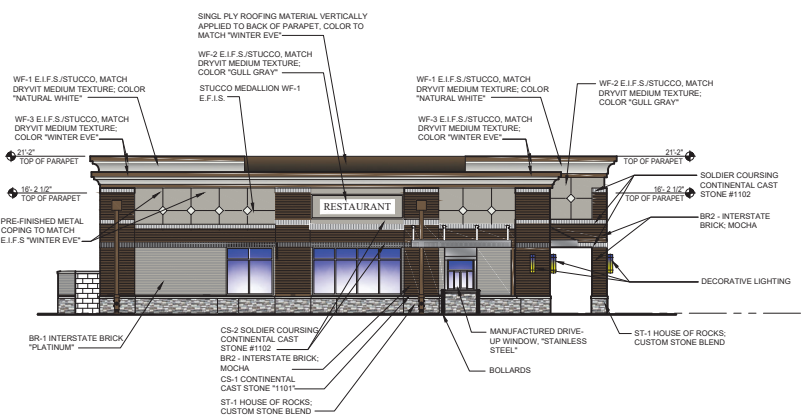
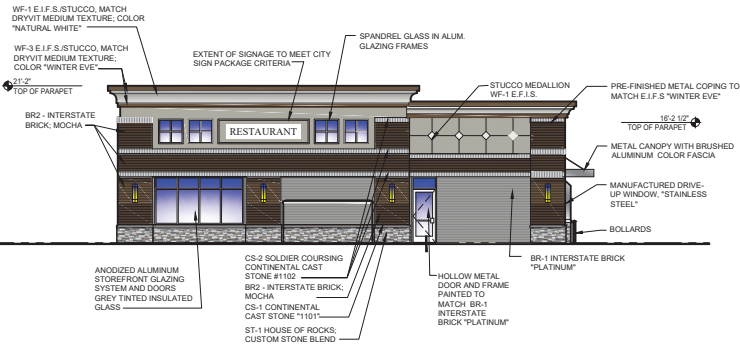
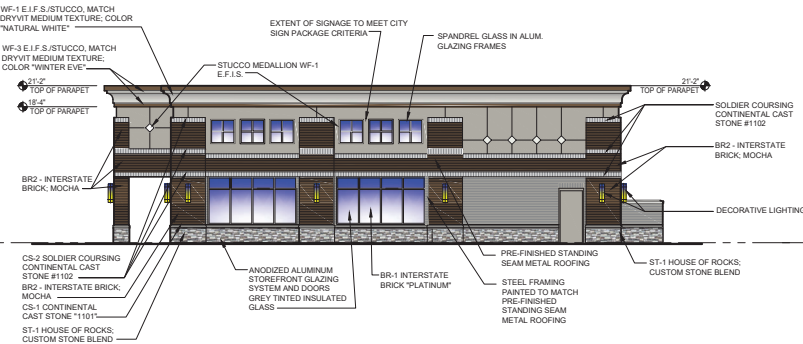
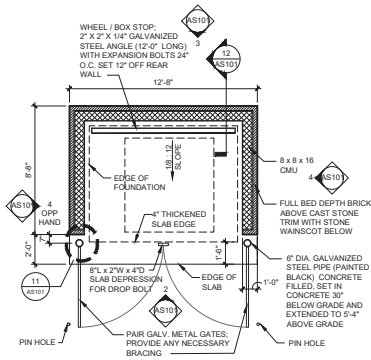
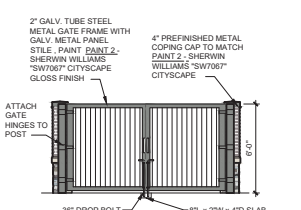
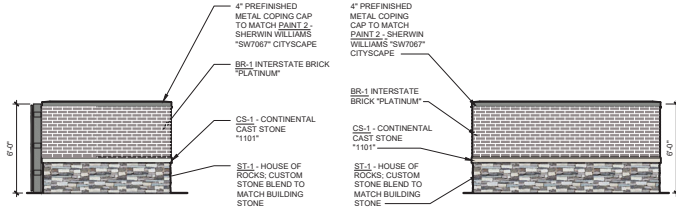
NO.	DATE	DESCRIPTION	BY	
			CDW	CDW
1	10.23.23	Site Layout Changes		
2	10.23.23	Per City Comments		
3				
4				
5				
6				
7				



7/21/2022

L101

PLOT DATE: 10/22/2023  
 PROJECT: 18-0041 - VISTA VILLAGE  
 DRAWING: 18-0041-LS-01 & FDP - LSP - 10.23.23.dwg - L101



THOMAS GRAY NOLTE ARCHITECTS, P.A.  
1005 W. BROADWAY, SUITE 200  
OVERLAND PARK, KANSAS 66207  
P. (913) 332-2444  
F. AX. (913) 332-2427



NEW CONSTRUCTION  
**VISTA #6 LLC**  
**TENANT BUILDING**  
RIDGEVIEW & APRIL STAR PARKWAY  
JOHNSON COUNTY

DATE PRINTED:	
DATE ISSUED:	
REVISIONS:	

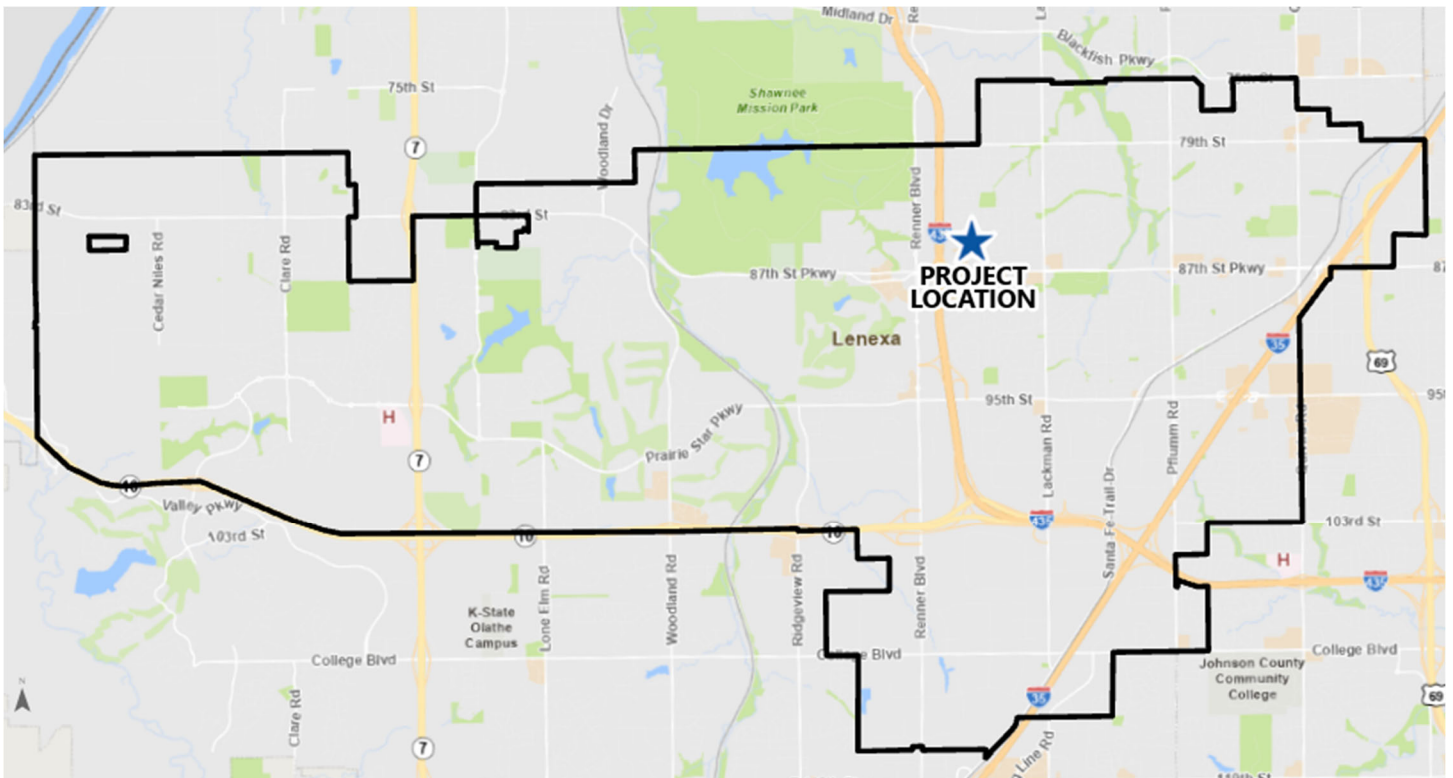
ARCHITECTURAL PROJECT NUMBER  
**2022010**

ELEVATIONS

SHEET NUMBER  
**A-201**

## LENEXA POINT, 2<sup>ND</sup> PLAT

<b>Project #:</b>	PT23-24F	<b>Location:</b>	Near the northwest corner of W 87 <sup>th</sup> St Parkway & Maurer Road
<b>Applicant:</b>	Aaron Perrine, GBA	<b>Project Type:</b>	Final Plat
<b>Staff Planner:</b>	Will Sharp	<b>Proposed Use:</b>	Commercial Development



### PROJECT SUMMARY

The applicant requests approval of a final plat of 3.34 acres for commercial development located north of W 87<sup>th</sup> Street Parkway between I-435 and Maurer Road. The plat consists of one lot intended for development and one tract for stormwater detention. A new drainage easement is dedicated to the City with this plat, which requires approval from the Governing Body. This project does not require a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

The subject property is located in the vicinity of the northwest corner of W 87<sup>th</sup> Street Parkway and Maurer Road. Tract A, a stormwater detention area, is included in the current proposal. The center was originally developed in the late 1980s and is not platted. This is the second plat for the Lenexa Point commercial development. Lenexa Point, 1<sup>st</sup> Plat was approved in 2016 (PT16-25F).

Recently approved plans for this lot include a preliminary plan (PL22-21P) and a final plan (PL23-04F). A special use permit (SU22-11) was approved in December 2022 to allow a self-service storage use in the CP-2, Planned Community Commercial District. The final plan shows a proposal for a three-story, 118,560 SF self-service storage facility.

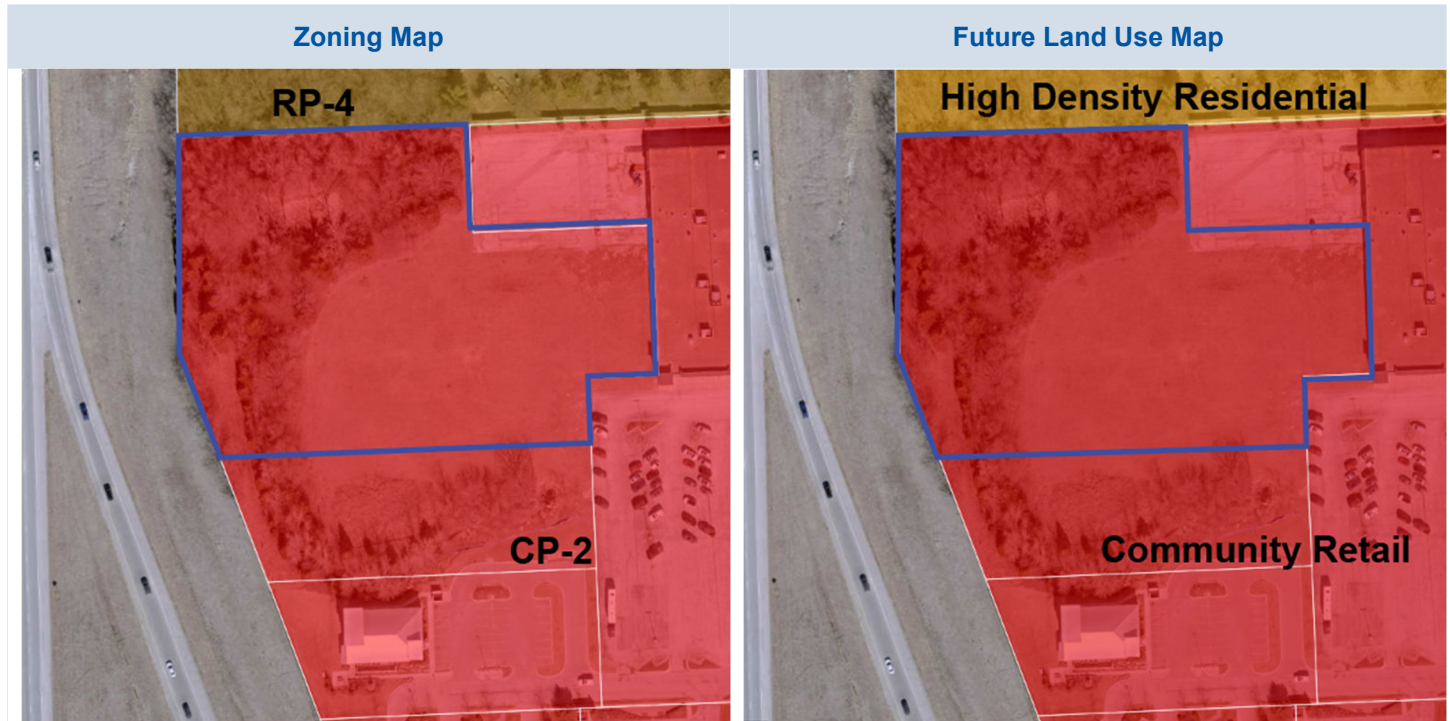
LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
3.34	N/A	CP-2	Community Retail



**Exhibit 1: Aerial Image of Subject Site**

## LAND USE REVIEW

The proposed application is for a one-lot and one-tract final plat for a new self-storage development in the Lenexa Pointe retail center. Commercial development is permitted in the CP-2, Planned Community Commercial District.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Community Retail	CP-2	Undeveloped
<b>North</b>	High Density Residential	RP-4	Multifamily Residential
<b>South</b>	Community Retail	CP-2	Undeveloped
<b>East</b>	Community Retail	CP-2	Retail
<b>West</b>	I-435 Right-of-Way	N/A	I-435 Right-of-Way

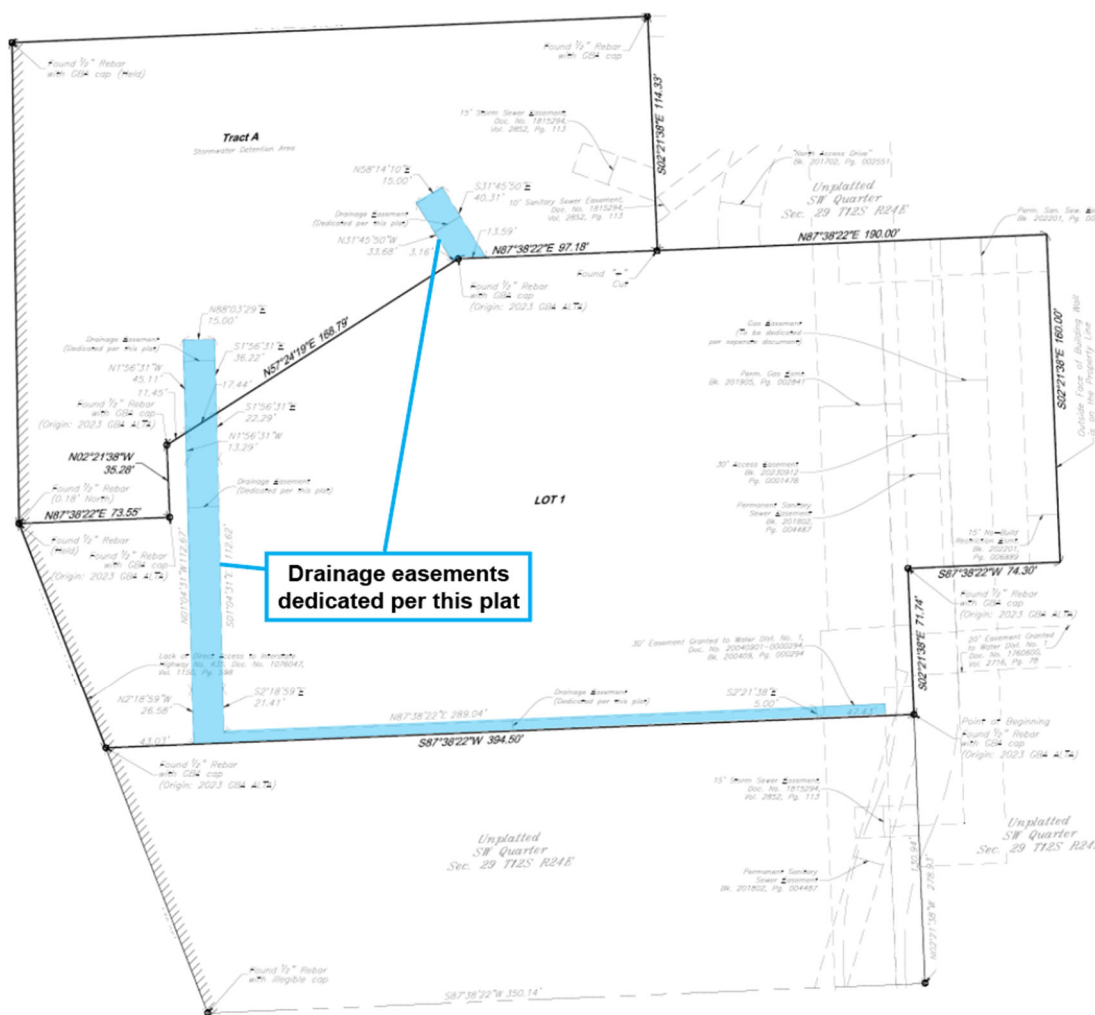
The proposed use is compatible in the context of existing land uses. Commercial uses are compatible with the area’s designation as Community Retail on the Future Land Use Map in the Comprehensive Plan. Current development in the vicinity is in accordance with the Comprehensive Plan.

## FINAL PLAT REVIEW

This application is for a 3.34-acre final plat. It consists of one lot (Lot 1, 2.18 acres) intended for commercial development and one tract (Tract A, 1.16 acres) intended for stormwater detention.

There are numerous existing easements within the plat boundary, with new drainage easements being dedicated by this plat. These dedications are highlighted in Exhibit 2 below.

Access to the site will come from connections with Maurer Road to the east. A perpetual access easement to this property was dedicated by Lenexa Point, 1<sup>st</sup> Plat (PT16-25F). Limits of no access to Interstate 435 are indicated on the western boundary of the plat.



**Exhibit 2: Lenexa Point, 2<sup>nd</sup> Plat (PT23-24F) Showing Easements.**

## DEVIATIONS

The applicant is not requesting any deviations.

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## REVIEW PROCESS

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- *This project requires approval by the Planning Commission and acceptance of dedications by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration by the City Council on November 21, 2023.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

---

## RECOMMENDATION FROM PROFESSIONAL STAFF

---

★ **Staff recommends approval of the proposed final plat for Lenexa Point, 2<sup>nd</sup> Plat.**

- The applicant requests approval of a final plat of 3.34 acres for commercial development located north of W 87<sup>th</sup> Street Parkway between I-435 and Maurer Road. The plat consists of one lot and one tract.
- The project is consistent with Lenexa's goals through ***Responsible Economic Development*** to create ***Thriving Economy***.

## FINAL PLAT

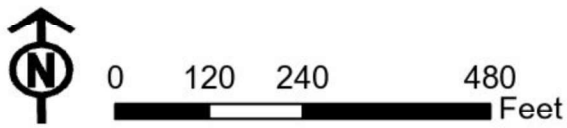
Staff recommends **approval** of the final plat for PT23-24F – **Lenexa Point, 2<sup>nd</sup> Plat** at the northwest corner of W 87<sup>th</sup> Street Parkway and Maurer Road for commercial development.



Document Path: C:\Users\cdwills\Documents\ArcGIS\Projects\MyProject24\MyProject24.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Lenexa Point, 2nd Plat PT23-24F





**FINAL PLAT OF  
LENEXA POINT, SECOND PLAT  
(PART OF THE SOUTHWEST QUARTER OF SECTION 29, TOWNSHIP 12 SOUTH, RANGE 24 EAST,  
IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS)**

**Description:**

A tract of land being part of the Southwest Quarter of Section 29, Township 12 South, Range 24 East, of the Sixth Principal Meridian, in the City of Lenexa, Johnson County, Kansas, more particularly described by Michael D. Lay, LS 1612, of George Butler Associates, Inc., C.L.S. 6, on October 20, 2023, as follows:

Commencing at the Southeast corner of the Southwest Quarter of said Section 29; thence South 87°38'22" West, along the South line of the Southwest Quarter of said Section 29, a distance of 585.00 feet; thence North 02°21'38" West, departing said South line, a distance of 45.00 feet to the intersection of the North 87°18'59" West line of 87th Street with the Westery Right-of-Way line of Maurer Road, as said street and road are now established; thence continuing North 02°21'38" West, along said Westery Right-of-Way line, a distance of 84.66 feet, to a point of curvature; thence Northwesterly, continuing along said Westery Right-of-Way line, along a curve to the right, having a radius of 900.00 feet, and a central angle of 11°25'34", a distance of 179.48 feet; thence South 87°38'22" West, departing said Westery Right-of-Way line, a distance of 322.24 feet; thence North 02°18'59" West, a distance of 26.66 feet; thence South 87°38'22" West, a distance of 151.23 feet, to the Southeast corner of Lot 1, LENEXA POINT, FIRST PLAT, as recorded in Book 201705, Page 003248; thence North 02°21'38" West, along the East line of said Lot 1, a distance of 117.69 feet, to the Point of Beginning; thence South 87°38'22" West, along the South line of said Lot 1, a distance of 394.50 feet, to a point on the Eastern Right-of-Way line of Interstate Highway No. 435, as now established; thence North 21°04'38" West, along said Eastern Right-of-Way line, a distance of 117.69 feet; thence North 02°18'59" West, continuing along said Eastern Right-of-Way line, a distance of 234.69 feet, to the Southeast corner of Lot 1, CRESCENT APARTMENTS, as recorded in Book 101, Page 34; thence North 87°38'22" East, departing said Eastern Right-of-Way line, along the South line of said Lot 1, a distance of 310.29 feet, to a point on the Eastern line of a tract of land as described in the Kansas General Warranty Deed, recorded in Book 200404, Page 011824; thence South 02°21'38" East, departing said South line, along the Eastern line of said tract of land, a distance of 114.33 feet; thence North 87°38'22" East, continuing along the Eastern line of said tract of land, a distance of 190.00 feet; thence South 02°21'38" East, continuing along the Eastern line of said tract of land, a distance of 160.00 feet; thence South 87°38'22" West, continuing along the Eastern line of said tract of land, a distance of 74.30 feet; thence South 02°21'38" East, continuing along the Eastern line of said tract of land, a distance of 71.74 feet, to the Point of Beginning, containing 145,415.02 square feet, or 3.34 acres, more or less.

**Description:**

The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner shown on the accompanying plat, which hereafter shall hereinafter be known as "LENEXA POINT, SECOND PLAT".

The undersigned proprietor of said property shown on this plat does hereby authorize for public use and public ways and thoroughfares, all parcels and lots of land indicated on this plat as streets, avenues, places, roads, drives, lanes, avenues and alleys not heretofore dedicated. An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of conduits, pipes, inlets, manholes, surface drainage facilities, and other similar facilities, upon, over and under those areas outlined and designated on this plat as "Drainage Easement" or "D/E" is hereby granted to the City of Lenexa, Johnson County, Kansas, for its use. Drainage easements shall remain free of fences, shrubs, and other obstacles that would restrict the flow of drainage. The City of Lenexa is under no duty or obligation to maintain or repair the stormwater drainage facilities placed within the limits of this easement, and in no event shall this easement be construed to impose any such obligation on the City of Lenexa.

**Restrictions:**

The Property is subject to the Easements, Covenants and Restrictions Agreement recorded in Book 201702, Page 002551, of the Johnson County, Kansas records and that hereby becomes a part of the dedication of this plat as though fully set forth herein.

The Property is subject to Declaration of Restrictions and Grant of Easements recorded in Book 2852, Page 113, as amended by First Amendment to Declaration of Restrictions and Grant of Easement recorded in Book 3224, Page 975, and Second Amendment of Declaration of Restrictions, recorded in Book 201702, Page 002550. Paragraph 8 of this document grants a Perpetual Non-Exclusive Easement for storm sewers and the right to discharge and release drainage and storm water from said storm sewers into the Stormwater Detention Area to the North, shown on Exhibit "B" of document.

**Consent to Levy:**

The undersigned proprietor of the above described land hereby agrees and consents that the Board of County Commissioners of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to release such land proposed to be dedicated for public ways and thoroughfares, or parts thereof, for public use, from the lien and effect of any special assessment, and that the amount of unpaid special assessments on such land as dedicated shall become and remain a lien on the realtor of this land financing or abutting on such dedicated public way of thoroughfare.

**Execution:**

IN TESTIMONY WHEREOF, the undersigned proprietor has hereunto subscribed its name, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

Smart Storage Lenexa, LLC

Robert Brooks, Manager

STATE OF \_\_\_\_\_, S.S.

COUNTY OF \_\_\_\_\_, S.S.

BE IT REMEMBERED that on this \_\_\_\_ day of \_\_\_\_\_, 20\_\_, before me, the undersigned, a Notary Public in and for said County and State, personally appeared Robert Brooks, to me personally known to be the person described in and who executed the foregoing instrument on behalf of said Company, and he duly acknowledged the execution of the same to be the free act and deed of said city.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year last above written.

Notary Public \_\_\_\_\_ My Appointment Expires: \_\_\_\_\_

IN TESTIMONY WHEREOF, the undersigned proprietor has hereunto subscribed its name, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

Penn Properties Holding, L.L.C.

Jon Copaken, Administrative Member

STATE OF \_\_\_\_\_, S.S.

COUNTY OF \_\_\_\_\_, S.S.

BE IT REMEMBERED that on this \_\_\_\_ day of \_\_\_\_\_, 20\_\_, before me, the undersigned, a Notary Public in and for said County and State, personally appeared Jon Copaken, to me personally known to be the person described in and who executed the foregoing instrument on behalf of said Company, and he duly acknowledged the execution of the same to be the free act and deed of said city.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year last above written.

Notary Public \_\_\_\_\_ My Appointment Expires: \_\_\_\_\_

APPROVED by the Planning Commission of the City of Lenexa, Johnson County, Kansas, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

By: Chris Pass, Chairperson

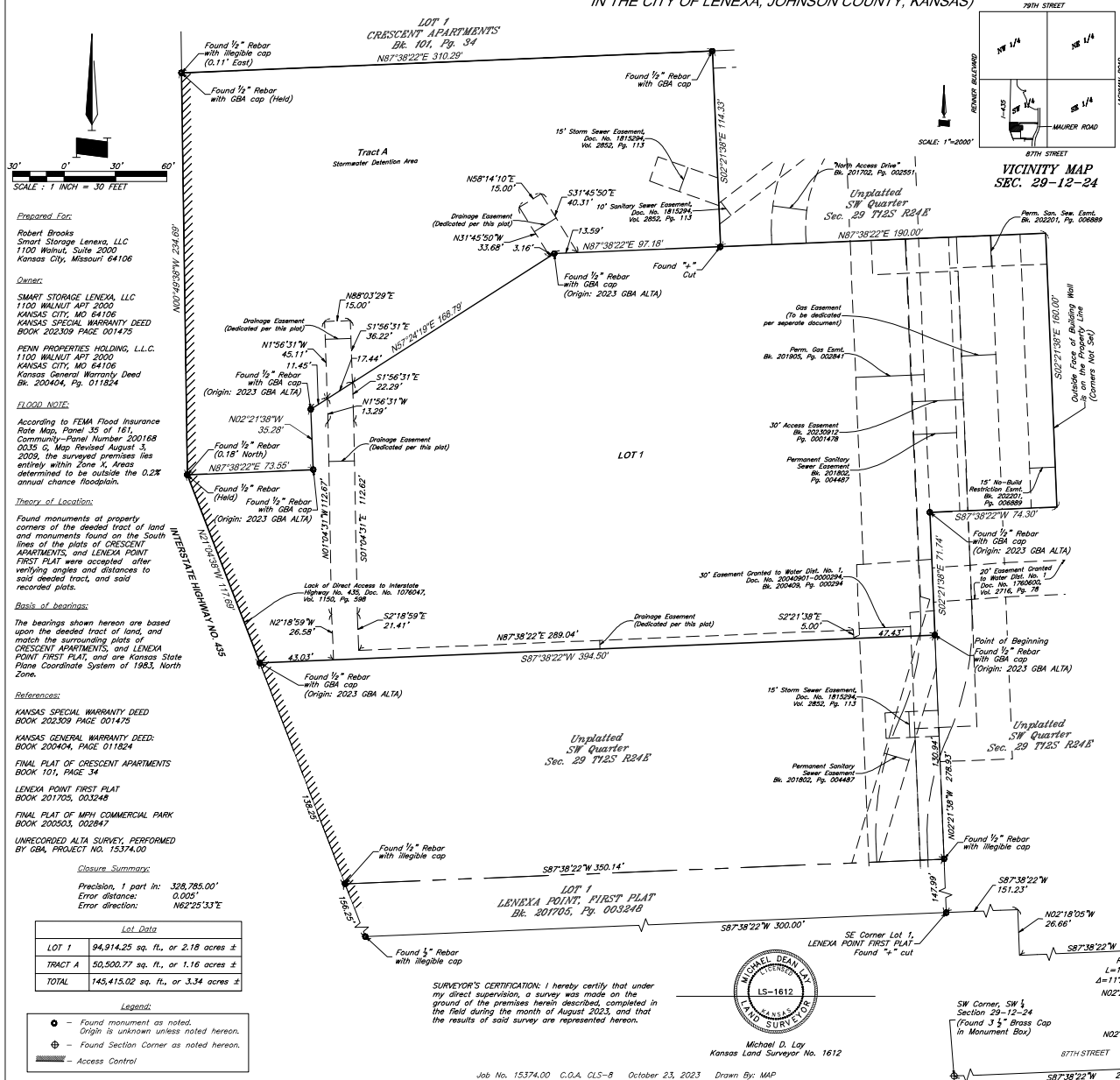
APPROVED by the City Council of the City of Lenexa, Johnson County, Kansas, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

By: Michael Boehm, Mayor

ATTEST: Jennifer Martin, City Clerk



**FINAL PLAT OF  
LENEXA POINT, SECOND PLAT  
Section 29, Township 12, Range 24  
City of Lenexa, Johnson County, Kansas**



**Prepared For:**  
Robert Brooks  
Smart Storage Lenexa, LLC  
1100 Walnut, Suite 2000  
Kansas City, Missouri 64106

**Owner:**  
SMART STORAGE LENEXA, LLC  
1100 WALNUT APT 2000  
KANSAS CITY, MO 64106  
KANSAS SPECIAL WARRANTY DEED  
BOOK 202309 PAGE 001475

**PERM PROPERTIES HOLDING, L.L.C.**  
1100 WALNUT APT 2000  
KANSAS CITY, MO 64106  
KANSAS GENERAL WARRANTY DEED  
BK. 200404, Pg. 011824

**LEGEND:**  
According to FEMA Flood Insurance Rate Map, Panel 35 of 161, Community-Panel Number 200168 0035 G, Map Revised August 3, 2008, the surveyed premises lies entirely within Zone X, Areas determined to be outside the 0.2% annual chance floodplain.

**Theory of Location:**  
Found monuments at property corners of the deeded tract of land and monuments found on the South lines of the plots of CRESCENT APARTMENTS, and LENEXA POINT FIRST PLAT, were accepted after verifying angles and distances to said deeded tract, and said recorded plots.

**Basis of bearings:**

The bearings shown hereon are based upon the deeded tract of land, and match the surrounding plots of CRESCENT APARTMENTS, and LENEXA POINT FIRST PLAT, and are Kansas State Plane Coordinate System of 1983, North Zone.

**References:**

- KANSAS SPECIAL WARRANTY DEED BOOK 202309 PAGE 001475
- KANSAS GENERAL WARRANTY DEED BOOK 200404, PAGE 011824
- FINAL PLAT OF CRESCENT APARTMENTS BOOK 101, PAGE 34
- LENEXA POINT FIRST PLAT BOOK 201705, 003248
- FINAL PLAT OF MPH COMMERCIAL PARK BOOK 200503, 002847
- UNRECORDED ALTA SURVEY, PERFORMED BY GBA, PROJECT NO. 15374.00

**Closure Summary:**

Precision, 1 part in: 328,785.00'  
Error distance: 0.005'  
Error direction: N02°25'33"E

Lot Data	Area
LOT 1	94,914.25 sq. ft., or 2.18 acres ±
TRACT A	50,500.77 sq. ft., or 1.16 acres ±
TOTAL	145,415.02 sq. ft., or 3.34 acres ±

**Legend:**

- Found monument as noted. Origin is unknown unless noted hereon.
- ⊙ Found Section Corner as noted hereon.
- Access Control

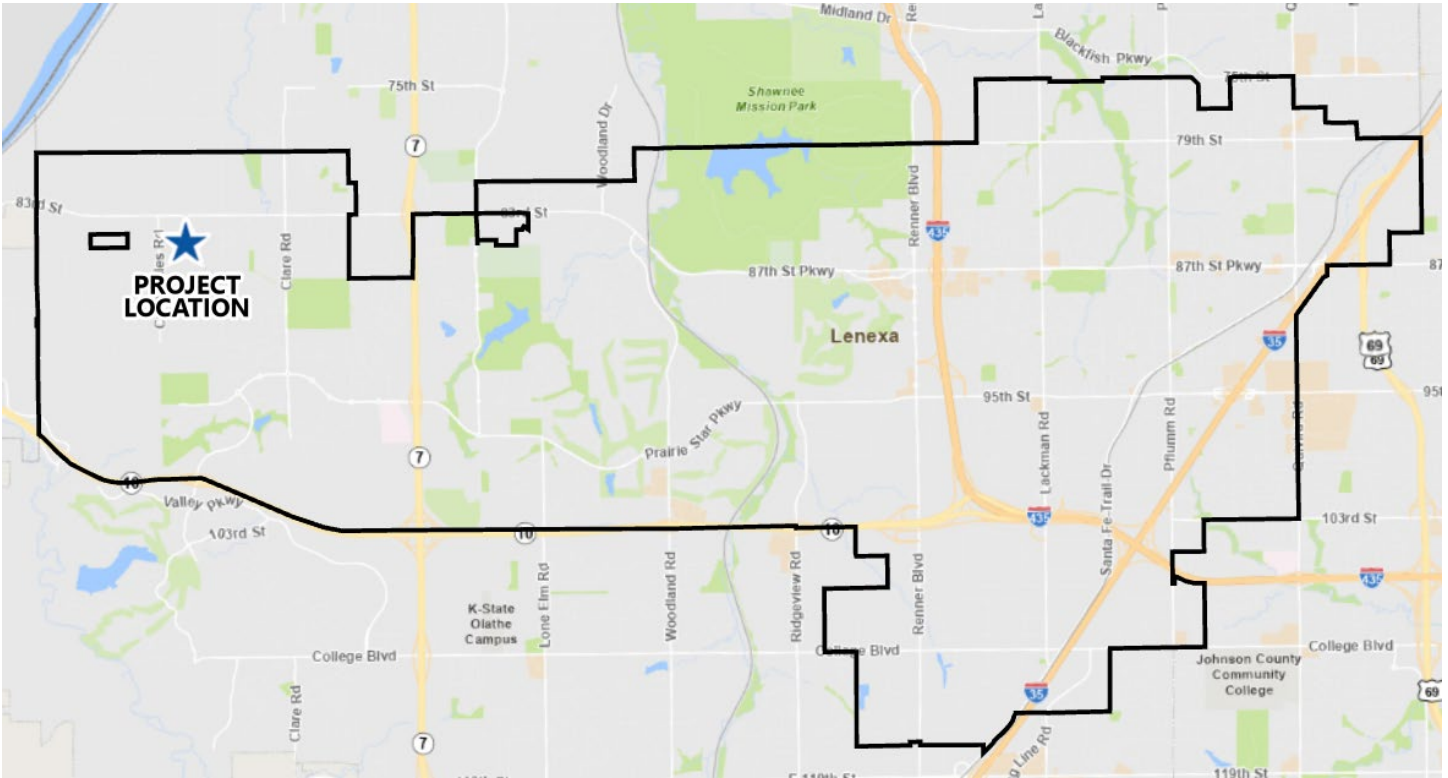
**SURVEYOR'S CERTIFICATION:** I hereby certify that under my direct supervision, a survey was made on the ground of the premises herein described, completed in the field during the month of August 2023, and that the results of said survey are represented hereon.



Michael D. Lay  
Kansas Land Surveyor No. 1612

## VISCEK ESTATES

<b>Project #:</b>	PT23-25F	<b>Location:</b>	25925 W. 83 <sup>rd</sup> Street
<b>Applicant:</b>	Matt Cox, Allenbrand-Drews	<b>Project Type:</b>	Final Plat
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Single-Family Residential



### PROJECT SUMMARY

The applicant requests approval of a final plat containing 2.65 acres with one, two-acre lot and rights-of-way. The lot contains one house and will remain as a single-family use. The plat includes dedication of rights-of-way and easements for landscaping and utilities. The proposed final plat is consistent with the approved preliminary plat (PT23-05P), which was approved by the Governing Body on October 17, 2023. This project does not require a Public Hearing.

### STAFF RECOMMENDATION: APPROVAL

## SITE INFORMATION

This site was annexed into the City of Lenexa in the late 1980s and has held an agricultural zoning classification and use since. One home, built in 1910, is located at the northeast corner of the property. A preliminary plat of ‘Viscek Estates’ and rezoning of Lot 1 from the AG, Agricultural Zoning District, to the RP-1, Planned Residential Single-Family (Low-Density) Zoning District, was approved by the Governing Body on October 17, 2023.

<b>LAND AREA (AC)</b> 2.65	<b>BUILDING AREA (SF)</b> N/A	<b>CURRENT ZONING</b> RP-1	<b>COMP. PLAN</b> Suburban Residential
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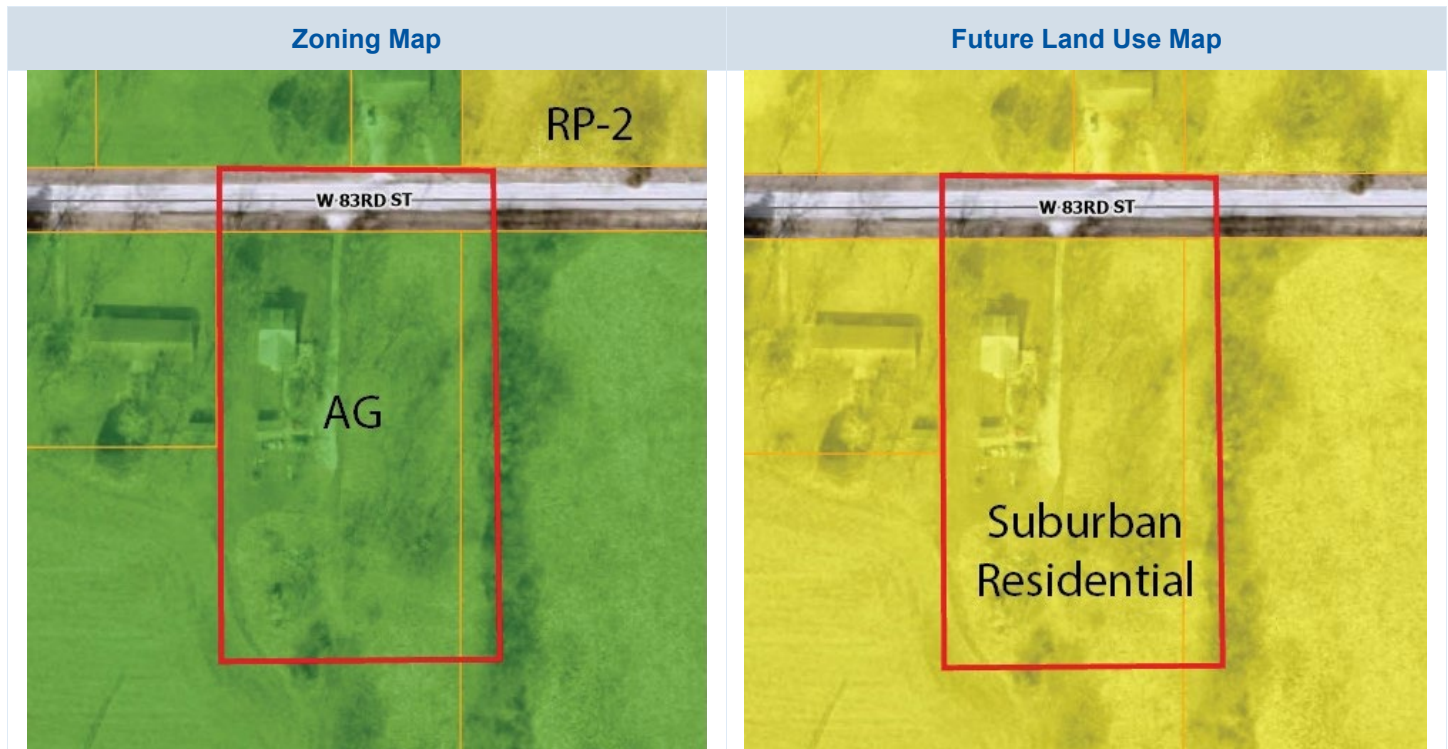
*Exhibit 1: Aerial Image of Subject Site*



*Exhibit 2: Street View of existing home and site.*

## LAND USE REVIEW

The property is zoned RP-1, Planned Residential Single-Family. The proposed use is compatible with existing and planned surrounding uses which include agricultural uses with single-family, the planned Stoneridge single-family subdivision to the west and planned Stoneridge North single-family and two-family development to the north. The future land use of Suburban Residential allows a density of up to 3.5 dwelling units per acre.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low-Density) District	Single-Family Residential
<b>North</b>	Suburban Residential	AG, Agricultural District	Single-Family Residential
<b>South</b>	Suburban Residential	AG, Agricultural District	Agricultural
<b>East</b>	Suburban Residential	AG, Agricultural District	Agricultural
<b>West</b>	Suburban Residential	AG, Agricultural District	Agricultural

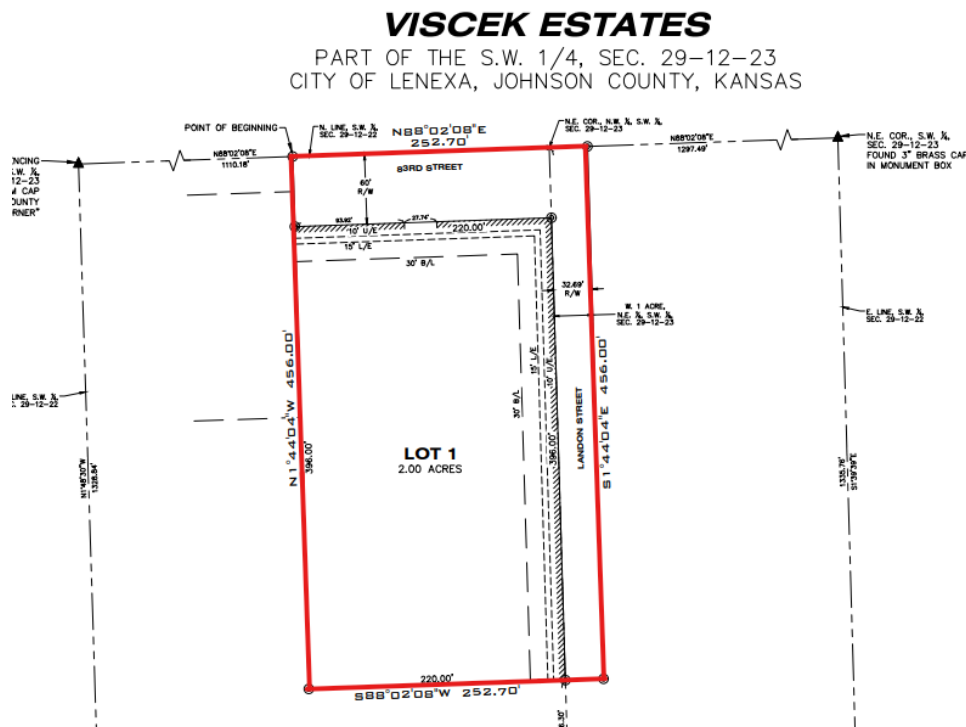
## FINAL PLAT REVIEW

This is a final plat of 2.65 acres for one lot in the RP-1, Planned Residential Single-Family (Low-Density) Zoning District. No improvements are proposed as a part of this project.

### Lot 1:

- 2.0 acres
- Remaining a single-family residential use with one dwelling
- Zoned RP-1, Planned Residential Single-Family
- Access is available from W. 83<sup>rd</sup> Street. When the future street network is developed for the currently undeveloped surrounding area, it is planned that the lot will then be accessed from a future local street to the south or east of the lot. This plat reflects a limit of no access along the planned right-of-way to the east (future Landon Street) and along W. 83<sup>rd</sup> Street, with the exception of the existing driveway location. It is anticipated that the limits of no access will change with future development to facilitate a new driveway location and close the driveway access from W. 83<sup>rd</sup> Street.
- A 15' landscape easement and 10' utility easement will be dedicated to the City of Lenexa along W. 83<sup>rd</sup> Street and along the future right-of-way to the east. It shall be the responsibility of the landowner to maintain the easements.
- The lot does not have sanitary sewer service but instead has an existing septic system. The lot area is two acres to meet the minimum requirements to have a septic system as set forth by Johnson County Wastewater.

The remaining 0.65 acre of site area will be dedicated to the City of Lenexa as rights-of-way for future street improvements as shown on the plat. This dedication requires acceptance by the Governing Body at the time of final plat.



*Exhibit 3: Viscek Estates Final Plat*

## DEVIATIONS

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The applicant is not requesting any deviations.

## REVIEW PROCESS

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- This project requires approval from the Planning Commission and acceptance of dedications by the City Council. Pending approval from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on November 21, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

## RECOMMENDATION FROM PROFESSIONAL STAFF

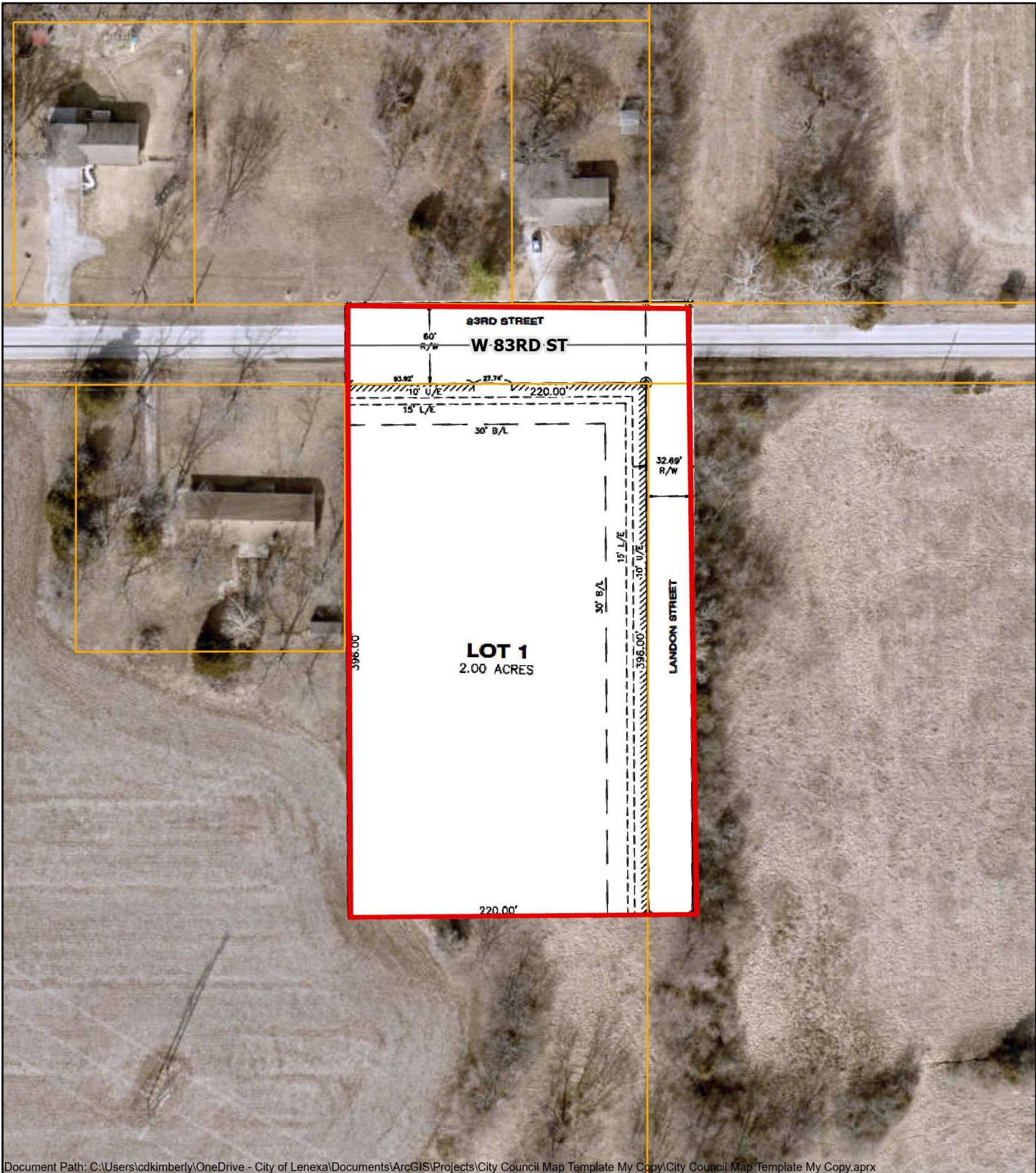
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★ **Staff recommends approval of the proposed Final Plat for Viscek Estates.**

- This is a final plat for one, single-family lot known as Viscek Estates.
- The project is consistent with Lenexa's goals through ***Responsible Economic Development*** to create ***Vibrant Neighborhoods***.

## FINAL PLAT

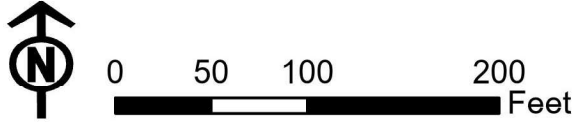
Staff recommends **approval** of the final plat for PT23-25F - **Viscek Estates** at 25925 W. 83<sup>rd</sup> Street, for a single-family residential use.



Document Path: C:\Users\cdkimberly\OneDrive - City of Lenexa\Documents\ArcGIS\Projects\City Council Map Template My Copy\City Council Map Template My Copy.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Final Plat of Viscek Estates PT23-25F



FINAL PLAT OF  
**VISCEK ESTATES**  
 PART OF THE S.W. 1/4, SEC. 29-12-23  
 CITY OF LENEXA, JOHNSON COUNTY, KANSAS

LEGAL DESCRIPTION

All that part of the Northwest Quarter of the Southwest Quarter and the West 1.00 acre of the Northeast Quarter of the Southwest Quarter of Section 29, Township 12 South, Range 23 East, in the City of Lenexa, Kansas, being more particularly described by Matthew R. Cox, P.S.-1637 on September 18, 2022 as follows:

Commencing at the Northwest corner of the Southwest Quarter of said Section 29; thence North 88 degrees 02 minutes 08 seconds East, along the North line of the Southwest Quarter of said Section 29, a distance of 1110.18 feet to the point of beginning; thence continuing North 88 degrees 02 minutes 08 seconds East, along the North line of the Southwest Quarter of said Section 29, a distance of 252.70 feet; thence South 1 degree 44 minutes 04 seconds East, parallel with and 32.69 feet East of the West line of the Northwest Quarter of the Southwest Quarter of said Section 29, a distance of 456.00 feet; thence South 88 degrees 02 minutes 08 seconds West, a distance of 252.70 feet; thence North 1 degree 44 minutes 04 seconds West, a distance of 456.00 feet to the point of beginning, containing 2.65 acres, more or less.

DEDICATION

The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner shown on the accompanying plat which hereafter shall be known as "VISCEK ESTATES".

The streets, terraces and roads shown on this plat and not heretofore dedicated for public use and public ways and thoroughfares are hereby dedicated to the City of Lenexa, Johnson County, Kansas for its use and that of its designees or duly authorized agents.

An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of conduits, pipes, poles, wires, drainage facilities, ditches and cables, and similar facilities, upon, over and under those areas outlined and designated on this plat as "Utility Easement" or "U/E" is hereby granted to the City of Lenexa, Johnson County, Kansas.

The undersigned proprietor of the above described land does hereby grant an easement to the City of Lenexa, Johnson County, Kansas, to enter upon, locate, or plat soil, trees, bushes, shrubs or fences within those areas indicated on this plat as "Landscape Easement" or "L/E". The owners of the lots and tracts shown herein shall have the responsibility to replant, replace, mow, clip, trim, spray, chemically treat, repair, and otherwise maintain any grass, trees, shrubs, plants and other landscaping within the aforementioned "Landscape Easement" or "L/E".

All existing easements or rights-of-way heretofore dedicated shall remain and be unaffected with the filing of this plat.

CONSENT TO LEVY

The undersigned proprietor of the above described land hereby consents and agrees that the Board of County Commissioners of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to release such land proposed to be dedicated for public use from the lien and effect of any special assessments, and that the amount of the unpaid special assessments on such land dedicated shall become and remain a lien on the remainder of this land fronting and abutting on such dedicated public way or thoroughfare.

EXECUTION

IN TESTIMONY WHEREOF, Unified School District #232 has caused this instrument to be executed this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

By: \_\_\_\_\_  
 Cory Gibson, USD 232 Superintendent

STATE OF KANSAS )  
 ) SS:  
 COUNTY OF JOHNSON )

BE IT REMEMBERED that on this \_\_\_\_ day of \_\_\_\_\_, 20\_\_ before me, the undersigned, a Notary Public in and for said County and State, came Cory Gibson, Superintendent of Unified School District 232, who is personally known to me to be such person who executed the within instrument.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my notarial seal the day and year last above written.

Notary Public: \_\_\_\_\_ My appointment expires: \_\_\_\_\_

APPROVALS

APPROVED BY the Planning Commission of the City of Lenexa, Kansas, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

By: \_\_\_\_\_  
 Chris Poss, Chairman

APPROVED BY the Governing Body of the City of Lenexa, Kansas, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

By: \_\_\_\_\_ Attest: \_\_\_\_\_  
 Michael A. Boehm, Mayor Jennifer Martin, City Clerk

Classification Note: The minimum ground floor area classification for ICR in this plat is Class "J".

**VISCEK ESTATES**

**CIVIL ENGINEERS**  
**LAND SURVEYORS - LAND PLANNERS**

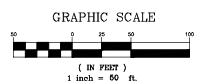
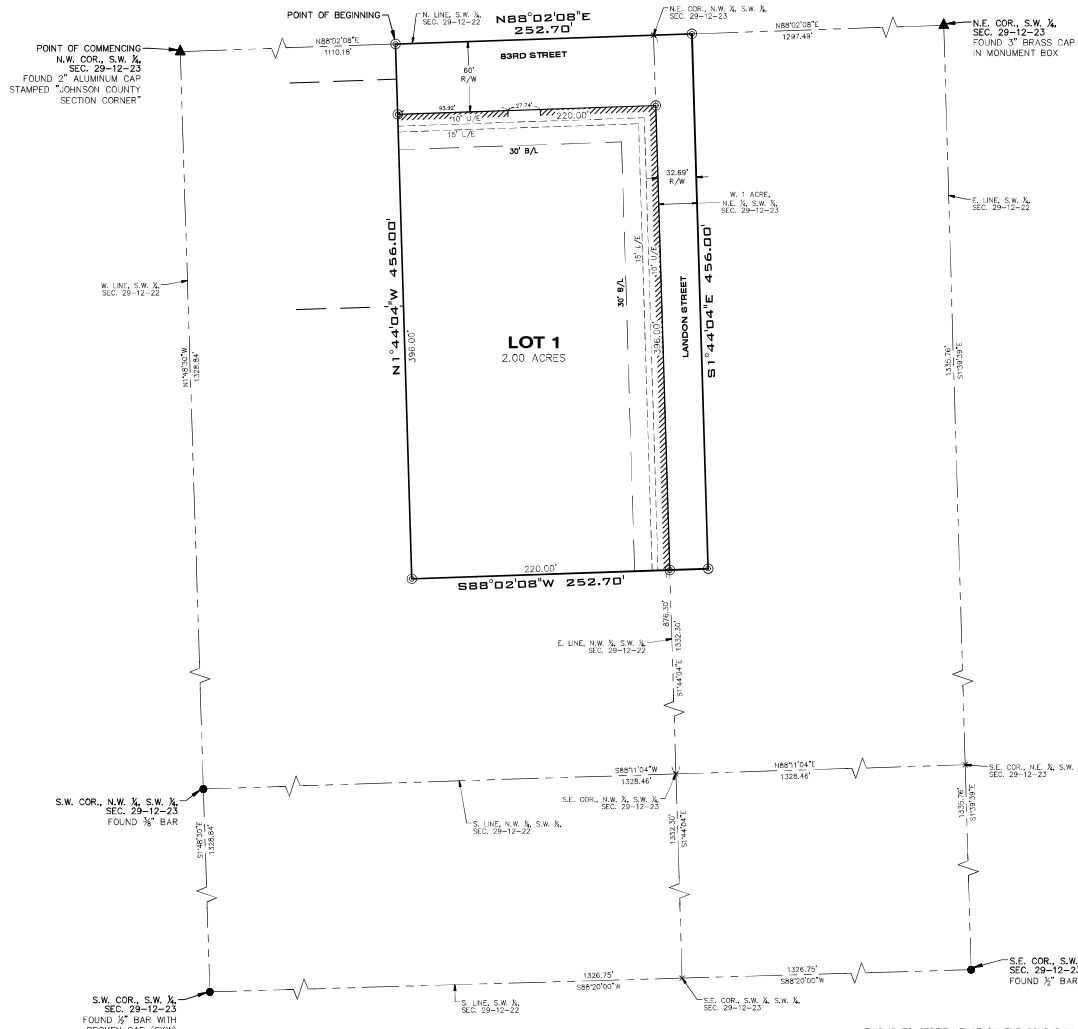


122 N. WATER STREET  
 OLATHE, KANSAS 66061  
 PHONE: (913) 764-1076  
 FAX: (913) 764-8635

14 W. PEORIA  
 PAOLA, KANSAS 66071  
 PHONE: (913) 557-1076  
 FAX: (913) 557-6904

AD PROJECT #38619 29-12-23 FINAL PLAT

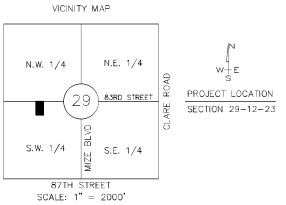
THIS IS TO CERTIFY THAT ON THE 22ND DAY OF SEPTEMBER 2023, THIS FIELD SURVEY WAS COMPLETED ON THE GROUND BY ME OR UNDER MY DIRECT SUPERVISION AND THAT SAID SURVEY MEETS OR EXCEEDS THE "KANSAS MINIMUM STANDARDS" FOR BOUNDARY SURVEYS PURSUANT TO K.A.R. 66-12-1.



- NOTES:
1. Basis of Bearings: Kansas State Plane, North Zone
  2. All bearings and distances shown are record and measured unless otherwise noted.

- LEGEND
- ▲ MONUMENT FOUND AS DESCRIBED
  - BAK FOUND AS DESCRIBED
  - UNKNOWN ORIGIN UNLESS NOTED
  - ⊗ SET 1/2" X 24" REBAR WITH PLASTIC IS (LS 93 CAP (N CONC)
  - B/L BUILDING LINE
  - U/E UTILITY EASEMENT
  - L/E LANDSCAPE EASEMENT
  - R/W RIGHT-OF-WAY
  - ////// LIMITS OF NO ACCESS

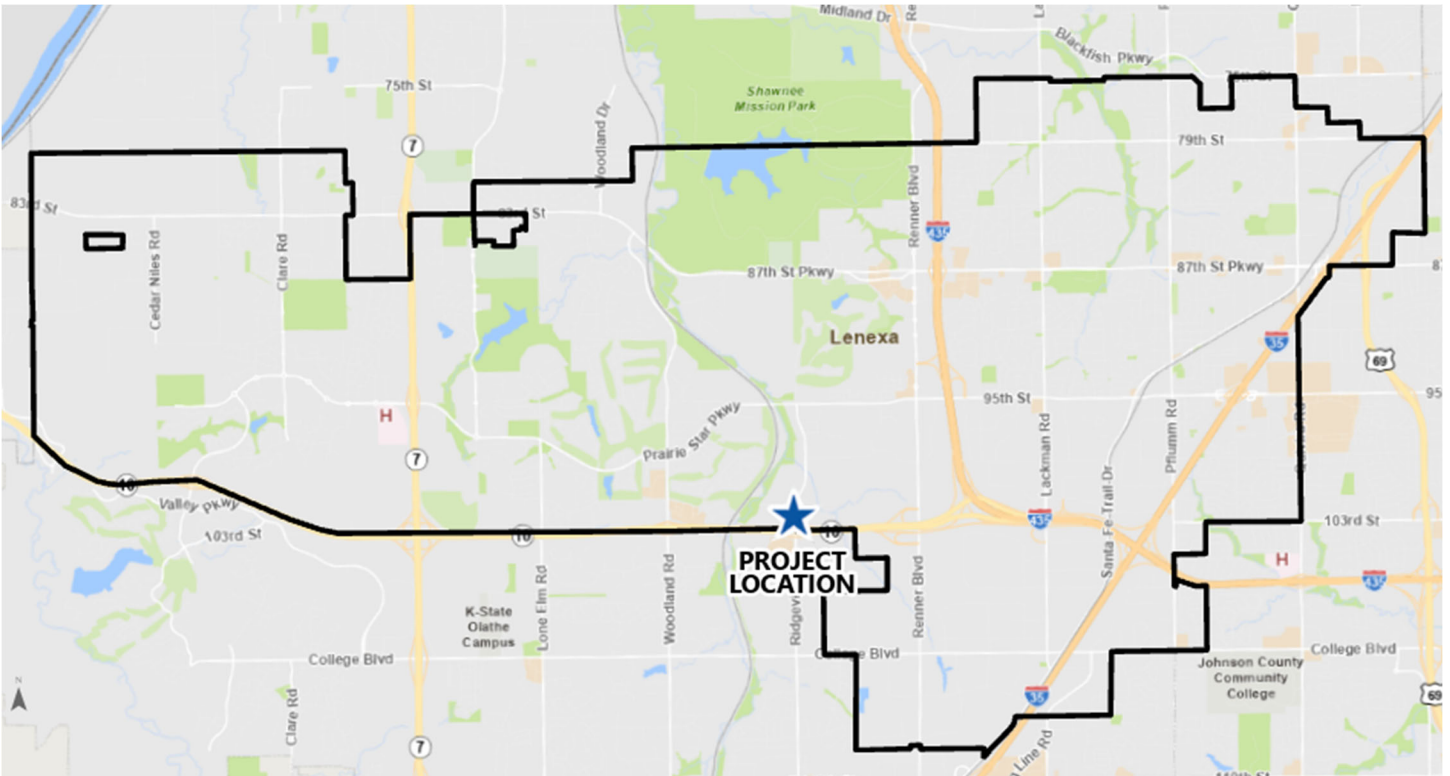
Error of Closure:  
 Perimeter: 1417.40'  
 Error Closure: 0.0000 Course: N00°00'00"E  
 Error North: 0.00000 East: 0.00000  
 Precision 1:417400000.00





## VISTA RIDGE, 2<sup>ND</sup> PLAT

<b>Project #:</b>	PT23-26F	<b>Location:</b>	Near the Northeast Corner of K-10 and Ridgeview Road
<b>Applicant:</b>	Chad Porter, Payne & Brockway	<b>Project Type:</b>	Final Plat
<b>Staff Planner:</b>	Will Sharp	<b>Proposed Use:</b>	Commercial Development



### PROJECT SUMMARY

The applicant requests approval of a final plat of 2.17 acres for commercial development located on the northeast corner of K-10 and Ridgeview Road. The proposed final plat is a replat of a portion of the previously approved Vista Ridge, 1<sup>st</sup> Plat (PT22-02FR). The new plat will create two new lots of the existing Lot 2. The new lots are Lot 6 and Lot 7 to be consistent with the existing Vista Ridge, 1<sup>st</sup> Plat lot numbering scheme. The location of the new property line of this final plat generates a request for a setback deviation for two buildings as approved on the final plan for the newly created Lots 6 and 7. This project does not require a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

The subject property is located on the northeast corner of K-10 and Ridgeview Road. Vista Ridge, 1<sup>st</sup> Plat was approved in 2022 (PT22-02FR). The site is zoned CP-3, Planned Regional Commercial District (RZ14-06), and is designated for Mixed Use development on the Future Land Use Map.

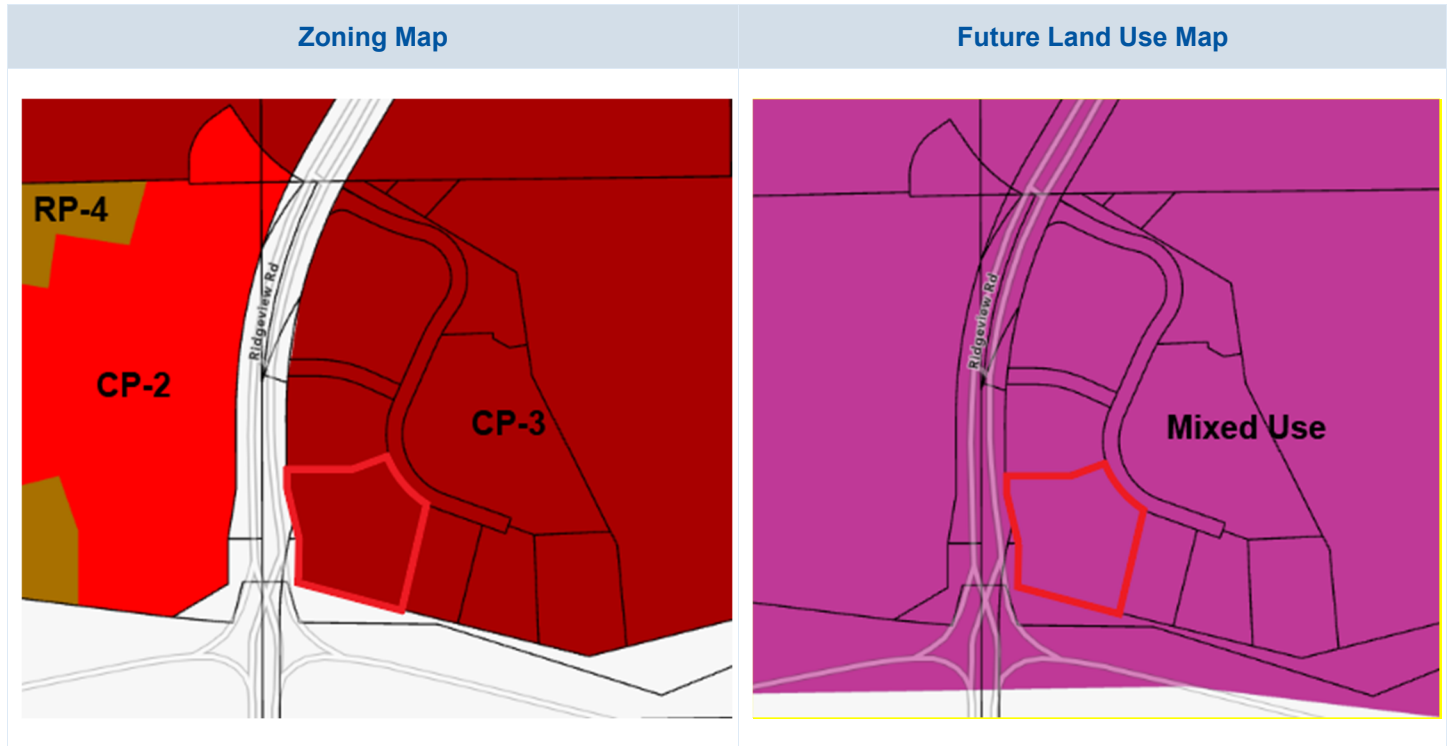
LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
2.17	N/A	CP-3	Mixed Use



**Exhibit 1: Aerial image of subject site.**

## LAND USE REVIEW

The proposed application is for a final plat that divides an existing lot into two lots for commercial development. Commercial development is permitted in the CP-3, Planned Regional Commercial District.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Mixed Use	CP-3	Undeveloped
<b>North</b>	Mixed Use	CP-3	Undeveloped
<b>South</b>	<i>Employment Area City of Olathe Across K-10 Highway</i>	<i>BP, Business Park City of Olathe Across K-10 Highway</i>	<i>Hotel and Conference Center City of Olathe Across K-10 Highway</i>
<b>East</b>	Mixed Use	CP-3	Restaurant
<b>West</b>	Mixed Use	CP-2	Undeveloped

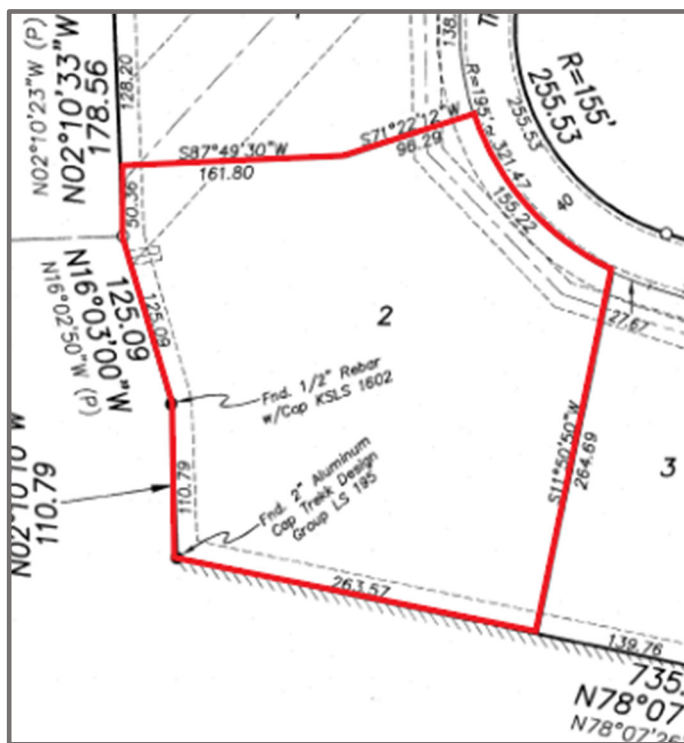
The proposed use is compatible in the context of existing land uses. Commercial uses are compatible with the area's designation as Mixed Use on the Future Land Use Map in the Comprehensive Plan. Current development in the vicinity is in accordance with the Comprehensive Plan.

## FINAL PLAT REVIEW

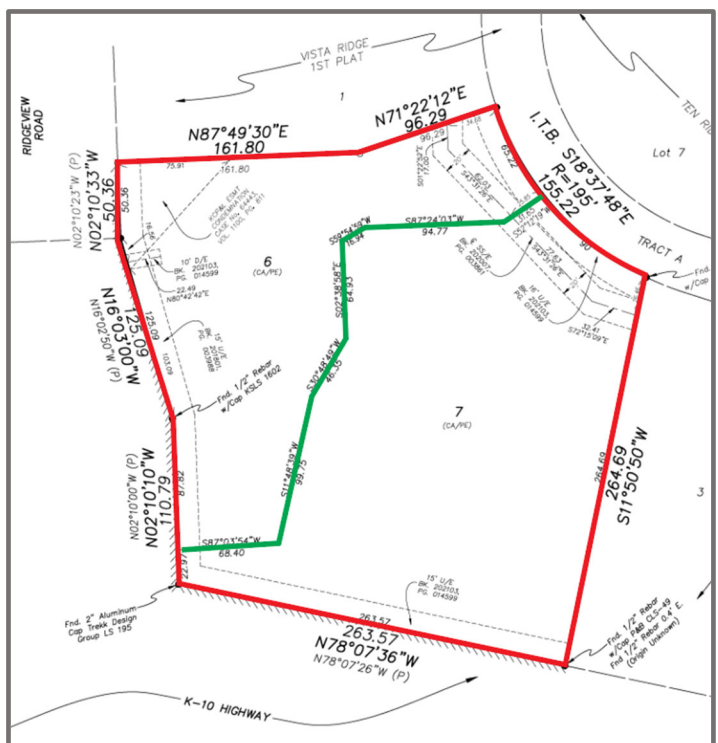
This final plat is a replat of a portion of PT22-02FR – Vista Ridge, 1<sup>st</sup> Plat. The proposed plat consists of two lots. Lot 6 is 37,330.4 SF (0.86 acres) and Lot 7 is 57,147.2 SF (1.31 acres), for a total land area of 2.17 acres.

Access to the site is on the northeast property line from W 101<sup>st</sup> Street, which is classified as a Local roadway. There are restrictions for access onto Ridgeview Road and K-10 Highway.

There are several existing easements on the plat. A cross-access/parking easement between Lots 6 and 7 is new with this proposal. There are no new dedications that require acceptance by the City.



**Exhibit 2: Vista Ridge, 1<sup>st</sup> Plat (Approved)**



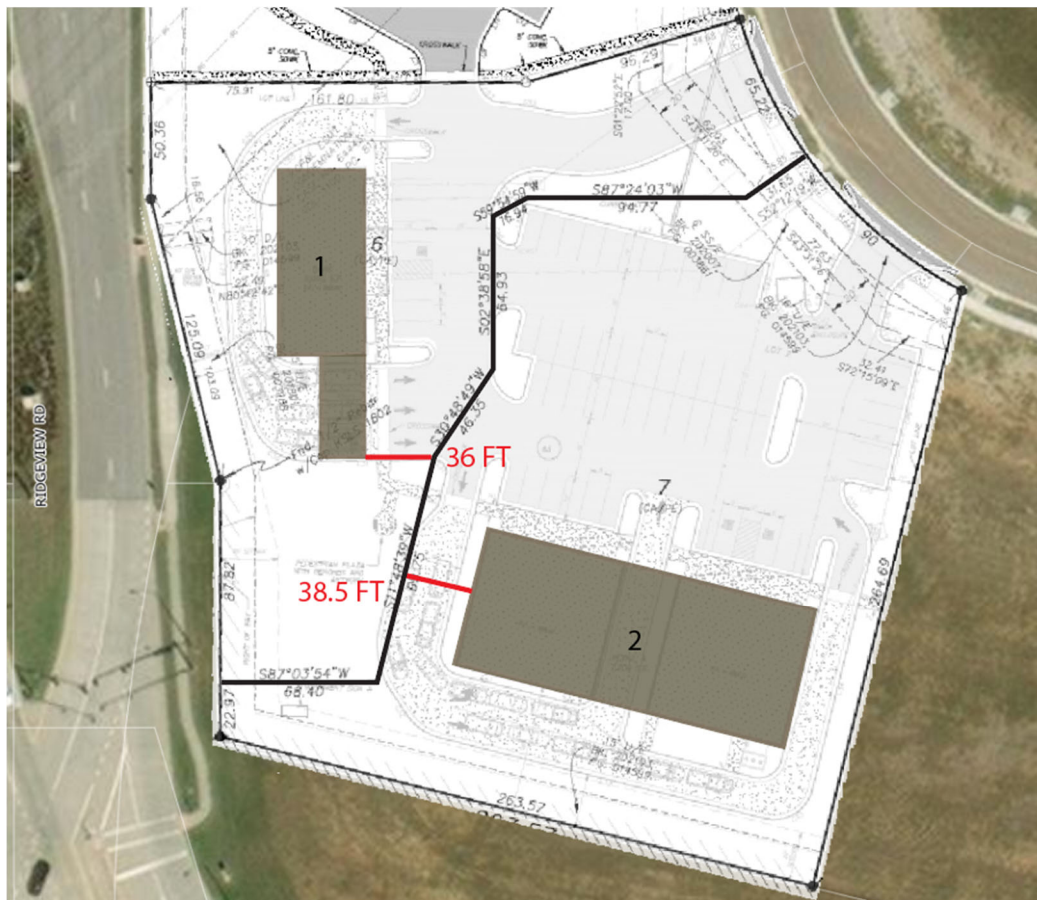
**Vista Ridge, 2<sup>nd</sup> Plat (Proposed)**

## DEVIATIONS

The result of the division of the existing lot into two lots is that the building approved with final plan PL22-08F will require a setback deviation because the buildings are now closer to the property line. Section 4-1-B-15-F of the Unified Development Code (UDC) states that a 50-foot building setback is required from the property line. Multiple deviations were granted to allow a reduced setback for buildings in this development at the time of the preliminary plan consideration. These setbacks were not part of the original consideration; therefore, the setback deviation is to be considered as part of this replat. Staff is supportive of this deviation request.

TABLE 2: SETBACK ANALYSIS			
Type	Required Minimum Setback	Proposed Setback	Difference
Building setback (Building 1)	50 ft	36 ft at closest point	14 ft
Building setback (Building 2)	50 ft	38.5 ft at closest point	11.5 ft

This development contains uses that are more typical of the CP-2 Zoning District, such as restaurants and service-oriented uses. The CP-2 Zoning District has a required setback of 30 feet. At its closest point, Building 1 is 36 feet and Building 2 is 38.5 ft from the proposed lot line, as shown in Exhibit 3 below.



**Exhibit 3: Approved Final Plan (PL22-08F) with the proposed plat superimposed.**

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## REVIEW PROCESS

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- *The Planning Commission is the final authority for approval of this project.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

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## RECOMMENDATION FROM PROFESSIONAL STAFF

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★ **Staff recommends approval of the proposed final plat for Vista Ridge, 2<sup>nd</sup> Plat.**

- The applicant requests approval of a final plat of 2.17 acres for commercial development located on the northeast corner of K-10 and Ridgeview Road. The new plat will create two new lots with a shared cross access/parking easement.
- The project is consistent with Lenexa's goals through ***Responsible Economic Development*** to create ***Thriving Economy*** and ***Vibrant Neighborhoods***.

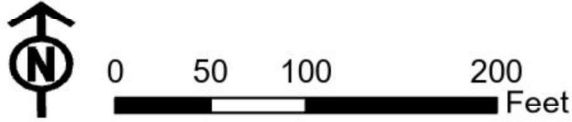
## FINAL PLAT

Staff recommends **approval** of the final plat for PT23-26F – **Vista Ridge, 2<sup>nd</sup> Plat** near the northeast corner of K-10 Highway and Ridgeview Road for commercial development, with deviations from the required building setback as described in the Staff Report.



Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Vista Ridge, 2nd Plat PT23-26F



REVISIONS			
NO.	DATE	DESCRIPTION	BY
1	9/29/23	completed	CTA
2	9/29/23	city comments	CTA

# FINAL PLAT OF VISTA RIDGE, 2ND PLAT A Replat of All of Lot 2, VISTA RIDGE, 1ST PLAT

This is a replat and resurvey of All of Lot 2, VISTA RIDGE, 1ST PLAT, a subdivision in the City of Lenexa, Johnson County, Kansas, prepared by Gerald L. Conn, P.S. No. 1128, September 29, 2023, containing 94,477.6 square feet, more or less.

The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner as shown on the accompanying plat, which hereafter shall be known as "VISTA RIDGE, 2ND PLAT".

An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of streets, curbs, sidewalks, jogging trails, conduits, pipes, manholes, inlets, manholes, surface drainage facilities, utilities, landscaping and other similar facilities upon, over, and under those areas outlined and designated on this plat as "Utility Easement" or "U/E" is hereby granted to the City of Lenexa, Johnson County, Kansas, for its use and that of its designers or duly authorized agents. A "Utility Easement" or "U/E" off lots adjacent and parallel with all street right-of-way lines, when shown hereon, is hereby dedicated to the City of Lenexa, Johnson County, Kansas, for its use and that of its designees or duly authorized agents, for the purposes previously described. No above ground facilities associated with the uses herein permitted shall be constructed, located or maintained in any area of the U/E that crosses or intersects with a drainage easement "D/E".

An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of conduits, pipes, inlets, manholes, surface drainage facilities and other similar facilities, upon, over and under those areas outlined and designated on this plat as "Drainage Easements" or "D/E" is hereby granted to the City of Lenexa, Kansas. Drainage Easements shall remain free of fences, shrubs, and other obstacles that would restrict the flow of drainage. The City of Lenexa is under no duty or obligation to maintain or repair the stormwater drainage facilities pieces within the limits of this easement, and in no event shall this easement be construed to impose any such obligation on the City of Lenexa.

An easement or license to lay, construct, maintain, alter, repair, replace and operate one or more sewer lines and all appurtenances convenient for the collection of sanitary sewage, together with the right of ingress and egress, over and through those areas designated as "Sanitary Sewer Easement" or "SSE" on this plat, together with the right of ingress and egress over and through adjoining land as may be reasonably necessary to access said easement and is hereby granted to the Consolidated Man Sewer District of Johnson County, Kansas or their assigns. Alteration of land contours will be permitted only with the express written approval of COW. Any planting of improvements or planting of trees on said permanent right-of-way will be done at the risk of subsequent damage thereto without compensation thereof.

A perpetual easement of access over, under, across, and upon the areas designated as "Cross Access/Parking Easement" or "CA/PE" and Lots 6 and 7 is hereby reserved to the undersigned proprietor, his heirs, assigns, etc., for ingress and egress of all persons and occupants of lots and parcels depicted on this plat, their guests and invitees. Owners and occupants of adjoining land to the North, South, East, and West of the plat, as well as other properties within the development shall have the same right of ingress and egress as all owners and occupants of lots and parcels depicted on this plat. No obstacles or obstructions that would prohibit the free flow of traffic shall be allowed to be constructed or maintained within said "Cross Access/Parking Easement".

The use of all lots in this subdivision shall hereafter be subject to the restrictions which will be executed as a separate instrument of writing and will be recorded in the Office of the Register of Deeds of Johnson County, Kansas, said restrictions will thereby be made a part of the dedication of this plat as though fully set forth herein.

The undersigned proprietor of the above described tract of land hereby consents and agrees that the Board of County Commissioners of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to release such land proposed to be dedicated for public use, from the lien and effect of any special assessments and that the amount of unpaid special assessments on such land dedicated, shall become and remain a lien on the remainder of this land fronting or abutting on said dedicated public ways or thoroughfares.

IN TESTIMONY WHEREOF, the undersigned proprietor has caused this instrument to be executed this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

TEN RIDGE, LLC

\_\_\_\_\_  
MICHAEL A. CHRISTIE, Manager

STATE OF \_\_\_\_\_ (KANSAS) SS  
COUNTY OF \_\_\_\_\_ (JOHNSON)

BE IT REMEMBERED, that on this \_\_\_\_\_ day of \_\_\_\_\_, 2023, before me a Notary Public in and for said County and State, came MICHAEL A. CHRISTIE, Manager of TEN RIDGE, LLC, who is personally known to me to be the same person who executed the foregoing instrument of writing on behalf of said Corporation, and he duly acknowledged the execution of the same to be the act and deed of said Corporation.

IN WITNESS WHEREOF, I have hereunto set my hand and seal on the day and year last written above.

\_\_\_\_\_  
Notary Public My Appointment Expires \_\_\_\_\_

APPROVED by the Planning Commission of the City of Lenexa, Kansas, on this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

By: \_\_\_\_\_  
CHRIS POSS, Chairman

I hereby certify that on August 8, 2023, I or someone under my direct supervision completed a field survey of the above described tract of land and this is an accurate and true plat of said survey, on this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

Gerald L. Conn  
Kansas P.S. No. 1128

Prepared by: Payne & Brockway Engineers, P.A.  
P. O. Box 128  
Olathe, Kansas 66051  
913-782-4800

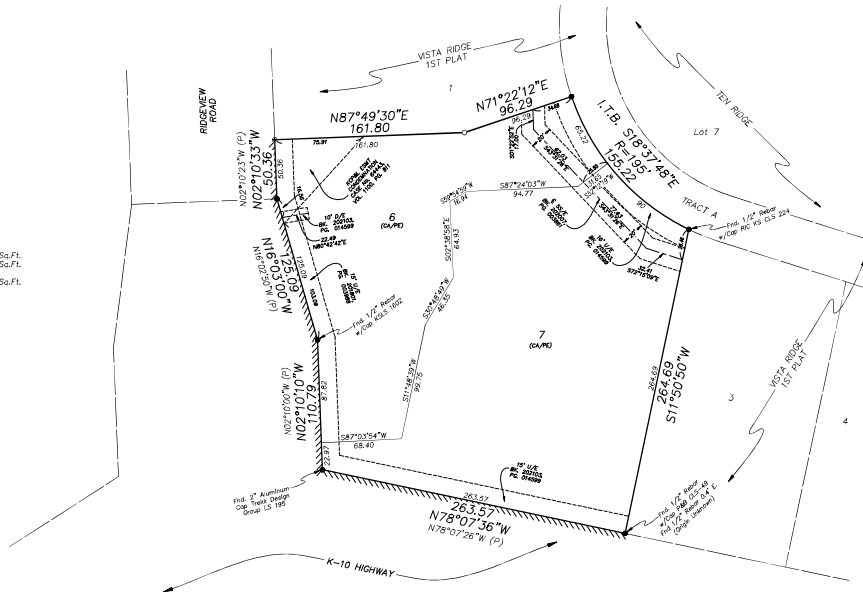
Developed by: Ten Ridge, LLC  
c/o Michael A. Christie, Manager  
10550 S. Warwick Street  
Olathe, Kansas 66061  
913-498-8000

Date Prepared: September 29, 2023

**VISTA RIDGE  
2ND PLAT  
SEC. 6, T13S, R24E  
JOHNSON COUNTY, KANSAS**



LOT AREAS  
LOT 6 37,330.4 Sq.Ft.  
LOT 7 57,147.2 Sq.Ft.  
TOTAL 94,477.6 Sq.Ft.



**NOTES:**

The basis for bearings on this plat is Grid North, Kansas North Zone NAD83.

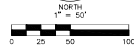
In approving this plat by the City of Lenexa and recording this plat with the Office of the Register of Deeds, restrictions and covenants, building lines and lot lines that lie within those parts of VISTA RIDGE, 1ST PLAT being replatted, are hereby now vacated, except as shown on this plat.

Lot 6 shall have No Direct Vehicular Access to Ridgeview Road. Lots 6 and 7 shall have No Direct Vehicular Access to K-10 Highway.

According to F.I.R.M. Map No. 2009\*CD0490 Johnson County, Kansas, dated August 3, 2009 this entire site is determined to be outside the 100-year floodplain.

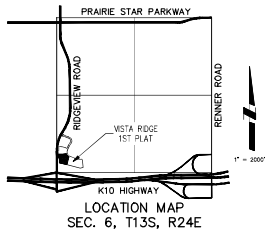
1,227.82' (plat boundary distance) / 0.0073 (closing distance) = unadjusted error of closure 1 in 168,193.15

ALL AREAS DESIGNATED ON THE PLAT AS DRAINAGE EASEMENT SHALL REMAIN FREE OF FENCES, SHRUBS, AND OTHER OBSTACLES THAT WOULD RESTRICT THE FLOW OF DRAINAGE.



**LEGEND:**

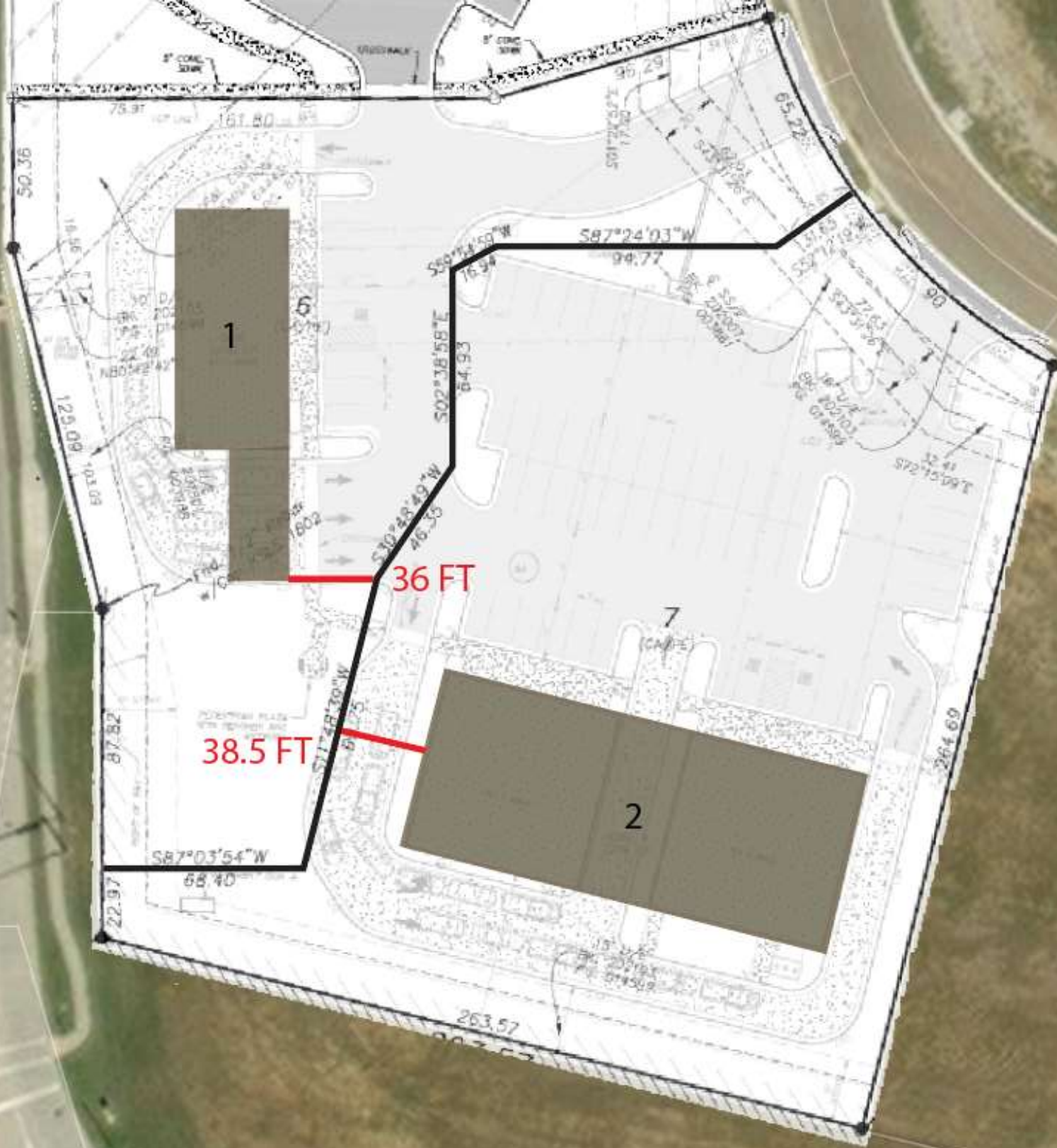
- DENOTES 1/2" REBAR SET w/P&B PLASTIC CAP QLS-49
- DENOTES 1/2" REBAR FOUND w/P&B PLASTIC CAP QLS-49 (UNLESS NOTED)
- ◆ DENOTES "X" CUT FOUND ON CONCRETE
- U/E DENOTES UTILITY EASEMENT
- SSE DENOTES SANITARY SEWER EASEMENT
- D/E DENOTES DRAINAGE EASEMENT
- CA/PE DENOTES CROSS ACCESS/PARKING EASEMENT
- P DENOTES PLATTED
- ===== DENOTES LIMITS OF NO ACCESS



23-1024-P(1).dwg



RIDGEVIEW RD



1

6

36 FT

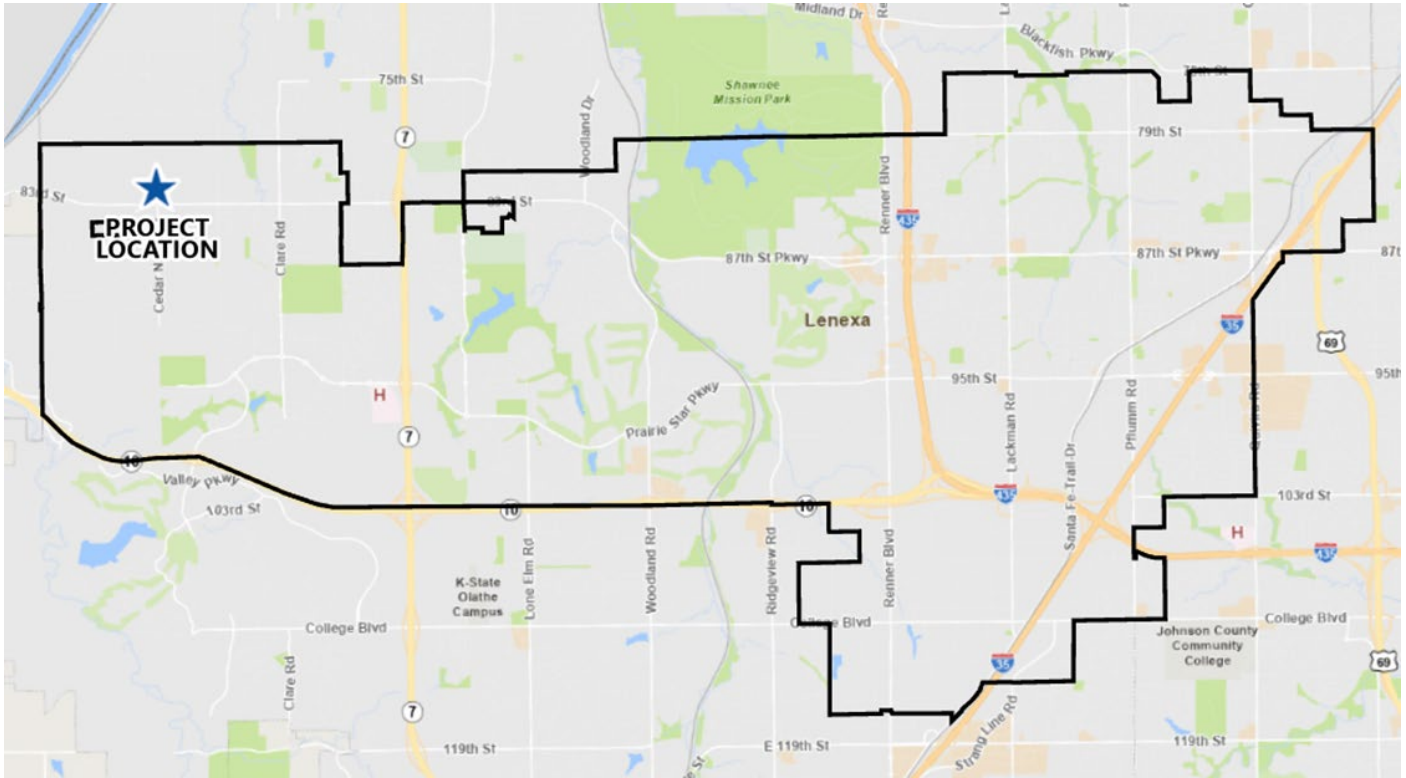
38.5 FT

2

7

# STONERIDGE NORTH, SECOND PLAT

<b>Project #:</b>	PT23-27F	<b>Location:</b>	NEC of W. 83 <sup>rd</sup> St & Cedar Niles Rd
<b>Applicant:</b>	Matt Mabe, Complete LLC	<b>Project Type:</b>	Final Plat
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Single-Family Residential



## PROJECT SUMMARY

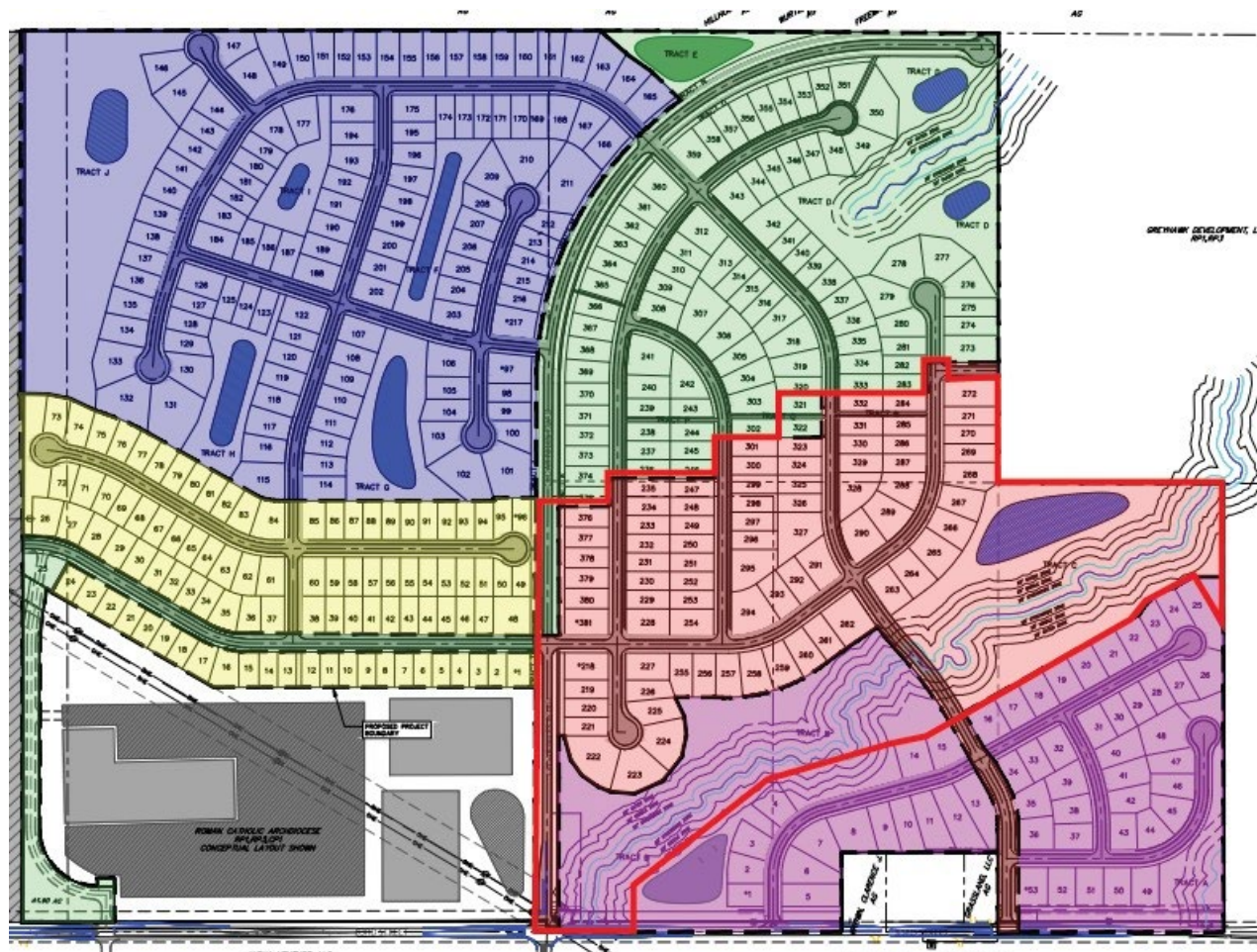
The applicant requests approval of a final plat for a single-family subdivision containing 80 lots, six tracts, and rights-of-way on 40.7 acres in the RP-1 Planned Residential Single-Family (Low-Density) and RP-2, Residential Planned (Intermediate-Density) District. This is the second plat of the Stoneridge North subdivision, a single-family and two-family planned residential development. The final plat includes construction of new public streets, related infrastructure improvements and private amenity spaces. The applicant requests approval of a final plat for the development and three revised deviations from the Unified Development Code (UDC). The deviation requests are related to lot width, lot area, and front yard setback. The updated deviations are due to refinements from preliminary to final plan stage and are supported by Staff. The proposed final plat is consistent with the preliminary plat (PT22-01P), which was approved by the Governing Body on February 7, 2023, with the exception of the revised deviation requests. This project does not require a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL**

**SITE INFORMATION**

This site was annexed into the City of Lenexa in November of 1986, at which time it was zoned and used as agricultural. It was subsequently rezoned to NP-O, RP-1, and AG in 2009 as part of the larger Cedar Niles Estates rezoning and concept plan (RZ09-02 and PL09-01CP), which included a total of 278.63 acres north of W. 83<sup>rd</sup> Street and west of Mize Boulevard. The 2009 plan did not progress beyond plan approvals.

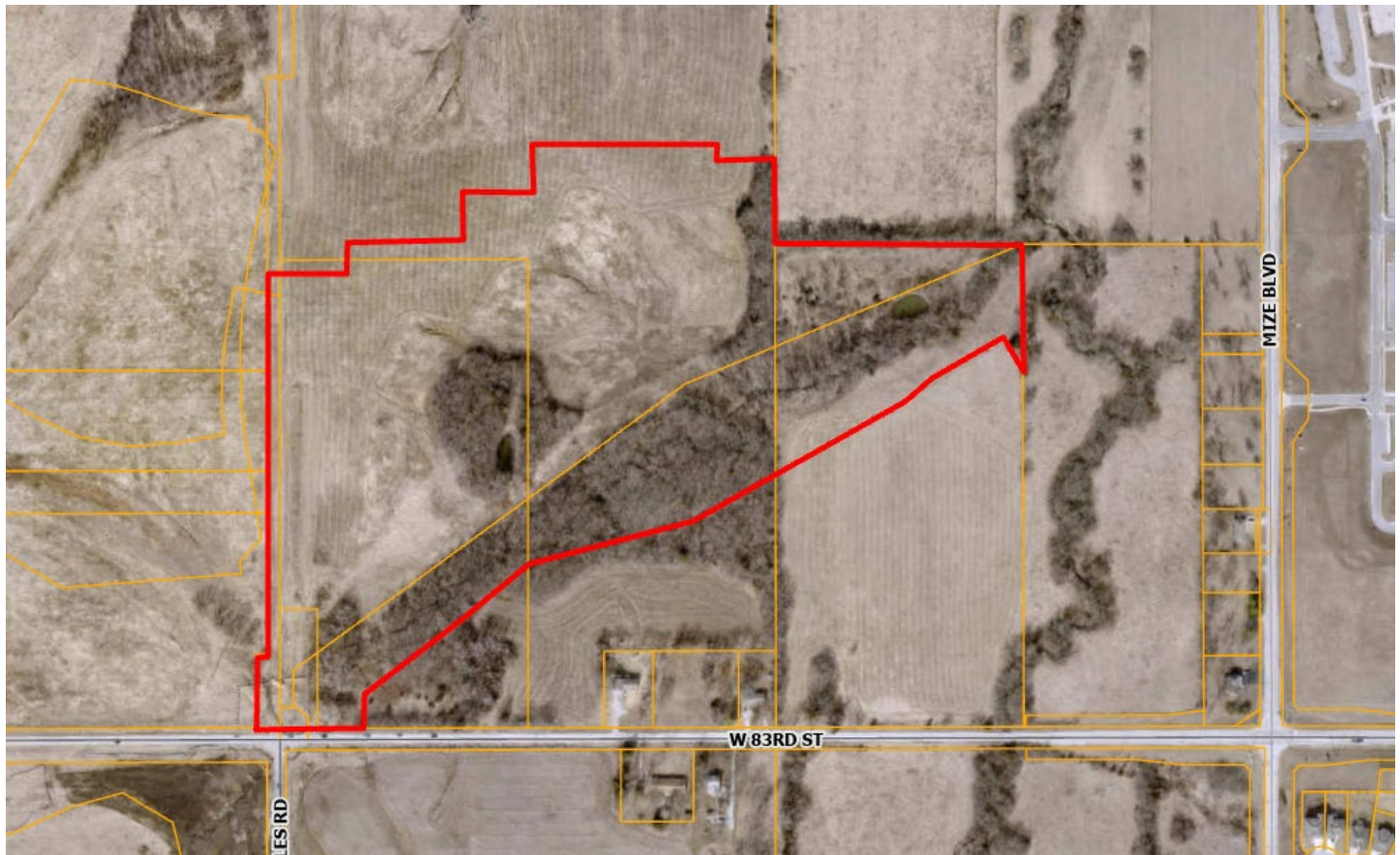
Arise Homes rezoned 168 acres from AG, RP-1, NP-O, and CP-1 to RP-1 and RP-2 in 2022 as part of a new residential subdivision known as Stoneridge North. The subject site is a portion of the area that was rezoned to RP-1. A preliminary plat (PT22-01P) was also approved at the same time as the rezoning (RZ22-05). The preliminary plat included approved deviations in the RP-1 Zoning District for minimum lot area, minimum lot width, and front yard setback.



- LEGEND**
- PHASE 1 - 53 MULTIFAMILY LOTS
  - PHASE 1 - 79 SINGLE FAMILY LOTS
  - PHASE 2 - 86 SINGLE FAMILY LOTS
  - PHASE 3 - 121 SINGLE FAMILY LOTS
  - PHASE 4 - 95 SINGLE FAMILY LOTS

**Exhibit 1: Preliminary Plat PT22-01P with Subject Final Plat PT23-27F Outlined in Red**

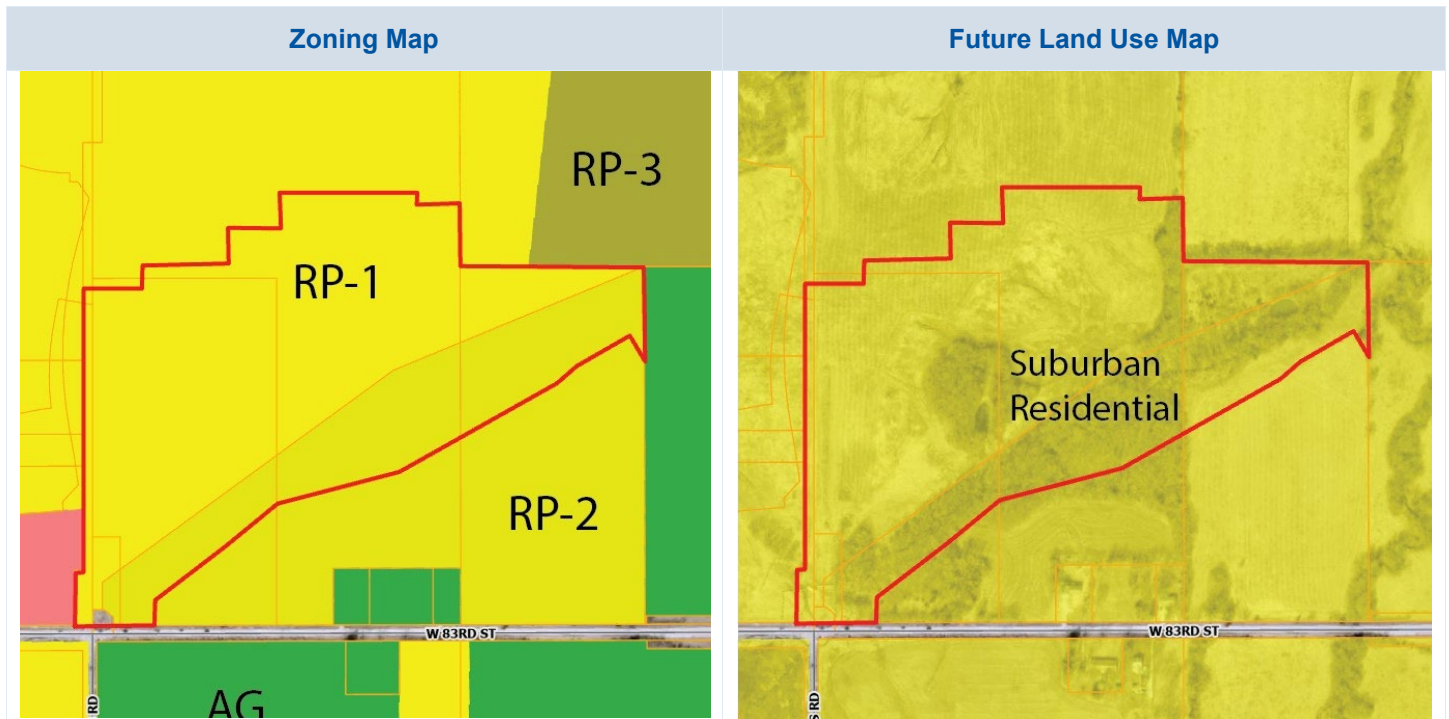
<b>LAND AREA (AC)</b> 40.7	<b>BUILDING AREA</b> N/A	<b>CURRENT ZONING</b> RP-1, RP-2	<b>COMP. PLAN</b> Suburban Residential
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**Exhibit 2: Aerial Image of Subject Site**

## LAND USE REVIEW

The proposed use is a residential subdivision. Stoneridge North is planned to include both single-family and two-family lots. This final plat is for the second plat and includes only single-family lots but does include a portion of land, Tract G, which is within the RP-2 Zoning District boundary. The Future Land Use designation is Suburban Residential. Single-family homes are an allowable use within the RP-1 Zoning District. The proposed use is similar to recent approvals in the area, including the Stoneridge development on the south side of W. 83<sup>rd</sup> Street. Other uses in the area include a nearby school (Mill Creek Middle School) and additional planned residential development.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low-Density) & RP-2, Residential Planned (Intermediate-Density)	Undeveloped
<b>North</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low-Density) & RP-3, Residential Planned (Medium High-Density)	Undeveloped
<b>South</b>	Suburban Residential	RP-2, Residential Planned (Intermediate-Density) & AG, Agricultural	Single-Family Residential and Agricultural
<b>East</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low-Density) & AG, Agricultural	Undeveloped
<b>West</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low-Density) & CP-1, Planned Neighborhood Commercial	Undeveloped

## FINAL PLAT REVIEW

This is a final plat of 80 single-family residential lots on 40.7 acres and is the second plat of Stoneridge North subdivision, for which a preliminary plan/plat was approved in February 2023. The final plat includes tracts and amenity areas that will serve the larger Stoneridge North subdivision. A plat note states that all amenity recreation trails shall be installed with subdivision infrastructure.

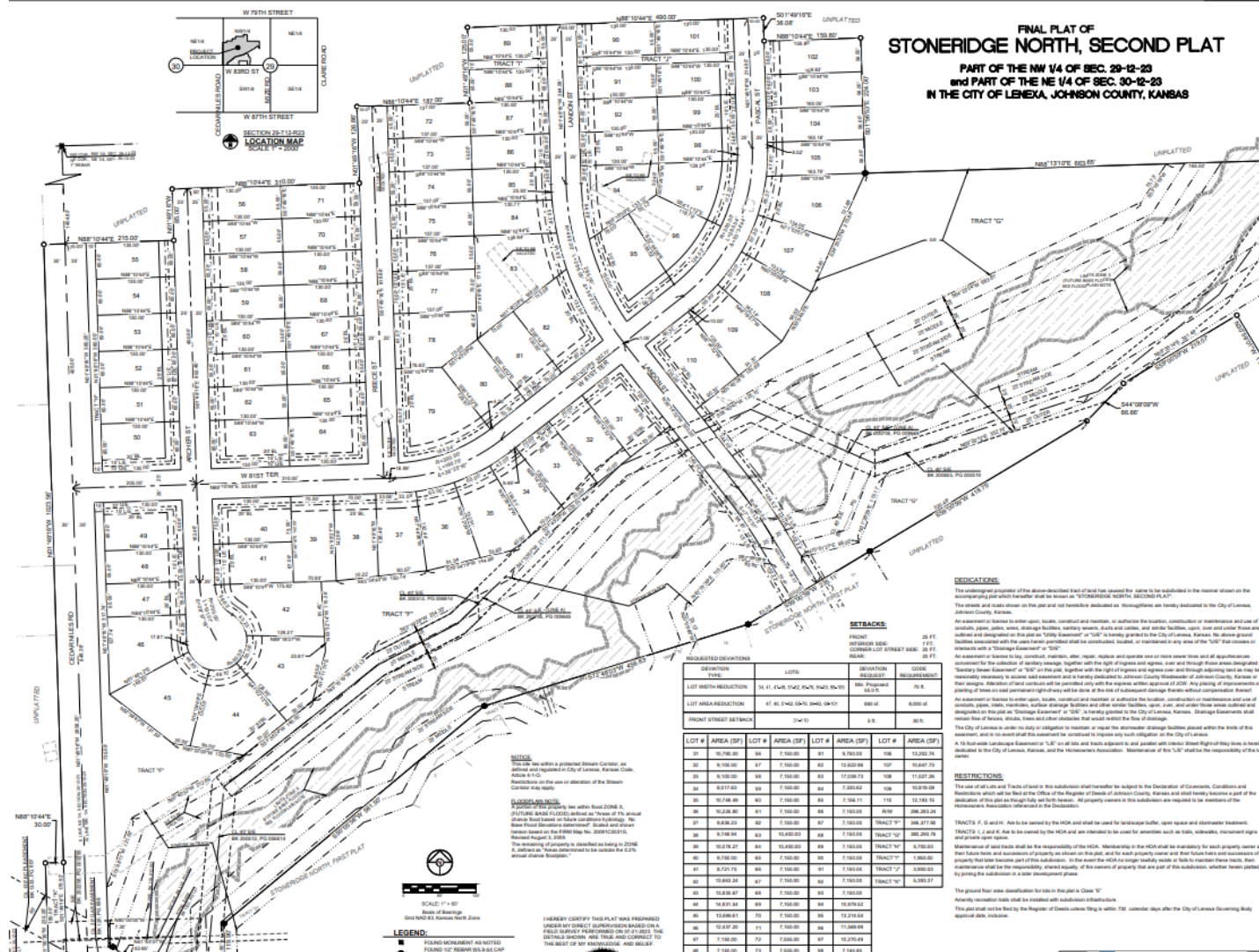
Street development includes approximately 1,300 linear feet of Cedar Niles Road north of W. 83<sup>rd</sup> Street on the western edge of the plat and a north-south expansion of Landon Street from what was approved in the first phase. An east-west street to be known as W. 81<sup>st</sup> Terrace and three additional north-south streets, Archer Street, Reece Street, and Pascal Street, will serve as local roads internal to the subdivision. Sidewalks will be provided for pedestrian connectivity, along with mid-block connections (Tract I and J), and private trails within the tracts.

**TABLE 2: PLAT DEDICATIONS**

Type of Dedication	Dedicated To
Rights-of-Way	City of Lenexa
Landscape Easements	City of Lenexa
Utility Easements	City of Lenexa
Drainage Easements	City of Lenexa
Sanitary Sewer Easements	Johnson County Wastewater

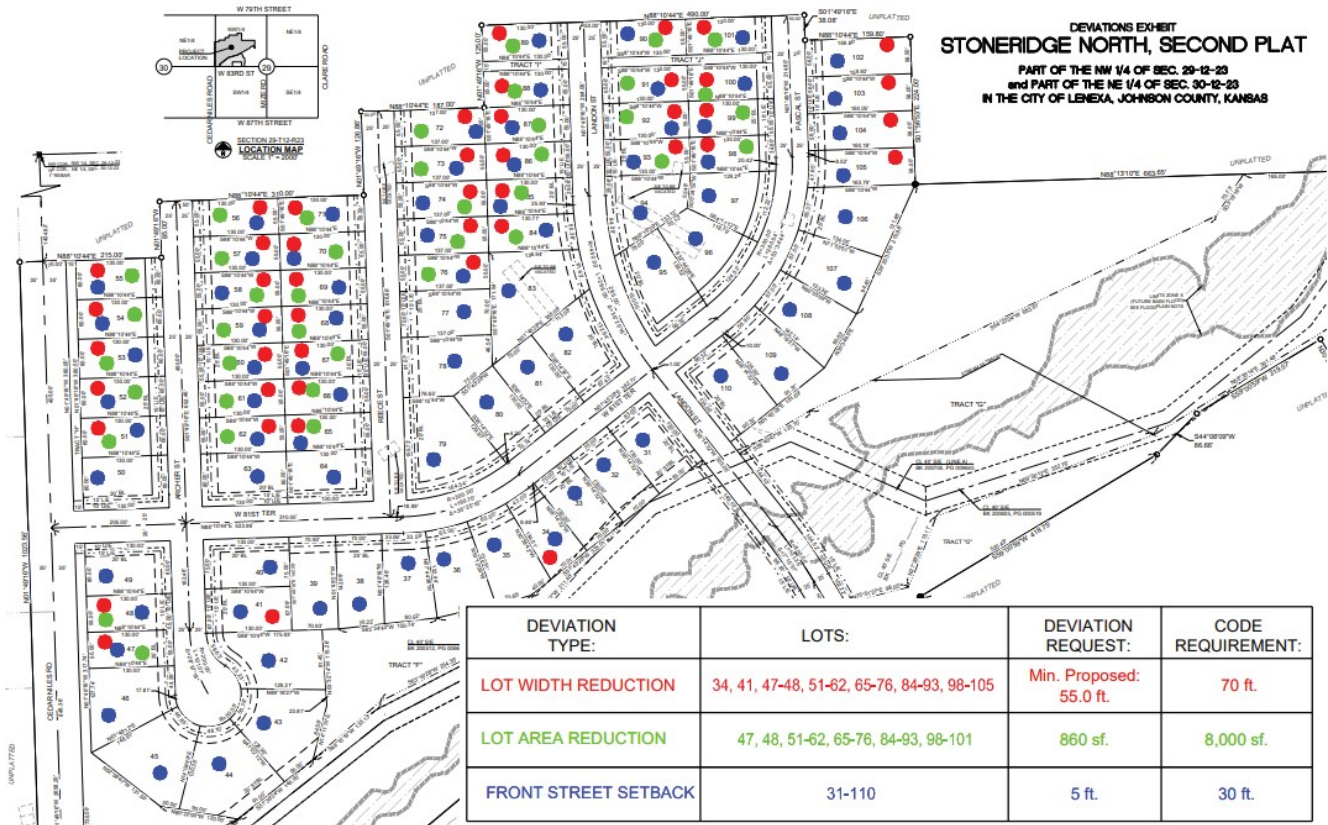
**TABLE 3: TRACT PURPOSES**

Tract	Purpose
<b>F</b>	Landscape buffer, open space and stormwater treatment
<b>G</b>	Landscape buffer, open space and stormwater treatment
<b>H</b>	Landscape buffer, open space and stormwater treatment
<b>I</b>	Mid-block pedestrian connection
<b>J</b>	Mid-block pedestrian connection
<b>K</b>	Open space and landscaping
Tracts F, G, H, I, J, and K shall be owned and maintained by the Homeowners Association	



## DEVIATIONS

The preliminary plan/plat (PT22-01P) was approved with deviations from the RP-1 standards for minimum lot width, minimum lot area, and front yard setback. Due to slight changes within the plan/plat from preliminary to final stage, slightly fewer lots need deviations for these dimensional aspects than originally requested.



**Exhibit 4: RP-1 Deviations Requested with Final Plat**

TABLE 4: COMPARISON OF DEVIATIONS FROM PRELIMINARY TO FINAL			
Deviation	Amount Requested	# of Lots at Preliminary	# of Lots at Final
Lot Width	-15 FT (Min. 55 FT)	59	46
Lot Area	-860 SF* (Min 1,740 SF)	42	40
Front Yard Setback	-5 FT (Min 25 FT)	80	80

\* The deviation approved with the preliminary plat was for a lot area reduction of 850 SF, resulting in a minimum lot area of 7,150 SF. All lots on the final plat have a lot area of 7,150 SF or greater except for Lot 98 which has a lot area of 7,144 SF.

## REVIEW PROCESS

- This project requires approval by the Planning Commission and acceptance of dedications by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on November 21, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.



## RECOMMENDATION FROM PROFESSIONAL STAFF

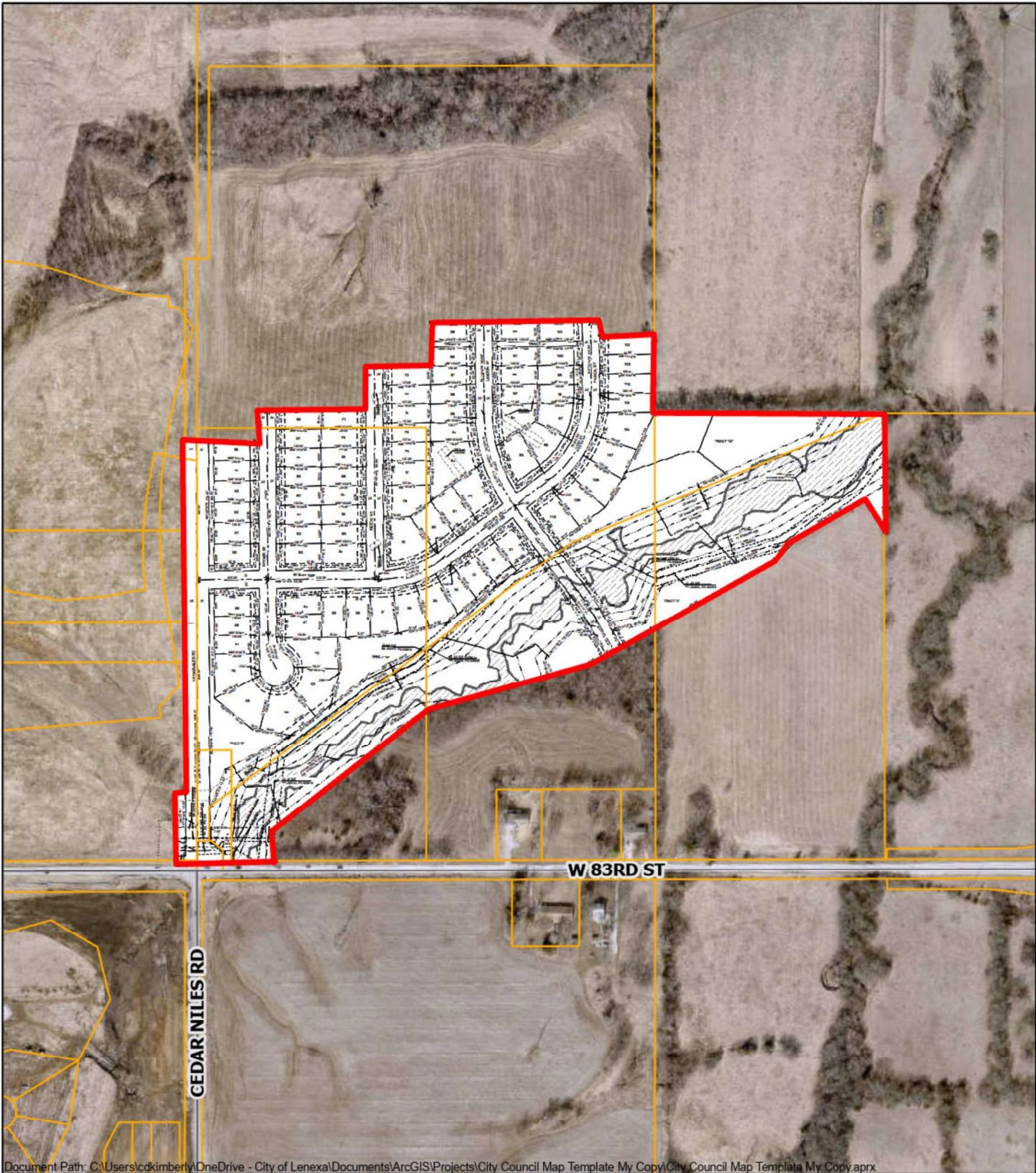
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★ **Staff recommends approval of the proposed Final Plat for Stoneridge North, Second Plat.**

- This is a final plat of 80 single-family lots and 6 tracts in the RP-1 and RP-2 Zoning District, with deviations for lot width, lot area, and front yard setback. This is the second plat of the planned Stoneridge North single-family and two-family residential development.
- The project is consistent with Lenexa's goals through *Responsible Economic Development* and *Strategic Community Investment* to create *Vibrant Neighborhoods*.

### FINAL PLAT

Staff recommends **approval** of the final plat for PT23-27F – **Stoneridge North, Second Plat** near the northeast corner of W. 83<sup>rd</sup> Street and Cedar Niles Road, for a single-family residential subdivision.



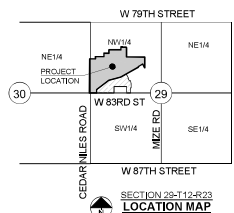
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Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

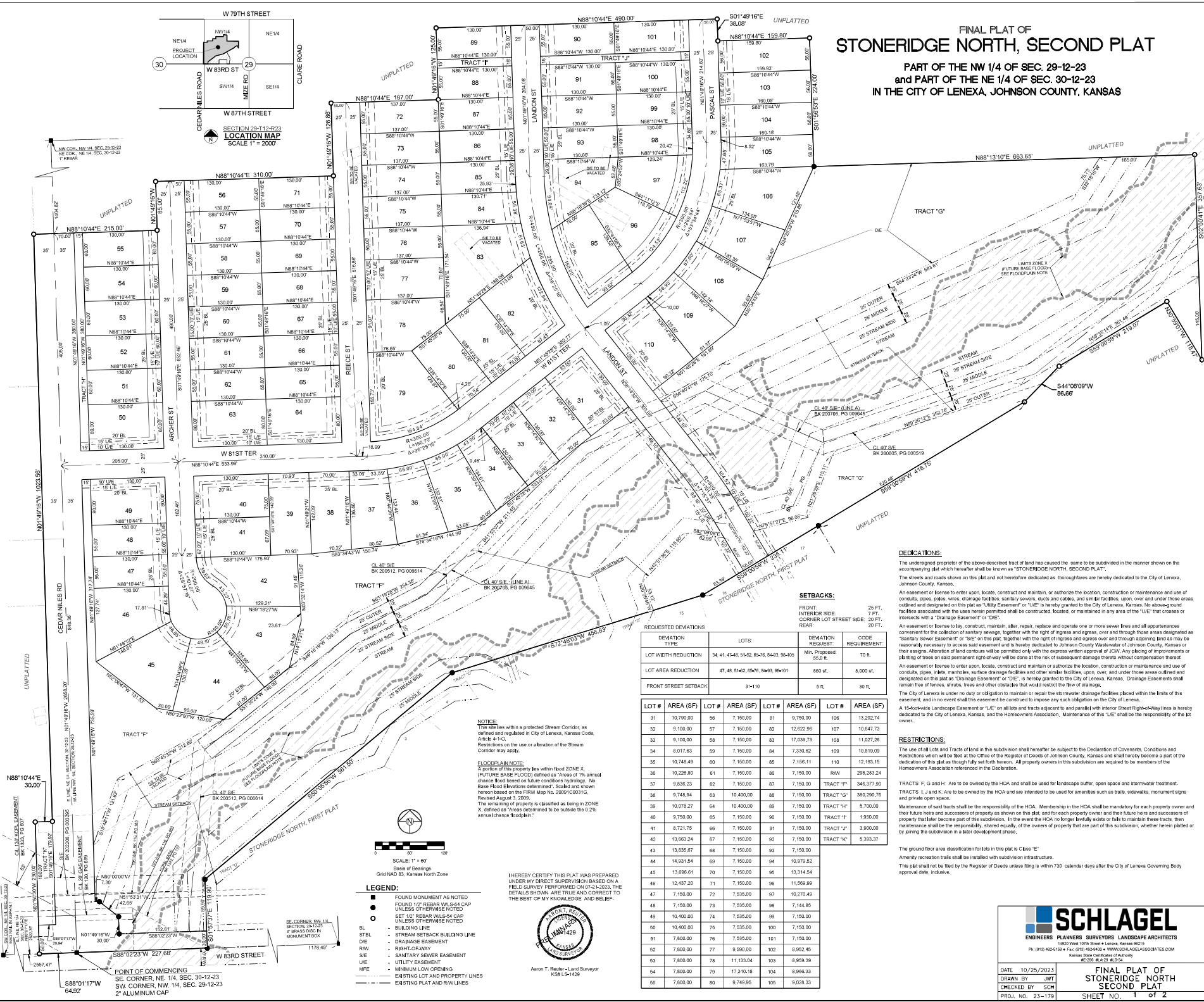
# Stoneridge North, Second Plat PT23-27F



**FINAL PLAT OF  
STONERIDGE NORTH, SECOND PLAT**  
PART OF THE NW 1/4 OF SEC. 29-12-23  
and PART OF THE NE 1/4 OF SEC. 30-12-23  
IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS



SECTION 29-12-23 & 30-12-23  
LOCATION MAP  
SCALE 1" = 2000'



**DEDICATIONS:**  
The undersigned proprietor of the above-described tract of land has caused the same to be subdivided in the manner shown on the accompanying plat which hereafter shall be known as "STONERIDGE NORTH, SECOND PLAT".  
The streets and roads shown on this plat and not heretofore dedicated as thoroughfares are hereby dedicated to the City of Lenexa, Johnson County, Kansas.  
An easement or license to enter upon, locate, construct and maintain, or authorize the location, construction or maintenance and use of conduits, pipes, poles, wires, drainage facilities, sanitary sewers, ditches and cables, and similar facilities, upon, over and under those areas outlined and designated on this plat as "Utility Easement" or "UE" is hereby granted to the City of Lenexa, Kansas. No above-ground facilities associated with the uses herein permitted shall be constructed, located, or maintained in any area of the "UE" that crosses or intersects with a "Drainage Easement" or "DE".  
An easement or license to enter upon, locate, construct and maintain or operate one or more sewer lines and all appurtenances convenient for the collection of sanitary sewage, together with the right of ingress and egress, over and through those areas designated as "Sanitary Sewer Easement" or "SSE" on this plat, together with the right of ingress and egress over and through adjoining lands as may be reasonably necessary to access said easement and is hereby granted to the City of Lenexa, Kansas. No above-ground facilities associated with the uses herein permitted shall be constructed, located, or maintained in any area of the "SSE" that crosses or intersects with a "Drainage Easement" or "DE".  
An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of conduits, pipes, poles, wires, drainage facilities and other similar facilities, upon, over and under those areas outlined and designated on this plat as "Drainage Easement" or "DE", is hereby granted to the City of Lenexa, Kansas. Drainage Easements shall remain free of fences, shrubs, trees and other obstructions that would restrict the flow of drainage.  
The City of Lenexa is under no duty or obligation to maintain or repair the stormwater drainage facilities placed within the limits of this easement, and in no event shall this easement be construed to impose any such obligation on the City of Lenexa.  
A 15-foot-wide Landscape Easement or "LE" on all lots and tracts adjacent to and parallel with interior Street Right-of-Way lines is hereby dedicated to the City of Lenexa, Kansas, and the Homeowners Association. Maintenance of this "LE" shall be the responsibility of the lot owner.

**SETBACKS:**

FRONT INTERIOR SIDE	25 FT.
CORNER LOT STREET SIDE	20 FT.
REAR	20 FT.

**REQUESTED DEVIATIONS**

DEVIATION TYPE	LOTS	DEVIATION REQUESTED	CODE	REQUIREMENT
LOT WIDTH REDUCTION	34, 41, 41A, 51A-52, 60-61, 84-85, 90-91	MIN. Proposed 50.0 ft.		70 FT.
LOT AREA REDUCTION	47, 48, 51A-52, 55-56, 84-85, 89-90, 91	860 sq. ft.		8,000 sq. ft.
FRONT STREET SETBACK	3-10	5 ft.		30 ft.

LOT #	AREA (SF)	LOT #	AREA (SF)	LOT #	AREA (SF)	LOT #	AREA (SF)
31	10,790.00	56	7,150.00	81	9,750.00	106	13,202.74
32	9,100.00	57	7,150.00	82	12,622.96	107	10,647.73
33	9,100.00	58	7,150.00	83	17,039.73	108	11,027.26
34	8,017.63	59	7,150.00	84	7,330.62	109	10,819.09
35	10,748.49	60	7,150.00	85	7,156.11	110	12,193.15
36	10,226.80	61	7,150.00	86	7,150.00	RAW	296,283.24
37	8,836.23	62	7,150.00	87	7,150.00	TRACT "F"	346,377.90
38	9,748.94	63	7,150.00	88	7,150.00	TRACT "G"	382,279.76
39	10,078.27	64	10,400.00	89	7,150.00	TRACT "H"	5,700.00
40	9,750.00	65	7,150.00	90	7,150.00	TRACT "I"	1,350.00
41	8,721.75	66	7,150.00	91	7,150.00	TRACT "J"	3,900.00
42	13,663.24	67	7,150.00	92	7,150.00	TRACT "K"	5,393.37
43	13,835.67	68	7,150.00	93	7,150.00		
44	14,931.54	69	7,150.00	94	10,979.52		
45	13,896.61	70	7,150.00	95	13,314.54		
46	12,437.20	71	7,150.00	96	11,568.99		
47	1,150.00	72	7,036.00	97	10,270.49		
48	7,150.00	73	7,536.00	98	7,144.85		
49	10,400.00	74	7,536.00	99	7,150.00		
50	10,400.00	75	7,535.00	100	7,150.00		
51	7,800.00	76	7,535.00	101	7,150.00		
52	7,800.00	77	6,990.00	102	8,962.45		
53	7,800.00	78	11,133.04	103	8,969.33		
54	7,800.00	79	17,310.18	104	9,996.33		
55	7,800.00	80	9,749.95	105	9,028.33		

**NOTICE:**  
This plat lies within a protected Stream Corridor, as defined and regulated in City of Lenexa, Kansas Code, Article 4-1-0.  
Restrictions on the use or alteration of the Stream Corridor may apply.

**FLOODPLAIN NOTE:**  
A portion of the property lies within Flood Zone X (FUTURE BASE FLOOD) defined as "Areas of 1% annual chance flood based on future conditions hydrology. No Base Flood Elevations determined". Scaled and shown herein based on the FEMA Map No. 2209C0001G, Revised August 3, 2009.  
The remaining property is classified as being in Zone X, defined as "Areas determined to be outside the 0.2% annual chance floodplain".

I HEREBY CERTIFY THIS PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION BASED ON A FIELD SURVEY PERFORMED ON 07-23-2023. THE DETAILS SHOWN ARE TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.



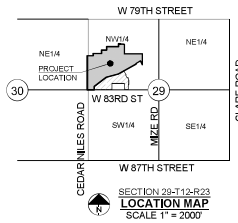
Aaron T. Reuter - Land Surveyor  
KSR 15-1-929

- LEGEND:**
- FOUND MONUMENT AS NOTED
  - FOUND 1/2" REBAR W/LS-4 CAP UNLESS OTHERWISE NOTED
  - SET 1/2" REBAR W/LS-4 CAP UNLESS OTHERWISE NOTED
  - BUILDING LINE
  - STREAM SETBACK BUILDING LINE
  - DRAINAGE EASEMENT
  - RIGHT-OF-WAY
  - SANITARY SEWER EASEMENT
  - UTILITY EASEMENT
  - MINIMUM LOW OPENING
  - EXISTING LOT AND PROPERTY LINES
  - EXISTING PLAT AND RAW LINES

**SCHLAGEL**  
ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS  
1402 West 107th Street • Lenexa, Kansas 66216  
Ph: (913) 664-6119 • Fax: (913) 664-8442 • WWW.SCHLAGELASSOCIATES.COM  
Kansas State Certified Surveyor  
KCS-20-23-85-04

# FINAL PLAT OF STONERIDGE NORTH, SECOND PLAT

PART OF THE NW 1/4 OF SEC. 29-12-23  
and PART OF THE NE 1/4 OF SEC. 30-12-23  
IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS



**APPROVALS:**

Approved by the Planning Commission of the City of Lenexa, Johnson County, Kansas,  
this \_\_\_ day of \_\_\_\_\_ 20\_\_\_

Chris Pass, Chairman  
Approved by the Governing Body of the City of Lenexa, Johnson County, Kansas, this  
\_\_\_ day of \_\_\_\_\_ 20\_\_\_

Michael A. Boehm, Mayor      Jennifer Martin, City Clerk

**CONSENT TO LEVY:**

The undersigned proprietor of the above described land hereby consents and agrees that the Board of County Commissioners of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to release such land proposed to be dedicated for public use from the lien and effect of any special assessments, and that the amount of the unpaid special assessments on such land dedicated shall secure and remain a lien on the remainder of this land fronting and abutting on such dedicated public way or thoroughfare.

**EXECUTION:**

IN TESTIMONY WHEREOF, \_\_\_\_\_  
has caused this instrument to be executed, this \_\_\_ day of \_\_\_\_\_ 20\_\_\_

By: \_\_\_\_\_

By: \_\_\_\_\_

**ACKNOWLEDGMENT:**

STATE OF KANSAS             )  
  ) ss.  
COUNTY OF JOHNSON     )

BE IT REMEMBERED that on this \_\_\_ day of \_\_\_\_\_ 20\_\_\_ before me, the undersigned, a Notary Public, in and for said County and State, came \_\_\_\_\_ who personally knows me to be the same person who executed the foregoing instrument of writing on behalf of said company, and such duly acknowledged the execution of the same to be the act and deed of same.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my notarial seal on the day and year last above written.

\_\_\_\_\_  
Notary Public                                     My Commission Expires \_\_\_\_\_

\_\_\_\_\_  
Print Name

**DESCRIPTION:**

Part of the Northwest One-Quarter of Section 29, Township 12, Range 23 and the Northeast One-Quarter of Section 30, Township 12, Range 23, both in the City of Lenexa, Johnson County, Kansas, and both together being more described as follows:

Commencing at the Southeast corner of said Northeast One-Quarter; thence along the South line of said Northeast One-Quarter, South 89 degrees 01 minutes 17 seconds West, a distance of 84.92 feet; thence North 01 degrees 50 minutes 30 seconds West, a distance of 230.00 feet; thence North 88 degrees 10 minutes 44 seconds East, a distance of 30.00 feet; thence along a line 35.00 feet West of and parallel with the East line of said Northeast One-Quarter, North 01 degrees 49 minutes 16 seconds East, a distance of 1023.56 feet; thence North 88 degrees 10 minutes 44 seconds East, a distance of 215.00 feet; thence North 01 degrees 49 minutes 16 seconds East, a distance of 85.00 feet; thence North 88 degrees 10 minutes 44 seconds East, a distance of 310.00 feet; thence North 01 degrees 49 minutes 16 seconds West, a distance of 125.86 feet; thence North 88 degrees 10 minutes 44 seconds East, a distance of 187.00 feet; thence North 01 degrees 49 minutes 16 seconds West, a distance of 125.00 feet; thence North 88 degrees 10 minutes 44 seconds East, a distance of 490.00 feet; thence South 01 degrees 49 minutes 16 seconds East, a distance of 36.08 feet; thence North 88 degrees 10 minutes 44 seconds East, a distance of 159.80 feet; thence South 01 degrees 56 minutes 53 seconds East, a distance of 224.00 feet; thence North 88 degrees 13 minutes 10 seconds East, a distance of 663.65 feet; thence South 02 degrees 00 minutes 41 seconds East, a distance of 357.63 feet; thence North 30 degrees 59 minutes 01 seconds West, a distance of 116.47 feet; thence South 59 degrees 00 minutes 59 seconds West, a distance of 219.07 feet; thence South 44 degrees 08 minutes 09 seconds West, a distance of 86.66 feet; thence South 59 degrees 00 minutes 59 seconds West, a distance of 418.75 feet to the Northeast corner of STONERIDGE NORTH, FIRST PLAT, a subdivision of land in the said City of Lenexa; thence along the Northernly and Westerly lines of said STONERIDGE NORTH, FIRST PLAT for the following four courses, continuing South 59 degrees 00 minutes 59 seconds West, a distance of 235.11 feet; thence South 72 degrees 48 minutes 03 seconds West, a distance of 456.83 feet; thence South 50 degrees 05 minutes 56 seconds West, a distance of 561.50 feet; thence South 01 degrees 57 minutes 37 seconds East, a distance of 119.90 feet to a point on the South line of the said Northwest One-Quarter; thence along said South line, South 88 degrees 02 minutes 23 seconds West, a distance of 227.68 feet, to the Point of Beginning, and containing 45,741 acres, more or less.

I HEREBY CERTIFY THIS PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION BASED ON A FIELD SURVEY PERFORMED ON 07-2-2023. THE DETAILS SHOWN ARE TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

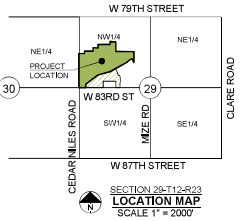
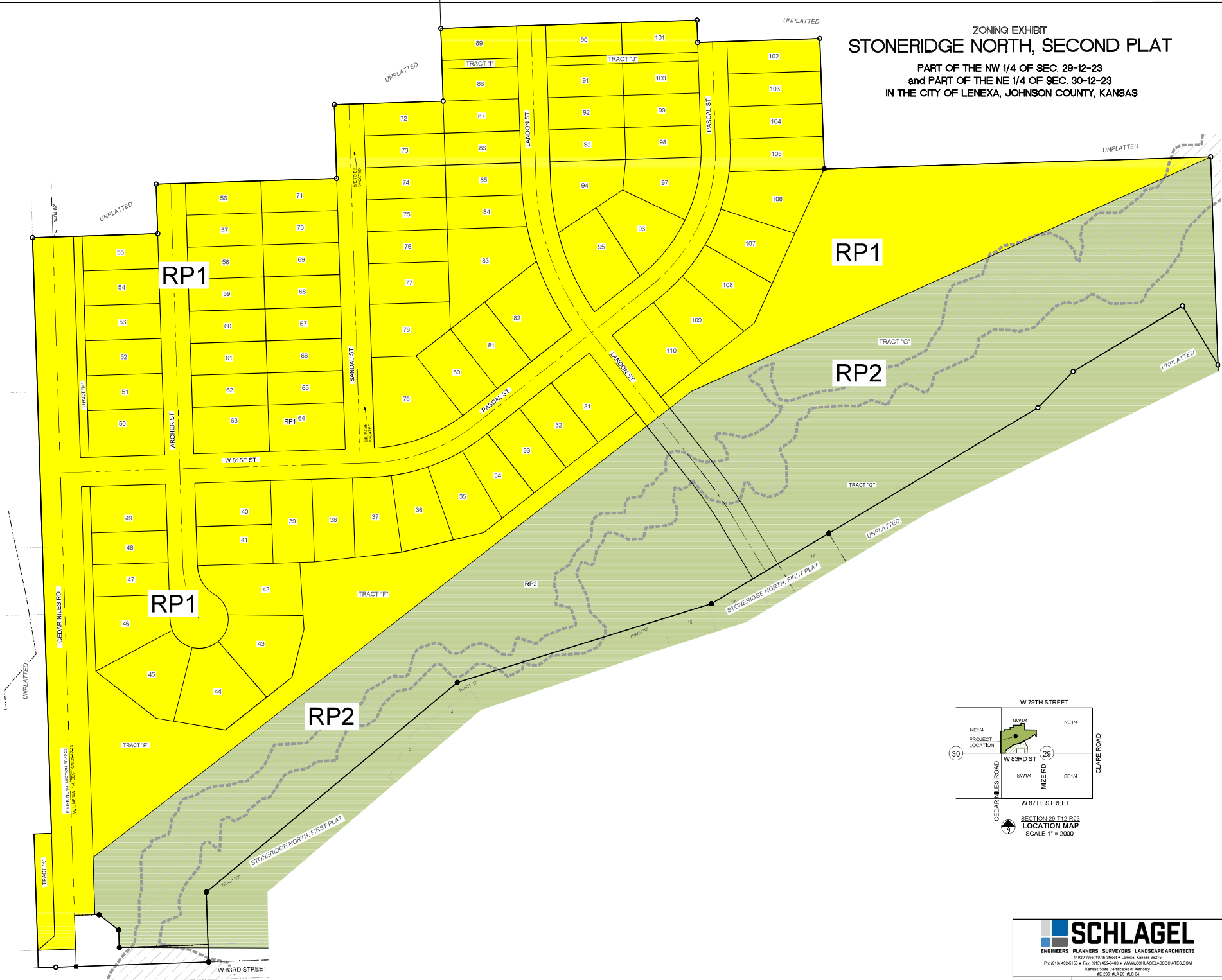


Aaron T. Reuter - Land Surveyor  
KS LS-1429

 <b>SCHLAGEL</b> ENGINEERS   PLANNERS   SURVEYORS   LANDSCAPE ARCHITECTS 1402 West 107th Street • Lenexa, Kansas 66216 Ph: (913) 492-6116 • Fax: (913) 492-8400 • WWW.SCHLAGELANDSCAPEARCHITECTS.COM Kansas State Certificates of Authority: 85-208, 84-3-29, 85-5-54	
DATE   10/25/2023 DRAWN BY   JMT CHECKED BY   SCH PROJ. NO.   23-179	<b>FINAL PLAT OF STONERIDGE NORTH SECOND PLAT</b>  SHEET NO.   2   of   2

ZONING EXHIBIT  
**STONERIDGE NORTH, SECOND PLAT**

PART OF THE NW 1/4 OF SEC. 29-12-23  
 and PART OF THE NE 1/4 OF SEC. 30-12-23  
 IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS

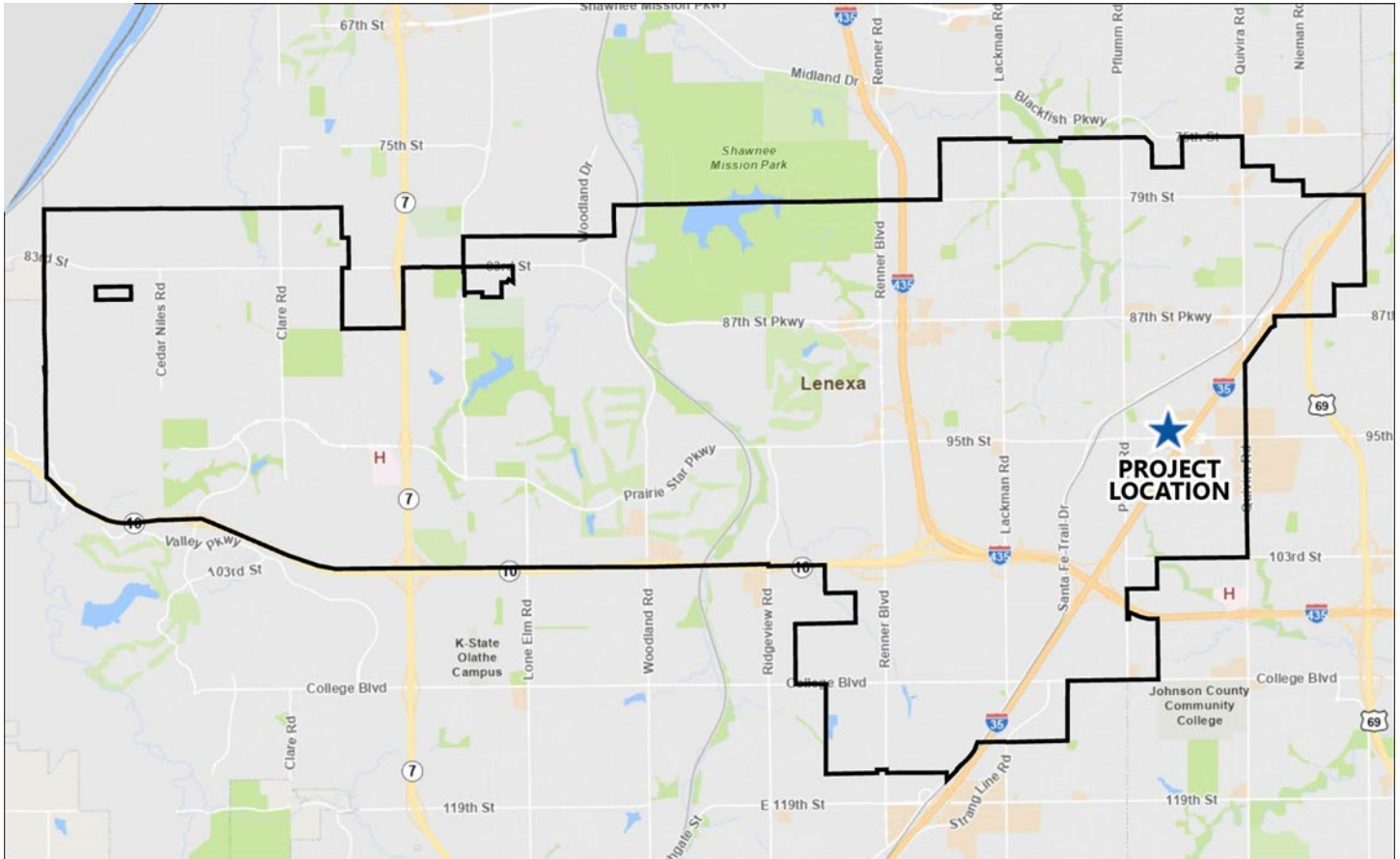


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 Ph: (913) 492-6169 • Fax: (913) 492-8402 • WWW.SCHLAGELASSOCIATES.COM  
 Kansas State Certificate of Authority  
 85-208, R-0-23, R-5-54

DATE	10/20/2023	FINAL PLAT OF
DRAWN BY	JWT	STONERIDGE NORTH
CHECKED BY	SCM	SECOND PLAT
PROJ. NO.	23-179	SHEET NO. 1 of 2

## FINELINE SOLUTIONS

<b>Project #:</b>	SU23-10	<b>Location:</b>	13100 W 95 <sup>th</sup> Street, Suite 2B
<b>Applicant:</b>	Cathy Moss, Business Owner	<b>Project Type:</b>	Special Use Permit
<b>Staff Planner:</b>	Logan Strasburger	<b>Proposed Use:</b>	Medical Clinic and Personal Services



### PROJECT SUMMARY

The applicant is requesting approval of a special use permit (SUP) to operate *medical clinic* and *personal services* businesses known as FineLine Solutions, Tivona Naturals, and Parkway Direct Primary Care Clinic (PDPCC) located at 13100 W. 95<sup>th</sup> Street, Suite 2B. The property is within the NP-O, Planned Neighborhood Office District. Per the Unified Development Code (UDC) [Section 4-1-B-11](#), a SUP is required to operate a *medical clinic* and/or a *personal services* business within the NP-O Zoning District. According to the applicant, FineLine Solutions provides aesthetic services as well as prescription weight loss injections; Tivona Naturals provides aesthetic services and primary care, and; Parkway Direct Primary Care Clinic provides only primary care services. Staff recommends that exterior property maintenance be performed as a condition of approval for this SUP. This request requires a public hearing at the Planning Commission meeting and consideration by the Governing Body.

**STAFF RECOMMENDATION: APPROVAL FOR THREE YEARS**

## SITE INFORMATION

The property at 13100 W. 95<sup>th</sup> Street was originally built and occupied as a residence in 1960. In May 1986, the property was rezoned from R-1 (Low Density Residential) to CP-O (Planned Office District) and later was rezoned to NP-O. Building additions were made to the structure in 1986 to accommodate office-related uses and multiple tenants. The structure has four levels with 12 units that are rented out to a variety of businesses.

There are four existing special use permits for this building, which are specific to suites. A special use permit for a *medical clinic* in Suite 3A was recently approved by the Governing Body on September 19, 2023.

1. Suite 3A: Personal Services (Ordinance [No. 4344](#)),
2. Suite 3A: Medical Clinic (Ordinance [No. 5957](#)),
3. Suite 3B: Personal Services (Ordinance [No. 4321](#)), and;
4. Suite 1A: Massage Services (Ordinance [No. 4262](#)).

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
0.40	2,700 (total) 668 (tenant space)	NP-O	Office/Employment Center



**Exhibit 1: Aerial Image of Subject Site**



***Exhibit 2: Photo from west side of subject property facing southeast/95<sup>th</sup> Street.***



***Exhibit 3: Photo from west side of subject property facing east towards Noland Road.***



## LAND USE REVIEW

The subject property is in a NP-O, Neighborhood Planned Office District. The *medical clinic* and *personal services* uses require a special use permit in the NP-O Zoning District. Per UDC [Section 4-3-C-3](#), the *personal services* use is defined as:

*“An establishment primarily engaged in the provision of frequently or recurrently needed services of a personal nature. Typical uses include, beauty and barbershops, tanning and nail salons, weight loss centers, shoeshines, portrait studios, custom tailoring and seamstress, and establishments which provide massage therapy subject to the licensing provisions in this City Code.”*

And the *medical or dental clinic* use is defined as:

*“An establishment where patients, who are not lodged overnight except for observation or emergency treatment, are admitted for examination and treatment by a person or group of persons practicing any form of healing or health-building services, whether such persons be medical doctors, chiropractors, osteopaths, chiropodists, naturopaths, optometrists, dentists or any such profession, the practice of which is lawful in the State, and also includes establishments which provide massage therapy subject to the licensing provisions in this City Code.”*

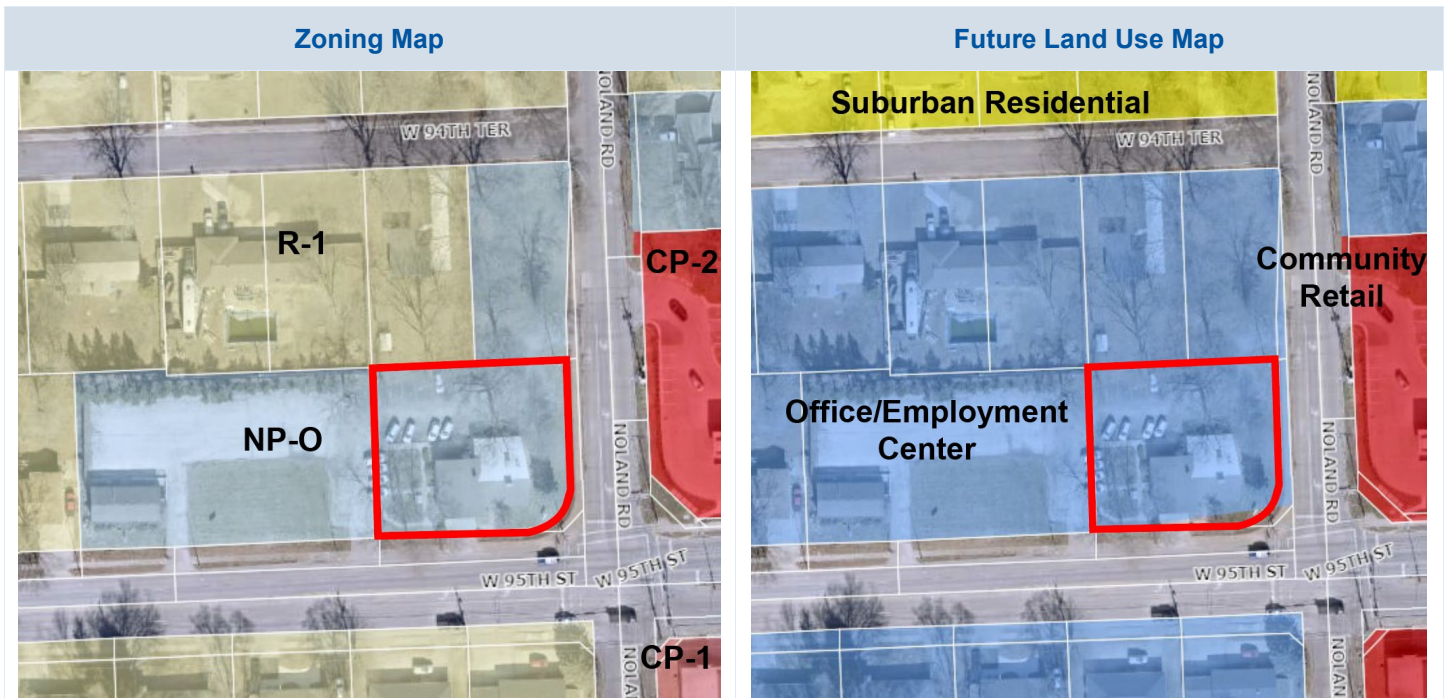
**TABLE 1: PROPOSED USES**

Business	Use Determinations	Services Offered
FineLine Solutions	Personal Services, Medical Clinic	Micro-needling, administration of neurotoxins and dermal fillers, other aesthetic personal services; prescription weight-loss injections and monitoring.
Tivona Naturals	Personal Services, Medical Clinic	Micro-needling, administration of neurotoxins and dermal fillers, IV hydration, and other aesthetic personal services; primary care.
Parkway Direct Primary Care Clinic (PDPCC)	Medical Clinic	Primary Care

Three businesses propose to occupy the 668 square-foot space within Suite 2B. According to the applicant, owner of FineLine Solutions, the medical and personal services provided include the administration of neurotoxins (Botox) for the treatment of wrinkles; dermal fillers used to treat deep lines, volume loss, skin folds and lip augmentation; prescription weight loss injections; and micro-needling treatments to reduce the appearance of hyperpigmentation, mild acne scars, and stretch marks. Primary care services will also be offered by both Tivona Naturals and Parkway Direct Primary Care Clinic (PDPCC).

FineLine Solutions will be available weekday evenings and weekends by appointment only. Tivona Naturals will be available Monday, Wednesdays, Fridays, and Saturdays from 10 AM to 7 PM by appointment only. PDPCC will operate at most two days a week, excluding weekends, and by appointment only.

Staff believes this is an appropriate location for low volume medical clinic and personal services businesses. The proposed use is compatible with the surrounding existing land uses and those uses which are designated on the future land use map.



**TABLE 2: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Office/Employment Center	NP-O, Neighborhood Planned Office District	Office
<b>North</b>	Office/Employment Center, Suburban Residential	NP-O, Neighborhood Planned Office District & R-1, Residential Single-Family District	Residential – Single-Family Detached, Undeveloped
<b>South</b>	Office/Employment Center	R-1, Residential Single-Family District	Residential – Single-Family Detached
<b>East</b>	Community Commercial	CP-2, Planned Community Commercial District	Service Station, Auto Service, Restaurant
<b>West</b>	Neighborhood Office	NP-O, Neighborhood Planned Office District	Medical or Dental Office

## SPECIAL USE PERMIT REVIEW

The applicant requests approval of a special use permit for a *medical clinic* and *personal services* uses at 13100 W. 95th Street, Suite 2B, in the NP-O, Planned Neighborhood Office Zoning District. Staff provides the following analysis for the review criteria within Section 4-1-G-5 of the UDC.

### 1. The character of the neighborhood.

The neighborhood is comprised of single-family residential homes and community commercial uses. Directly east of the subject site, across Noland Road and along 95<sup>th</sup> Street, is Philips 66 gas station, an auto service business known as Dare 2 Dream Motorsports, and Mi Ranchito restaurant. One lot that lies directly to the north of the subject property is zoned NP-O and is undeveloped except for the most

southeastern part of the lot, which is utilized for an access drive for the subject site. Residential single-family homes are to the north of the subject property. The west adjacent property is a dental clinic. W. 95<sup>th</sup> Street borders the south edge of the property. Medical clinic and personal service uses are compatible with the neighborhood because it is an appropriate use to buffer more intense uses in the CP-2 Zoning District from surrounding residential single-family homes.

**2. The zoning and use of properties nearby.**

Surrounding zoning and uses are described in [Table 1](#). The uses associated with the subject property are various types of office and a medical clinic. The NP-O District allows medical clinics and personal services as a special use. FineLine Solutions and its partnering businesses will be a low volume medical clinic that also provides personal services. It is staff's opinion the proposed special use is compatible with the surrounding zoning and uses.

**3. The suitability of the subject property for the uses to which it has been restricted.**

There are four existing special use permits issued to individual suites within the subject property. A special use permit was recently approved for a medical clinic in Suite 3A. Staff believes that these low-volume medical clinic and personal service businesses are appropriate at this location.

**4. The extent to which the proposed use will detrimentally affect nearby property.**

Staff does not believe there is a potential for the medical clinic and personal services businesses to be detrimental to nearby property. The site has adequate on-site parking and a cross-parking and cross-access agreement with the adjacent dentist office to the west.

**5. The length of time the subject property has remained vacant as zoned.**

The property is mostly occupied. Tenant spaces become available from time to time.

**6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.**

Staff does not believe that the denial of the application would be a relative gain in public health, safety, or welfare to the community. Staff believes that if the application were denied, the hardships imposed on the applicant would be greater than any negative externalities the use may impose. Staff does not believe there are operational impacts that need to be mitigated by the applicant.

**7. Recommendation of City's permanent professional staff.**

Please see Staff's recommendation at the end of this report.

**8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.**

The proposed medical clinic and personal services businesses will not change the primary land use of the subject property. The land use is in conformity with the Future Land Use Map designation of Office/Employment Center.

**9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.**

The subject property is an established development. The property is adequately served by required utilities and services.

**10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.**

See the parking standards table below. The subject property exceeds the requirement and has 20 spaces provided. The site has adequate on-site parking and a cross-parking and cross-access agreement with the adjacent dentist office to the west.

<b>TABLE 3: PARKING STANDARDS</b>			
<b>Use</b>	<b>Parking Formula</b>	<b>Required Parking</b>	<b>Provided Parking</b>
Personal Services	1 space per 200 SF & 1 Space Per Employee	17	20
Medical Clinic	1 space per 200 SF		
Office	1 space per 250 SF		

**11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.**

The proposed use will not generate negative environmental impacts. New construction or additional pavement is not proposed with this SUP.

**12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.**

There are no proposed modifications to the stormwater system or increases to the overall amount of impervious surfaces. The proposed SUP would not adversely affect the capacity or quality of water of the stormwater system. No natural streams are within the vicinity of the subject property.

**13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.**

Staff previously conducted a site visit to the subject property for a separate SUP application that was recently approved by the Governing Body (SU23-08). During the visit, the following observations were made:

- Deteriorated parking surface and missing parking space striping,
- Overgrowth of vegetation and damaged/diseased trees, and;
- Missing screening around waste receptacle.



**Exhibit 4: Waste receptacle without required screening.**

The property owner has been notified of Staff’s concerns. The property owner began work to remedy the issues and a condition of approval for SU23-08 was to remedy the property maintenance conditions.

Since notification in early August, the parking lot has been repaved and restriped as shown in Exhibit 6 and have had trees trimmed and overgrown vegetation removed late September. Staff understands that the remaining property maintenance concern related to the dumpster screening is in process and will monitor its completion against the condition related to SU23-08. The deadline to meet the condition is Monday, December 18, 2023.



**Exhibit 5: Overgrown vegetation and dead limbs in parking spaces.**



**Exhibit 6: Parking lot repair and restriping completed.**

## DEVIATIONS

The applicant is not requesting any deviations.

## REVIEW PROCESS

- *This Special Use Permit requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the Special Use Permit is tentatively scheduled for consideration from the City Council on November 21, 2023.*

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## RECOMMENDATION FROM PROFESSIONAL STAFF

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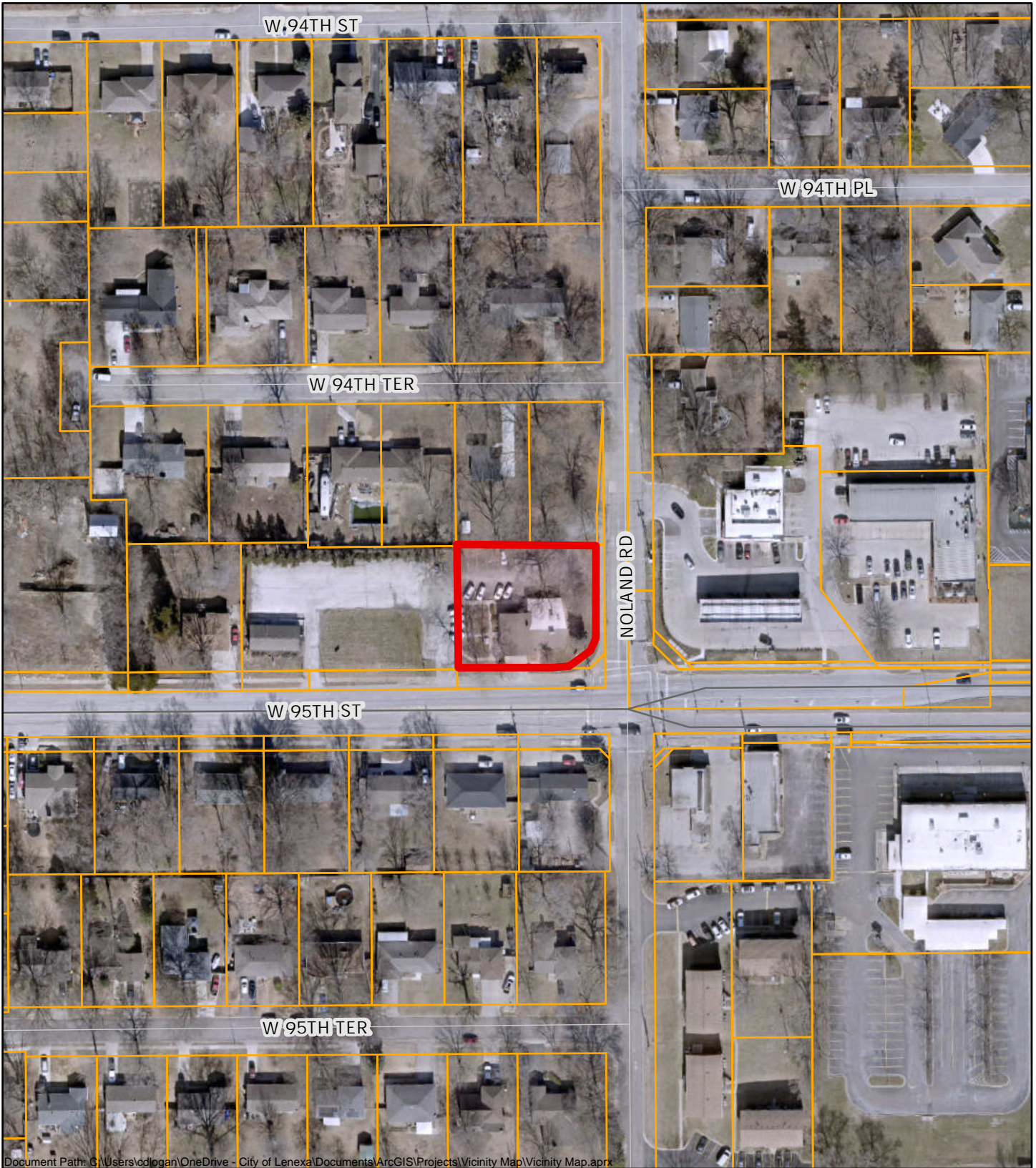
★ **Conduct a Public Hearing.**

★ **Staff recommends approval of the proposed special use permit for FineLine Solutions and its co-tenants, Tivona Naturals and Parkway Direct Primary Care Clinic.**

- The uses, “*Medical Clinic*” and “*Personal Services*”, are appropriate at the subject property.
- The project is consistent with Lenexa’s goals through ***Strategic Community Investment*** to create ***Vibrant Neighborhoods*** and ***Healthy People***.

### SPECIAL USE PERMIT

Staff recommends **approval** of SU23-10 - a special use permit for *medical clinic* and *personal services* uses for **FineLine Solutions, Tivona Naturals, and Parkway Direct Primary Care Clinic** at 13100 W. 95<sup>th</sup> Street, Suite 2B, for a period of three years.



# FineLine Solutions

## SU23-10



**To whom it may concern,**

FineLine Solutions LLC provides Aesthetic services using Neurotoxins and filler injections, microneedling, and weight loss management.

**Description of each service provided:**

- 1. Neurotoxins: Injectable for the treatment of wrinkles to reduce the appearance of fine lines in the face. Can also be used to treat excessive sweating and migraines.**
- 2. Dermal fillers: Used for the treatment of deep lines, volume loss, skin folds and lip augmentation.**
- 3. Microneedling: Minimally-invasive procedure. Stimulates the body's natural healing process and is used to reduce the appearance of hyperpigmentation, mild acne scars, wrinkles, irregular skin tone and stretch marks.**
- 4. Weight loss management: Use of FDA approved Semaglutide weekly injections and oral Phentymine monthly prescriptions to treat obesity in patients with BMI of 30 and above.**
- 5. Other Aesthetic services to include facials, waxing, and dermaplanning.**

The business is open evenings and weekends by appointment. It is open part time. The maximum number at a time will be 3. Since I will work by appointments, I have to schedule to accommodate my client's needs.

The office space is a shared space with other providers that offer similar services and primary care.

The contact person is Catherine Moss FNP and one assistant Michelle Matthew RN.

Cathy Moss FNP-C  
KS NP license #53-81171-011  
KS Business license #  
FineLine Solutions LLC  
402 304 4622

Michelle Matthew  
480 334 5164



To whom it may concern,

Tivona Spa and Wellness  
KS Registration License 78949926  
Provide Primary Care and Licensed Aesthetic services

Description of service provided:

1. Primary Care, Direct and Virtual - Health services that cover a range of prevention, wellness, and treatment for common illnesses. As a Nurse Practitioner, I maintain long-term relationships with patients and advise and treat a range of health related issues.
2. Neurotoxins: Injectable for the treatment of wrinkles to reduce the appearance of fine lines in the face. Can also be used to treat excessive sweating and migraines.
3. Dermal fillers: Used for the treatment of deep lines, volume loss, skin folds and lip augmentation.
4. Microneedling: Minimally-invasive procedure. Stimulates the body's natural healing process and is used to reduce the appearance of hyperpigmentation, mild acne scars, wrinkles, irregular skin tone and stretch marks.
5. Weight loss management: Use of FDA approved Semaglutide weekly injections and oral medications with monthly prescriptions to treat obesity in patients with BMI of 30 and above.

While the office space is shared at times, I anticipate working part time and use of space is only by appointment. Services provided as needed twice a week or as needed by appointment. I accept some weekday evenings and weekends but only by appointment. Maximum use for this provider is about 2-3 days.

Please reach out if any questions or concerns

Thank you

Suzanne Gunn FNP  
Tivona Spa and Wellness  
913-488-2948

## **Parkway Direct Primary Care Clinic (PDPCC)**

**To whom it may concern**

**Parkway Direct Primary Care Clinic**

**Kansas Registration ID 8075459**

### **What I do**

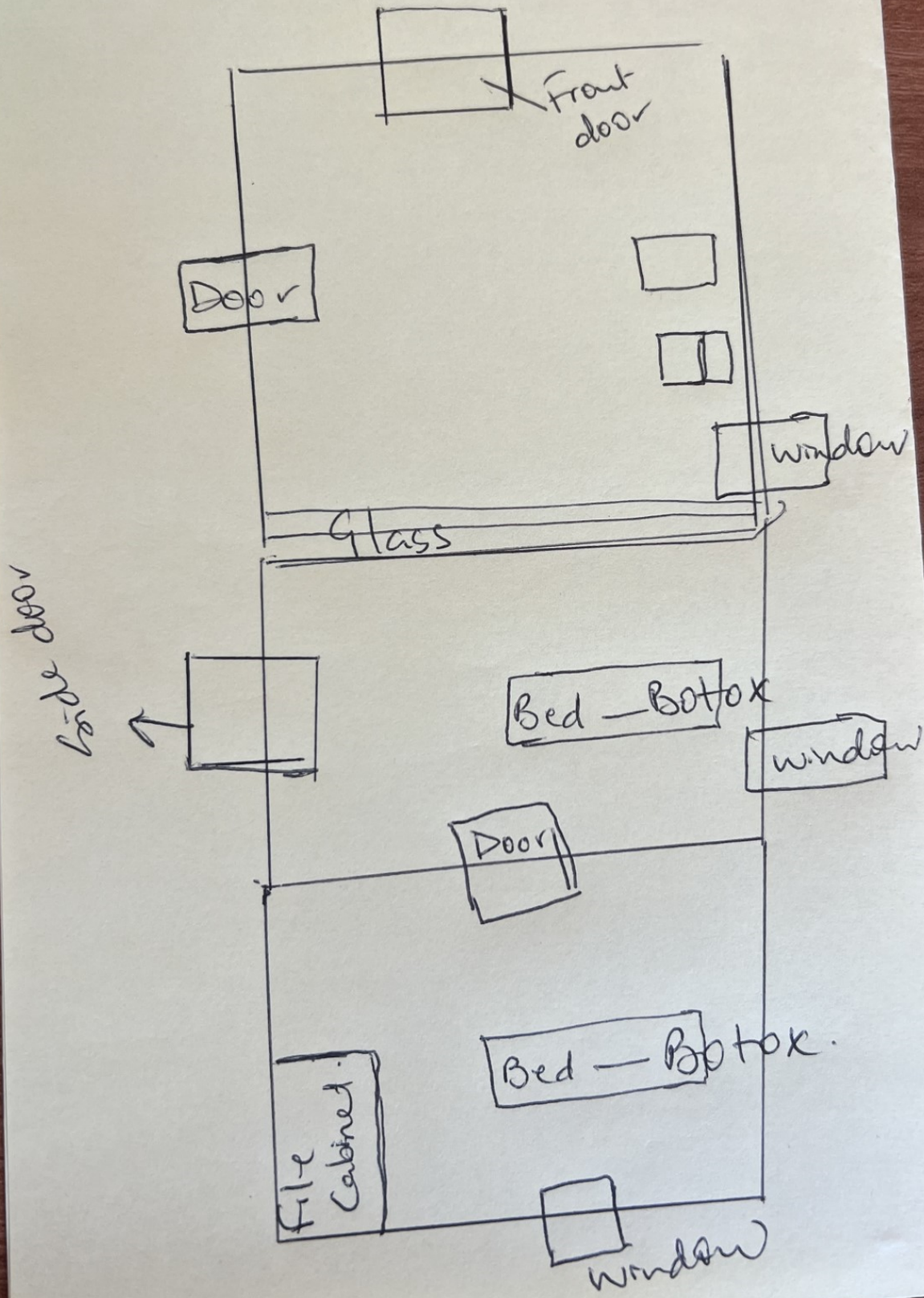
1. I provide primary direct medical care, during the initial stages of a patient's healthcare journey. The initial assessment or examination serves as a preliminary step to identify potential health issues, determine the appropriate course of action, and direct patients to further diagnostic tests or specialized care if necessary. In addition, I encompass various services to maintain health, diagnose and treat illnesses, and manage chronic conditions.
2. Although the office space is shared with other providers, my working hours are part time and strictly by appointment. I intend to see clients two days a week, excluding weekends.

Feel free to reach out for additional information

David Kimani; DNP-FNP

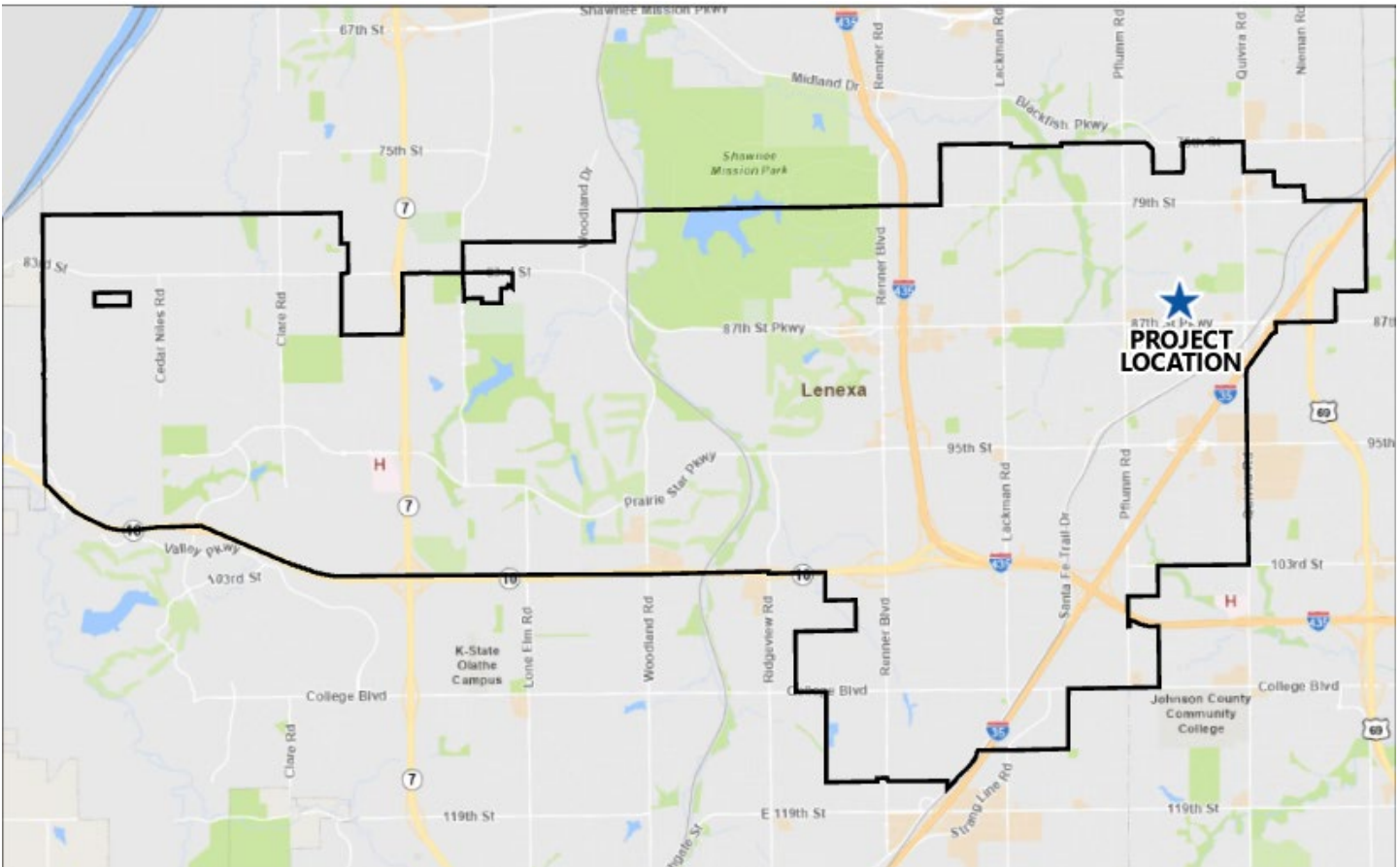
316-251-2548.

95th Street



**RAW HEALTH COMPANY**

<b>Project #:</b>	SU23-11	<b>Location:</b>	12760 W. 87 <sup>th</sup> Street Parkway, Suite 110
<b>Applicant:</b>	Suzanne Adams, Business Owner	<b>Project Type:</b>	Special Use Permit
<b>Staff Planner:</b>	Logan Strasburger	<b>Proposed Use:</b>	Medical Clinic, Chiropractor



**PROJECT SUMMARY**

The applicant, Suzanne Adams, is requesting approval of a special use permit (SUP) to operate a medical clinic known as Raw Health Company located at 12760 W. 87<sup>th</sup> Street Parkway, Suite 110, within the NP-O (Planned Neighborhood Office) District. Per Unified Development Code (UDC) [Section 4-1-B-11](#), a SUP is required to operate a medical clinic within the NP-O Zoning District. Raw Health Company is a medical practice that specializes in perinatal and pediatric adjustments, care for young athletes, and whole family chiropractic care. The clinic proposes to occupy a 700 square-foot space and will be open for appointments between 8:00 AM to 7:00 PM, Monday through Saturday. Visits are scheduled by appointment only. This request requires a public hearing at the Planning Commission meeting and consideration by the Governing Body.

**STAFF RECOMMENDATION: APPROVAL FOR FIVE YEARS**

## SITE INFORMATION

The property at 12760 W. 87<sup>th</sup> Street Parkway hosts one primary structure that was constructed in 1985. The property was initially zoned CP-1 (Planned Neighborhood Commercial) District and was later rezoned to NP-O. The structure is one level with eight total units that are occupied by a variety of businesses for general office and a nonconforming medical clinic uses.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
1.54	Total: 10,032 Raw Health: 700	NP-O	Office/Employment Center



*Exhibit 1: Aerial Image of Subject Site*



*Exhibit 2: Front Entrance of Subject Site*



*Exhibit 3: Parking Lot Directly Serving Subject Site*



Exhibit 4: Treatment Room 1



Exhibit 5: Treatment Room 2

## LAND USE REVIEW

The subject property is in a Neighborhood Planned Office District. A *medical or dental clinic*, which includes chiropractic, requires a special use permit in this zoning district. Per UDC [Section 4-3-C-3](#), a *medical or dental clinic* is defined as:

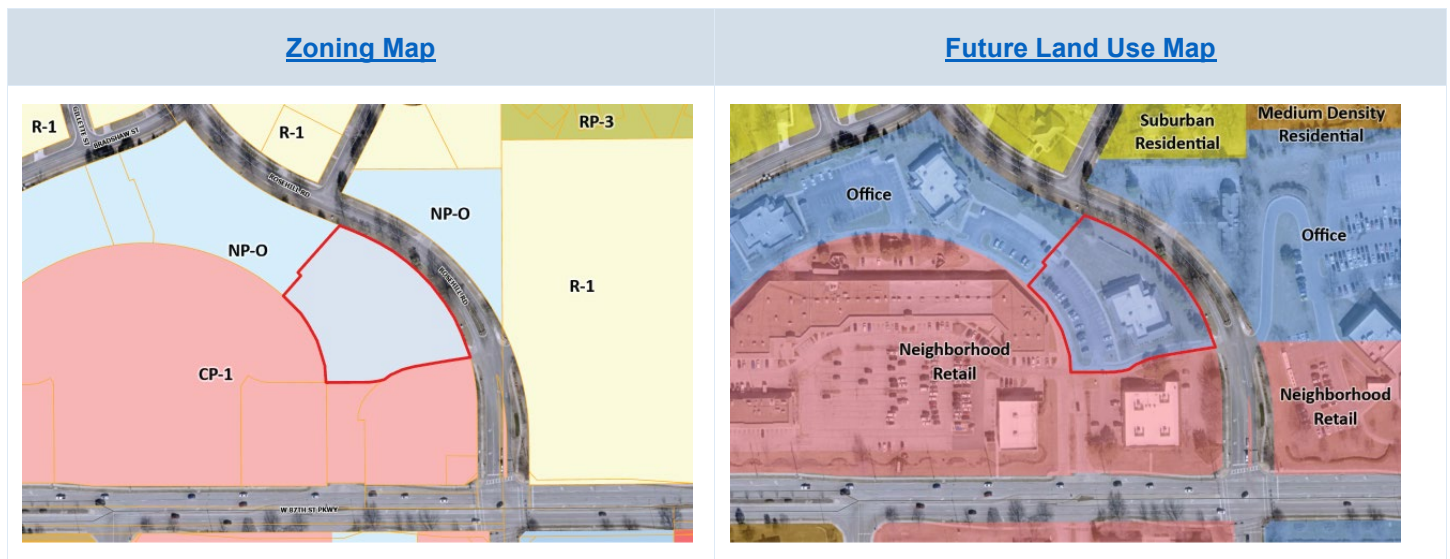
“an establishment where patients, who are not lodged overnight except for observation or emergency treatment, are admitted for examination and treatment by a person or group of persons practicing any form of healing or health-building services, whether such persons be medical doctors, chiropractors, osteopaths, chiropractists, naturopaths, optometrists, dentists or any such profession, the practice of which is lawful in the State, and also includes establishments which provide massage therapy subject to the licensing provisions in this City Code.”

Raw Health Company provides chiropractic services. The business will be open for appointments between 8:00 AM and 7:00 PM, Monday through Saturday. The applicant stated that they will be in the office a minimum of 13 hours weekly within that time frame. Appointments are made on an as-needed basis, as is typical with medical clinics. Appointments for chiropractic care are generally 15 to 30 minutes.

If this special use permit application is approved, Raw Health Company plans to expand their business to include massage therapy services, which is permitted within the *medical or dental clinic* use definition. Appointments for massage therapy range from 60-90 minutes, depending upon the modality of treatment requested by the client.

With the expansion of Raw Health Company’s business, it is anticipated that the maximum number of patients that could be seen at once is four, and an additional three to five patients could be in the waiting area. There would be a maximum of nine patients in the office at any one time. Raw Health Company anticipates working with independent contractors to provide chiropractic or massage therapy services, which means there would be a maximum of four independent contractors working in the office at one time. The anticipated client to practitioner ratio is 1:1.

The tenant space is within Building D of Greystone Plaza, which has a total of eight suites. Suite 110 is a 700 square-foot space comprised of four treatment rooms and one waiting room. Types of businesses that occupy other suites within Building D include immigration examination services, mental health services, law offices, an insurance broker, and a telecommunications service provider. Staff believes this is an appropriate location for a medical office. The future land use map supports office and neighborhood businesses.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Neighborhood Office	NP-O, Neighborhood Planned Office	Neighborhood Office – General Office, Medical Clinic
<b>North</b>	Suburban Residential, Medium-Density Residential	R-1, Residential Single-Family District, R-3, Residential Planned (Medium-High Density) District	Residential – Single-Family Detached, Single-Family Attached; Commercial – Dental Clinic
<b>South</b>	Neighborhood Retail, Neighborhood Office, Community Retail, High-Density Residential	CP-1, Planned Neighborhood Commercial District, CP-O, Planned General Office District, NP-O, Neighborhood Planned Office, RP-4, Residential Planned (High-Density) District	Neighborhood Commercial - Personal Instruction; Commercial – Retail, Restaurants, Bank; Residential – Multifamily
<b>East</b>	Medium Density Residential, Office, Neighborhood Retail, Community Retail	CP-2, Planned Community Commercial District CP-O, Planned General Office District, R-1, Residential Single-Family District, R-3, Residential Planned (Medium-High Density) District	Residential – Single-family detached, single-family attached, Church
<b>West</b>	Neighborhood Retail, Neighborhood Office, Suburban Residential	NP-O, Neighborhood Planned Office, CP-1, Planned Neighborhood Commercial District	Office, Commercial - Retail, Restaurants

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## SPECIAL USE PERMIT REVIEW

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The applicant requests approval of a special use permit for a medical clinic (chiropractic) use at 12760 W. 87<sup>th</sup> Street Parkway in the NP-O Zoning District. Staff provides the follow analysis to the review criteria within [Section 4-1-G-5](#) of the UDC.

### 1. The character of the neighborhood.

The subject site includes other nonconforming medical uses such as behavioral health services, out-patient psychiatric treatment, and a medical examination clinic. Conforming uses within the site include lawyers, insurance agents, and a telecommunications service provider. Directly to the north of the subject site are single-family residential homes. East adjacent is the Central Church of the Nazarene, a nonconforming dentist office, medium density two-family residential homes, and the City of Lenexa Municipal Complex. Directly to the south is a liquor store, retail, restaurants, community office spaces, and multifamily apartment homes. West adjacent is the remainder of the Greystone South Plaza development that features Tanner’s Bar and Grill, AutoZone, Hermes Landscaping Office, and other general low volume retail or restaurants. The request is well-suited to the character of the neighborhood.

It should be noted that the City’s review processes have been enhanced over the years and new nonconforming uses are less likely to be established in the future. Staff will work to bring the site into compliance over time as new tenants seek certificates of occupancy for the nonconforming suites.

### 2. The zoning and use of properties nearby.

Surrounding zoning and uses are described in [Table 1](#). The uses associated with the subject property include various types of office and nonconforming medical or dental clinic uses. The NP-O District allows medical or dental clinics as a special use. Raw Health Company is a small but growing medical clinic. Staff does not anticipate negative externalities such as noise or traffic to be a concern. It is Staff’s opinion the proposed special use is compatible with the surrounding zoning and uses.

### 3. The suitability of the subject property for the uses to which it has been restricted.

The subject site has hosted many nonconforming medical offices and clinics since inception. There are no existing special use permits for the site. However, in 2019, a special use permit application was submitted by an individual that sought to grant a special use permit for medical clinic use to the entirety of the building. The sale of the building to a medical group was contingent on the approval of the special use permit. The applicant withdrew their application before it made it to the Planning Commission, as the sale fell through. The site is suitable for the uses to which it is restricted as well as to the requested SUP for medical use.

### 4. The extent to which the proposed use will detrimentally affect nearby property.

Staff does not believe there is a potential for the medical clinic to be detrimental to nearby property. The site features adequate on-site parking. There is also a legal covenant and restrictions document that stipulates cross-parking/access agreements with prior, existing, and future owners of property within Greystone South Plaza.

### 5. The length of time the subject property has remained vacant as zoned.

The property has no vacancy. However, tenant spaces become available from time to time.



**6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.**

Staff does not believe that the denial of the application would be a relative gain in public health, safety, or welfare to the community. Staff believes that the presence of a medical clinic within close proximity to residential neighborhoods would increase the public health and welfare of the community. Staff believes that if the application were denied, the hardships imposed on the applicant would be greater than any negative externalities the use may impose. Staff does not believe there are impacts that need to be mitigated by the applicant.

**7. Recommendation of City's permanent professional staff.**

Please see Staff's recommendation at the end of this report.

**8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.**

The proposed chiropractic clinic will not change the primary land use of the subject property. The land use is in conformity with the Future Land Use Map designation of Office/Employment Center.

**9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks, and recreation facilities, etc.**

The subject property is an established development. The property is adequately served by required utilities and services.

**10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.**

See the parking standards table below. The subject property has 40 of 41 required parking spaces that immediately serve the building. The Greystone Plaza development has a shared parking agreement between all lots. The remaining required parking space is located within the parking lot to the northwest. The parking requirements of the UDC are fulfilled between the available on-site parking and nearby shared parking.

TABLE 2: PARKING STANDARDS			
Use	Parking Formula	Required Parking	Provided Parking
Medical Clinic	1 space per 200 SF	41	40
Office	1 space per 250 SF		

**11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.**

The proposed use will not generate negative environmental impacts.

**12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.**

There are no proposed modifications to the stormwater system or increases to the overall number of impervious surfaces. The proposed SUP would not adversely affect the capacity or quality of water of the stormwater system. No natural streams are within the vicinity of the subject property.

**13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.**

Staff conducted a site visit as part of the review for the SUP application. Staff did not observe any zoning or code compliance issues on site.

## DEVIATIONS

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The applicant is not requesting any deviations.

## REVIEW PROCESS

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- *This special use permit requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the special use permit is tentatively scheduled for consideration from the City Council on November 21, 2023.*

## RECOMMENDATION FROM PROFESSIONAL STAFF

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★ **Conduct a Public Hearing.**

★ **Staff recommends approval of the proposed special use permit for Raw Health Company.**

- The use, “*Medical or Dental Clinic*”, is appropriate at the subject property for a period of five years, at which time the use can be reviewed to ensure compliance with the criteria for review.
- The project is consistent with Lenexa’s goals through **Strategic Community Investment** to create **Healthy People** and **Vibrant Neighborhoods**.

## SPECIAL USE PERMIT

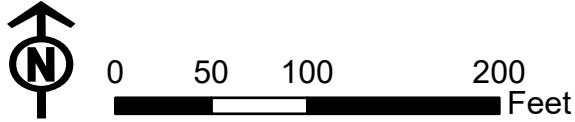
Staff recommends **approval** of SU23-11 - a special use permit for a medical clinic known as Raw Health Company at 12760 W. 87<sup>th</sup> Street, Suite 110, for a period of five years.



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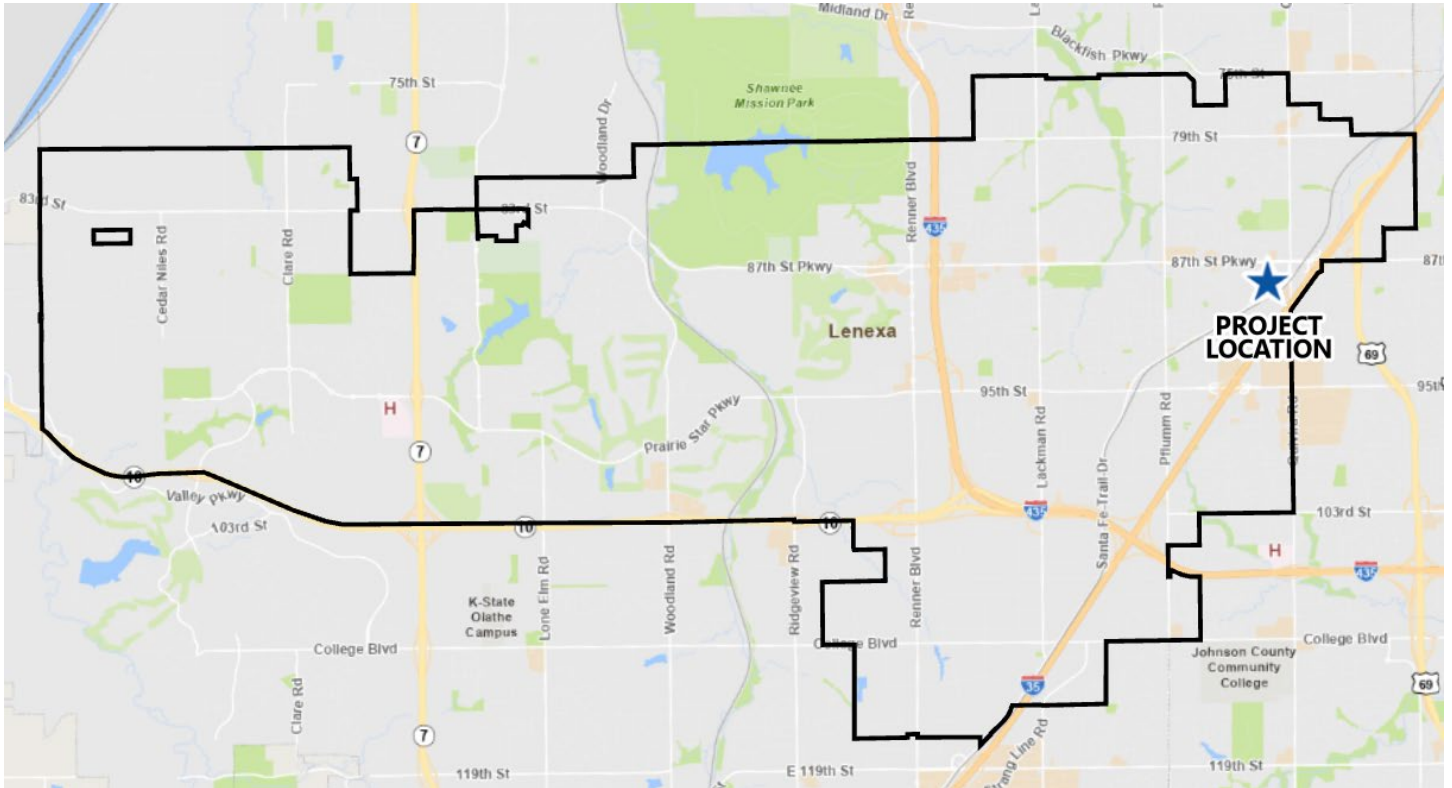
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For further information, please call 913-477-7500

# Raw Health Company SU23-11



## CEVA ANIMAL HEALTH

<b>Project #:</b>	PL23-09P	<b>Location:</b>	8940 Long Street
<b>Applicant:</b>	Jay Healy, GBA	<b>Project Type:</b>	Preliminary Plan
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Manufacturing



### PROJECT SUMMARY

The applicant requests approval of a preliminary plan/plat for a manufacturing facility at 8940 Long Street. The applicant proposes to construct a 42,000 SF building, parking, and related site improvements. The proposed building is two stories and 43.5 feet tall. The development includes construction of new public sidewalks along Long Street and Santa Fe Trail Drive. The applicant seeks approval of three deviations from the Unified Development Code (UDC). The deviation requests are related to setbacks and perimeter landscape areas. Staff supports the proposed deviations. This project does not require a Public Hearing.

### STAFF RECOMMENDATION: APPROVAL

## SITE INFORMATION

The site contains two parcels.

- 8940 Long Street is 2.7 acres and is part of Lot 3 of Ceva Biomune, Third Plat which was recorded April 29, 2016. This lot is owned by Ceva and has an auxiliary parking lot for the Ceva campus and outdoor storage.
- 12510 Santa Fe Trail Drive is .92 acre and is unplatted. This parcel is owned by Ceva and was formerly an office building for Cy Young Industries.

Previous applications on this site include:

- PL21-05P “Ceva 8940 Long Street” preliminary plan for an 18,500 SF manufacturing building and site improvements. This plan was approved by Governing Body on June 15, 2021.
- PL21-15FS “Ceva Long Street Mech Yard Addition” administrative final plan for the development of the first phase for parking and civil infrastructure. The final plan was approved by Staff on January 10, 2022 but was not constructed.

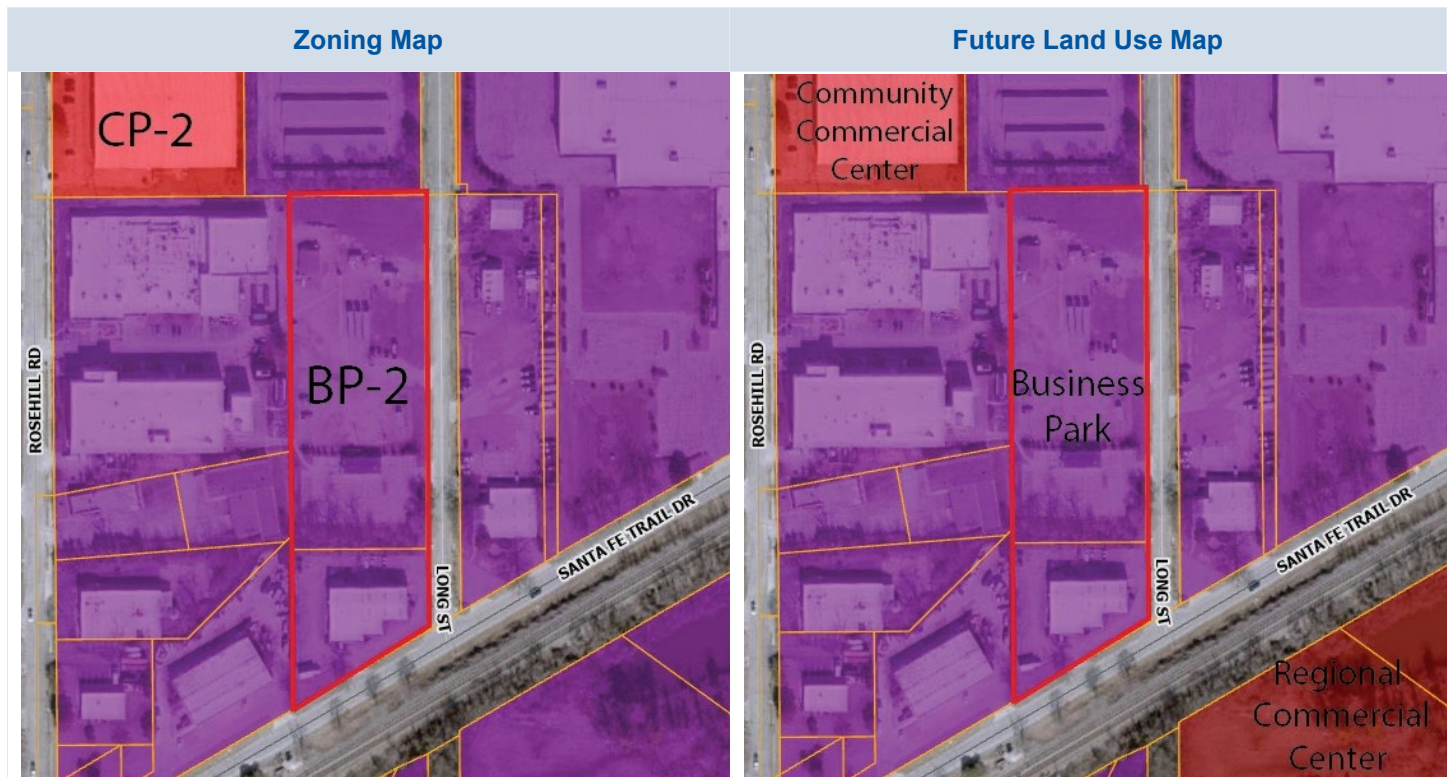
<b>LAND AREA (AC)</b> 3.62	<b>BUILDING AREA (SF)</b> 42,000	<b>CURRENT ZONING</b> BP-2	<b>COMP. PLAN</b> Business Park
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**Exhibit 1: Aerial Image of Subject Site**

## LAND USE REVIEW

The proposed use is a manufacturing facility with office space that will produce agricultural vaccines. This site will be part of the larger Ceva campus, which is located between Rosehill Road and Long Street, north of Santa Fe Trail Drive. Manufacturing is an allowable use within the BP-2, Planned Manufacturing Zoning District and complies with the designated Future Land Use of Business Park. Surrounding uses, other than the adjacent Ceva campus, include outdoor storage, warehousing, and other similar industrial uses that would be compatible with the proposed development.

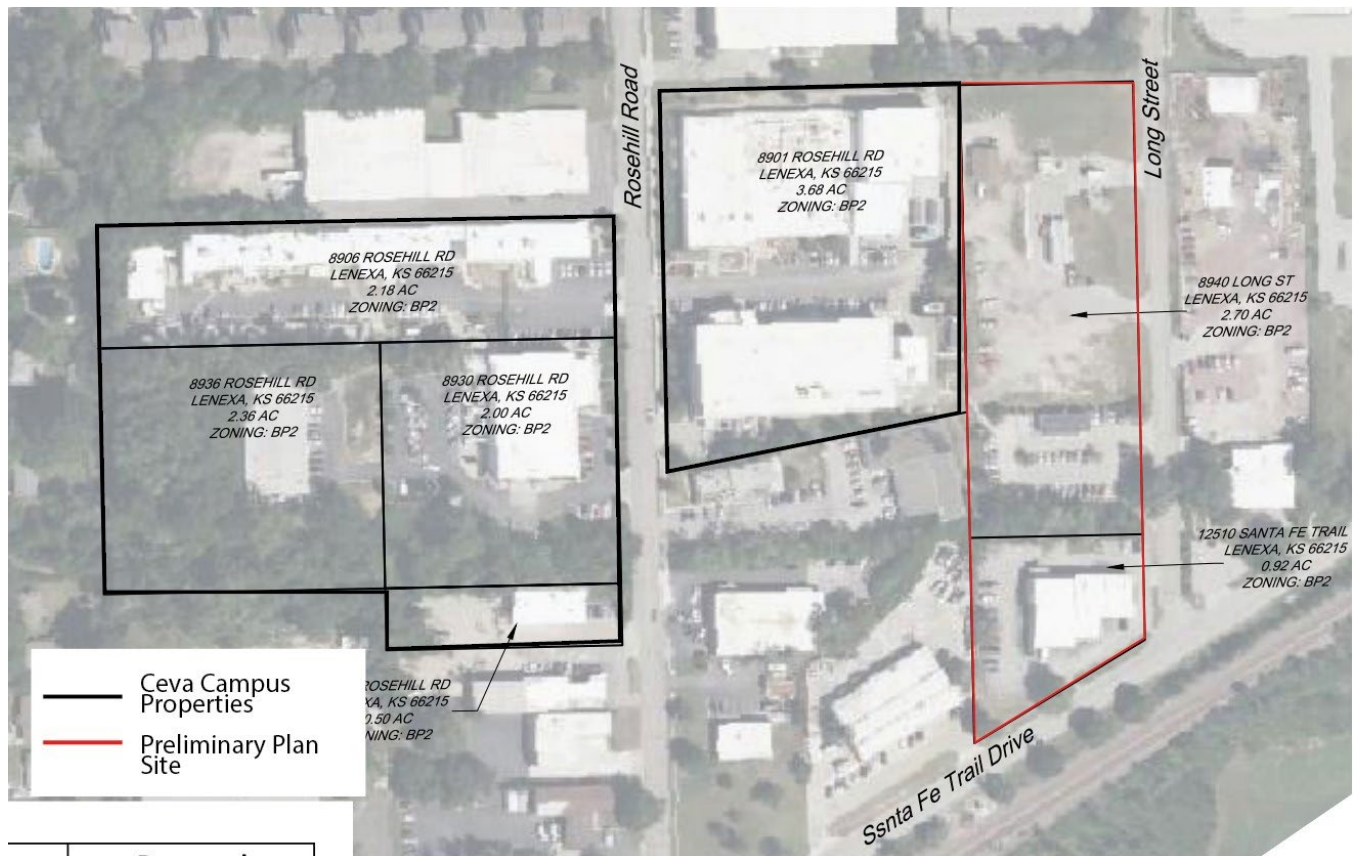


**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Business Park	BP-2, Planned Manufacturing District	Warehouse, Office & Surface Parking
<b>North</b>	Business Park	BP-2, Planned Manufacturing District	Self-Storage
<b>South</b>	Business Park	BP-2, Planned Manufacturing District	Warehouse
<b>East</b>	Business Park	BP-2, Planned Manufacturing District	Warehouse
<b>West</b>	Business Park	BP-2, Planned Manufacturing District	Manufacturing

## PRELIMINARY PLAN REVIEW

The site plan proposes a 42,000 SF industrial building located central to the site with a north parking lot and truck maneuvering area and a second parking lot to the south adjacent to Santa Fe Trail Drive. The site abuts the existing Ceva campus to the west and has a cross-access drive. Other improvements include public sidewalk installation and site landscaping. The preliminary plan is also intended to serve as the preliminary plat.



**Exhibit 2: Ceva Campus**

### DIMENSIONAL STANDARDS

The plans meet the dimensional standards of the zoning district with some requested deviations as discussed further in the Deviations section of the Staff Report. The proposal meets required building height limitations but does not meet all setback requirements.

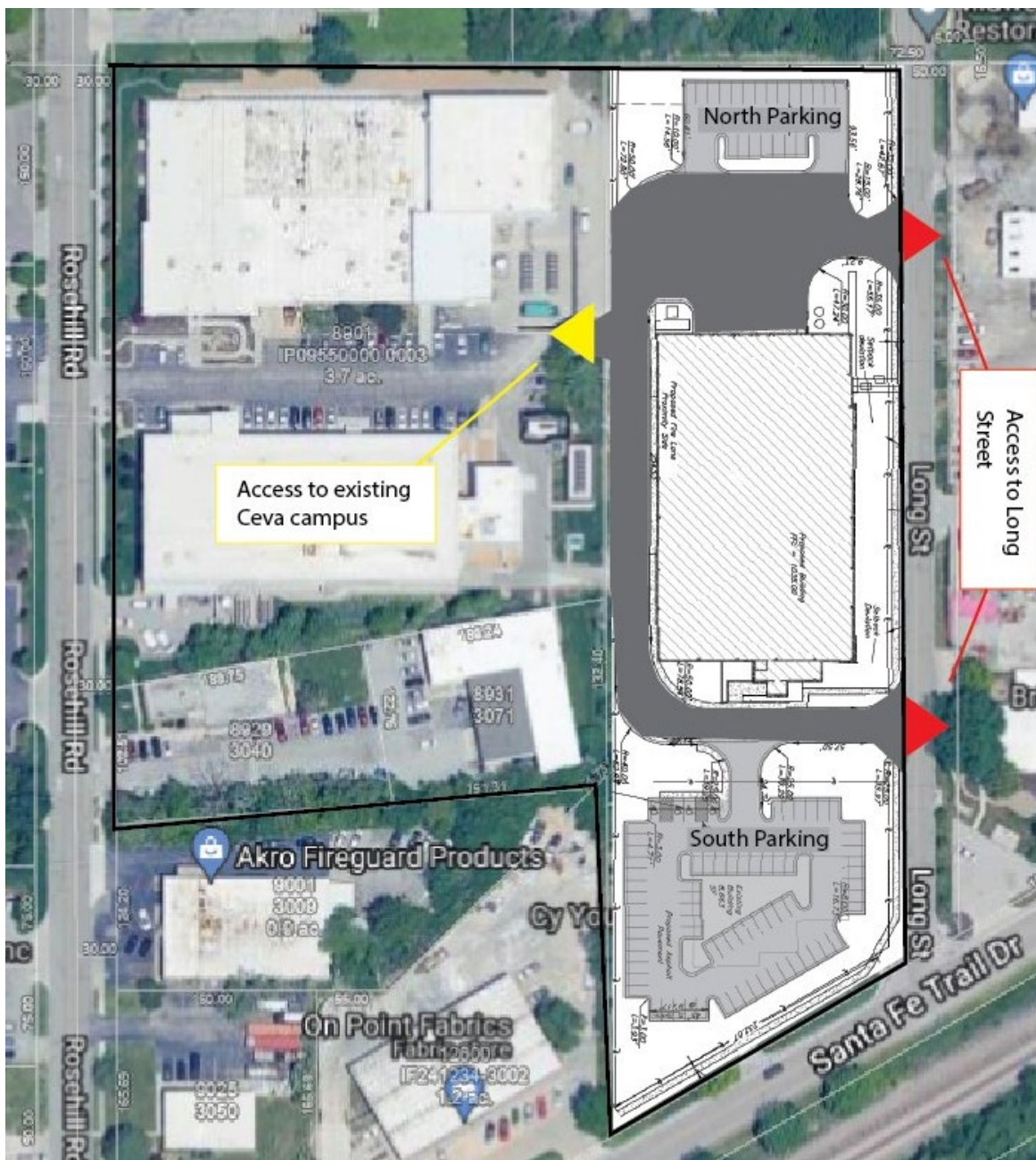
### PUBLIC IMPROVEMENTS

The site plan shows new public sidewalks on the west side of Long Street and north side of Santa Fe Trail Drive. The plan currently depicts a 5' sidewalk; however, Staff would like to work with the applicant to further refine site design and provide a 10' trail along Santa Fe Trail Drive, where a future trail is planned.

**TRAFFIC, ACCESS, AND PARKING**

The site abuts Long Street to the east and Santa Fe Trail Drive to the south. Santa Fe Trail Drive is designated as a collector street.

The site has two drive entrances from Long Street. The northern drive provides access to a truck maneuvering area and the north parking lot. The southern drive accesses the primary entrance of the building and the southern parking lot. A drive connects both entrances on the west side of the building, where there is also a connecting drive into the adjacent Ceva property to the west and access through the parking lot to Rosehill Road.



**Exhibit 3: Access points, parking and drive locations.**



The site has a current parking lot that serves as overflow parking for the Ceva property to the west. The new development proposal includes a surplus of 33 parking spaces above what is required by code for the new building. The additional parking will serve to replace some parking for the building to the west. An internal sidewalk connection is required to connect the new southern parking lot to the west to serve those parking in that area and walking to the other building. Additional sidewalk connections are provided from the parking lot to the main entrance and from the internal sidewalk network to the new public sidewalks on Long Street and Santa Fe Trail Drive. The applicant will also need to identify the location for four bicycle parking stalls with the final plan.

**TABLE 2: PARKING ANALYSIS**

Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Office (4,700 SF)	1/250 SF	19		
Warehousing (4,000 SF)	1/1,500 SF	3		
Manufacturing/ Production (33,000 SF)	1/1,000 SF	33		
	<b>Total</b>	<b>55</b>	<b>88</b>	<b>+33</b>

**STORMWATER**

The applicant submitted a preliminary stormwater management plan, indicating the intent to meet the City’s stormwater standards and requirements.

The existing parking lot has an existing underground infiltration facility, which will be removed with the demolition of the existing parking lot. A new underground infiltration facility will be constructed on the north end of the property, which will provide both water quality components as well as the detention components required of the site.

The existing segment of the open drainage channel running through the property is proposed to be boxed for a short distance while it is adjacent to the new parking lot/drive aisle.

**FIRE PREVENTION**

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

**LIGHTING**

A complete photometric plan is required at the time of final plan submittal and will be reviewed at that time.

**LANDSCAPING**

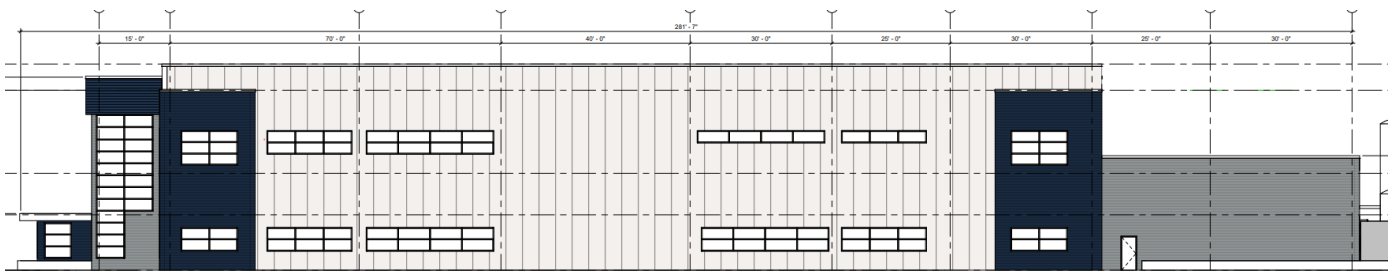
Santa Fe Trail Drive, a collector road, requires a landscape buffer area with 25 feet depth. The applicant proposes a reduced landscape area depth of 16 feet but is still providing the required number of plantings within the area. The deviation request is discussed further in the *Deviations* section of the Staff Report. To mitigate the

reduced landscape area along Santa Fe Trail Drive, the applicant is providing landscaping along the east and south walls of the building within greenspace areas as a countermeasure to the requested deviations. Additional perimeter plantings are required along Long Street and other property lines to the north and west. Parking lot landscaping is also provided within islands.

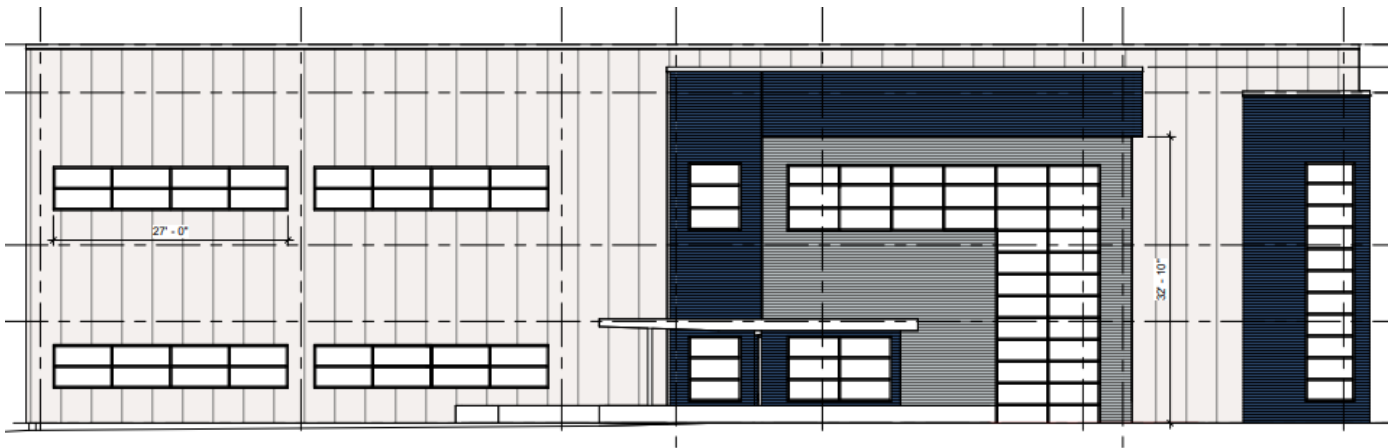
## ARCHITECTURE

The preliminary plan includes one, two-story building with a proposed floor area of 47,000 SF and a height of 43.5 feet.

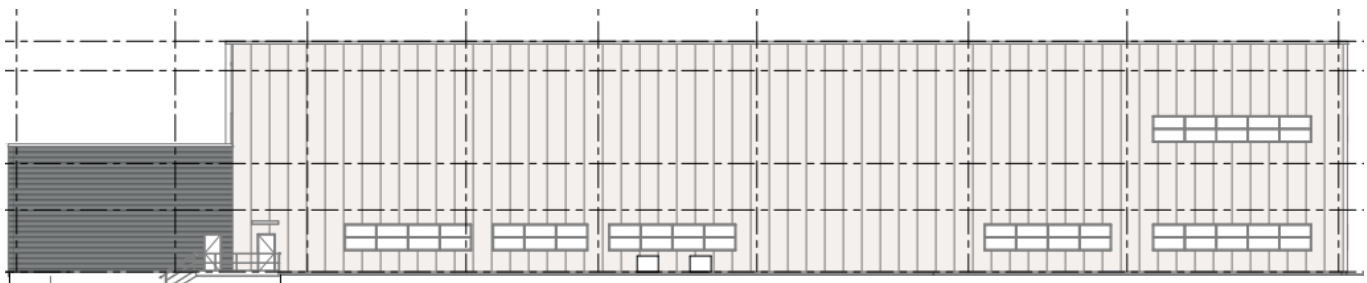
The architecture will match the aesthetic of other buildings on the Ceva campus. The new building will utilize the existing color scheme of white, gray, and blue for the campus. Proposed materials include vertical and horizontal insulated panels, which match the existing Ceva buildings.



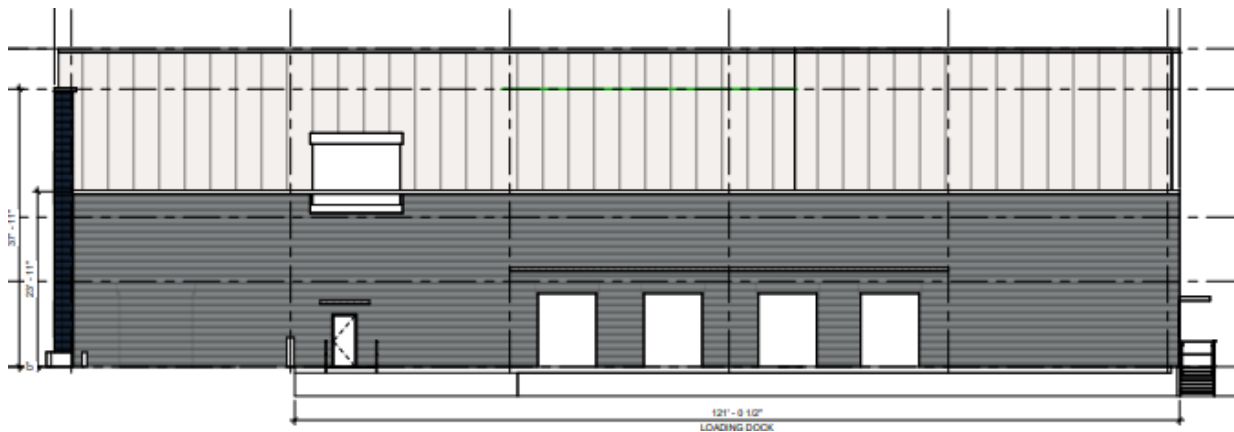
**Exhibit 4: East elevation facing Long Street.**



**Exhibit 5: South elevation facing Santa Fe Trail Drive.**



**Exhibit 6: West elevation.**



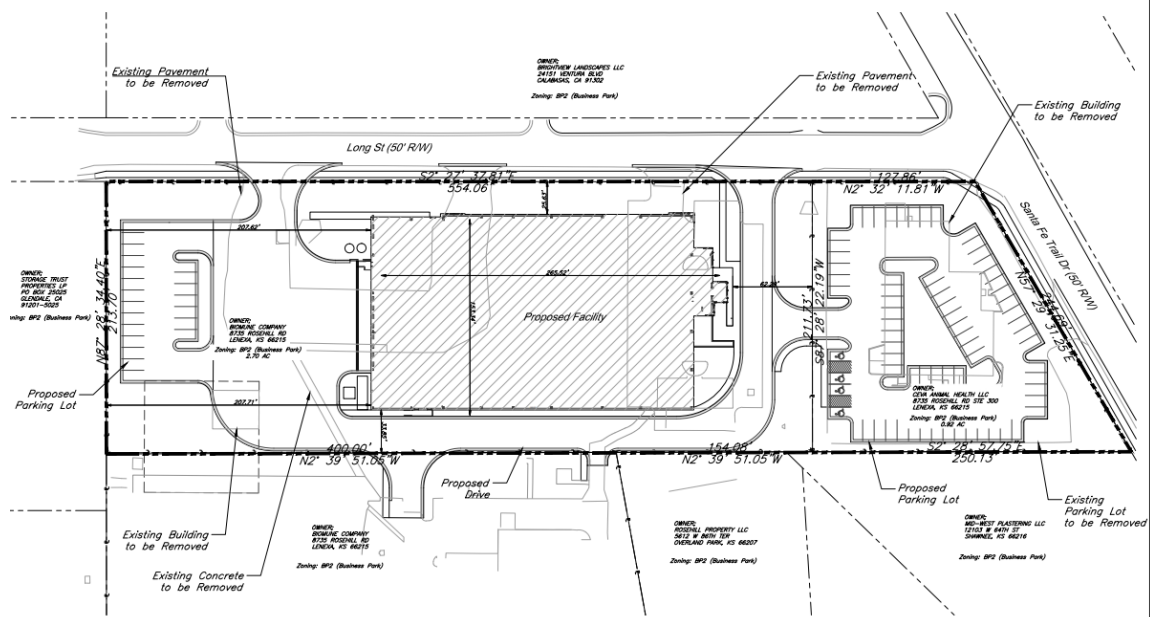
**Exhibit 7: North elevation.**

## PRELIMINARY PLAT REVIEW

Two parcels, one platted and one unplatted, will be joined into one lot covering 3.62 acres. The intended use of the lot is manufacturing and office. The preliminary plan is serving as the preliminary plat.

The lot has right-of-way frontage on Long Street to the east and Santa Fe Trail Drive to the south but only has vehicular access from Long Street and a cross-access drive shared with the adjacent Ceva properties to the west. With the final plat, a Limits of No Access will be marked along Santa Fe Trail Drive. Utilities are available to the site.

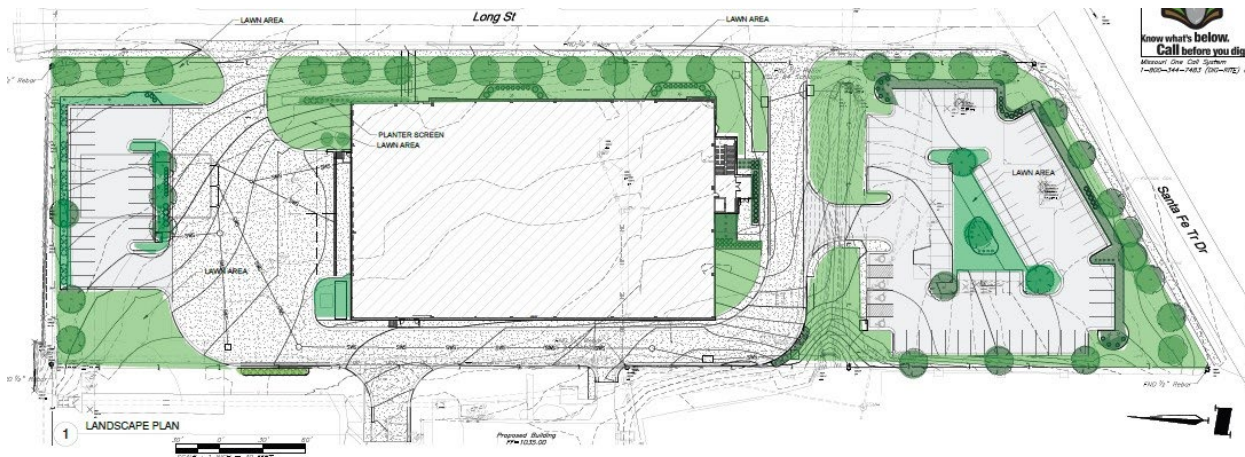
### PRELIMINARY PLAT FOR CEVA ANIMAL HEALTH



**Exhibit 8: Preliminary Plat**

## DEVIATIONS

The applicant is requesting deviations related to setbacks and landscaping. The applicant will provide enhancements beyond code requirements to landscaping around the base of the building to offset the loss of landscaped area where deviations are requested. Each deviation is discussed in further detail in the following paragraphs.



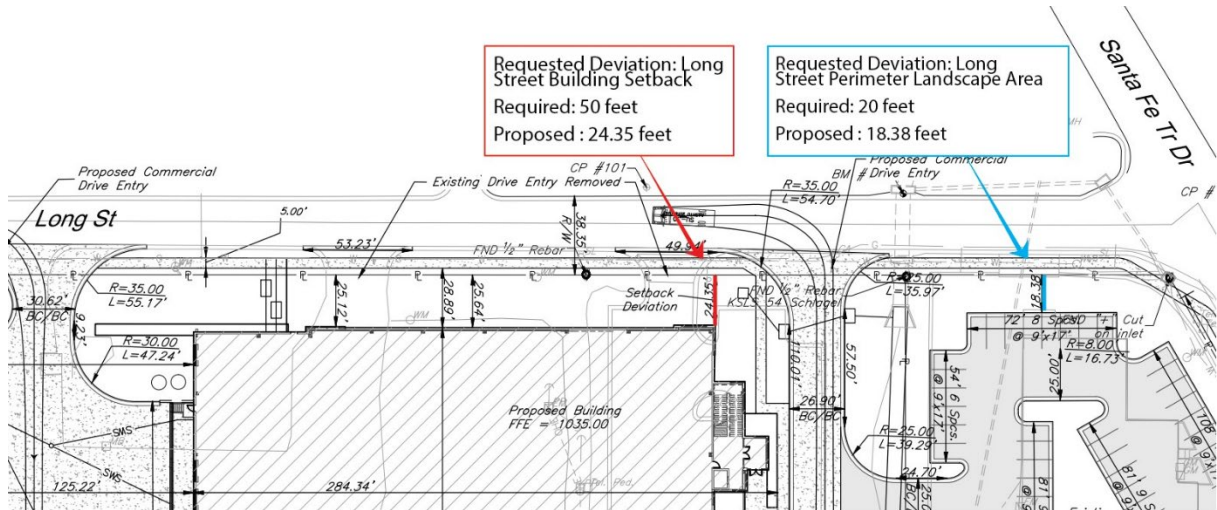
**Exhibit 9: Landscape plan**

**TABLE 3: SETBACK ANALYSIS**

Yard	Required Minimum Setback	Proposed Setback	Difference
<b>Building Setback (Long Street)</b>	50 feet	24.35 feet	- 25.65 feet
<b>Landscape Depth (SE Corner - Long Street)</b>	20 feet	18.38 feet	-1.62 feet
<b>Landscape Depth (West Property Line)</b>	10 feet	1 foot to 7.64 feet	- 9 to -2.36 feet
<b>Landscape Depth (Santa Fe Trail Drive)</b>	25 feet	16 feet	-9 feet

### LONG STREET: BUILDING SETBACK AND PERIMETER LANDSCAPE AREA

The applicant is requesting a reduced building setback from the standards of the BP-2 Zoning District. The required building setback of 50 feet from Long Street as designated by the zoning district requirements would be difficult to achieve given the long and narrow shape of the lot. Relocation of the building to be setback further from Long Street would mean the drive aisle to the west of the building would need to be removed. Staff understands this drive aisle is important for providing cross-access and connection with the larger Ceva campus to the west. The applicant has also requested a reduced landscape depth for a local road between Long Street and the south parking lot. The applicant proposed to reduce the required landscape area depth of 20 feet minimally by 1.6 feet but will still meet the required count for landscape material. For these reasons, Staff supports these deviation requests.



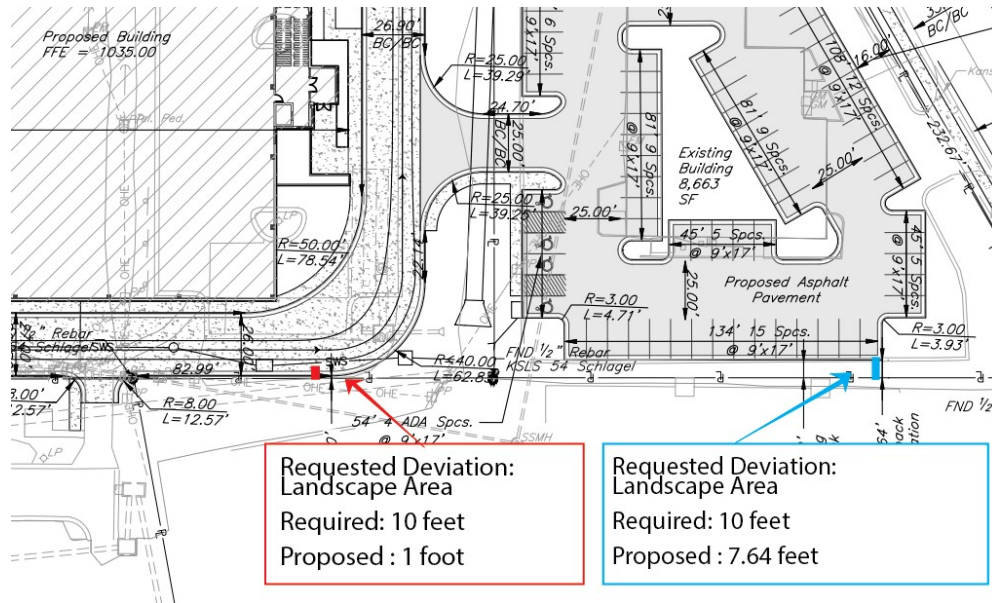
**Exhibit 10: Long Street Building Setback and Perimeter Landscape Area Deviation Request.**

**LANDSCAPE AREA: WEST PROPERTY LINE**

The applicant is requesting a reduction to the minimum depth of the landscape area along the west lot line. For perimeter lot lines not adjacent to rights-of-way, a minimum landscape area with a depth of 10 feet is required by Section 4-1-D-2-L. This deviation will cover a reduced setback of 1 foot from the west property line to the drive aisle and will also cover the south parking lot for which a landscape area setback of 7.64 feet from the west property line is proposed.

The reduction for the drive aisle is requested for two reasons, first again taking into consideration the narrow size of the lot, it would be difficult to maintain the drive aisle setback without requiring a further deviation from Long Street building setback. Second, the west property line is an internal property line that abuts the existing Ceva campus. The proposed new site will flow continuously into the existing development and is intended to function as a larger complex rather than a standalone building, requiring a lesser need for separation between the two lots. Reducing the landscape area to one foot does not leave adequate space for plantings between the drive aisle and the property line.

The reduced landscape area for the parking lot is proposed to be consistent with the previously approved parking lot in the same location, which had a setback of 6.82 feet, and also to preserve as much parking as possible for the shared campus.

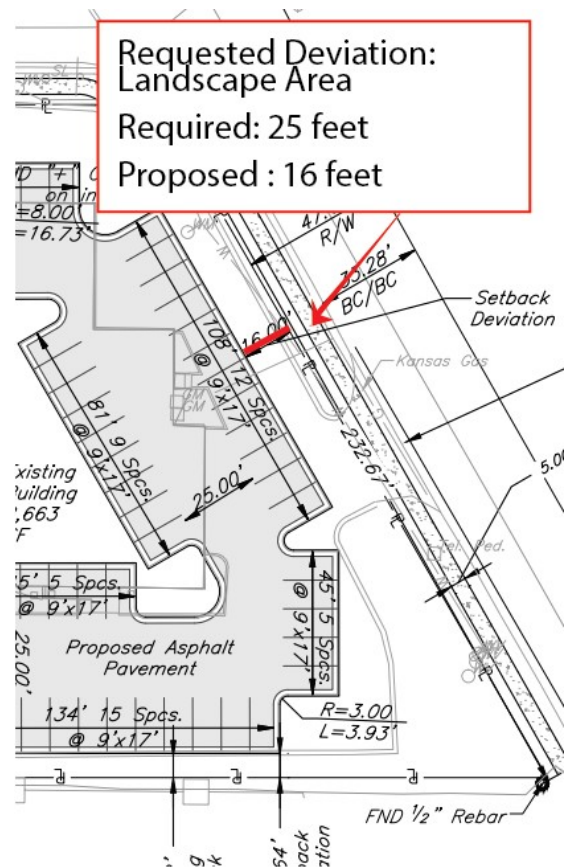


**Exhibit 11: West Perimeter Landscape Area Depth Deviation Request.**

**LANDSCAPE AREA: SANTA FE TRAIL DRIVE**

The applicant is requesting a reduced landscape area between the parking lot and right-of-way adjacent to Santa Fe Trail Drive. As a designated Collector Road, Santa Fe Trail Drive has a required minimum landscape area with a depth of 25 feet in accordance with Section 4-1-D-2-L of the UDC. The applicant is requesting a reduced landscape area depth of 16 feet. Despite the reduced depth of the landscape area, the proposal still meets the required count of plantings. The proposed landscape area of 16 feet with shrubs and trees is an improvement upon the current site layout which has approximately seven feet and two trees. The proposed layout is also consistent with or in greater compliance than adjacent properties which have landscape areas between approximately zero to 16 feet along Santa Fe Trail Drive. For these reasons, Staff supports the deviation request.

**At Right: Exhibit 12: Santa Fe Trail Drive Landscape Area Depth Deviation Request.**



★ **Discussion Item #1: The Planning Commission must consider the requested deviations for setback and landscape depth.**

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## REVIEW PROCESS

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- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration by the City Council on November 21, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

---

## RECOMMENDATION FROM PROFESSIONAL STAFF

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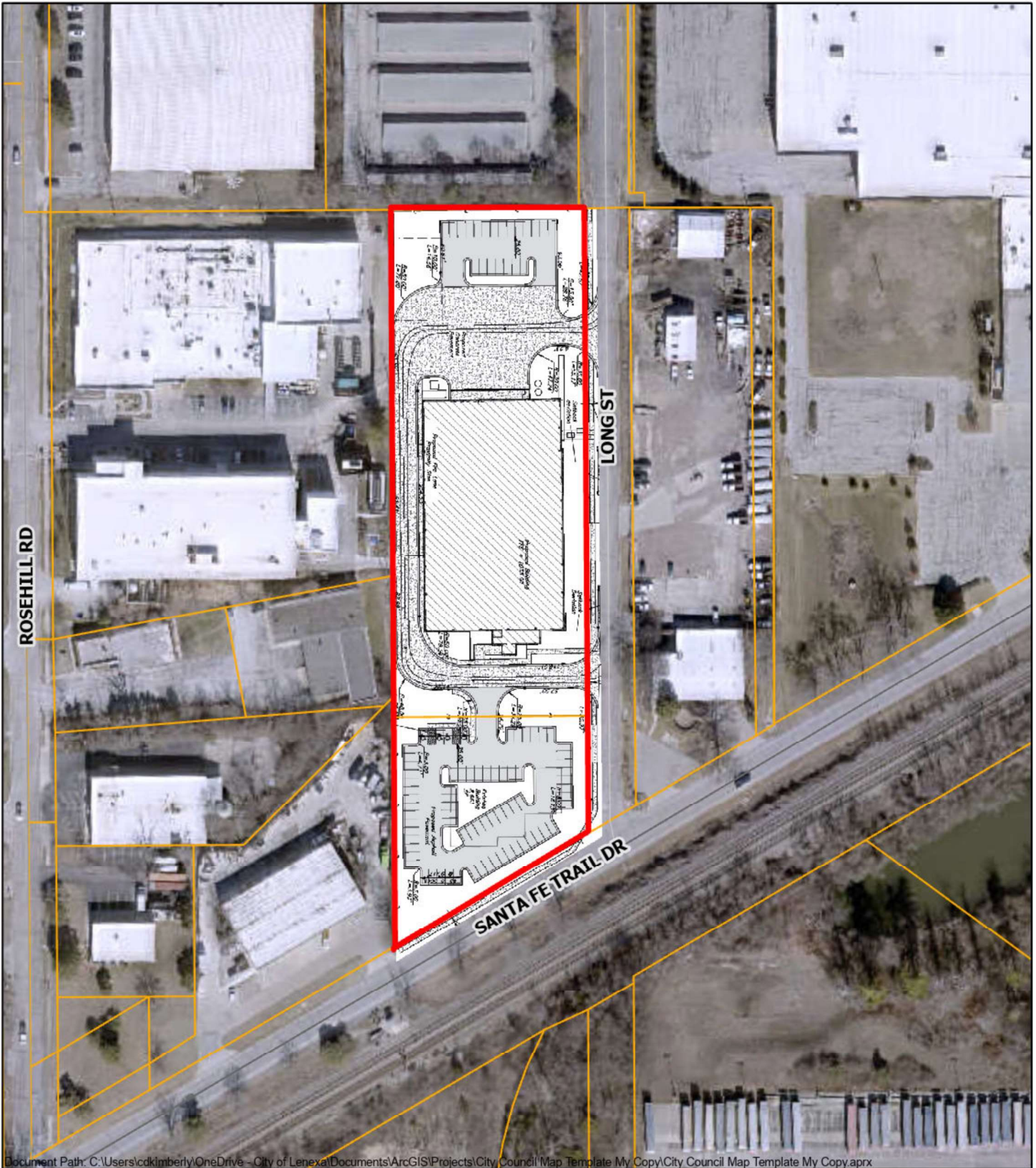
★ **Conduct a Public Hearing.**

★ **Staff recommends approval of the proposed Preliminary Plan for Ceva Animal Health.**

- This is a preliminary plan for one manufacturing building and parking for the Ceva campus near Santa Fe Trail Drive and Long Street. The preliminary plan serves as the preliminary plat.
- The project is consistent with Lenexa's goals through *Responsible Economic Development* and *Strategic Community Investment* to create a *Thriving Economy*.

### PRELIMINARY PLAN

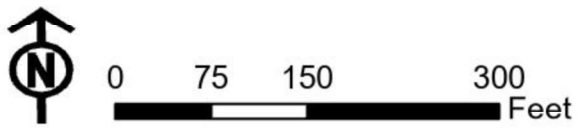
Staff recommends **approval** of the preliminary plan and deviations for PL23-09P - **Ceva Animal Health** at 8940 Long Street, for a manufacturing and office building.



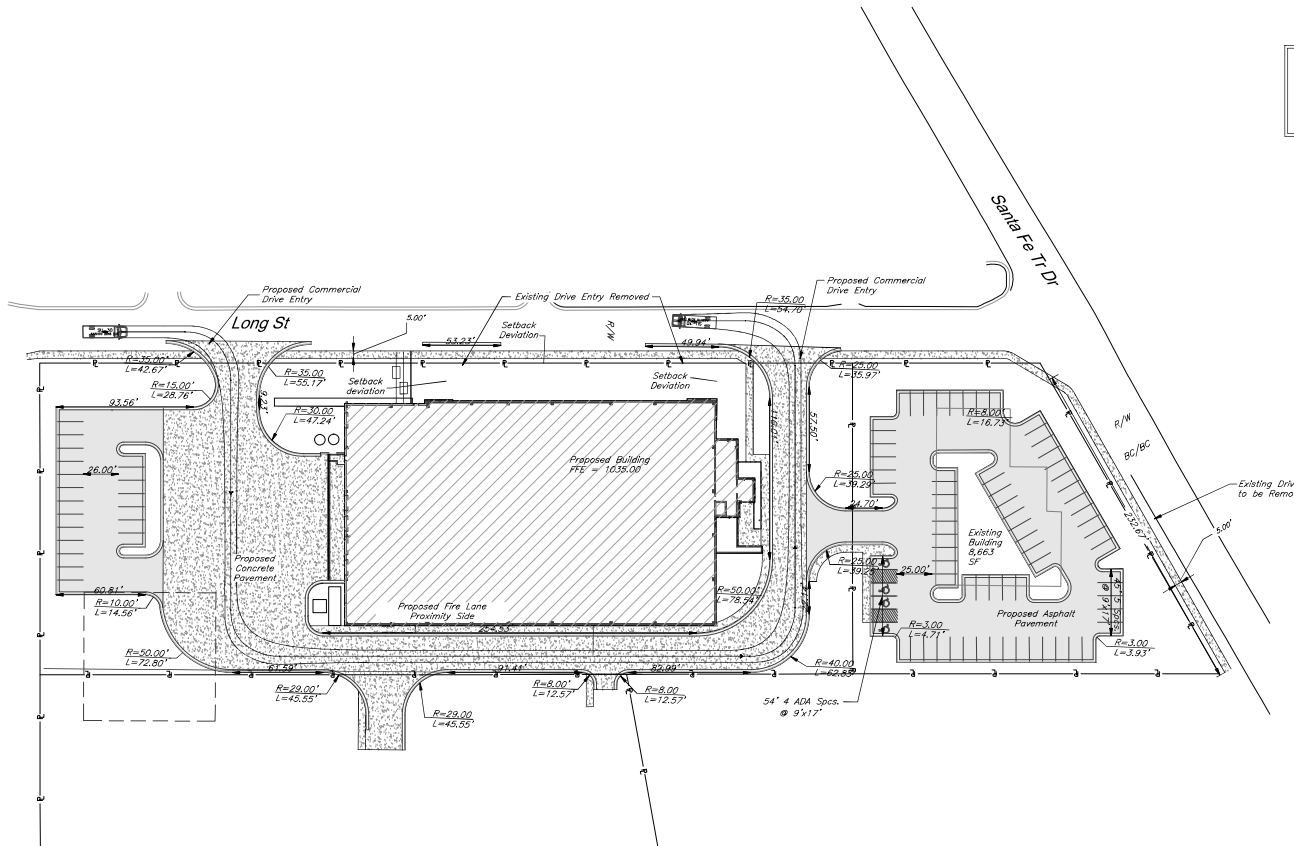
Document Path: C:\Users\cdkimberly\OneDrive - City of Lenexa\Documents\ArcGIS\Projects\City Council Map Template My Copy\City Council Map Template My Copy.aprx

Data Source: City of Lenexa and Johnson County Kennes  
For further information, please call 913-477-7500

# Ceva Animal Health PL23-09P



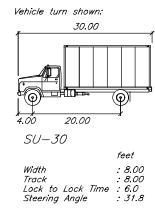




**CAUTION!**  
 Numerous Utilities on Site.  
 Contractor to verify location  
 and elevation of all utilities  
 prior to commencing  
 construction.

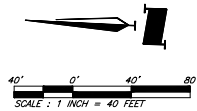


Missouri One Call System  
 1-800-344-7483 (DIG-RITE) or 811



**Legend**

⊙	Cable TV Pedestal	—	Centerline
⊕	Power Pole	—	Fiber Optic Line
⊖	Guy Anchor	—	Gas Line
⊕	Electrical Manhole	—	Guard Rail
⊕	Electric Meter	—	Over Head Electric
⊕	Electrical Transformer	—	Over Head Telephone
⊕	Electric Pedestal	—	Over Head Cable TV
⊕	Power Pole/Telephone Pole	—	Property Line
⊕	Power Pole/Light Pole	—	Right-of-Way Line
⊕	Bollard/Guard Post	—	Sanitary Sewer Line
⊕	Gas Meter	—	Stream
⊕	Gas Valve	—	Tire Line
⊕	Curb Inlet	—	Underground Electric
⊕	Junction Box	—	Underground Telephone
⊕	Sanitary Sewer Manhole	—	Underground Cable TV
⊕	Sanitary Cleanout	—	Water Line
⊕	Light Pole	—	Proposed Storm Sewers
⊕	Yard Light	—	Existing Storm Sewers
⊕	Benchmark	—	Fire Hydrant
⊕	Test Pit	—	Water Meter
⊕	Control Point	—	Asphalt Pavement
⊕	Telephone Manhole	—	Concrete Pavement
⊕	Telephone Pedestal	—	Fence
⊕	Telephone Pole	—	
⊕	Traffic Signal Controller Box	—	



**GBA**

9801 Renner Blvd., Ste. 300  
 Lenexa, KS 66219  
 913.662.0000  
 gbateam.com

**CEVA - LONG STREET PROJECT**  
 6940 LONG STREET  
 LENEXA, KANSAS 66215

PROPOSED FACILITY FOR:



REVISION  
 10/23/23 City Comments

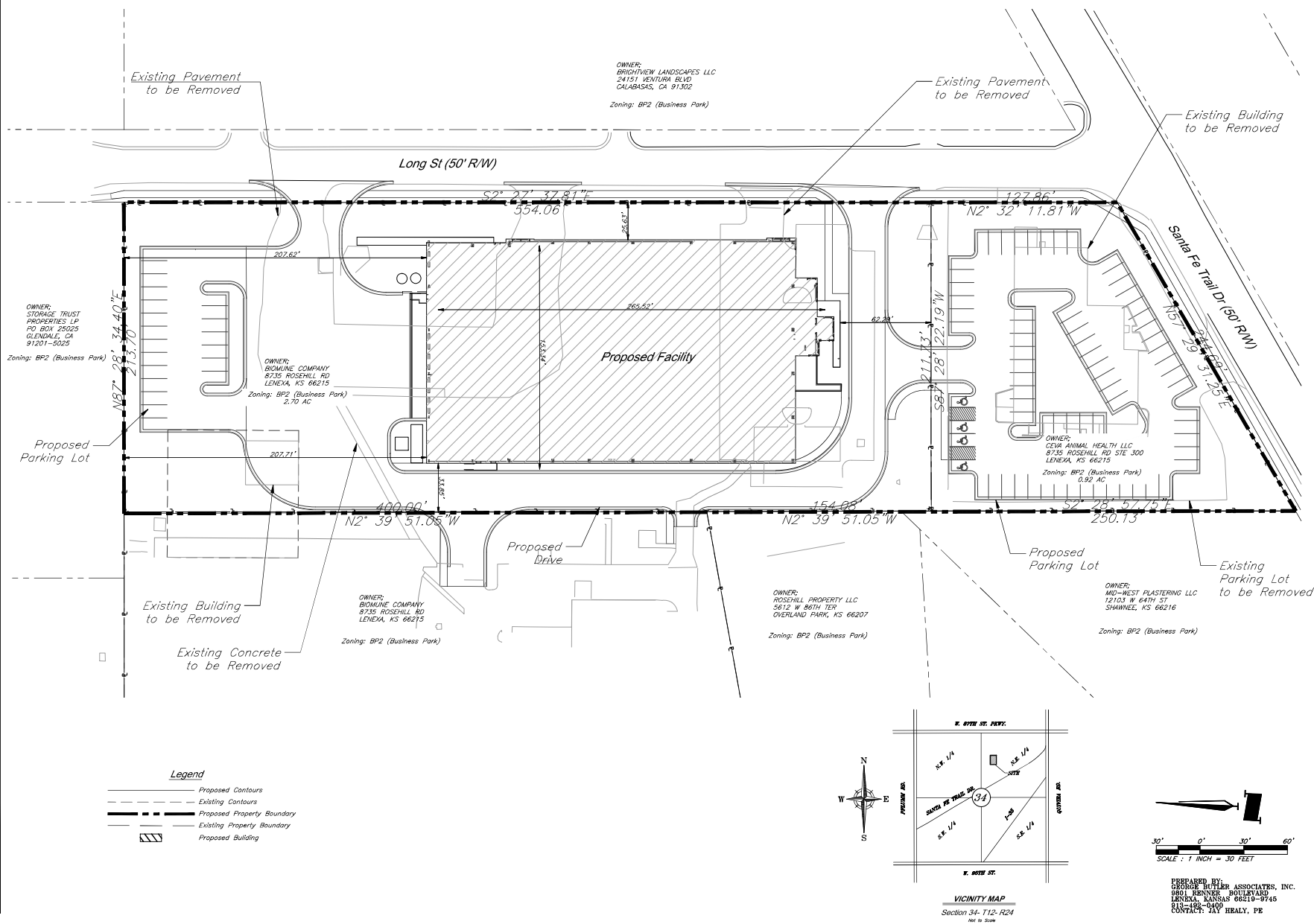
PROJECT NUMBER  
 15555  
 DATE  
 10/23/23

DESIGNED  
 JRH  
 DRAWN  
 BICK, ELS  
 REVIEWED  
 JRH  
 SHEET TITLE

**Dimension Plan**

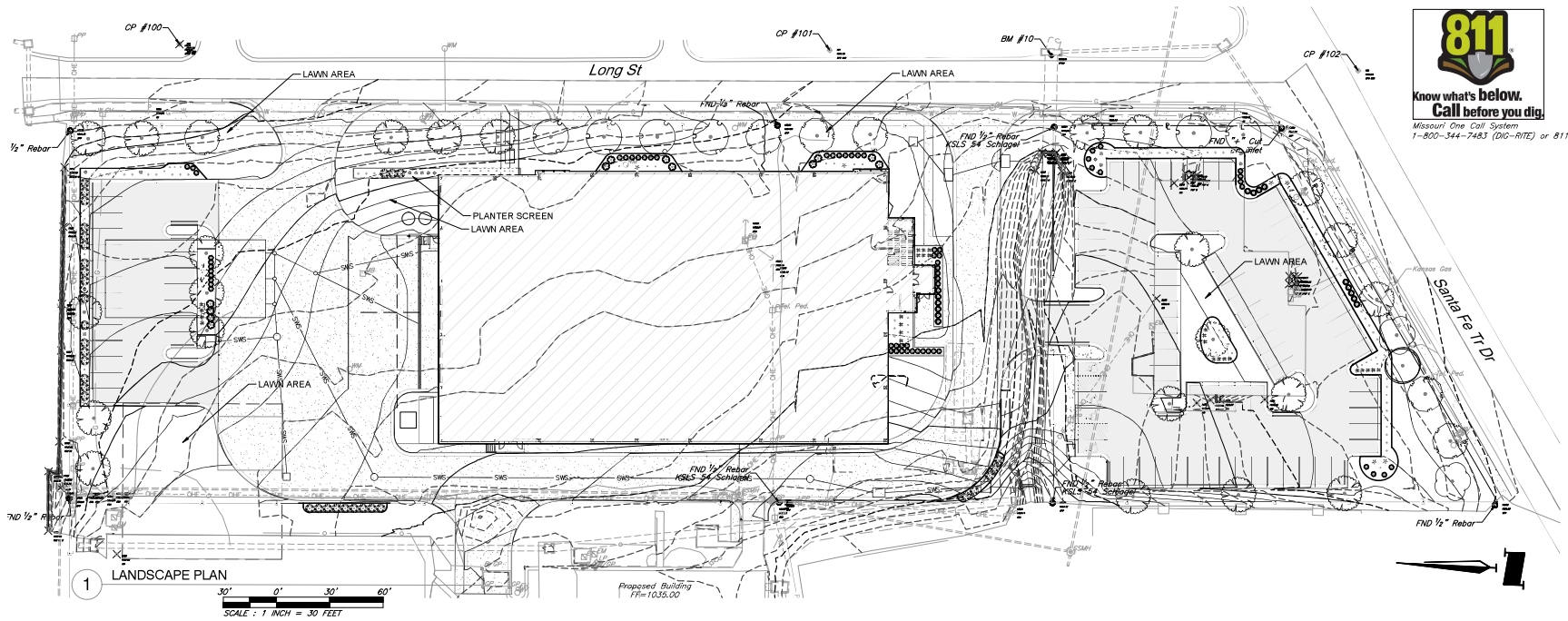
SHEET NUMBER  
**C3**

# PRELIMINARY PLAT FOR CEVA ANIMAL HEALTH



G:\15555\Civil\3D\Production Drawings\Preliminary Development\Plan\1555505050.dwg Layout: SB Preliminary Plat Monday, October 23, 2023, 11:22am Copyright 2023, George Butler Associates, Inc.

C:\Users\Gibb\OneDrive\Documents\Drawings\Preliminary Development\Plan\15555510100.dwg Layout: L1 Landscape Plan Monday, October 23, 2023, 11:13 am Copyright 2023, George Buler Associates, Inc.



PROPOSED FACILITY FOR:  
**CEVA - LONG STREET PROJECT**  
 6940 LONG STREET  
 LENEXA, KANSAS 66215

**PLANT SCHEDULE**

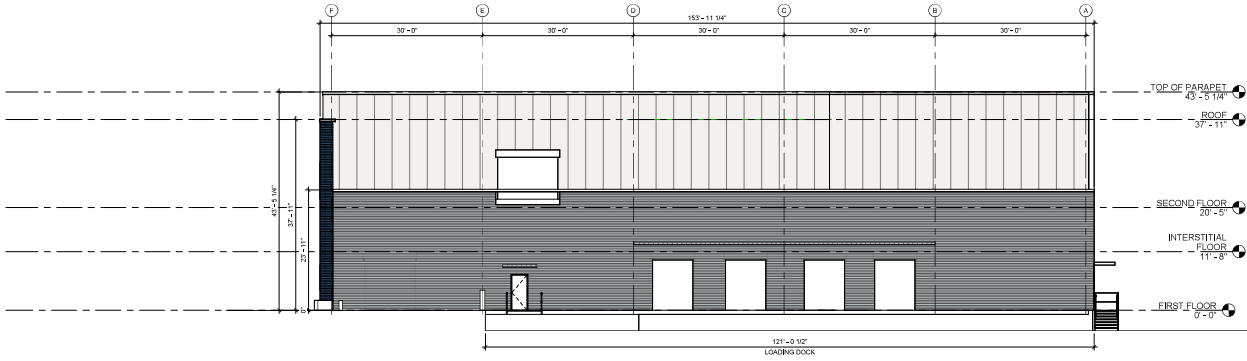
TREES	CODE	BOTANICAL / COMMON NAME	SIZE	CONTAINER	QTY
	ACE MTN	Acer miyabei 'Morton' / State Street™ Miyabe Maple	2" Cal.	B&B	6
	GLE LNE	Gleditsia triacanthos inermis 'Skycode' / Skyline® Honey Locust	2" Cal.	B&B	23
	QUE BIC	Quercus bicolor / Swamp White Oak	2" Cal.	B&B	6
FLOWERING TREES	CODE	BOTANICAL / COMMON NAME	SIZE	CONTAINER	QTY
	CER FOR	Cercis canadensis 'Forest Pansy' / Forest Pansy Eastern Redbud	2" Cal.	B&B	1
	MAL JE2	Malus x 'Newbold' / Red Jewel™ Crabapple	2" Cal.	B&B	2

**PLANT SCHEDULE**

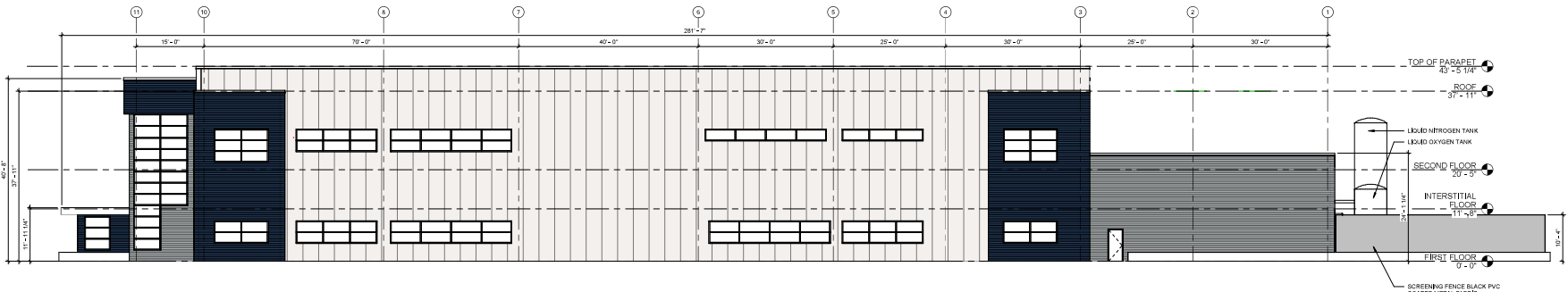
SHRUBS	CODE	BOTANICAL / COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	CHA GMP	Chamaecyparis pfeiffera 'Golden Mop' / Golden Mop Threadleaf Sawara Cypress	3 gal.	Pot	16	
	FOT BL5	Fothergilla major 'Blue Shadow' / Blue Shadow Fothergilla	3 gal.	Pot	7	
	FOT LOS	Fothergilla x intermedia 'NCF1' / Legend of the Small® Fothergilla	2 gal.	Pot	32	
	HYP JOH	Hypericum kalmianum / Kalm St. Johnswort	3 gal.	Pot	12	
	HYP LES	Hypericum x 'Depp' / Sunny Boulevard® St. John's Wort	3 gal.	Pot	3	
	LAG EDS	Lagerstroemia indica x laural 'SBLAG B9' / Enduring™ Summer Red Crape Myrtle	3 gal.	Pot	12	
	PHY TJ6	Physocarpus opulifolius 'SMPOTW' / Tiny Wine® Ninebark	3 gal.	Pot	19	
	SAM ORR	Sambucus racemosa 'SMNSRD' / Lemon Lace® Elderberry	3 gal.	Pot	6	
	WEI TFP	Weigela florida 'Verweig' / My Moral Purple Effect® Weigela	3 gal.	Pot	29	
EVERGREEN	CODE	BOTANICAL / COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	JUN FOR	Juniperus chinensis 'Sea Green' / Sea Green Juniper	3 gal.	Pot	51	
	JUN BL7	Juniperus squamata 'Blue Star' / Blue Star Juniper	3 gal.	Pot	15	
	JUN SKY	Juniperus virginiana 'Skyrocket' / Skyrocket Eastern Redcedar	3 gal.	Pot	14	
	JUN AR2	Juniperus x pfitzeriana 'Armstrongii' / Armstrong Pfitzer Juniper	3 gal.	Pot	34	
	THU ATB	Thuja occidentalis 'Yrt Boe' / North Pole® Arborvitae	3 gal.	Pot	7	
GRASSES	CODE	BOTANICAL / COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	CAL STC	Callamagrostis x acutiflora 'Stricut' / Karl Foerster Feather Reed Grass	2 gal.	Pot	26	

REVISION
10/23/23 City Comments
PROJECT NUMBER
15555
DATE
10/2/23
DESIGNED
JRH
DRAWN
BCK, ELS
REVIEWED
JRH
SHEET TITLE
Landscape Plan

SHEET NUMBER  
**L1**  
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 Revision 008 8-15  
 Last Printing 008 03-4



**OVERALL NORTH ELEVATION**  
 B-1108-A3.01 SCALE: 1/4" = 10'-0"



**OVERALL EAST ELEVATION**  
 B-1108-A3.01 SCALE: 1/4" = 10'-0"



**MATERIAL & FINISH LEGEND**

- VERTICAL INSULATED METAL PANEL  
TYPE 1 BONE WHITE
- HORIZONTAL COLLECTED INSULATED METAL PANEL  
TYPE 2 TANGIE BLUE
- HORIZONTAL COLLECTED INSULATED METAL PANEL  
TYPE 3 DOVE GRAY

REV	DATE	DESCRIPTION

NOT FOR CONSTRUCTION

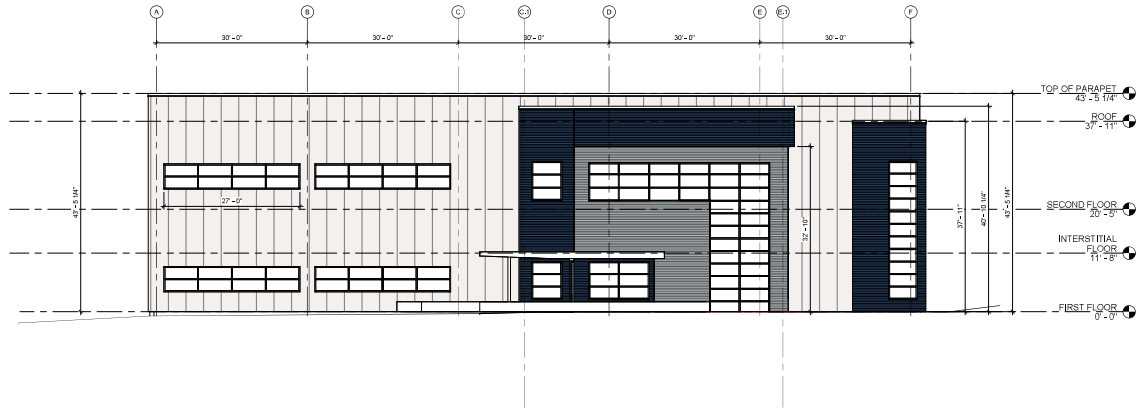
PROJECT NUMBER  
 PLANNING AND ZONING

DATE  
 10/23/2023

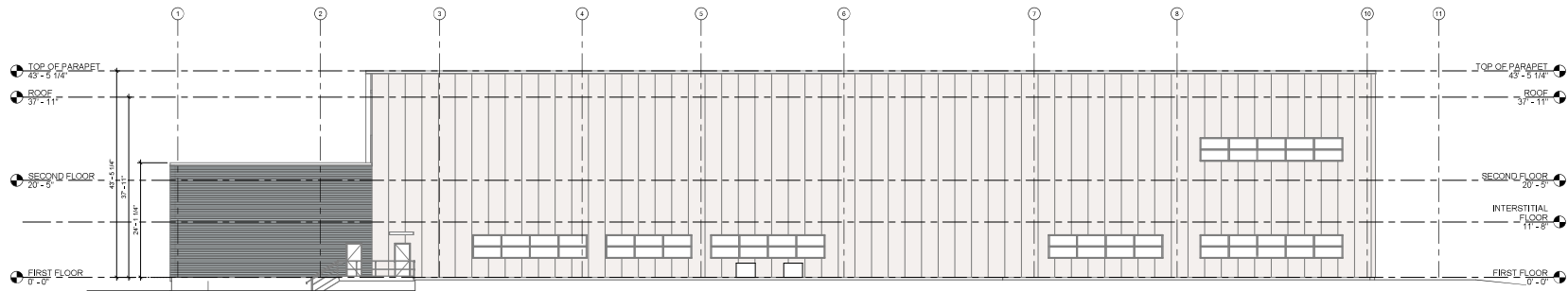
DESIGNED: Designer  
 DRAWN: Author  
 REVIEWED: Checker

SHEET TITLE  
 EXTERIOR BUILDING  
 ELEVATIONS

SHEET NUMBER  
**B-A3.01**



**OVERALL SOUTH ELEVATION**  
SCALE: 1" = 10'-0"



**OVERALL WEST ELEVATION**  
SCALE: 1" = 10'-0"



**MATERIAL & FINISH LEGEND**

- [Vertical Insulated Metal Panel] VERTICAL INSULATED METAL PANEL TYPE 1 BONE WHITE
- [Dark Blue Panel] HORIZONTAL COLLABATED INSULATED METAL PANEL TYPE 2 TAHOE BLUE
- [Dark Gray Panel] HORIZONTAL COLLABATED INSULATED METAL PANEL TYPE 3 SOVE GRAY

**CEVA ANIMAL HEALTH  
LONG STREET FACILITY  
8940 LONG STREET  
LENEXA, KANSAS 66215**

REV	DATE	DESCRIPTION

*NOT FOR CONSTRUCTION*

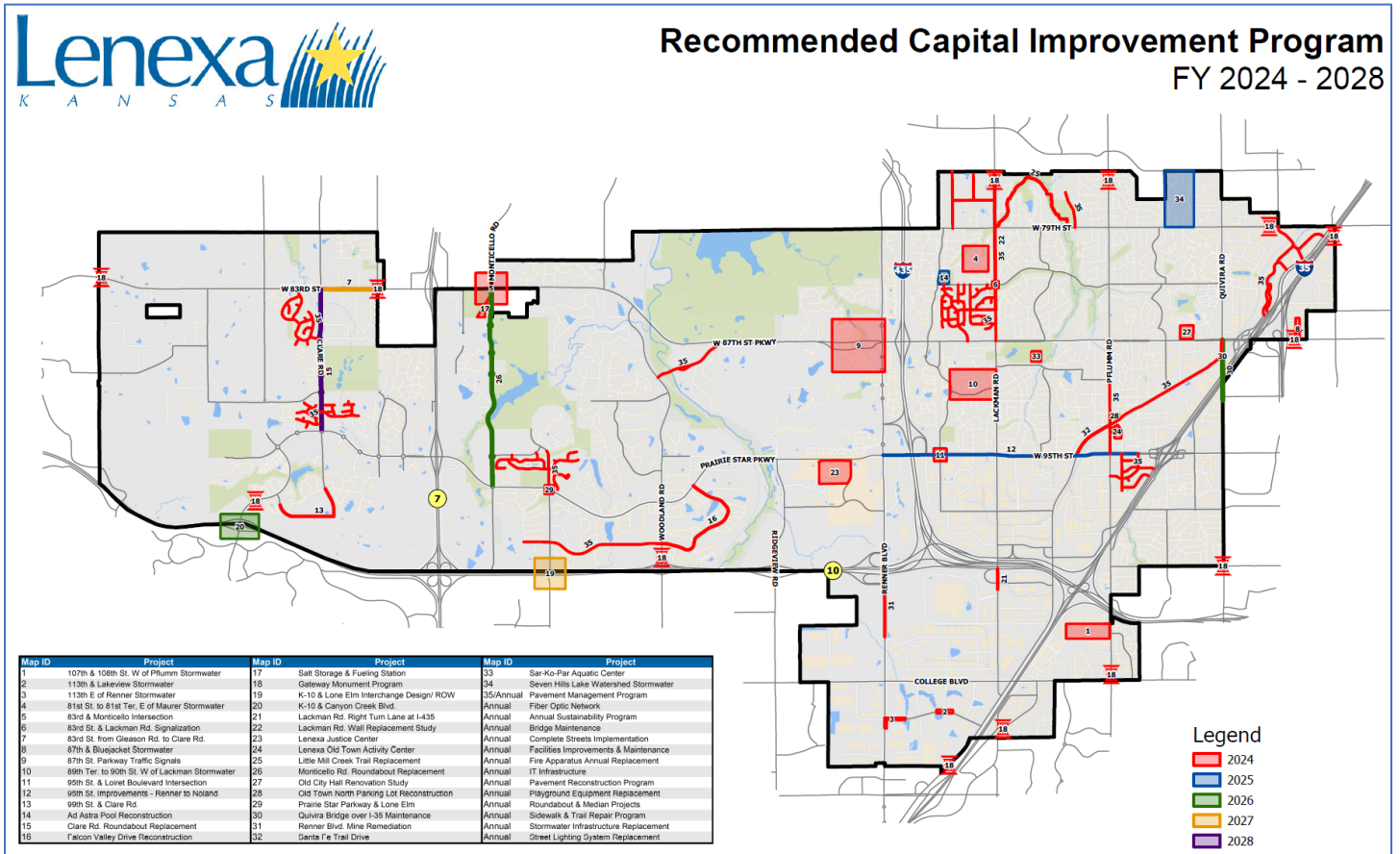
PROJECT NUMBER	PLANNING AND ZONING
DATE	10/29/2023

DESIGNED:	Designer
DRAWN:	Author
REVIEWED:	Checker

SHEET TITLE  
**EXTERIOR BUILDING ELEVATIONS**

SHEET NUMBER  
**B-A3.02**

# CONSIDERATION OF THE RECOMMENDED 2024 – 2028 CAPITAL IMPROVEMENT PROGRAM



## PROJECT SUMMARY

The Capital Improvement Program “CIP” serves as a guide for planning, funding and scheduling capital projects over a five-year period and includes a variety of projects, usually relating to street and bridge construction and maintenance, improvements and maintenance related to parks and recreation activities, and projects related to public safety. The CIP includes non-recurring projects with a cost of at least \$300,000 and a useful life of at least five years.

Kansas State Statute K.S.A. 12-748 sets forth the Planning Commission’s responsible for reviewing the City’s Capital Improvement Program and determining if it is consistent with the City’s Comprehensive Plan. The statute emphasizes the link between public improvements and adopted land use plans.

On an annual basis, in conjunction with the Governing Body adopting the annual budget, the Planning Commission reviews the CIP. An overview of the proposed CIP was provided to the Governing Body at their October 10, 2023, Committee of the Whole meeting.

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## REQUESTED ACTION

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Consideration that the recommended 2024 – 2028 Capital Improvement Program for the City of Lenexa is in conformity with the Comprehensive Plan.

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## PROJECT INFORMATION

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The recommended FY 2024 – 2028 CIP is \$275.2. As in past years, funding for the CIP includes the following sources:

- General Fund Revenues
- General Obligation Bonds
- 3/8-Cent Sales Tax
- External Grants
- County ¼-Cent Sales Tax
- Stormwater Revenues
- Special Highway Fund (Gas Tax)
- Excise Tax
- TIP Impact Fees
- Other (includes CARS and SMAC revenues, Park impact fees and revenues, etc.)

The CIP includes 48 funded projects. Four projects comprise approximately 55% of the recommended CIP:

- Lenexa Justice Center (\$73.0 million)
- Pavement Management Program (\$44.9 million)
- 83<sup>rd</sup> Street Improvements from Gleason Road to Clare Road (13.7 million)
- Lenexa Old Town Activities Center Improvements (\$12.2 million)

Seventeen new projects have been added to the proposed 2024-2028 Capital Improvement Program:

- Street, Bridge and Trail Maintenance Projects:
  - 95<sup>th</sup> Street Improvements – Renner Blvd. to Noland Road.
  - Clare Road Roundabout Replacement.
  - Monticello Road Roundabout Replacement
  - Lackman Road Wall Replacement Study
  - Quivira Bridge over I-35 Maintenance
  - Little Mill Creek Trail Replacement
- Stormwater Infrastructure Replacement:
  - 87<sup>th</sup> Street and Bluejacket
  - 107<sup>th</sup> and 108<sup>th</sup> Streets west of Pflumm
  - 89<sup>th</sup> Terrace to 90<sup>th</sup> Street west of Lackman Road
  - Seven Hill Lake Watershed Phase 1
- 83<sup>rd</sup> Street and Lackman Road Signalization
- 83<sup>rd</sup> Street from Gleason Road to Clare Road
- K-10 and Canyon Creek Blvd. Improvements
- Old Town North Parking Lot Reconstruction
- Roundabout and Median Improvement Project
- IT Infrastructure Replacement Program
- Annual Sustainability Program

---

## **COMPREHENSIVE PLAN**

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The City of Lenexa’s Comprehensive Plan is our official policy guide for future growth and development. Within the context of the comprehensive plan, the issues of sustainability, land use, transportation, watershed management, and natural resources are addressed.

The City’s Vision 2040 framework identifies and the describes the visions for five topic areas:

- Healthy People
- Inviting Places
- Vibrant Neighborhoods
- Integrated Infrastructure and Transportation
- Thriving Economy

CIP projects such as the Pavement Management and Reconstruction Programs, annual bridge maintenance, and annual street light replacement programs are all related to preserving the condition and safety of our roadways and travel ways. The annual stormwater infrastructure replacement project is important to the continued operation of the city’s stormwater system. These projects directly correlate to the Governing Body’s vision relative to Vibrant Neighborhoods, Inviting Places and Integrated Infrastructure and Transportation.

Projects such as improving the community center and senior center, as well as reconstructing Ad Astra pool and improving Indian Trails Aquatics Center contribute to providing recreational opportunities for residents and visitors. These projects correlate to Healthy People and Vibrant Neighborhoods.

One of the goals of the comprehensive plan is to ensure Lenexa continues to be a quality place to live, work, and play. By undertaking the projects included in the CIP, we are promoting a balanced and orderly future development pattern that enhances the built environment for residents, employees, and businesses.

---

## **RECOMMENDATION FROM PROFESSIONAL STAFF**

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Staff recommends that the Planning Commission find the 2024 - 2028 Capital Improvement Program for the City of Lenexa to be in conformity with the Comprehensive Plan.



# FY 2024 – FY 2028 RECOMMENDED CAPITAL IMPROVEMENT PROGRAM (CIP)

## Executive Summary

As required by the City’s Capital Improvement Program (CIP) policy, staff is submitting the recommended fiscal year 2024-2028 CIP for review by the Governing Body. The recommended CIP includes non-recurring projects with a cost of at least \$300,000 and a useful life of at least five years.

The recommended fiscal year 2024-2028 CIP totals \$275.2 million and includes 48 capital projects.

## Sources of Funding

The recommended CIP is financed with a diverse group of funding sources. General obligation bonds used to finance the CIP are repaid through special assessments, property taxes (the debt service mill levy is estimated at approximately 5.033 mills for fiscal year 2024), and stormwater fees (the equivalent dwelling unit, or EDU, fee is \$109 for fiscal year 2024).

The funding sources in the CIP are:

Funding Sources: FY 2024-2028 CIP		
General Fund Revenues	\$88,901,610	32%
General Obligation Bonds (includes SBD Bonds)	\$67,907,168	25%
3/8 - Cent Sales Tax	\$44,694,997	16%
County 1/4 - Cent Sales Tax	\$16,000,000	6%
Stormwater Revenues	\$13,528,479	5%
External Grants	\$12,565,084	5%
Excise Tax	\$9,407,812	3%
Special Highway Fund (Gas Tax)	\$8,771,100	3%
TIP Impact Fees	\$2,981,831	1%
Other (includes CARS and SMAC revenues, Park impact fees and revenues, etc.)	\$10,437,861	4%
<b>Total</b>	<b>\$275,195,942</b>	<b>100%</b>

The general obligation bonds category includes \$44 million for the Lenexa Justice Center, \$21.3 million for street projects, and \$2.6 million for Stormwater projects. In addition, external funding sources finance 12% of the CIP for FY 2024 through FY 2028. Examples of external funding include the Special Highway Fund (gas tax), the County Assistance Road System program (CARS), the Stormwater Management Advisory Council program (SMAC), and federal grants.

## Uses of Funding

The uses of funding by project category are:

Uses of Funding: FY 2024-2028 CIP		
Streets/Bridges/Traffic Projects	\$122,164,368	44%
Facility Projects	\$91,844,966	34%
Parks & Recreation	\$25,978,683	9%
Stormwater Projects	\$21,104,925	8%
Capital Equipment/Other Projects	\$14,103,000	5%
Total	\$275,195,942	100%

Highlighted projects in the recommended CIP include:

- Lenexa Justice Center (\$73.0 million)
- Pavement Management Program (\$44.9 million)
- Pavement Reconstruction Program (\$11.0 million)
- Stormwater Infrastructure Replacement (\$7.5 million)
- 83rd Street Improvements from Gleason Road to Clare Road (\$13.7 million)
- 99th Street and Clare Road construction (\$8.1 million)
- Lenexa Old Town Activity Center Improvements (\$12.2 million)
- Ad Astra Pool Reconstruction (\$8.0 million)

Please see the “Funded Project Expenditures by Year” document for a full list of projects included in the 2024 – 2028 CIP.

## CIP Adoption and Amendments

In developing communities like Lenexa, the CIP represents a “snapshot” in time. As such, it is the best plan that can be formulated today based on the information available. Staff anticipates presenting amendments as conditions change to assure the CIP continues to implement the City’s Comprehensive Plan and achieve the Governing Body’s goals.

# SOURCES OF FUNDING BY YEAR

2024-2028 RECOMMENDED CIP									
SOURCES OF FUNDING:	Prior Years	2023	2024	2025	2026	2027	2028	Totals	% share
General Obligation Bonds	\$6,468,541	\$45,183,261	\$5,240,590	\$-	\$8,431,200	\$-	\$-	\$65,323,592	24%
Stormwater Bonds	-	-	-	2,583,576	-	-	-	2,583,576	1%
Special Benefit District Bonds	-	-	-	-	-	-	-	-	0%
General Fund Revenues	33,704,127	15,041,047	11,835,000	4,896,700	12,102,950	6,939,036	4,382,750	88,901,610	32%
Stormwater Revenues	-	2,543,450	3,979,529	3,030,500	1,325,000	1,325,000	1,325,000	13,528,479	5%
Excise Tax	931,500	1,504,312	272,000	2,900,000	-	300,000	3,500,000	9,407,812	3%
TIP Fees	1,509,250	1,393,298	79,283	-	-	-	-	2,981,831	1%
PRIF Fees	-	-	-	-	-	-	-	-	0%
3/8 cent sales tax	883,635	11,768,400	6,843,768	9,900,804	4,998,820	5,098,797	5,200,773	44,694,997	16%
County 1/4 cent sales tax	9,618,780	6,381,220	-	-	-	-	-	16,000,000	6%
Parks Revenues	150,000	89,642	60,000	260,349	25,000	25,000	25,000	634,991	0%
Other funding	-	450,000	300,000	-	-	-	-	750,000	0%
<b>SUBTOTAL:</b>	<b>\$53,265,833</b>	<b>\$84,354,630</b>	<b>\$28,610,170</b>	<b>\$23,571,929</b>	<b>\$26,882,970</b>	<b>\$13,687,833</b>	<b>\$14,433,523</b>	<b>\$244,806,888</b>	<b>88%</b>

EXTERNAL SOURCES OF FUNDING:									
SOURCES OF FUNDING:	Prior Years	2023	2024	2025	2026	2027	2028	Totals	% share
Federal & State Grants	\$2,807,680	\$6,044,584	\$596,500	\$160,000	\$160,000	\$2,636,320	\$160,000	\$12,565,084	5%
CARS Program	-	575,000	-	1,000,000	-	-	-	1,575,000	1%
SMAC Program	-	147,550	2,188,896	2,506,424	-	-	-	4,842,870	2%
Special Highway fund (gas tax)	-	1,546,000	1,505,100	1,430,000	1,430,000	1,430,000	1,430,000	8,771,100	3%
Other local funding	-	2,185,000	-	-	291,870	158,130	-	2,635,000	1%
<b>TOTAL EXTERNAL SOURCES OF FUNDING:</b>	<b>\$2,807,680</b>	<b>\$10,498,134</b>	<b>\$4,290,496</b>	<b>\$5,096,424</b>	<b>\$1,881,870</b>	<b>\$4,224,450</b>	<b>\$1,590,000</b>	<b>\$30,389,054</b>	<b>12%</b>
<b>GRAND TOTAL SOURCES OF FUNDING:</b>	<b>\$56,073,513</b>	<b>\$94,852,764</b>	<b>\$32,900,666</b>	<b>\$28,668,353</b>	<b>\$28,764,840</b>	<b>\$17,912,283</b>	<b>\$16,023,523</b>	<b>\$275,195,942</b>	<b>100%</b>

# USES OF FUNDING BY YEAR

2024-2028 RECOMMENDED CIP									
USES OF FUNDING:	Prior Years	2023	2024	2025	2026	2027	2028	Totals	% share
Streets & Bridges	\$4,560,238	\$17,408,721	\$21,730,178	\$14,951,784	\$18,640,640	\$22,175,483	\$13,113,524	\$112,580,568	41%
Traffic	1,453,464	4,150,524	1,539,812	610,000	610,000	610,000	610,000	9,583,800	3%
Stormwater	-	1,991,500	7,117,925	8,095,500	1,300,000	1,300,000	1,300,000	21,104,925	8%
Facilities	7,467,831	64,651,031	17,726,104	500,000	500,000	500,000	500,000	91,844,966	34%
Parks	2,283,128	14,280,752	1,929,803	7,485,000	-	-	-	25,978,683	9%
Capital Equipment & Miscellaneous	7,382	3,402,618	4,292,000	2,514,000	1,629,000	1,758,000	500,000	14,103,000	5%
<b>TOTAL USES OF FUNDING:</b>	<b>\$15,772,043</b>	<b>\$105,885,146</b>	<b>\$54,335,822</b>	<b>\$34,156,284</b>	<b>\$22,679,640</b>	<b>\$26,343,483</b>	<b>\$16,023,524</b>	<b>\$275,195,942</b>	<b>100%</b>

# YEAR CAPITAL EXPENDITURES ARE INCURRED

Project Title	Prior Years	Budget 2023	2024	2025	2026	2027	2028	Total
<b>STREETS/ BRIDGES</b>								
Pavement Management Program	\$-	\$6,563,400	\$6,651,868	\$7,687,804	\$7,837,820	\$7,991,797	\$8,149,773	\$44,882,462
Pavement Reconstruction Program	39,041	2,470,959	5,000,000	-	-	-	-	7,510,000
Bridge Maintenance	-	460,000	920,000	270,000	270,000	270,000	270,000	2,460,000
K-10 & Lone Elm Interchange Design and ROW Acquisition	3,434,274	-	-	-	-	2,849,726	-	6,284,000
99th Street & Clare Road	708,733	3,057,079	4,243,000	-	-	-	-	8,008,812
95th Street & Loiret Boulevard Intersection Improvements	378,190	1,765,283	35,000	-	-	-	-	2,178,473
Sidewalk & Trail Repair Program	-	300,000	350,000	350,000	350,000	350,000	350,000	2,050,000
Lackman Road Right Turn Lane at I-435	-	160,000	515,000	-	-	-	-	675,000
Santa Fe Trail Drive - 95th Street to Park Street	-	132,000	1,575,000	-	-	-	-	1,707,000
95th Street Improvements - Renner to Noland (NEW)	-	-	200,000	3,900,000	-	-	-	4,100,000
Clare Road Roundabout Replacement (NEW)	-	-	-	-	-	300,000	3,500,000	3,800,000
Falcon Valley Drive Reconstruction (NEW)	-	2,500,000	1,500,000	-	-	-	-	4,000,000
Lackman Road Wall Replacement Study (NEW)	-	-	150,000	-	-	-	-	150,000
Monticello Road Roundabout Replacement (NEW)	-	-	-	300,000	5,460,000	-	-	5,760,000
83rd Street from Gleason Road to Clare Road (NEW)	-	-	590,310	2,304,280	2,346,000	8,431,200	-	13,671,790
K-10 and Canyon Creek Blvd Improvements (NEW)	-	-	-	57,200	532,450	-	-	589,650
Old Town North Parking Lot Reconstruction (NEW)	-	-	-	-	291,870	1,913,160	-	2,205,030
Quivira Bridge over I-35 Maintenance (NEW)	-	-	-	82,500	1,552,500	-	-	1,635,000
Roundabout and Median Improvement Projects (NEW)	-	-	-	-	-	69,600	843,750	913,350
<b>SUBTOTAL</b>	<b>\$4,560,238</b>	<b>\$17,408,721</b>	<b>\$21,730,178</b>	<b>\$14,951,784</b>	<b>\$18,640,640</b>	<b>\$22,175,483</b>	<b>\$13,113,523</b>	<b>\$112,580,567</b>

Project Title	Prior Years	Budget 2023	2024	2025	2026	2027	2028	Total
<b>TRAFFIC</b>								
Street Lighting System Replacement	\$-	\$500,000	\$500,000	\$500,000	\$500,000	\$500,000	\$500,000	\$3,000,000
87th Street Parkway Traffic Signals	610,688	260,000	315,312	-	-	-	-	1,186,000
Complete Streets Implementation	580,000	110,000	110,000	110,000	110,000	110,000	110,000	1,240,000
83rd & Monticello Intersection Improvements	139,619	2,480,381	50,000	-	-	-	-	2,670,000
Prairie Star Parkway & Lone Elm Intersection Improvements	123,157	800,143	50,000	-	-	-	-	973,300
83rd & Lackman Road Traffic Signals (NEW)	-	-	514,500	-	-	-	-	514,500
<b>SUBTOTAL</b>	<b>\$1,453,464</b>	<b>\$4,150,524</b>	<b>\$1,539,812</b>	<b>\$610,000</b>	<b>\$610,000</b>	<b>\$610,000</b>	<b>\$610,000</b>	<b>\$9,583,800</b>
<b>PUBLIC BUILDINGS</b>								
Facilities Improvements & Maintenance	\$-	\$3,982,966	\$500,000	\$500,000	\$500,000	\$500,000	\$500,000	\$6,482,966
Old City Hall/Fire Station 6 Phase 1 Design	-	-	210,000	-	-	-	-	210,000
Lenexa Old Town Activities Center	78,620	1,223,000	10,850,380	-	-	-	-	12,152,000
Lenexa Justice Center	7,389,211	59,445,065	6,165,724	-	-	-	-	73,000,000
<b>SUBTOTAL</b>	<b>\$7,467,831</b>	<b>\$64,651,031</b>	<b>\$17,726,104</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$91,844,966</b>
<b>PARKS</b>								
Playground Equipment Replacement	1,493,684	200,000	-	285,000	-	-	-	1,978,684
Sar-Ko-Par Aquatic Center Improvements	789,444	14,080,752	129,804	-	-	-	-	15,000,000
Ad Astra Pool Reconstruction	-	-	800,000	7,200,000	-	-	-	8,000,000
Little Mill Creek Trail Replacement (NEW)	-	-	1,000,000	-	-	-	-	1,000,000
<b>SUBTOTAL</b>	<b>\$2,283,128</b>	<b>\$14,280,752</b>	<b>\$1,929,804</b>	<b>\$7,485,000</b>	<b>\$-</b>	<b>\$-</b>	<b>\$-</b>	<b>\$25,978,684</b>
<b>STORMWATER</b>								
Stormwater Infrastructure Replacement	\$-	\$1,300,000	\$1,034,000	\$1,300,000	\$1,300,000	\$1,300,000	\$1,300,000	\$7,534,000
113th Street East of Renner Boulevard	-	30,000	1,540,000	-	-	-	-	1,570,000
81st Street to 81st Terrace (East of Maurer)	-	661,500	323,925	-	-	-	-	985,425
113th Street and Lakeview Road	-	-	2,950,000	-	-	-	-	2,950,000
87th and Bluejacket (NEW)	-	-	1,270,000	-	-	-	-	1,270,000
107th and 108th Streets West of Pflumm (NEW)	-	-	-	1,870,000	-	-	-	1,870,000
89th Terrace to 90th Street West of Lackman (NEW)	-	-	-	1,970,000	-	-	-	1,970,000
Seven Hills Lake Watershed Phase 1 (NEW)	-	-	-	2,955,500	-	-	-	2,955,500
<b>SUBTOTAL</b>	<b>\$-</b>	<b>\$1,991,500</b>	<b>\$7,117,925</b>	<b>\$8,095,500</b>	<b>\$1,300,000</b>	<b>\$1,300,000</b>	<b>\$1,300,000</b>	<b>\$21,104,925</b>

Project Title	Prior Years	Budget 2023	2024	2025	2026	2027	2028	Total
<b>MAJOR CAPITAL EQUIPMENT &amp; MISCELLANEOUS</b>								
Gateway Monument Program	7,382	610,618	232,000	-	-	-	-	850,000
Fire Apparatus Annual Replacement	-	2,492,000	-	1,934,000	1,034,000	1,148,000	-	6,608,000
Renner Boulevard Mine Remediation	-	-	1,500,000	-	-	-	-	1,500,000
Freedom Fields Salt Storage and Fueling Station	-	-	1,653,750	-	-	-	-	1,653,750
Fiber Optic Network Expansion, Upgrade & Repair	-	300,000	200,000	200,000	200,000	200,000	200,000	1,300,000
IT Infrastructure Replacement Program (NEW)	-	-	656,250	330,000	345,000	360,000	250,000	1,941,250
Annual Sustainability Program (NEW)	-	-	50,000	50,000	50,000	50,000	50,000	250,000
<b>SUBTOTAL</b>	<b>\$7,382</b>	<b>\$3,402,618</b>	<b>\$4,292,000</b>	<b>\$2,514,000</b>	<b>\$1,629,000</b>	<b>\$1,758,000</b>	<b>\$500,000</b>	<b>\$14,103,000</b>
<b>Total Cost Funded Projects</b>	<b>\$15,772,043</b>	<b>\$105,885,146</b>	<b>\$54,335,823</b>	<b>\$34,156,284</b>	<b>\$22,679,640</b>	<b>\$26,343,483</b>	<b>\$16,023,523</b>	<b>\$275,195,942</b>

# PAVEMENT MANAGEMENT PROGRAM

## Project Purpose

The Annual Pavement Management Program aims to prolong the lifespan and quality of the City’s road infrastructure. The City takes a proactive, data-driven approach and utilizes various maintenance strategies to ensure a smoother and safer transportation network for the City.

## Project Description

The City evaluates the pavement network and utilizes alternative maintenance techniques, such as crack sealing, ultra-thin bonded asphalt surface (UBAS) and mill & overlay to create a cost effective program. Curb and gutter and sidewalk in each project area is assessed and replaced as needed. A portion of funds in the pavement management program will be dedicated toward the maintenance of city owned parking lots. Funding sources in prior years included Stormwater and Parks funds which are moving to the sidewalk and trails program in future years.



## Cost

\$44,882,462

## Construction Timeline

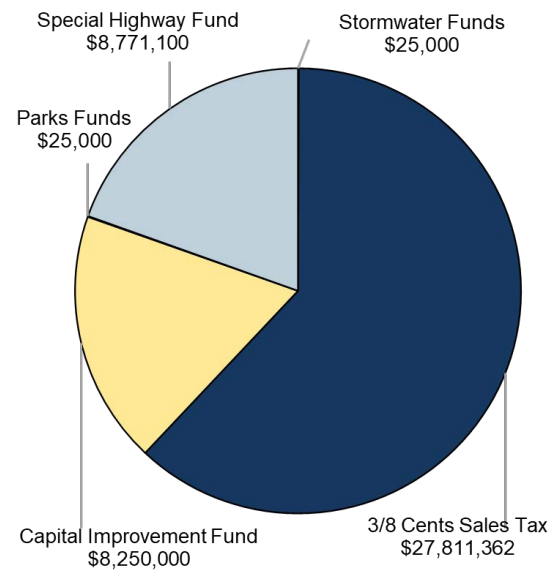
Ongoing

## Vision 2040 Themes

- Vibrant Neighborhoods
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Strategic Community Investment



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$1,199,000	\$1,303,000	\$1,357,000	\$1,409,000	\$1,463,000	\$1,519,000	\$8,250,000
Stormwater Funds	25,000	0	0	0	0	0	25,000
3/8 Cent Sales Tax	3,768,400	3,843,768	4,900,804	4,998,820	5,098,797	5,200,773	27,811,362
Parks Funds	25,000	0	0	0	0	0	25,000
Special Hwy Fund	1,546,000	1,505,100	1,430,000	1,430,000	1,430,000	1,430,000	8,771,100
<b>Total</b>	<b>\$6,563,400</b>	<b>\$6,651,868</b>	<b>\$7,687,804</b>	<b>\$7,837,820</b>	<b>\$7,991,797</b>	<b>\$8,149,773</b>	<b>\$44,882,462</b>



# PAVEMENT RECONSTRUCTION PROGRAM

## Project Purpose

The Pavement Reconstruction Program includes full or partial reconstruction of roadway infrastructure that has failed and is past their useful life. Typical pavement maintenance strategies are no longer viable for these locations as the roadway quickly fails after the maintenance strategy is completed, so major reconstruction is needed.

## Project Description

The Pavement Reconstruction Program will fully or partially reconstruct failed roadway infrastructure throughout the City. Locations will be identified as part of the City’s pavement evaluation process. The 2023 Project was Falcon Valley Drive from Woodland Road to Prairie Star Parkway and 2024 Projects are scheduled to be the Penny Green subdivision and Deer Run street. Construction for Pavement Reconstruction Program Projects may also include replacement of sidewalk and curb and gutter, new sidewalks, replacement of street lights and stormwater improvements.

## Cost

\$7,510,000

## Construction Timeline

Ongoing

## Vision 2040 Themes

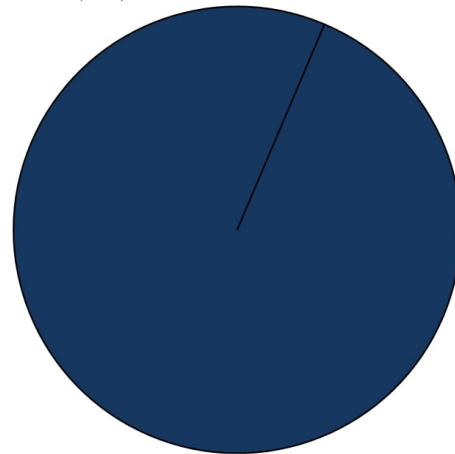
- Vibrant Neighborhoods
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Strategic Community Investment



Capital Improvement Fund  
\$7,510,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$7,510,000	\$-	\$0	\$0	\$0	\$0	\$7,510,000
<b>Total</b>	<b>\$7,510,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$7,510,000</b>

# BRIDGE MAINTENANCE PROGRAM

## Project Purpose

Without routine maintenance, the useful life of Lenexa’s bridges will be significantly shortened.

## Project Description

The City of Lenexa is required to have all 29 bridges inspected biennially and report to the Kansas Department of Transportation. Our consultant has identified several maintenance needs to keep our bridges safe and to extend their useful life. Specific improvements include expansion joint repairs, crack sealing, guardrail repair and scour protection.

## Cost

\$2,460,000

## Construction Timeline

Ongoing

## Vision 2040 Themes

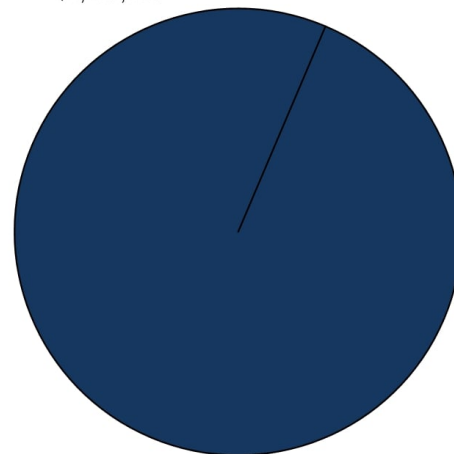
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment
- Sustainable Policies and Practices



Capital Improvement Fund  
\$2,460,000

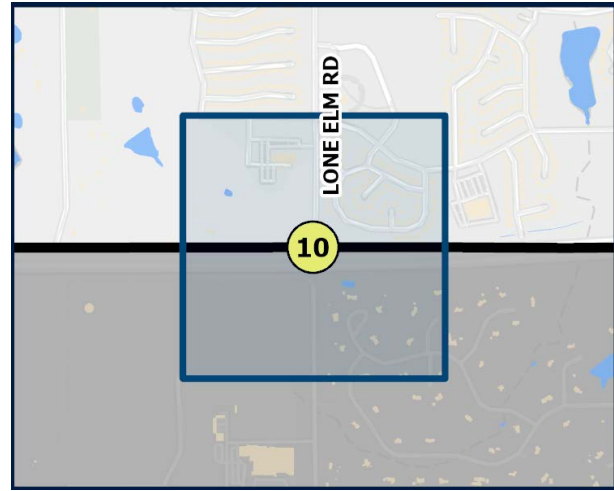


Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$460,000	\$920,000	\$270,000	\$270,000	\$270,000	\$270,000	\$2,460,000
<b>Total</b>	<b>\$460,000</b>	<b>\$920,000</b>	<b>\$270,000</b>	<b>\$270,000</b>	<b>\$270,000</b>	<b>\$270,000</b>	<b>\$2,460,000</b>

# K-10 & LONE ELM INTERCHANGE

## Project Purpose

The construction of an interchange with K10 Highway at Lone Elm is a critical element in supporting economic development in this area of the community. It will improve access to the anticipated commercial and residential growth in the vicinity, relieve traffic congestion at adjacent interchanges on K10 and K7 and provide improved access to the Olathe high school and activity centers in the northeast corner of 11th Street and Lone Elm Road, where a significant number of Lenexa students attend. This project designed the interchange and purchased the majority of Lenexa right-of-way necessary for construction. Future phases of the project will complete design and construction as either a standalone project or in conjunction with KDOT improvements to K10 Highway.



## Project Description

Design of interchange at K10 Highway and Lone Elm, including auxiliary lanes on K10; widening of Lone Elm Road to four-lane arterial street section from 101st Street to Prairie Star Parkway; and realignment of frontage road.

## Cost

\$6,284,000

## Construction Timeline

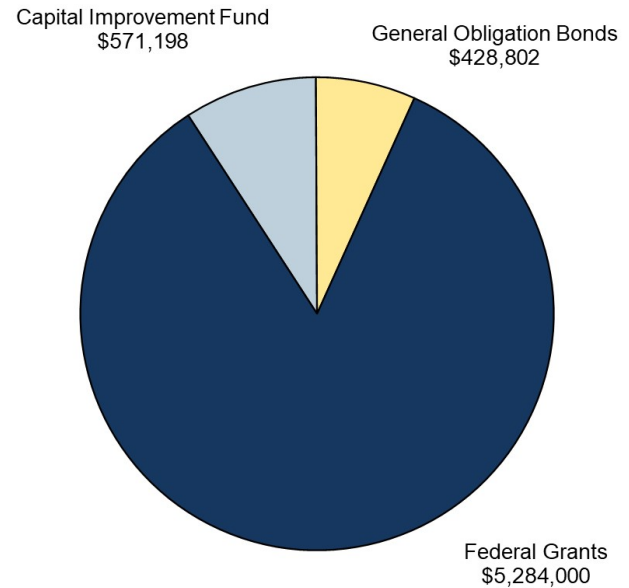
TBD

## Vision 2040 Themes

- Integrated Infrastructure & Transportation
- Thriving Economy

## Guiding Principles Supported

- Strategic Community Investment
- Responsible Economic Development



<b>Funding Sources</b>							
	<b>Prior Years</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>2027</b>	<b>2028</b>	<b>Total</b>
General Obligation Bonds	\$428,802	\$0	\$0	\$0	\$0	\$0	\$428,802
Federal Grants	2,807,680	0	0	0	2,476,320	0	5,284,000
Capital Improvement Fund	197,792	0	0	0	373,406	0	571,198
<b>Total</b>	<b>\$3,434,274</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,849,726</b>	<b>\$0</b>	<b>\$6,284,000</b>

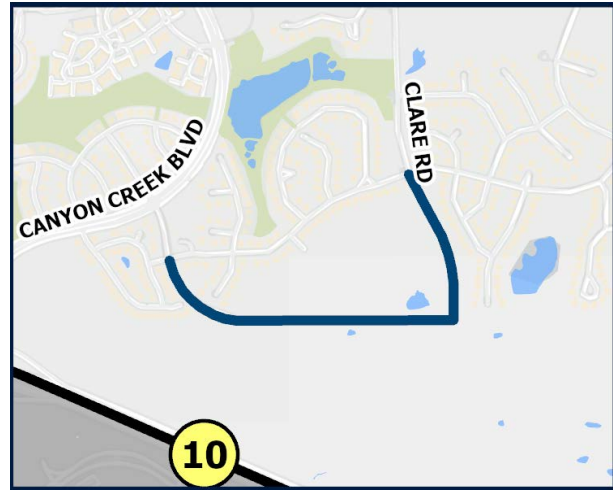
# 99TH ST. & CLARE RD.

## Project Purpose

To facilitate development of new residential subdivisions.

## Project Description

Construct 99th Street as a four-lane, divided roadway from the current end of pavement (EOP) at 98th Street easterly 2,550' to future Clare Road. In addition, the project would construct Clare Road from the current EOP at 97th Terrace southerly 1,350' to the proposed 99th Street extension. Clare Road would require grading for the ultimate four-lane section, but only constructing the easternmost two-lanes of asphalt at this time.



## Cost

\$8,008,812

## Construction Timeline

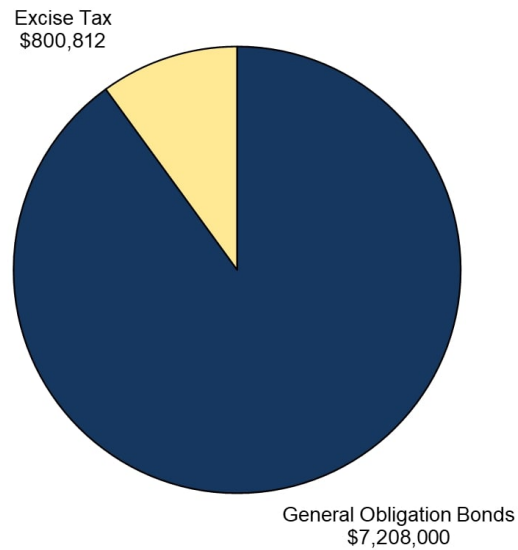
2022 - 2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Prudent Financial Management



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
General Obligation Bonds	\$7,208,000	\$0	\$0	\$0	\$0	\$0	\$7,208,000
Excise Tax	800,812	0	0	0	0	0	800,812
<b>Total</b>	<b>\$8,008,812</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$8,008,812</b>

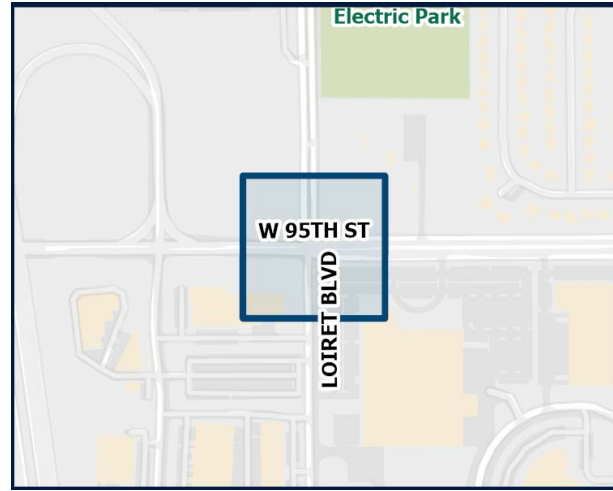
# 95TH ST. & LOIRET BLVD. INTERSECTION

## Project Purpose

Due to the adjacent development of Grundfos Headquarters, Skyline, Collins Aerospace, along with the extension of Maurer Road and associated development, this intersection now warrants a traffic signal. To provide more efficient operation of the intersection, additional turn lanes are necessary.

## Project Description

Install new traffic signal with ADA ramps, pedestrian signals and ADA push buttons on all approaches. Other work includes construction of a westbound right turn lane on 95th Street; a southbound right turn lane on Loiret Blvd; and an extension of the existing northbound right turn lane on Loiret Blvd.



## Cost

\$2,178,473

## Construction Timeline

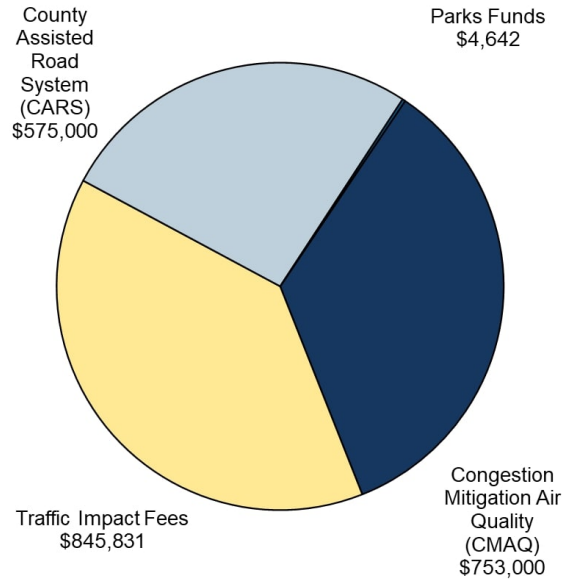
2023-2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Prudent Financial Management



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
TIP	\$845,831	\$0	\$0	\$0	\$0	\$0	\$845,831
Parks Funds	4,642	0	0	0	0	0	4,642
Federal Grants	753,000	0	0	0	0	0	753,000
CARS	575,000	0	0	0	0	0	575,000
<b>Total</b>	<b>\$2,178,473</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,178,473</b>

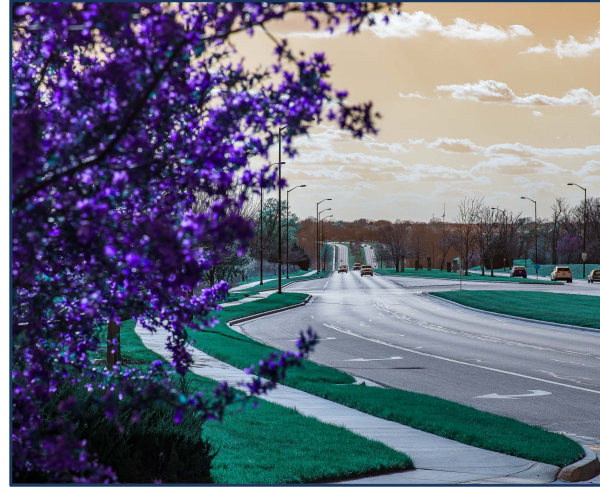
# SIDEWALK & TRAIL REPAIR PROGRAM

## Project Purpose

The Sidewalk & Trail Repair Program enhances pedestrian safety and accessibility by rehabilitating and maintaining sidewalks and addressing sidewalk network connectivity throughout the City.

## Project Description

The Sidewalk & Trail Repair Program repairs existing deteriorated sidewalks and connects missing sidewalk gaps throughout the City. Through targeted repairs and expansion, the program will create a more cohesive and walkable environment for Lenexa. Funding sources include Parks and Stormwater funds. Specific projects are identified annually.



## Cost

\$2,050,000

## Construction Timeline

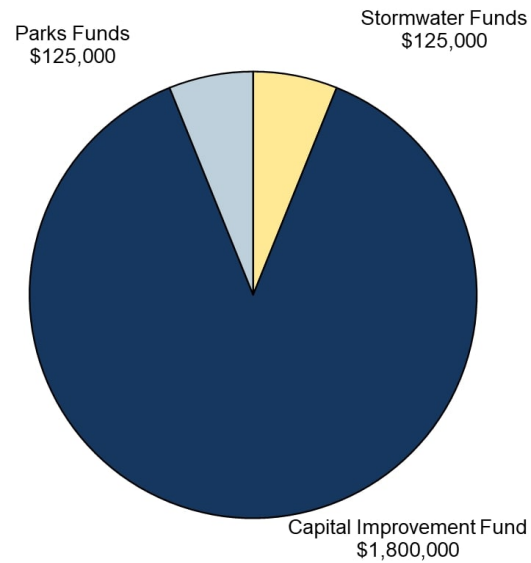
Ongoing

## Vision 2040 Themes

- Healthy People
- Inviting Places
- Vibrant Neighborhoods
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$300,000	\$300,000	\$300,000	\$300,000	\$300,000	\$300,000	\$1,800,000
Stormwater Funds	0	25,000	25,000	25,000	25,000	25,000	125,000
Parks Funds	0	25,000	25,000	25,000	25,000	25,000	125,000
<b>Total</b>	<b>\$300,000</b>	<b>\$350,000</b>	<b>\$350,000</b>	<b>\$350,000</b>	<b>\$350,000</b>	<b>\$350,000</b>	<b>\$2,050,000</b>

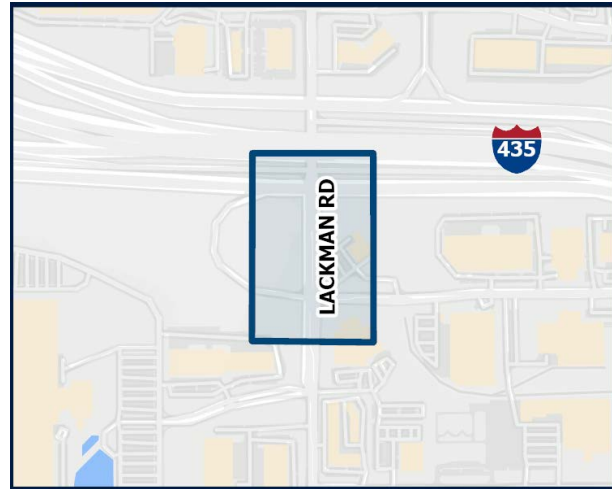
# LACKMAN ROAD RIGHT TURN LANE AT I-435

## Project Purpose

Northbound Lackman Road traffic currently backs up over 700' to 105th Street during peak hours. The right-turn lane would add additional capacity and reduce delays at the intersection. In addition, the intersection has seen numerous crashes, because of this, the project has been selected for KDOT Safety Funds.

## Project Description

Construct a northbound right-turn lane at the I-435 entrance ramp. The project would construct a turn lane approximately 24' wide to accommodate the future Phase II Gateway Improvements.



## Cost

\$675,000

## Construction Timeline

2023 - 2024

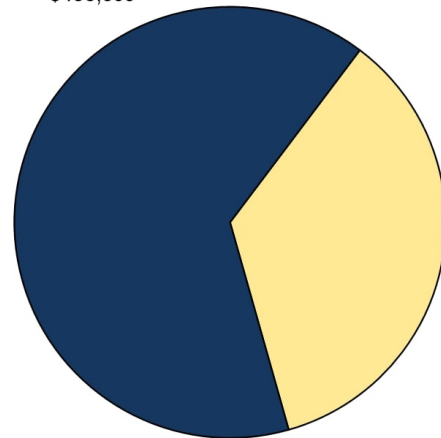
## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment

Surface Transportation Program  
\$436,500



Capital Improvement Fund  
\$238,500

## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$238,500	\$0	\$0	\$0	\$0	\$0	\$238,500
Federal Grants	0	436,500	0	0	0	0	436,500
<b>Total</b>	<b>\$238,500</b>	<b>\$436,500</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$675,000</b>



# SANTA FE TRAIL DR. STREET & TRAIL

## Project Purpose

The proposed street improvements are needed to increase safety and bring the section of road up to City standards.

## Project Description

The project will add new curb, gutter, street lights, storm sewer and trail. Additionally, the project will make upgrades to the traffic signal at 95th Street and Pflumm Road.

## Cost

\$1,707,000

## Construction Timeline

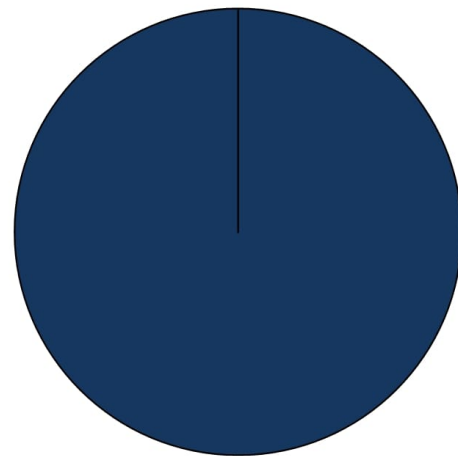
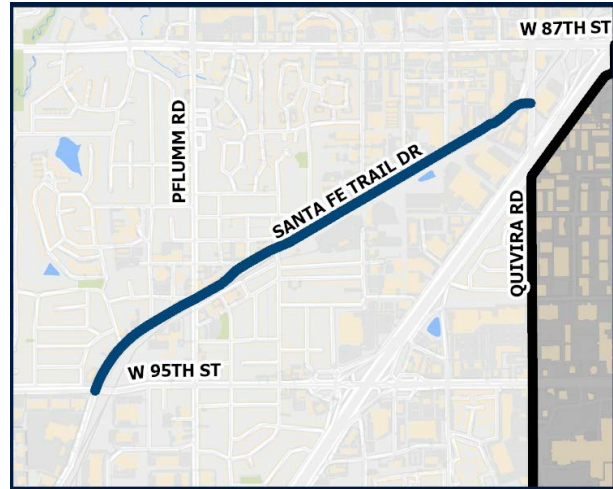
2023 - 2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation
- Inviting Places

## Guiding Principles Supported

- Strategic Community Investment
- Sustainable Policies & Practices



Excise Tax  
\$1,707,000

Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Excise Tax	\$1,635,000	\$72,000	\$0	\$0	\$0	\$0	\$1,707,000
<b>Total</b>	<b>\$1,635,000</b>	<b>\$72,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,707,000</b>

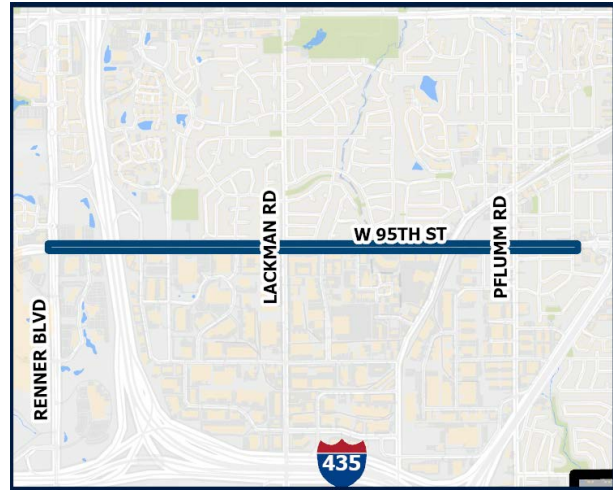
# 95TH ST. - RENNER BLVD. TO NOLAND RD.

## Project Purpose

The 95th Street Project will complete heavy preventative maintenance on a 2.25 mile roadway from Renner Boulevard to Noland Road. Portions of the pavement, storm sewer, and a section of street lights throughout this section of roadway are in need of repair and/or replacement. This project will complete the preventative maintenance activities before heavy deterioration occurs.

## Project Description

The 95th Street Project will consist of storm sewer pipe replacement and pipe lining at several locations, a small section of street light replacement, island beautification from Loiret Boulevard and Lackman Road, spot replacement of deteriorated sidewalk, trail, and curb and gutter, an asphalt mill and overlay, and pavement markings. This project will utilize 2025 CARS funding.



## Cost

\$4,100,000

## Construction Timeline

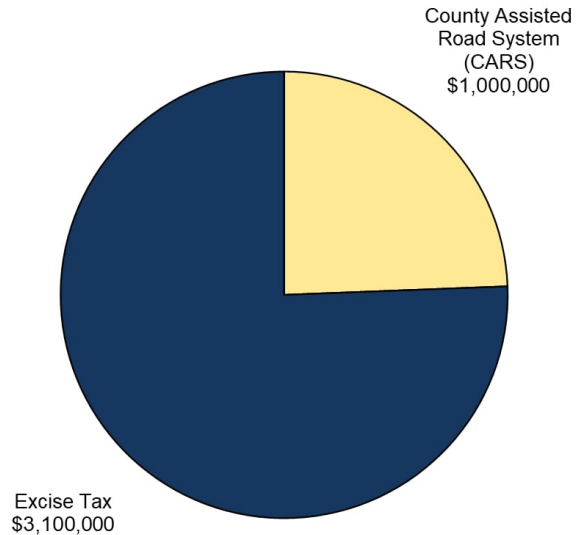
2024 - 2025

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment
- Superior Quality Services
- Prudent Financial Management
- Sustainable Policies & Practices



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Excise Tax	\$0	\$200,000	\$2,900,000	\$0	\$0	\$0	\$3,100,000
CARS	0	0	1,000,000	0	0	0	1,000,000
<b>Total</b>	<b>\$0</b>	<b>\$200,000</b>	<b>\$3,900,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$4,100,000</b>

# CLARE RD. ROUNDABOUTS

## Project Purpose

The asphalt roundabouts along Clare Road have shown failures related to pushing, shoving, and rutting due to the heavy turn movements and stop and start traffic. Typical mill and overlay maintenance strategies will need to be completed approximately every 5 years to ensure the asphalt base does not start deteriorating. Concrete roundabouts are not susceptible to these failures and maintenance would need to occur every 15 to 20 years.

## Project Description

The Clare Road Roundabout Replacement Project will reconstruct the pavement portion of the four roundabouts on Clare Road between 83rd Street and Prairie Star Parkway. This project will remove the asphalt pavement, prepare the subgrade, and place back 8" of concrete pavement at the roundabout and on each approach. Deteriorating curb and gutter, sidewalk, and brick pavers would be removed and replaced as needed.

## Cost

\$3,800,000

## Construction Timeline

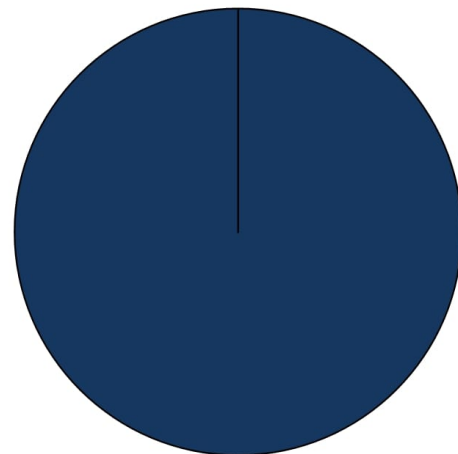
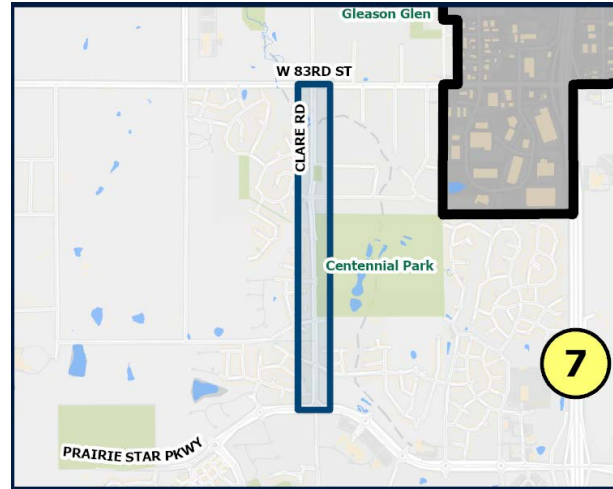
2027 - 2028

## Vision 2040 Themes

- Vibrant Neighborhoods
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Strategic Community Investment
- Sustainable Policies & Practices



Excise Tax  
\$3,800,000

Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Excise Tax	\$0	\$0	\$0	\$0	\$300,000	\$3,500,000	\$3,800,000
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$300,000</b>	<b>\$3,500,000</b>	<b>\$3,800,000</b>

# FALCON VALLEY DR. RECONSTRUCTION

## Project Purpose

Falcon Valley Drive from Woodland Road to Prairie Star Parkway was the first of the Pavement Reconstruction Program Projects. This 3.1 lane miles of roadway was past its useful life, and typical pavement management strategies were not holding up as they should. Due to this, the section of roadway was scheduled for complete removal and replacement.

## Project Description

The Falcon Valley Drive Reconstruction Project consisted of the complete removal and replacement of the pavement section from Woodland Road to Prairie Star Parkway. The project also included spot curb and gutter and sidewalk replacement, the addition of a new sidewalk along the entire length of the roadway, upgraded street lights, pavement markings, and sodding.

## Cost

\$4,000,000

## Construction Timeline

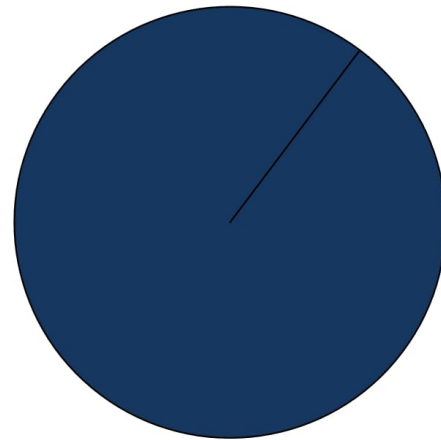
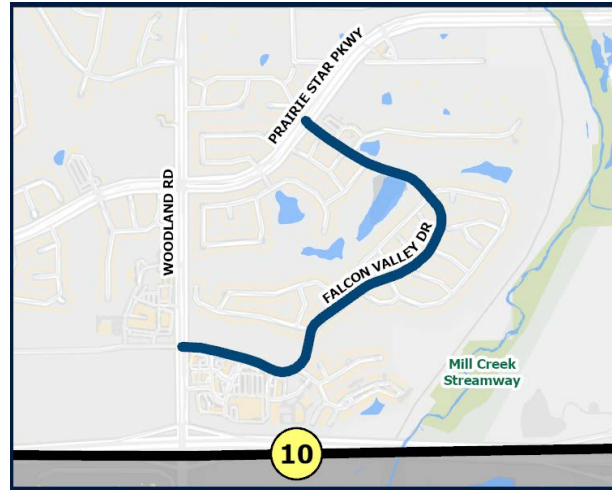
2023-2024

## Vision 2040 Themes

- Vibrant Neighborhoods
- Integrated Infrastructure & Transportation
- Inviting Places

## Guiding Principles Supported

- Strategic Community Investment
- Superior Quality Services
- Prudent Financial Management
- Sustainable Policies & Practices



Capital Improvement Fund  
\$4,000,000

Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$2,500,000	\$1,500,000	\$0	\$0	\$0	\$0	\$4,000,000
<b>Total</b>	<b>\$2,500,000</b>	<b>\$1,500,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$4,000,000</b>

# LACKMAN RD. WALL REPLACEMENT STUDY

## Project Purpose

The Lackman Road Wall Replacement Study will review and assess the useful life of 9 Integral Sidewalk and Retaining Walls (ISRWs) along Lackman Road from 84th Terrace to the North City Limits. The existing ISRWs are nearly 50 years old and would be assessed to determine if maintenance improvements could be completed or if removal and replacement are warranted. Depending on the results of the Study, initial design efforts would be completed to provide a cost estimate and determine potential utility and right-of-way issues.

## Project Description

The nine ISRWs along Lackman Road are showing signs of distress and failure. Failure to any of these walls would result in major and lengthy closures along Lackmand Road, as the planning, design, right-of-way and utility work would all be significant hurdles to overcome before major construction could begin. The walls do appear to have useful life in them but planning for their replacement is justified.

## Cost

\$150,000

## Construction Timeline

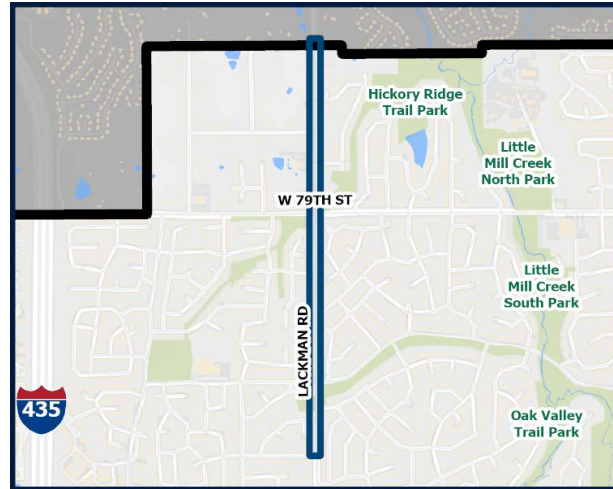
TBD

## Vision 2040 Themes

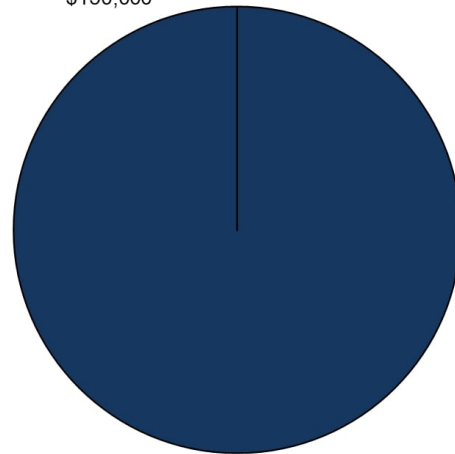
- Integrated Infrastructure & Transportation
- Vibrant Neighborhoods
- Inviting Places

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Strategic Community Investment
- Sustainable Policies & Practices



Capital Improvement Fund  
\$150,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$150,000	\$0	\$0	\$0	\$0	\$150,000
<b>Total</b>	<b>\$0</b>	<b>\$150,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$150,000</b>

# MONTICELLO RD. ROUNDABOUTS

## Project Purpose

The asphalt roundabouts along Monticello Road have shown failures related to pushing, shoving, and rutting due to the heavy turn movements and stop and start traffic. These roundabouts will need a large project with extensive asphalt base repairs and a mill and overlay, then after that, mill and overlay maintenance strategies will need to be completed approximately every 5 years to ensure the repaired asphalt base does not start deteriorating. Concrete roundabouts are not susceptible to these failures and maintenance would need to occur every 15 to 20 years.

## Project Description

The Monticello Road Roundabout Replacement Project will reconstruct the pavement portion of the four roundabouts on Monticello Road between 83rd Street and Prairie Star Parkway. This project will remove the asphalt pavement, prepare the subgrade, and place back 8" of concrete pavement at the roundabout and on each approach. Deteriorating curb and gutter, sidewalk, and brick pavers would be removed and replaced as needed.

## Cost

\$5,760,000

## Construction Timeline

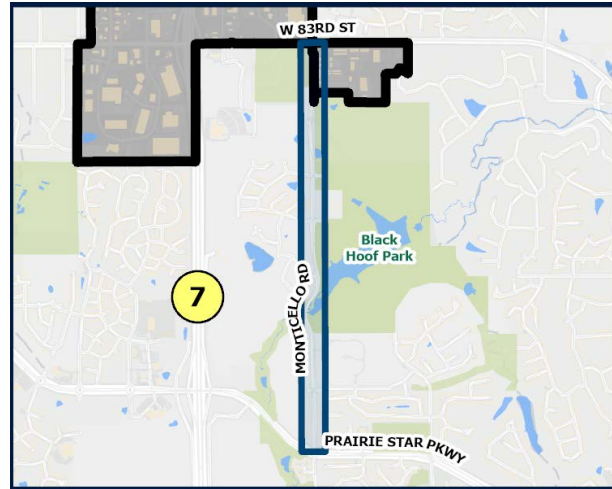
2025 - 2026

## Vision 2040 Themes

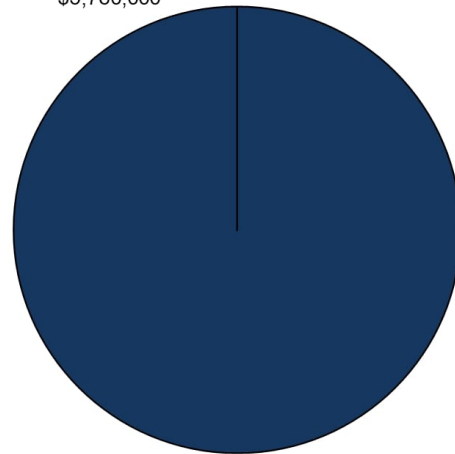
- Vibrant Neighborhoods
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Strategic Community Investment
- Sustainable Policies & Practices



Capital Improvement Fund  
\$5,760,000



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$0	\$300,000	\$5,460,000	\$0	\$0	\$5,760,000
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$300,000</b>	<b>\$5,460,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,760,000</b>

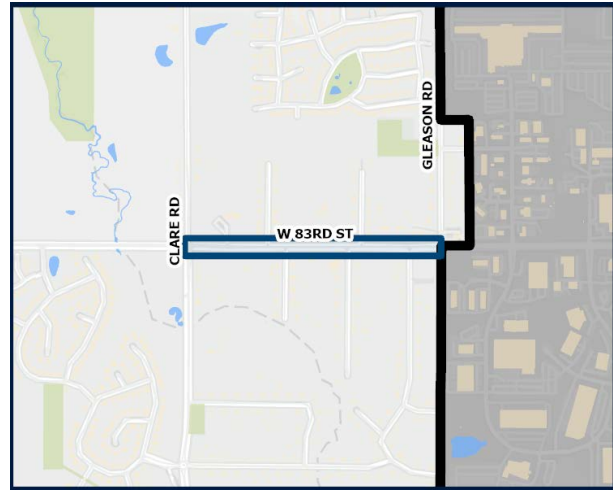
# 83RD ST. - GLEASON RD. TO CLARE RD.

## Project Purpose

Continued development in western Lenexa, along with potential traffic increases from the Panasonic Plant, show that 83rd Street will need capacity and safety improvements in the next five years. The current street is a two-lane road without adequate shoulders, stormwater system and street lighting.

## Project Description

The project will reconstruct the existing two-lane 83rd Street into a four-lane, divided street with new concrete curb & gutter, asphalt/concrete pavement, stormwater system, street lighting and traffic signals. Bike lanes and trails will also be evaluated during the design phase. Other work will include property acquisition and major utility relocations.



## Cost

\$13,671,790

## Construction Timeline

2024 - 2027

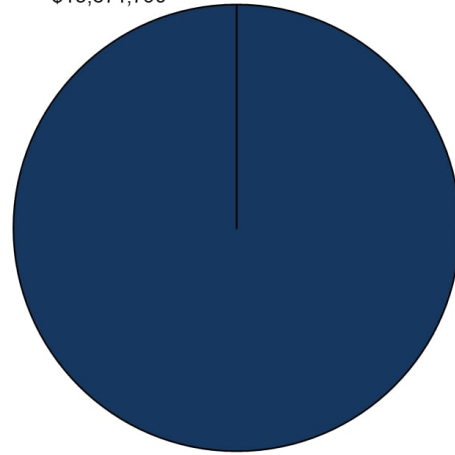
## Vision 2040 Themes

- Integrated Infrastructure & Transportation
- Healthy People

## Guiding Principles Supported

- Strategic Community Investment
- Superior Quality Services

General Obligation Bonds  
\$13,671,790



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
General Obligation Bonds	\$0	\$5,240,590	\$0	\$8,431,200	\$0	\$0	\$13,671,790
<b>Total</b>	<b>\$0</b>	<b>\$5,240,590</b>	<b>\$0</b>	<b>\$8,431,200</b>	<b>\$0</b>	<b>\$0</b>	<b>\$13,671,790</b>

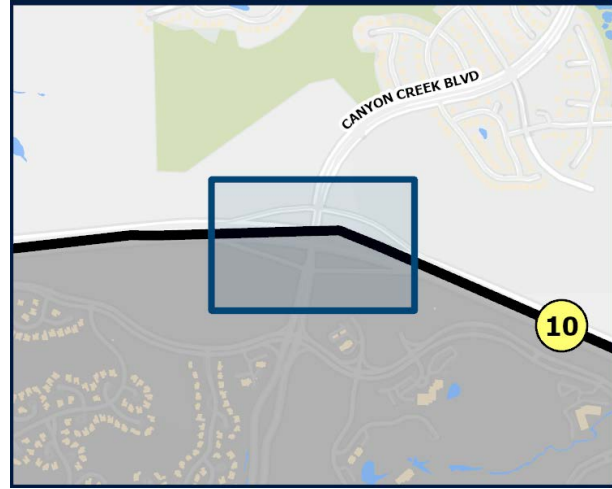
# K-10 & CANYON CREEK BLVD.

## Project Purpose

Due to increased traffic, additional turn lanes are needed at the interchange to support growth and development in the area. This area is a high-growth area of multi-family residential and commercial development based on the updated Comprehensive Plan.

## Project Description

Widen Canyon Creek Blvd. from the K-10 bridge north to the westbound K-10 entrance/exit ramps to accommodate a northbound left-turn lane. Mill & overlay Canyon Creek Blvd. while constructing the new turn lane. Other work includes widening the westbound K-10 exit ramp to add a right-turn lane. Staff is working with the Kansas Department of Transportation to include complete reconstruction of the interchange in future K-10 improvements. Based on those discussions and timelines for the K-10 improvements, will determine the need for these interim improvements.



## Cost

\$589,650

## Construction Timeline

2025 - 2026

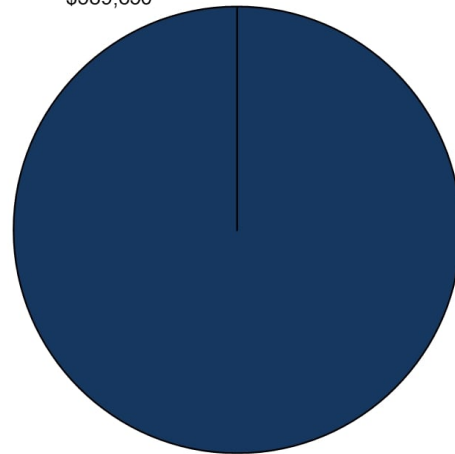
## Vision 2040 Themes

- Integrated Infrastructure & Transportation
- Thriving Economy

## Guiding Principles Supported

- Strategic Community Investment
- Responsible Economic Development

Capital Improvement Fund  
\$589,650



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$0	\$57,200	\$532,450	\$0	\$0	\$589,650
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$57,200</b>	<b>\$532,450</b>	<b>\$0</b>	<b>\$0</b>	<b>\$589,650</b>



# OLD TOWN NORTH PARKING LOT

## Project Purpose

The current parking lot needs major maintenance. The Planning Sustainable Places “A New Look at Old Town” study, recommends changes to the Old Town north parking lot at such time as major maintenance is due: “An additional drive connection to the public parking lot behind and north of the stores and reconfiguration of this parking lot should be considered to improve efficiency and safety for those using the lot as well as potentially increasing the total number of parking spaces.”

## Project Description

Remove and replace existing asphalt parking lot with new concrete parking lot. Install new storm sewer, sidewalks, curbs & gutters, street lighting, and landscaping. Also, connect the north parking lot with the front/south parking lot.

## Cost

\$2,205,030

## Construction Timeline

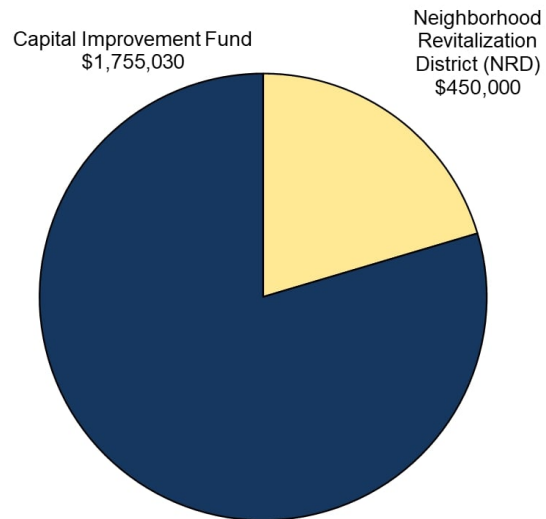
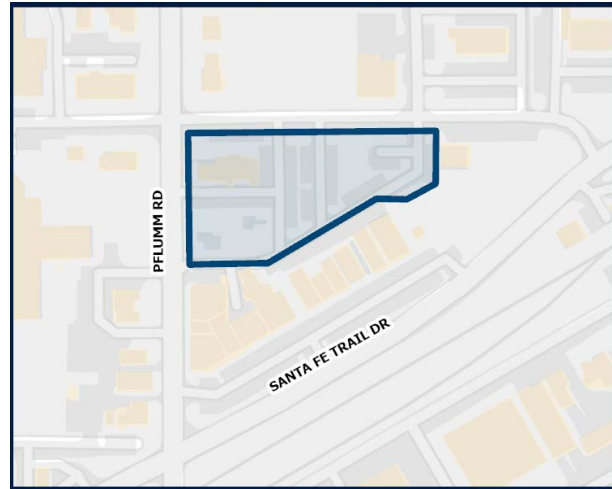
2026 - 2027

## Vision 2040 Themes

- Inviting Places
- Vibrant Neighborhoods

## Guiding Principles Supported

- Strategic Community Investment
- Extraordinary Community Pride



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$0	\$0	\$0	\$1,755,030	\$0	\$1,755,030
NRD	0	0	0	291,870	158,130	0	450,000
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$291,870</b>	<b>\$1,913,160</b>	<b>\$0</b>	<b>\$2,205,030</b>

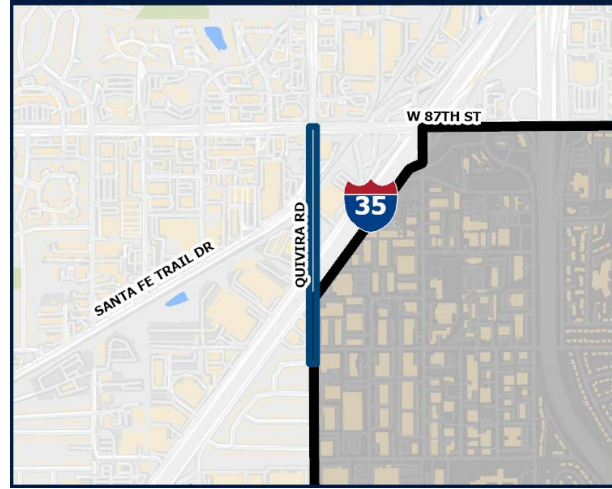
# QUIVIRA RD. BRIDGE OVER I-35

## Project Purpose

At this point, the bridge is structurally in good condition. However, the aesthetics of the bridge are showing their age. The proposed project would meet the Vision 2040 goals of Inviting Places and Vibrant Neighborhoods.

## Project Description

The existing bridge was constructed in 2002. The bridge needs maintenance to improve aesthetics. The handrails and streetlight are badly faded, and the epoxy coated chain link fence has rusted. The proposed project would replace the lighting and fencing, and remove, paint and replace the handrails.



## Cost

\$1,635,000

## Construction Timeline

2025 - 2026

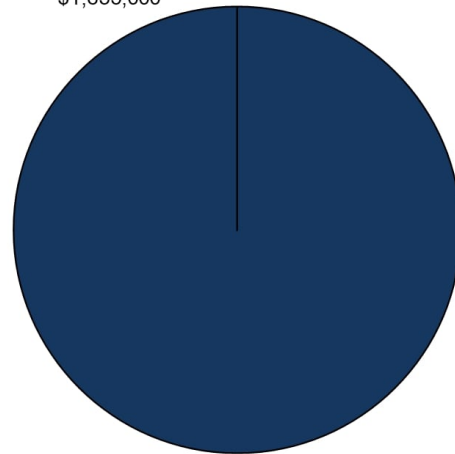
## Vision 2040 Themes

- Inviting Places
- Vibrant Neighborhoods

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment

Capital Improvement Fund  
\$1,635,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$0	\$82,500	\$1,552,500	\$0	\$0	\$1,635,000
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$82,500</b>	<b>\$1,552,500</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,635,000</b>

# ROUNDBABOUT & MEDIAN IMPROVEMENTS

## Project Purpose

Over the past five-plus years, staff has encountered several areas where minor improvement to the geometry of the street would greatly improve function and safety. These projects are too large to include in the Pavement Management Project, but are too small to include individually in the Capital Improvement Program. Staff has combined the highest priority projects into one project.

## Project Description

The project will make geometric improvements at the following locations:

- Extend island - Renner Blvd. north of 85th Street
- Extend island - 101st Street west of Woodland
- Extend roundabout island - Renner Blvd. @ City Center Drive
- Extend roundabout island - Renner Blvd. @ Apartment entrance
- Extend roundabout island - Renner Blvd. @ 90th Street
- Extend roundabout island - PSP & Dunraven Roundabout
- Extend roundabout island - PSP & Clare Roundabout

## Cost

\$913,350

## Construction Timeline

2027 - 2028

## Vision 2040 Themes

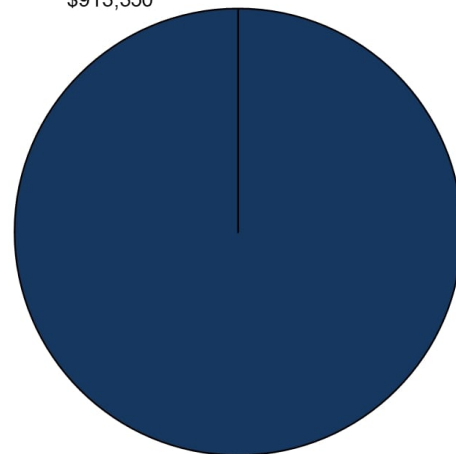
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment



Capital Improvement Fund  
\$913,350



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$0	\$0	\$0	\$69,600	\$843,750	\$913,350
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$69,600</b>	<b>\$843,750</b>	<b>\$913,350</b>

# STREETLIGHT REPLACEMENT PROGRAM

## Project Purpose

In 2009, the City purchased 2,437 streetlights from KCP&L. Most of these lights are over 35 years old, were installed under different lighting standards and are at or beyond their expected useful life. Since 2009, approximately 400 of these purchased lights have been replaced. This program provides ongoing funding to continue to replace a number of these purchased lights and other older streetlights in the City annually. The current average cost to replace a streetlight and pole is approximately \$7,500.

## Project Description

Replace older streetlights with new modern energy efficient lights meeting current city standards. The project also provides matching funds for street lighting projects receiving Community Development Block Grant (CDBG) funding annually. Specific projects are identified annually.

## Cost

\$3,000,000

## Construction Timeline

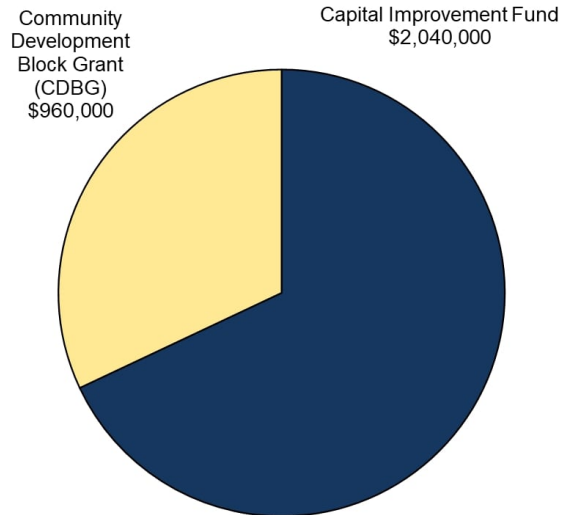
Ongoing

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$340,000	\$340,000	\$340,000	\$340,000	\$340,000	\$340,000	\$2,040,000
Federal Grants	160,000	160,000	160,000	160,000	160,000	160,000	960,000
<b>Total</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$3,000,000</b>

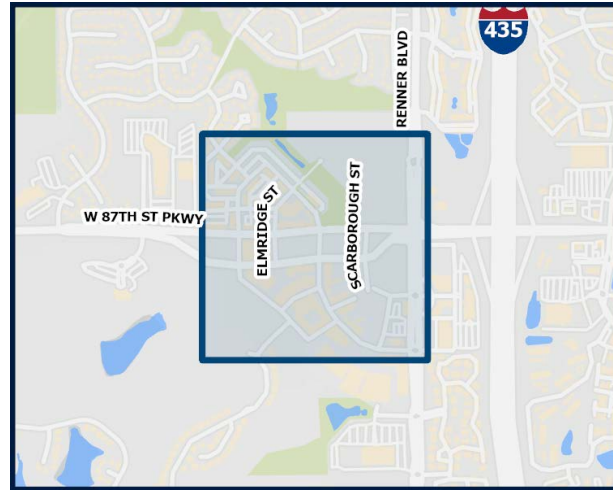
# 87TH ST. PKWY. TRAFFIC SIGNALS

## Project Purpose

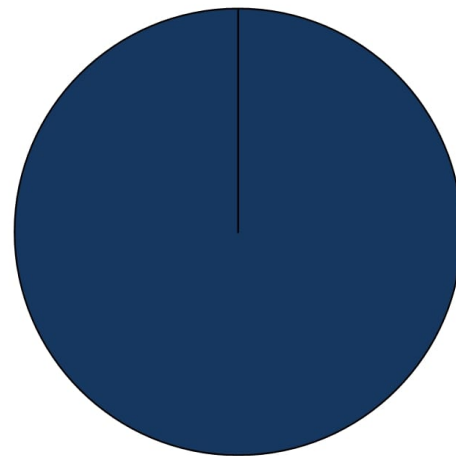
Due to increased traffic, signals are needed to support growth and development in the City Center area.

## Project Description

The original project included the design and construction of a traffic signal at eastbound 87th Street and Penrose Lane and also included the final signal design of the remaining seven (7) intersections on 87th Street from Scarborough to Winchester. The current project includes the installation of traffic signals at Eastbound 87th Street and Elmridge as well as Westbound 87th Street and Elmridge. The intent is to bid the project in 2023 with construction to take place in 2024. The remaining project will include the installation of traffic signals at Eastbound 87th Street and Scarborough and Westbound 87th Street and Scarborough. The intent is to bid and install these signals in 2024.



Traffic Impact Fees  
\$1,186,000



## Cost

\$1,186,000

## Construction Timeline

2023 - 2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment

Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
TIP	\$1,156,717	\$29,283	\$0	\$0	\$0	\$0	\$1,186,000
<b>Total</b>	<b>\$1,156,717</b>	<b>\$29,283</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,186,000</b>

# COMPLETE STREETS PROGRAM

## Project Purpose

The program will help fund improvements to the street system to address issues identified in the Complete Streets Plan and improve the overall transportation network to better accommodate all modes of transportation.

## Project Description

Implement complete streets improvements throughout the City including signing, pavement marking, filling missing sidewalk gaps, crosswalks, etc. as recommended in the Complete Streets Plan. Specific projects are identified annually.

## Cost

\$1,240,000

## Construction Timeline

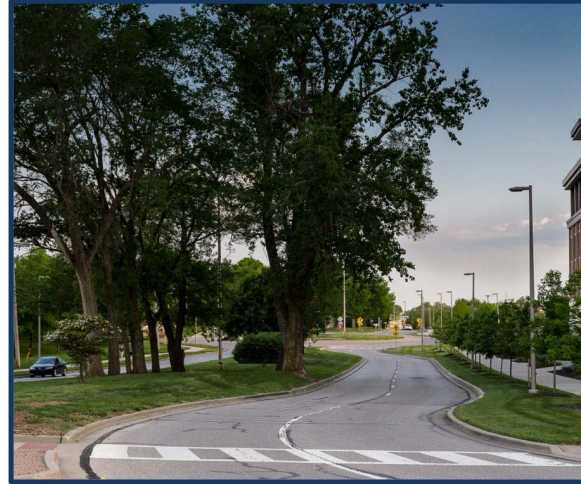
TBD

## Vision 2040 Themes

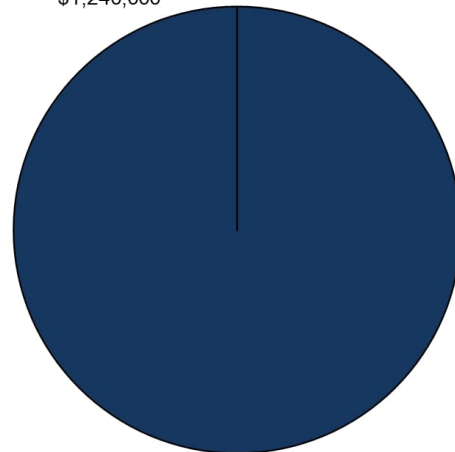
- Integrated Infrastructure & Transportation
- Healthy People

## Guiding Principles Supported

- Strategic Community Investment
- Sustainable Policies & Practices
- Inclusive Community Building



Capital Improvement Fund  
\$1,240,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$690,000	\$110,000	\$110,000	\$110,000	\$110,000	\$110,000	\$1,240,000
<b>Total</b>	<b>\$690,000</b>	<b>\$110,000</b>	<b>\$110,000</b>	<b>\$110,000</b>	<b>\$110,000</b>	<b>\$110,000</b>	<b>\$1,240,000</b>

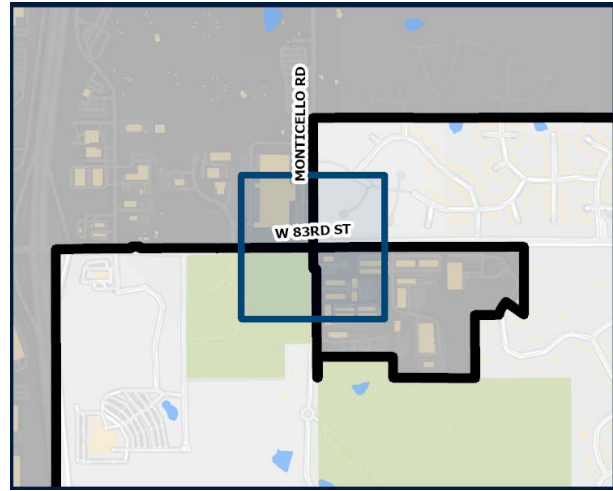
# 83RD ST. & MONTICELLO RD. INTERSECTION

## Project Purpose

Due to increased traffic, a traffic signal is needed to support growth and development in the area.

## Project Description

The project includes the installation and construction of a traffic signal, an eastbound and westbound right-turn lane, street lighting and a trail on 83rd Street (Monticello to Woodsonia), sidewalk on the north side of 83rd Street (McCoy to Woodsonia), mill and overlay of 83rd Street (K-7 to east of Valley Road), curb and gutter replacement, pavement replacement on 83rd Street east of K-7, and a pedestrian crossing at 83rd Street and Woodsonia.



## Cost

\$2,670,000

## Construction Timeline

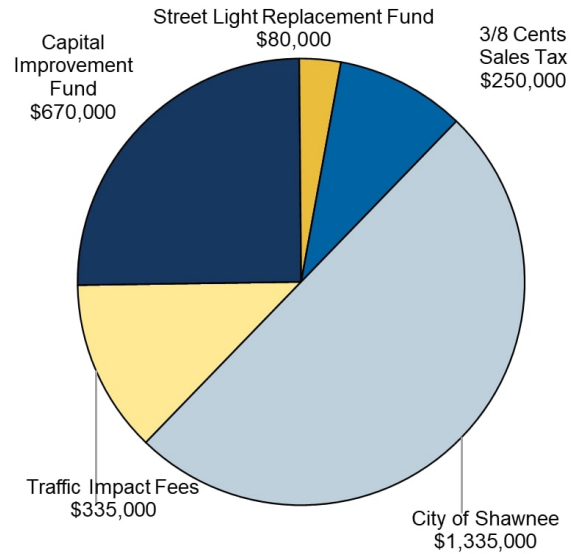
2023 - 2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$670,000	\$0	\$0	\$0	\$0	\$0	\$670,000
TIP	335,000	0	0	0	0	0	335,000
City of Shawnee	1,335,000	0	0	0	0	0	1,335,000
Street Light Replacement	80,000	0	0	0	0	0	80,000
3/8 Cent Sales Tax	250,000	0	0	0	0	0	250,000
<b>Total</b>	<b>\$2,670,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,670,000</b>

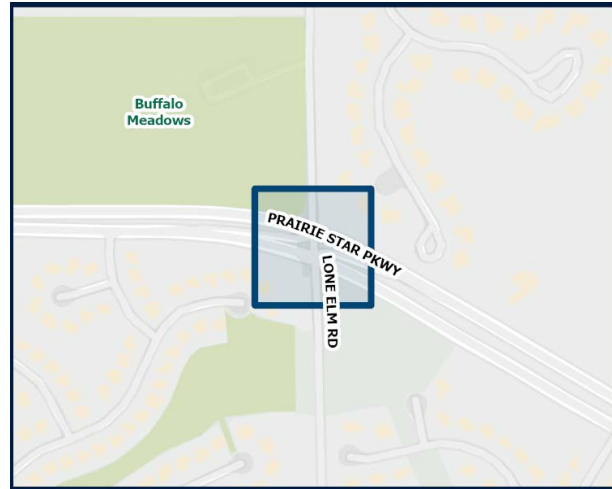
# PRAIRIE STAR PKWY. & LONE ELM RD.

## Project Purpose

Due to increased traffic, a traffic signal is needed to support growth and development in the area.

## Project Description

The project includes the installation of a new traffic signal at Prairie Star Parkway and Lone Elm Road. The project will also include modifications to median islands, modifications to the curb line on the southeast corner of Prairie Star Parkway and Lone Elm Road, pedestrian access improvements, and replacement of the residential post-top street lights on Lone Elm Road from Prairie Star Parkway to 96th Street.



## Cost

\$973,300

## Construction Timeline

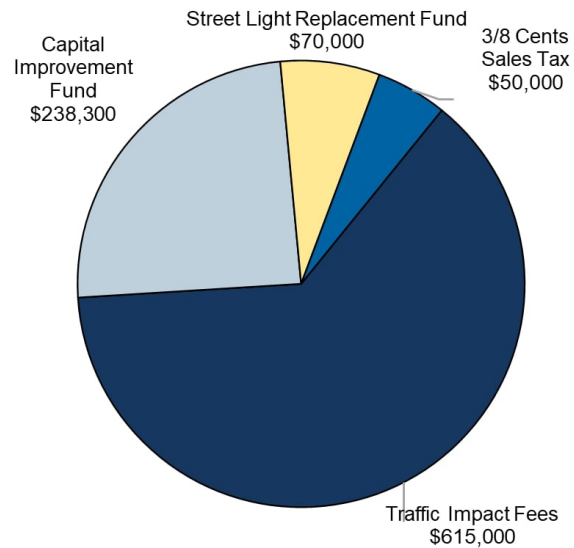
2023 - 2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$238,300	\$0	\$0	\$0	\$0	\$0	\$238,300
TIP	565,000	50,000	0	0	0	0	615,000
Street Light Replacement	70,000	0	0	0	0	0	70,000
3/8 cent sales tax	50,000	0	0	0	0	0	50,000
<b>Total</b>	<b>\$923,300</b>	<b>\$50,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$973,300</b>



# 83RD ST. & LACKMAN RD. TRAFFIC SIGNALS

## Project Purpose

Due to increased traffic, a traffic signal is needed to support growth and development in the area. In addition, with the closure of Ad Astra Pool in 2025, the increased pedestrian traffic crossing Lackman Road will benefit from the added safety of a traffic signal.

## Project Description

The project includes the installation of a new traffic signal at 83rd Street and Lackman Road. The project will also include pedestrian access improvements.

## Cost

\$514,500

## Construction Timeline

2024

## Vision 2040 Themes

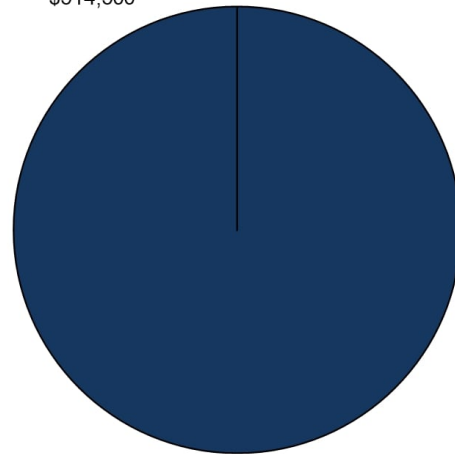
- Integrated Infrastructure & Transportation
- Vibrant Neighborhoods

## Guiding Principles Supported

- Strategic Community Investment
- Sustainable Policies & Practices



Capital Improvement Fund  
\$514,500



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$514,500	\$0	\$0	\$0	\$0	\$514,500
<b>Total</b>	<b>\$0</b>	<b>\$514,500</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$514,500</b>

# FACILITIES & MAINTENANCE IMPROVEMENTS

## Project Purpose

Facilities depreciate over time due to wear and tear, weather conditions, usage, and other factors. A dedicated fund ensures that necessary repairs and maintenance are carried out regularly, preventing small issues from becoming larger and costlier problems.

## Project Description

The facilities improvement and maintenance fund ensures the ongoing functionality, safety, and value of the City of Lenexa facilities while allowing for planned and cost-effective maintenance and improvements. It supports the overall operations, reputation, and long-term viability of the City of Lenexa employees and residents who work in and visit our facilities.



## Cost

\$6,482,966

## Construction Timeline

Ongoing

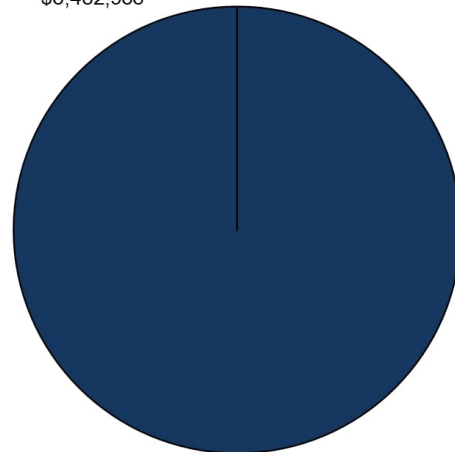
## Vision 2040 Themes

- Healthy People
- Inviting Places

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Sustainable Policies & Practices

Capital Improvement Fund  
\$6,482,966



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$3,982,966	\$500,000	\$500,000	\$500,000	\$500,000	\$500,000	\$6,482,966
<b>Total</b>	<b>\$3,982,966</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$500,000</b>	<b>\$6,482,966</b>

# OLD CITY HALL / FIRE STATION 6 PHASE 1 DESIGN

## Project Purpose

Funding for this phase of design will assess the mechanical and structural systems of the old City Hall and develop site plan recommendations and cost estimates for a future Fire Station 6. The first phase of design will also look at recommendations for reconfiguring the old City Hall for current and future City operations, including Information Technology, LiveWell employee health clinic, and Fire Administration.



## Project Description

The first phase of design for old City Hall and future Fire Station 6.

## Cost

\$210,000

## Construction Timeline

TBD

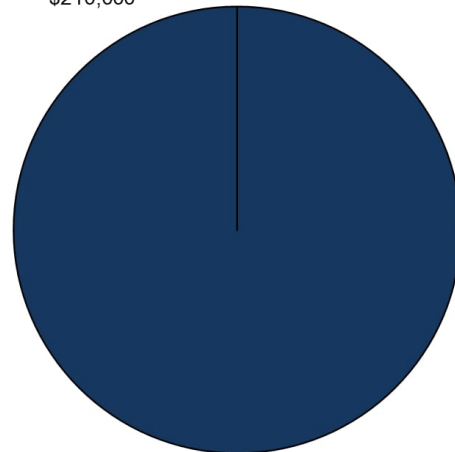
## Vision 2040 Themes

- Inviting Places

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment
- Values-based Organizational Culture

Capital Improvement Fund  
\$210,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$100,000	\$110,000	\$0	\$0	\$0	\$0	\$210,000
<b>Total</b>	<b>\$100,000</b>	<b>\$110,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$210,000</b>

# LENEXA OLD TOWN ACTIVITY CENTER

## Project Purpose

Recommendations from the Community Center and Senior Center Study included options for improving operations, programming, and function of the facilities while making the site a greater amenity for the surrounding neighborhood and the entire community.

## Project Description

Improvements to the Community Center and Senior Center campus including consolidation of buildings, ADA improvements, expansion of senior program areas, and site improvements.

## Cost

\$12,152,000

## Construction Timeline

2023 - 2024

## Vision 2040 Themes

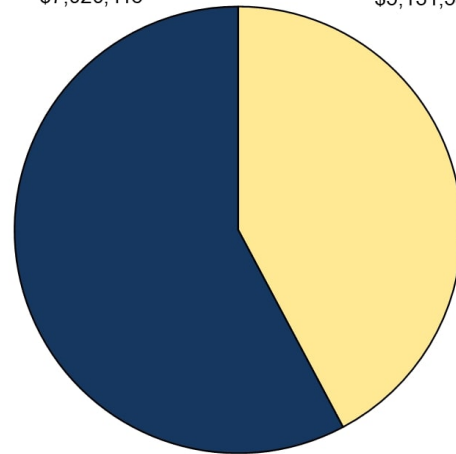
- Inviting Places
- Healthy People
- Vibrant Neighborhoods

## Guiding Principles Supported

- Strategic Community Investment
- Extraordinary Community Pride
- Inclusive Community Building



Capital Improvement Fund \$7,020,416      Federal Grants \$5,131,584



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$7,020,416	\$0	\$0	\$0	\$0	\$0	\$7,020,416
Federal Grants	5,131,584	0	0	0	0	0	5,131,584
<b>Total</b>	<b>\$12,152,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$12,152,000</b>

# LENEXA JUSTICE CENTER

## Project Purpose

The current Public Safety Complex was constructed in phases starting in 1980. The current building does not meet the future space needs of the Police Department and Municipal Court. The current building has reached its useful life and is currently not configured to meet current best practices in law enforcement. Additional space is needed in all functional areas including communications/dispatch, locker rooms, evidence and records processing, patrol, and training.

## Project Description

This project is the design and construction of a new facility to meet the future space needs of the Police Department and Municipal Court. The project enhances security, training, communications/dispatch, and records and evidence storage.



## Cost

\$73,000,000

## Construction Timeline

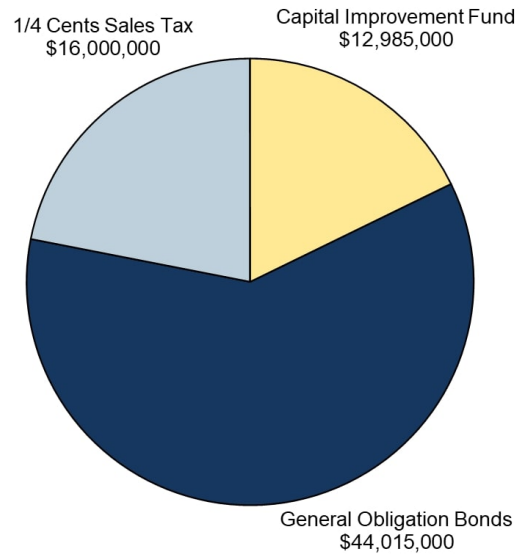
2022 - 2024

## Vision 2040 Themes

- Healthy People
- Inviting Places

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment
- Values-based Organizational Culture



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
General Obligation Bonds	\$44,015,000	\$0	\$0	\$0	\$0	\$0	\$44,015,000
Capital Improvement Fund	12,985,000	0	0	0	0	0	12,985,000
1/4 Cent Sales Tax	16,000,000	0	0	0	0	0	16,000,000
<b>Total</b>	<b>\$73,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$73,000,000</b>

# PLAYGROUND EQUIPMENT REPLACEMENT

## Project Purpose

Playgrounds have a useful lifespan and eventually have to be replaced. This project continues the replacement of equipment on a rotating basis according to the replacement schedule. Replacements for playgrounds located within neighborhood parks are anticipated between 20-25 years. Replacements for playgrounds located within community parks are anticipated between 16-22 years. Every playground is evaluated regularly and the schedule is adjusted appropriately to extend the life of the playgrounds as long as possible. The next replacements include both playgrounds located within Parkhurst Park in 2025. It is recommended that the replacement equipment be installed within Central Green Park in lieu of the current location along the Parkhurst trail which has very limited access.



## Project Description

Replacement of playground equipment and surfacing at various parks due to its age, safety, and availability of replacement parts. All playground replacements include a public engagement for interested parties.

## Cost

\$1,978,684

## Construction Timeline

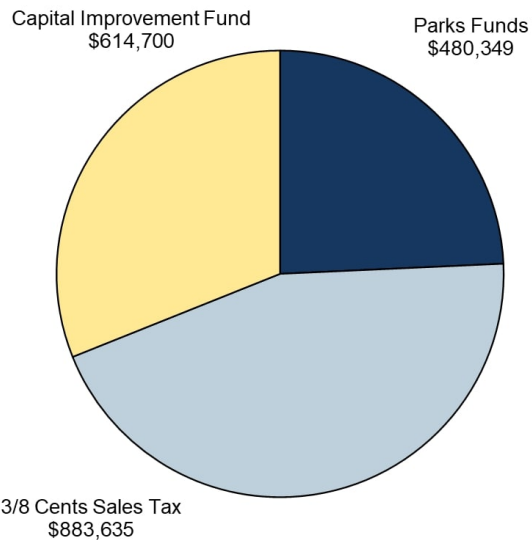
Ongoing

## Vision 2040 Themes

- Healthy People
- Inviting Places
- Vibrant Neighborhoods

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment
- Extraordinary Community Pride
- Inclusive Community Building



<b>Funding Sources</b>							
	<b>Prior Years</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>2027</b>	<b>2028</b>	<b>Total</b>
Capital Improvement Fund	\$614,700	\$0	\$0	\$0	\$0	\$0	\$614,700
3/8 Cent Sales Tax	883,635	0	0	0	0	0	883,635
Parks Funds	210,000	35,000	235,349	0	0	0	480,349
<b>Total</b>	<b>\$1,708,335</b>	<b>\$35,000</b>	<b>\$235,349</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,978,684</b>

# SAR-KO PAR AQUATIC CENTER

## Project Purpose

The majority of Indian Trails Aquatic Center is over 45 years old and many elements have reached the end of their useful life. This project will develop a modernized community-wide aquatic center with the appropriate mix of amenities. The amenities and programming needs were identified through a robust public engagement effort including a community-wide statistically valid survey.

## Project Description

Renovation and Expansion of Sar-Ko Par Aquatic Center.

## Cost

\$15,000,000

## Construction Timeline

2022 - 2024

## Vision 2040 Themes

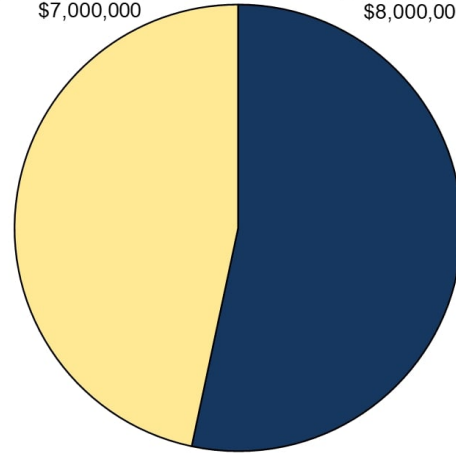
- Healthy People
- Inviting Places
- Vibrant Neighborhoods

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment
- Extraordinary Community Pride



Capital Improvement Fund \$7,000,000      3/8 Cents Sales Tax \$8,000,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$7,000,000	\$0	\$0	\$0	\$0	\$0	\$7,000,000
3/8 Cent Sales Tax	8,000,000	0	0	0	0	0	8,000,000
<b>Total</b>	<b>\$15,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$15,000,000</b>



# AD ASTRA POOL RECONSTRUCTION

## Project Purpose

Ad Astra Pool was built in 1988 and has been structurally problematic since its opening. While many investments and repairs have been made to keep this pool in operation, the ongoing and worsening conditions require it to be rebuilt to keep this location in operation. This project will rebuild Ad Astra Pool with the appropriate mix of amenities and programming to complement the nearby Indian Trails Aquatic Center while continuing to serve the surrounding neighborhoods and the community.

## Project Description

Reconstruction of Ad Astra pool and bathhouse, and reconfiguration of parking. The pool will be sized and programmed to serve the surrounding neighborhood similar to Flat Rock Creek Pool. This project will include public engagement and feedback on potential design elements.

## Cost

\$8,000,000

## Construction Timeline

2024 - 2025

## Vision 2040 Themes

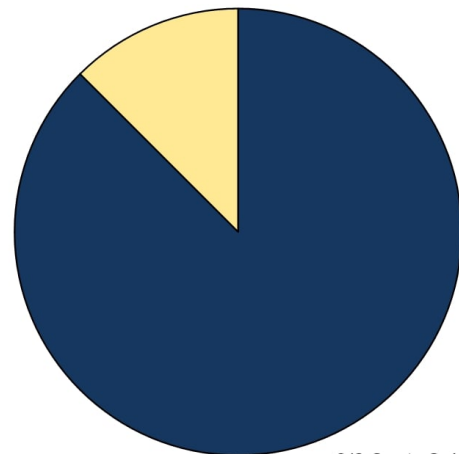
- Healthy People
- Vibrant Neighborhoods

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment
- Extraordinary Community Pride
- Inclusive Community Building



Capital Improvement Fund  
\$1,000,000



3/8 Cents Sales Tax  
\$7,000,000

## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$0	\$1,000,000	\$0	\$0	\$0	\$1,000,000
3/8 Cent Sales Tax	0	2,000,000	5,000,000	0	0	0	7,000,000
<b>Total</b>	<b>\$0</b>	<b>\$2,000,000</b>	<b>\$6,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$8,000,000</b>

# LITTLE MILL CREEK TRAIL

## Project Purpose

The Little Mill Creek Trail from the Lackman Road and 79th Street Intersection to the Little Mill Creek Park is the most heavily utilized trail in the Lenexa Trail System by both pedestrians and cyclists. The trail has reached the end of its useful life as there are major cracks, heaves, and heavily deteriorated sections. Typical maintenance strategies are no longer cost effective.

## Project Description

The Little Mill Creek Trail Replacement Project would consist of removing and replacing 1.45 lane miles of trail network from Lackman Road to the Little Mill Creek Park with a 10-foot wide concrete trail.

## Cost

\$1,000,000

## Construction Timeline

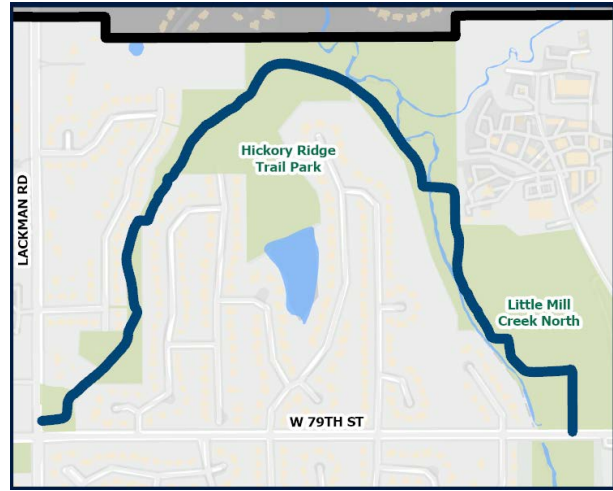
2024

## Vision 2040 Themes

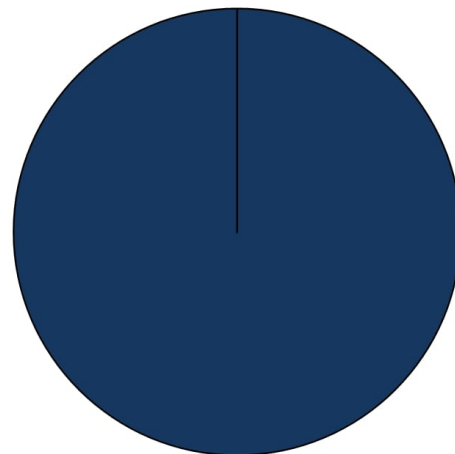
- Vibrant Neighborhoods
- Integrated Infrastructure
- Healthy People
- Inviting Places

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Strategic Community Investment
- Sustainable Policies and Practices



3/8 Cents Sales Tax  
\$1,000,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
3/8 Cent Sales Tax	\$0	\$1,000,000	\$0	\$0	\$0	\$0	\$1,000,000
<b>Total</b>	<b>\$0</b>	<b>\$1,000,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,000,000</b>

# STORMWATER INFRASTRUCTURE

## Project Purpose

Maintain city stormwater infrastructure in good condition to reduce the risk of flooding. Pipe failures can also be dangerous, especially under streets. The proposed project will analyze existing pipes and replace or line them before they fail.

## Project Description

Funding for emergency repairs or projects that are developed through system assessments.

## Cost

\$7,534,000

## Construction Timeline

Ongoing

## Vision 2040 Themes

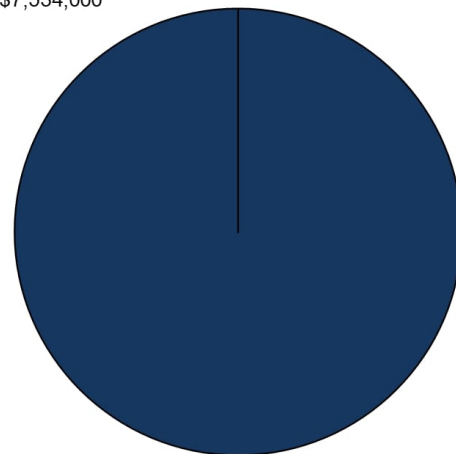
- Integrated Infrastructure & Transportation
- Vibrant Neighborhoods

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



Stormwater Funds  
\$7,534,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Funds	\$1,300,000	\$1,034,000	\$1,300,000	\$1,300,000	\$1,300,000	\$1,300,000	\$7,534,000
<b>Total</b>	<b>\$1,300,000</b>	<b>\$1,034,000</b>	<b>\$1,300,000</b>	<b>\$1,300,000</b>	<b>\$1,300,000</b>	<b>\$1,300,000</b>	<b>\$7,534,000</b>

# 113TH ST. - E OF RENNER

## Project Purpose

Replacement and rehabilitation of corrugated metal pipe that has reached the end of its useful life. This area was chosen due to active failures and the availability of SMAC funding.

## Project Description

Replace or rehabilitate corrugated metal pipe that has reached the end of its useful life.

## Cost

\$1,570,000

## Construction Timeline

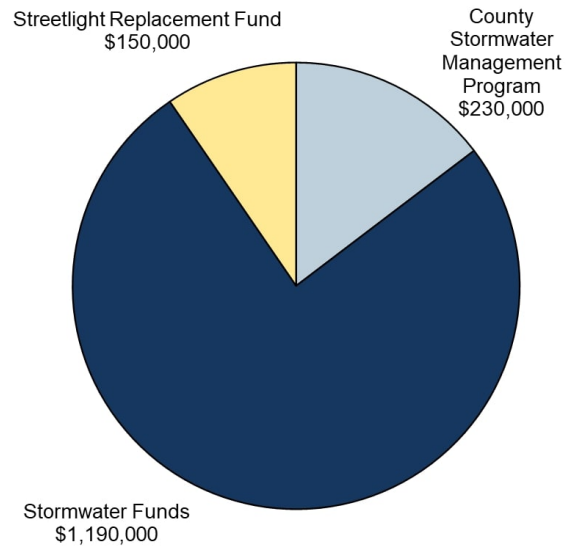
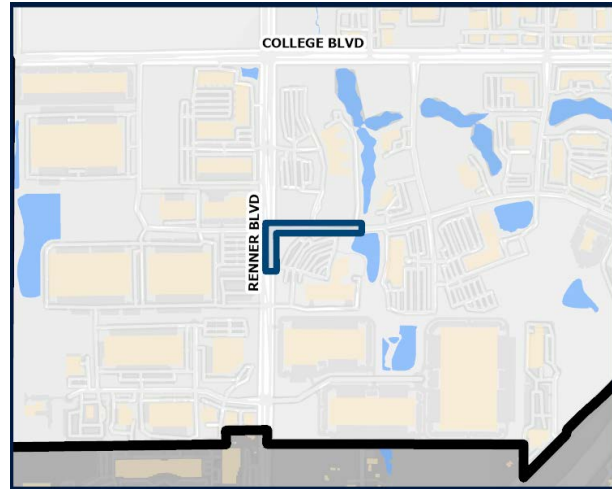
2023 - 2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Funds	\$704,500	\$485,500	\$0	\$0	\$0	\$0	\$1,190,000
SMAC	0	230,000	0	0	0	0	230,000
Street Light Replacement	0	150,000	0	0	0	0	150,000
<b>Total</b>	<b>\$704,500</b>	<b>\$865,500</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,570,000</b>

# 81ST ST. TO 81ST TER - E OF MAURER

## Project Purpose

The existing corrugated metal pipe has reached the end of its useful life. The project will replace or line the existing pipe to ensure its structurally sound and continues to convey stormwater runoff. This project is eligible for SMAC funding, thereby reducing the City's costs.

## Project Description

Replace or rehabilitate existing corrugated metal pipes through residential areas.

## Cost

\$985,425

## Construction Timeline

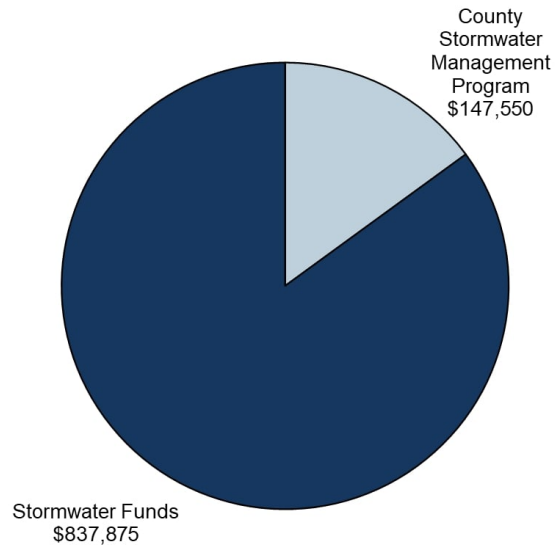
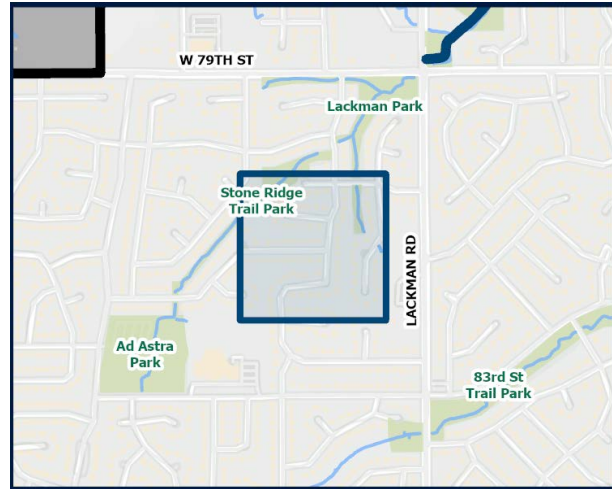
2023 - 2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Funds	\$513,950	\$323,925	\$0	\$0	\$0	\$0	\$837,875
SMAC	147,550	0	0	0	0	0	147,550
<b>Total</b>	<b>\$661,500</b>	<b>\$323,925</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$985,425</b>

# 113TH ST. & LAKEVIEW AVE.

## Project Purpose

The existing corrugated metal pipe has reached the end of its useful life. The project will replace or line the existing pipe to ensure it is structurally sound and continues to convey stormwater runoff. This project is eligible for SMAC funding, thereby reducing the City's costs.

## Project Description

Replace or rehabilitate corrugated metal pipe that has reached the end of its useful life.

## Cost

\$2,950,000

## Construction Timeline

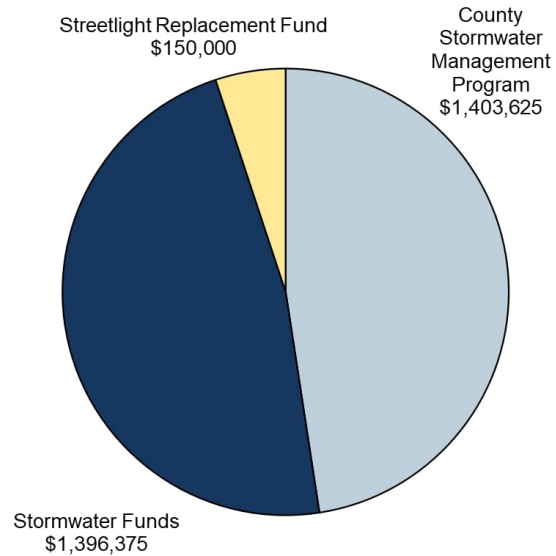
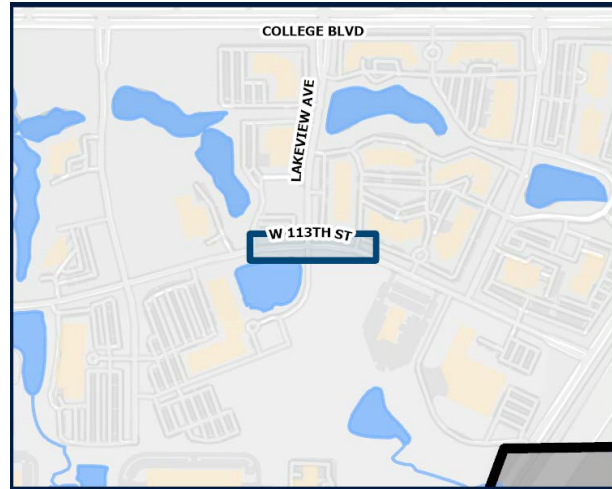
2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Funds	\$0	\$1,396,375	\$0	\$0	\$0	\$0	\$1,396,375
SMAC	0	1,403,625	0	0	0	0	1,403,625
Street Light Replacement	0	150,000	0	0	0	0	150,000
<b>Total</b>	<b>\$0</b>	<b>\$2,950,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,950,000</b>

# 87TH ST. & BLUEJACKET ST.

## Project Purpose

The existing corrugated metal pipe has reached the end of its useful life. The project will replace a 83"x57" corrugated metal pipe under Bluejacket, immediately north of 87th Street. The project will line or replace existing smaller pipes along Melrose Drive in the nearby Pine Ridge Business park. The project is eligible for SMAC funding, thereby reducing the City's costs.

## Project Description

Replace or rehabilitate corrugated metal pipe that has reached the end of its useful life.

## Cost

\$1,270,000

## Construction Timeline

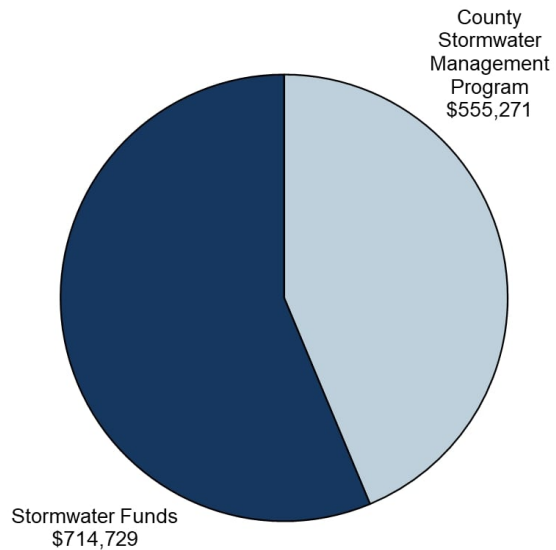
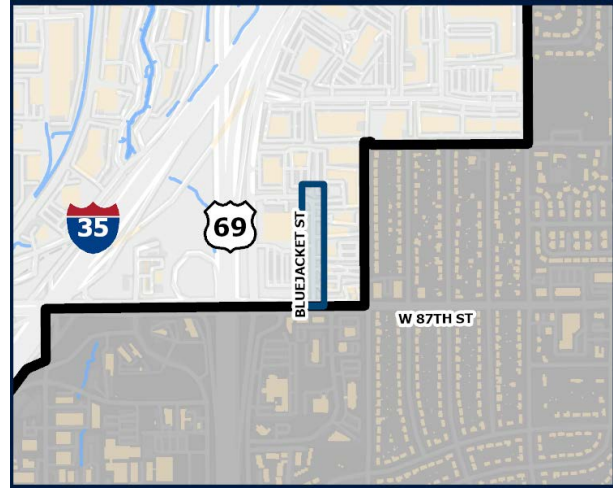
2024

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Funds	\$0	\$714,729	\$0	\$0	\$0	\$0	\$714,729
SMAC	0	555,271	0	0	0	0	555,271
<b>Total</b>	<b>\$0</b>	<b>\$1,270,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,270,000</b>

# 107TH ST. TO 108TH ST. - W OF PFLUMM RD.

## Project Purpose

The existing corrugated metal pipe has reached the end of its useful life. The project will replace or line multiple pipes ranging from 15" to 56" that convey stormwater across and along 107th and 108th Street in the industrial area west of Pflumm and east of Cottonwood Street. The project is eligible for SMAC funding, thereby reducing the City's costs.

## Project Description

Replace or rehabilitate corrugated metal pipe that has reached the end of its useful life.

## Cost

\$1,870,000

## Construction Timeline

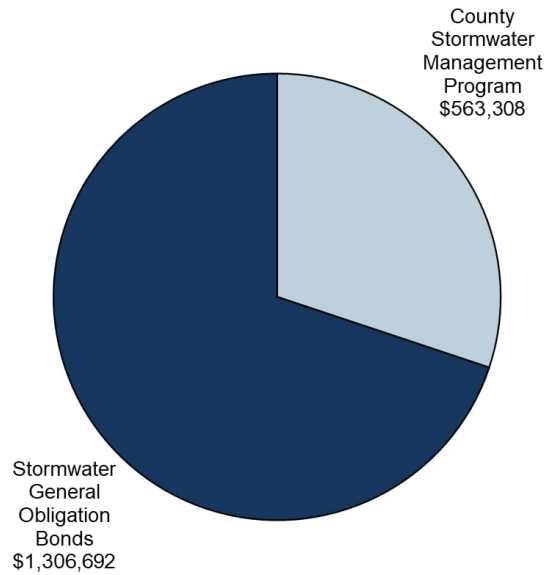
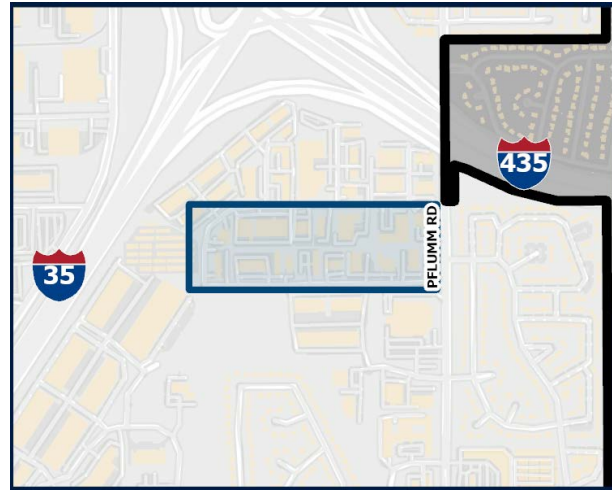
2025

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Bonds	\$0	\$0	\$1,306,692	\$0	\$0	\$0	\$1,306,692
SMAC	0	0	563,308	0	0	0	563,308
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,870,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,870,000</b>



# 89TH TER. TO 90TH ST. - W OF LACKMAN

## Project Purpose

The existing corrugated metal pipe has reached the end of its useful life. The project will replace 650 feet of 42" corrugated metal pipe that runs through the residential west of Lackman. The project will also replace or line multiple smaller pipes that tie into the large one. Work will only affect residential streets and yards. The project is eligible for SMAC funding, thereby reducing the City's costs.

## Project Description

Replace or rehabilitate corrugated metal pipe that has reached the end of its useful life.

## Cost

\$1,970,000

## Construction Timeline

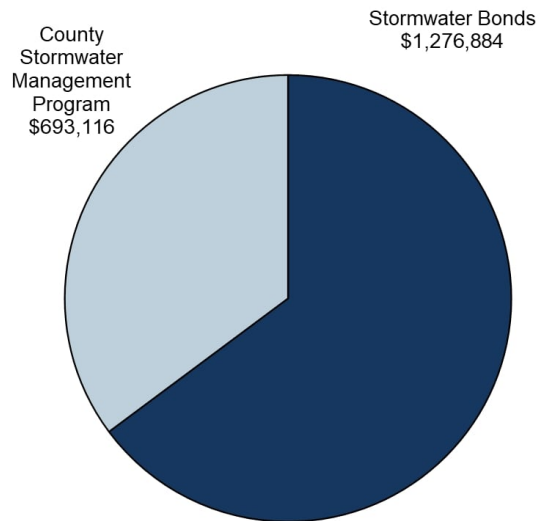
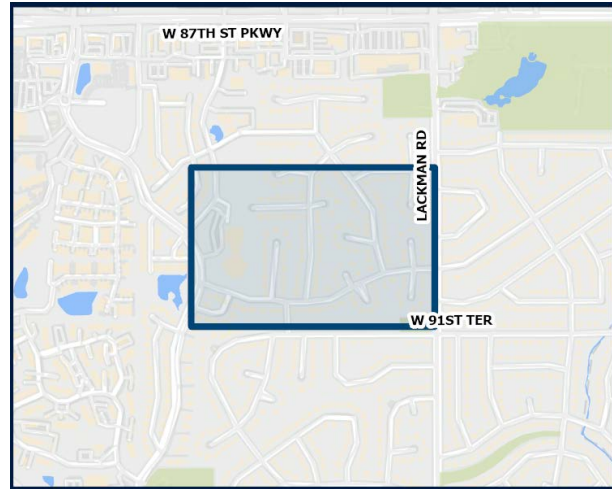
2025

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Bonds	\$0	\$0	\$1,276,884	\$0	\$0	\$0	\$1,276,884
SMAC	0	0	693,116	0	0	0	693,116
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,970,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,970,000</b>

# SEVEN HILLS LAKE WATERSHED PHASE 1

## Project Purpose

The existing corrugated metal pipe has reached the end of its useful life. The project will replace 750 feet of 77" x 52" corrugated metal pipe that runs through the residential area that drains into Seven Hills Lake. The project will also replace or line multiple smaller pipes that tie into the large one. Work will only affect residential streets and yards. The project is eligible for SMAC funding, thereby reducing the City's costs.

## Project Description

Replace or rehabilitate corrugated metal pipe that has reached the end of its useful life.

## Cost

\$2,955,500

## Construction Timeline

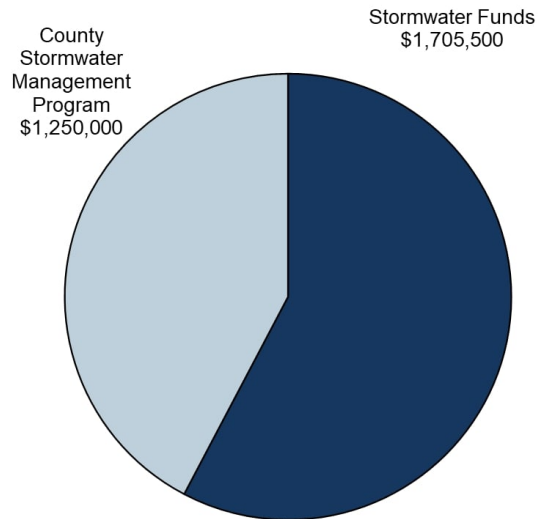
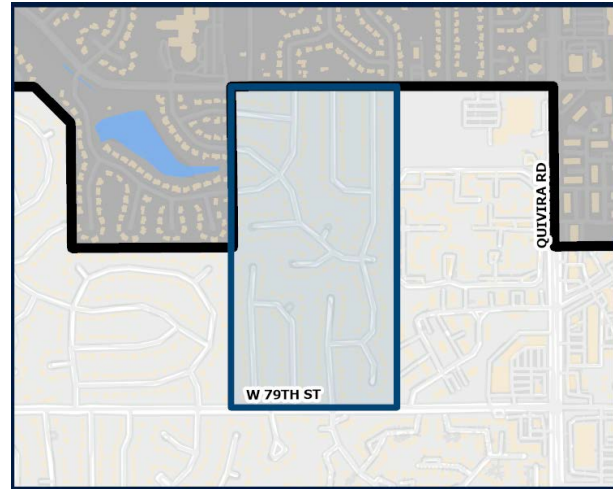
2025

## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Sustainable Policies & Practices
- Strategic Community Investment



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Stormwater Funds	\$0	\$0	\$1,705,500	\$0	\$0	\$0	\$1,705,500
SMAC	0	0	1,250,000	0	0	0	1,250,000
<b>Total</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,955,500</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,955,500</b>

# GATEWAY SIGN PROGRAM

## Project Purpose

The existing gateway signs are outdated and have been a point of emphasis from citizens across the City. Some locations have already been completed as part of a major road improvement in that particular area, however, without a dedicated project some of these locations may never get done. Signage, wayfinding, and branding are important to our community’s image and promote economic development and tourism by attracting businesses and visitors to the City.

## Project Description

This multi-phase project will replace the existing outdated metal gateway signs with stone monuments using the same style as the park monuments. This includes 15 total locations at major entry points into the city. The following locations will receive a three-column sign: Pflumm and College and K-10 and Woodland. There are 11 other locations that will receive a single column monument given the lower traffic volume or limited space.



## Cost

\$850,000

## Construction Timeline

2023 - 2024

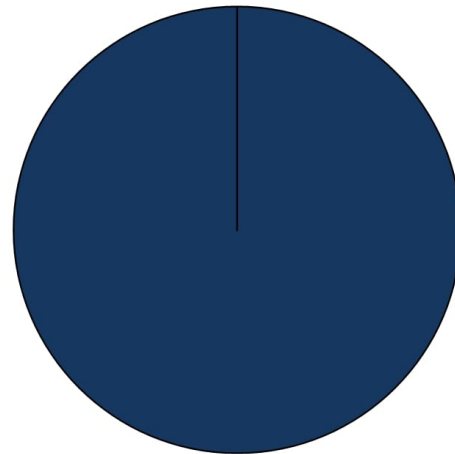
## Vision 2040 Themes

- Inviting Places

## Guiding Principles Supported

- Extraordinary Community Pride

Tourism Fund  
\$850,000



## Funding Sources

	Prior Years	2024	2025	2026	2027	2028	Total
Tourism Fund	\$850,000	\$0	\$0	\$0	\$0	\$0	\$850,000
Total	\$850,000	\$0	\$0	\$0	\$0	\$0	\$850,000

# FIRE APPARATUS REPLACEMENT

## Project Purpose

To maintain a high quality fleet of both front-line and reserve apparatus it is recommended that each fire apparatus be assigned 8 years of front line service and 8 years of reserve service. Delaying this apparatus replacement program will negatively affect front-line and reserve apparatus quality and reliability.

## Project Description

Annual replacement of fire apparatus in 2024 thru 2028. Due to increased production times for these apparatus, all apparatus funded through 2027 are already on order.

## Cost

\$6,608,000

## Construction Timeline

2024

## Vision 2040 Themes

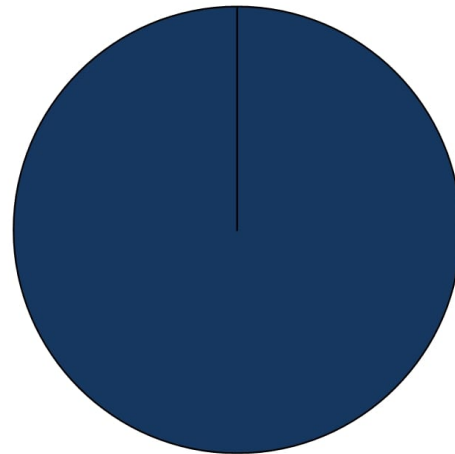
- Healthy People

## Guiding Principles Supported

- Superior Quality Services



Capital Improvement Fund  
\$6,608,000

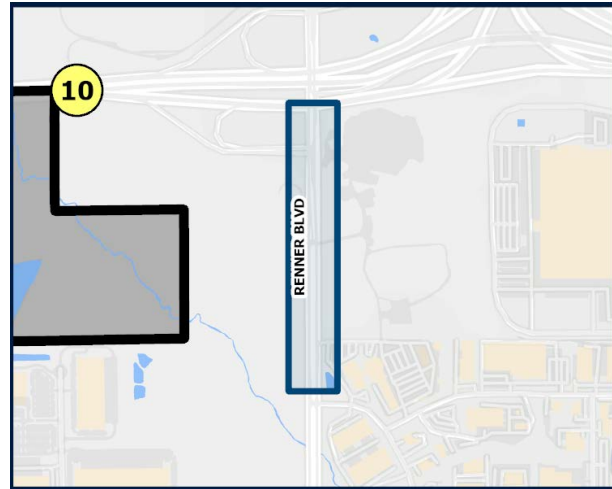


Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$4,426,000	\$0	\$0	\$1,034,000	\$1,148,000	\$0	\$6,608,000
<b>Total</b>	<b>\$4,426,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,034,000</b>	<b>\$1,148,000</b>	<b>\$0</b>	<b>\$6,608,000</b>

# RENNER BLVD. MINE REMEDIATION

## Project Purpose

In 1987, mining was completed on the west side of Renner Blvd, south of K-10. At that time, tunnels were mined under Renner Blvd. to provide access to the east side of Renner Blvd. for additional mining activities. Due to poor mining processes, the floors of the mine were excavated thin, leaving questionable structural capacity for the pillars. In certain areas of the mine, the pillars are punching through the floor of the mine, which is causing the roof to collapse. Staff has set monitoring devices along Renner Blvd. to ensure the tunnel ceilings are not moving. The funding for this project will keep remediation funds available should they be needed.



## Project Description

Fill the three quarried tunnels under Renner Blvd. by utilizing “rock slinging”. This is a process whereby a bore hole is drilled from the surface, into the mine, and small rocks are used to fill the voids.

## Cost

\$1,500,000

## Construction Timeline

2024 or TBD

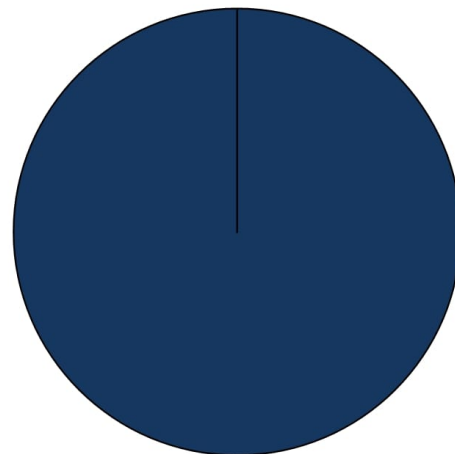
## Vision 2040 Themes

- Healthy People
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Strategic Community Investment

Capital Improvement Fund  
\$1,500,000

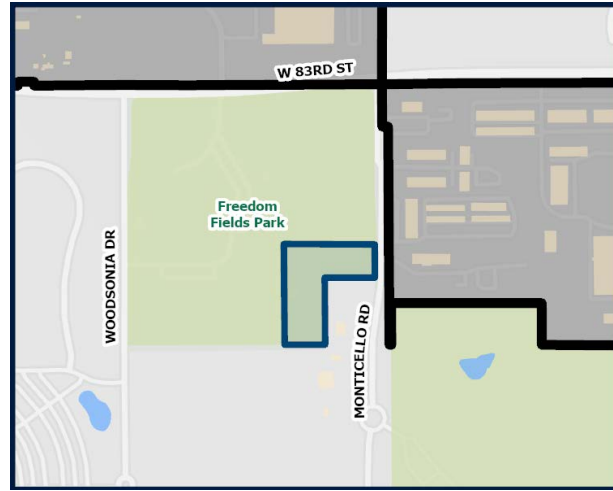


Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$1,500,000	\$0	\$0	\$0	\$0	\$1,500,000
<b>Total</b>	<b>\$0</b>	<b>\$1,500,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,500,000</b>

# SALT STORAGE & FUELING STATION

## Project Purpose

Construct a new fueling station and upgrade our current salt structure at Freedom Fields. Currently, a 750-ton salt structure at Freedom Fields that is used by our crews during winter events to reload trucks with salt. With the population growth and additional lane miles in the western part of the community, this structure is inadequate for use during a typical winter event. Replacing this structure with a larger 2,000-ton structure will improve efficiency and response to these neighborhoods. As the community continues to grow, it is inefficient to have only one fueling station for staff to refuel their vehicles and equipment. The current fueling station is located at 7700 Cottonwood. This additional fueling station will also improve fuel efficiencies and reduce staff travel time to refuel.



## Project Description

The salt structure will be replaced with a new 2,000-ton fabric structure, enhanced calcium chloride tanks and a new fueling station consisting of two diesel and two gasoline pumps. The fueling station will have a rain canopy with self-contained double lined above ground tanks. New pavement will be placed and the site will be fenced with access control capabilities. In addition, trees will be placed around the site for additional screening from neighboring properties.

## Cost

\$1,653,750

## Construction Timeline

2024

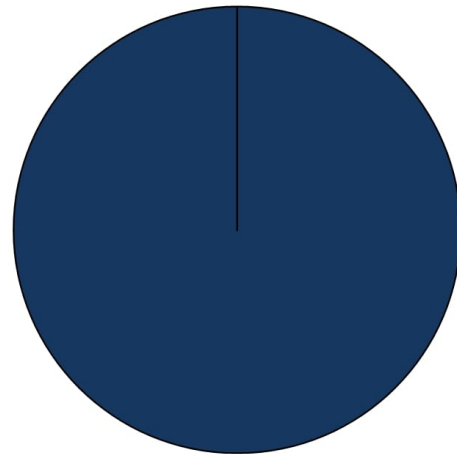
## Vision 2040 Themes

- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment
- Sustainable Policies & Practices

Capital Improvement Fund  
\$1,653,750



<b>Funding Sources</b>							
	<b>Prior Years</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>2027</b>	<b>2028</b>	<b>Total</b>
Capital Improvement Fund	\$472,500	\$1,181,250	\$0	\$0	\$0	\$0	\$1,653,750
<b>Total</b>	\$472,500	\$1,181,250	\$0	\$0	\$0	\$0	\$1,653,750

# FIBER OPTIC NETWORK

## Project Purpose

Expanding the fiber optic network will provide access to key locations that are currently not served. Fiber optic connections provide network access, security control, video connections, and traffic signal control. In addition to new facilities some of the existing fiber optic system is over 25 years old and in need of upgrades and repairs.

## Project Description

Over the past 25 years fiber optic cable has been installed with various projects and through various funding sources to develop an extensive network throughout the City. The network now has a loop around the City and most of the major facilities are connected, but several locations remain unserved. This project will provide dedicated funding for fiber to key unserved areas, replacement of outdated facilities and equipment and upgrades to the fiber optic network where needed. Specific projects are identified annually.

## Cost

\$1,300,000

## Construction Timeline

Ongoing

## Vision 2040 Themes

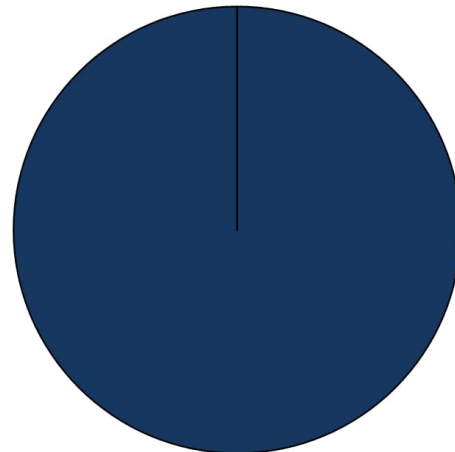
- Thriving Economy

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment
- Sustainable Policies & Practices



Capital Improvement Fund  
\$1,300,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$300,000	\$200,000	\$200,000	\$200,000	\$200,000	\$200,000	\$1,300,000
<b>Total</b>	<b>\$300,000</b>	<b>\$200,000</b>	<b>\$200,000</b>	<b>\$200,000</b>	<b>\$200,000</b>	<b>\$200,000</b>	<b>\$1,300,000</b>



# IT INFRASTRUCTURE PROGRAM

## Project Purpose

A robust IT infrastructure supports the City’s commitment to delivering efficient and reliable services to its employees and residents. Maintaining our “foundational” IT infrastructure is critical to providing transparent access and availability to all city data, applications and services. It is critical to maintain and replace these systems when these systems have exceeded their useful life, limiting any downtime due to hardware failure or risking unsupported security vulnerabilities.

## Project Description

The project focuses on server infrastructure with improved security and more efficient disaster recovery capabilities. Ongoing funding for upgrades and/or replacement network and wireless infrastructure, data storage, and cybersecurity enhancements. Specific projects will be identified annually.

## Cost

\$1,941,250

## Construction Timeline

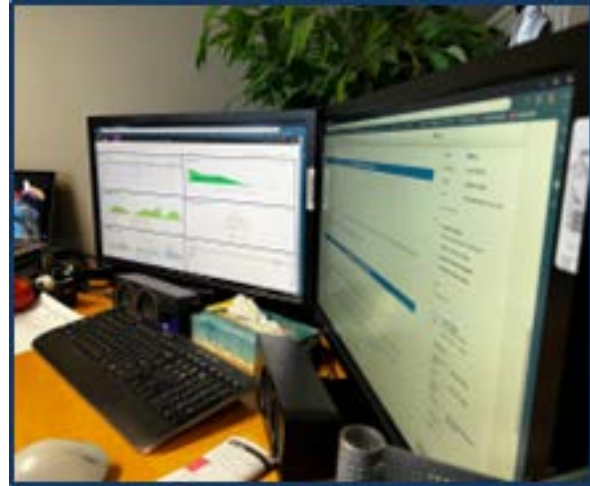
2024 - 2028

## Vision 2040 Themes

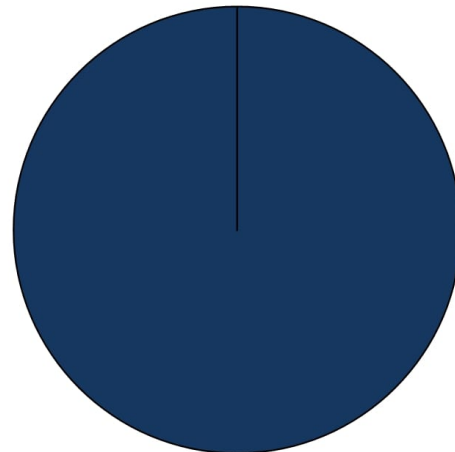
- Integrated Infrastructure & Transportation

## Guiding Principles Supported

- Superior Quality Services
- Strategic Community Investment



Capital Improvement Fund  
\$1,941,250



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$656,250	\$330,000	\$345,000	\$360,000	\$250,000	\$1,941,250
<b>Total</b>	<b>\$0</b>	<b>\$656,250</b>	<b>\$330,000</b>	<b>\$345,000</b>	<b>\$360,000</b>	<b>\$250,000</b>	<b>\$1,941,250</b>

# SUSTAINABILITY PROGRAM

## Project Purpose

Initiatives across the city to enhance energy efficiency, reduce waste, and promote self-sustainability in municipal operations and systems that align with the Climate Action KC plan endorsed by the Governing Body.

## Project Description

Potential projects include lighting enhancements, HVAC improvements, energy monitoring, alternative energy sources, capital equipment, or energy plans and studies. Specific projects will be identified annually.

## Cost

\$250,000

## Construction Timeline

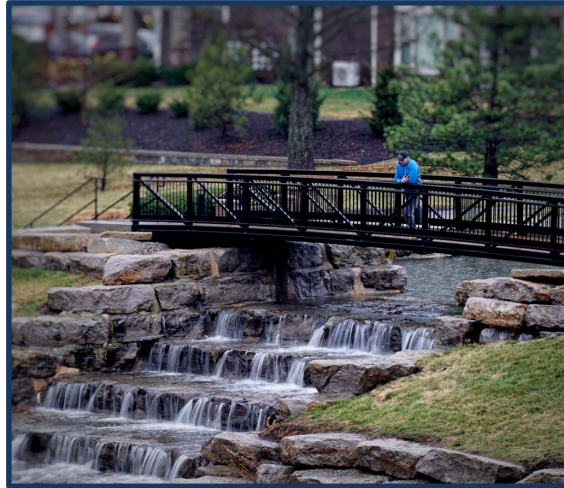
Ongoing

## Vision 2040 Themes

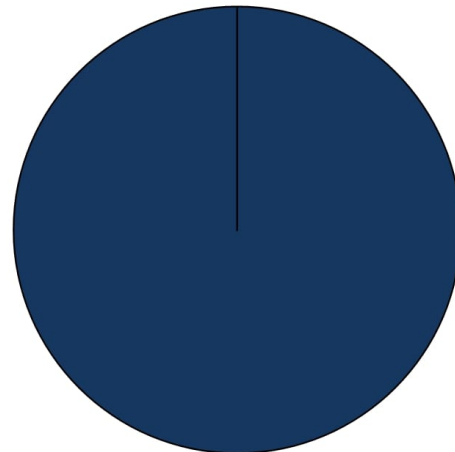
- Integrated Infrastructure & Transportation
- Vibrant Neighborhoods

## Guiding Principles Supported

- Superior Quality Services
- Prudent Financial Management
- Strategic Community Investment
- Sustainable Policies & Practices



Capital Improvement Fund  
\$250,000



Funding Sources							
	Prior Years	2024	2025	2026	2027	2028	Total
Capital Improvement Fund	\$0	\$50,000	\$50,000	\$50,000	\$50,000	\$50,000	\$250,000
<b>Total</b>	<b>\$0</b>	<b>\$50,000</b>	<b>\$50,000</b>	<b>\$50,000</b>	<b>\$50,000</b>	<b>\$50,000</b>	<b>\$250,000</b>

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## CALL TO ORDER

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Chairman Poss called the regular meeting of the Lenexa Planning Commission to order at 7:13 p.m. on Monday, October 2, 2023. The meeting was held in the Community Forum at Lenexa City Hall at 17101 W. 87<sup>th</sup> Street Parkway, Lenexa, Kansas.

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## ROLL CALL

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### COMMISSIONERS PRESENT

Chairman Chris Poss  
Vice-Chairman Mike Burson  
Commissioner Ben Harber  
Commissioner Don Horine  
Commissioner David Woolf  
Commissioner John Handley  
Commissioner Brenda Macke  
Commissioner Cara Wagner  
Commissioner Curt Katterhenry

### COMMISSIONERS ABSENT

None

### STAFF PRESENT

Scott McCullough, Director of Community Development  
Stephanie Kisler, Planning Manager  
Tim Collins, Engineering Construction Services Administrator  
Andrew Diekemper, Assistant Chief – Fire Prevention  
Steven Shroul, Assistant City Attorney  
Dave Dalecky, Planner II  
Kim Portillo, Planner III  
Will Sharp, Planning Intern  
Gloria Lambert, Senior Administrative Assistant

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## APPROVAL OF MINUTES

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The minutes of the August 28, 2023 meeting were presented for approval. Chairman Poss entertained a motion to **APPROVE** the minutes. Moved by Commissioner Horine, seconded by Commissioner Macke, and **APPROVED** by a unanimous voice vote.

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## CONSENT AGENDA

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1. **Brookhollow East, 4th Plat - Consideration of a final plat to combine two platted lots into a single lot in the Brookhollow East Business Park located at 8500 Marshall Drive within the CP-4, Planned Service Commercial District. PT23-20F**
2. **Canyon Creek Apartment Homes - Consideration of a final plat for a multi-family residential development located near the northeast corner of Canyon Creek Boulevard and K-10 Highway within the RP-2, Planned Residential (Intermediate Density) and RP-4, Planned Residential (High Density) Districts. PT23-23F**
3. **Falcon Farm, 1st Plat - Consideration of a final plat for a single-family subdivision located near the northwest corner of 101st Street and Lone Elm Road within the R-1, Single-Family Residential District. PT23-22F**
4. **Stoneridge North, First Plat - Consideration of a final plat for a two-family (duplex) residential subdivision located near the northeast corner of West 83rd Street and Cedar Niles Road within the RP-2, Planned Residential (Intermediate Density) District. PT23-19F**
5. **Westside Family Church Care Center - Consideration of a final plan and final plat for a two-story office building accessory to the church/place of worship located at 8500 Woodsonia Drive within the R-1, Single-Family Residential District. PL23-18F & PT23-21F**
6. **Kiewit K3 Building Sign - Consideration of a final plan for installation of a sign on the penthouse structure of the future Kiewit K3 Building located at the southwest corner of 89th Street and Hampton Street within the CC, Planned City Center District. PL23-19F**

Commissioner Wagner declared a conflict of interest regarding item no. 6 stating she works for the applicant.

Chairman Poss entertained a motion to **APPROVE** Consent Agenda Items 1-5. Moved by Commissioner Handley, seconded by Commissioner Macke, and carried by a unanimous voice vote.

Chairman Poss entertained a motion to **APPROVE** Consent Agenda Item 6. Moved by Commissioner Woolf, seconded by Commissioner Macke, and carried by a vote of 8-0-1, with Commissioner Wagner abstaining.

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## REGULAR AGENDA

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1. **Viscek Estates - Consideration of a rezoning and preliminary plan/plat to split a parcel into a single-family residential lot and an undeveloped lot for property located at 25925 West 83rd Street. The proposed rezoning from AG, Agricultural to RP-1, Planned Residential (Low-Density) Districts affects only the single-family residential lot. RZ23-06 & PT23-05P**

## APPLICANT PRESENTATION

Steve Deghand, a representative of the DeSoto School District presented on behalf of the applicant. He said Unified School District (USD) 232 intends to purchase the land for a future elementary school but would not begin development of the project for the next three to five years. The DeSoto School District will purchase the entire 40 acres of property but will sell two-acres back to the property owner that would like to remain in their home.

## STAFF PRESENTATION

Kim Portillo presented the Staff Report. She explained that Viscek Estates is currently 40-acres of property that DeSoto School District (USD 232) would like to split into two properties. One part would become a 35-acre agricultural lot and the second, a two-acre residential lot. The remainder of the land will be dedicated as rights-of-way to the City of Lenexa. She showed a graphic and pointed out the residential growth in that area, noting project applications that have been approved or currently going through the Planning Commission process. The property is currently zoned AG, Agricultural District with a Future Land Use classification of Suburban Residential. Both are in line with the applicant's proposal. The two-acres rezoned from Agricultural to RP-1, Planned Residential (Low Density) District is being rezoned because the AG District has a minimum lot size of 20-acres. She showed the criteria staff used for evaluating the rezoning application and explained they were outlined in greater detail in staff's report. Staff feels that the applicant has met all 13 criteria for the rezoning request. There will be a dedicated trailway through the subject property of Lot 2. Staff required the applicant to provide a trail easement that will be dedicated at time of final plat. She reviewed the access points for the residential lot to arterial roads and as the project goes forth and new development comes in, some access points may change. She noted all the right-of-way designations and roadway connections involved with the future of the project.

## PUBLIC HEARING

Chairman Poss **OPENED** the Public Hearing and asked if anyone wished to speak on this item.

Mike Jadud, who resides at 26580 West 90<sup>th</sup> Street, asked what will happen with the right-of-way in Cedar Niles with the coming project. He was concerned about traffic flow when the property becomes a school. He also mentioned the widening of the road at the southwest corner of Cedar Niles Road related to the Stoneridge subdivision to the west that has made the intersection dangerous.

Chairman Poss entertained a motion to **CLOSE** the Public Hearing. Moved by Commissioner Burson, seconded by Commissioner Katterhenry, and carried by a unanimous voice vote.

## PLANNING COMMISSION DISCUSSION

Chairman Poss referred to staff member Tim Collins to address the resident's questions.

Mr. Collins responded that no traffic studies have been conducted or site layouts provided from the school district to make any comments concerning the project. Further information will come with the submittal of the preliminary

and final plan that is projected within the next three to five years. He offered to meet with the gentleman at the conclusion of the meeting to discuss the issue concerning Cedar Niles Road.

Stephanie Kisler clarified that there will be proper public notification that will include a sign posted on the property, a legal notice in the newspaper and mailings to surrounding neighbors within 200 feet of the subject property at the time of any future rezoning request.

Chairman Poss said it was a pretty straight forward application and he did not have anything to add.

## MOTION

Chairman Poss entertained a motion to recommend **APPROVAL** of rezoning the 2-acre proposed Lot 1 from AG to RP-1 for RZ23-06 – **Viscek Estates** at 25925 W. 83<sup>rd</sup> Street, for a single-family home.

Moved by Commissioner Harber, seconded by Commissioner Macke, and carried by a unanimous voice vote.

Chairman Poss entertained a motion to recommend **APPROVAL** of the preliminary plan/plat for PT23-05P – **Viscek Estates** at 25925 W. 83<sup>rd</sup> Street, for a single-family home and agricultural uses.

Moved by Commissioner Handley, seconded by Commissioner Burson, and carried by a unanimous voice vote.

## STAFF REPORT

Scott McCullough updated the commissioners on the progress of the Comprehensive Plan.

Stephanie Kisler announced that Planning has developed a new more efficient and user-friendly public hearing sign that will take the place of the three signs previously used.

## ADJOURNMENT

Chairman Poss ended the regular meeting of the Lenexa Planning Commission at 7:31 p.m. on Monday, October 2, 2023.