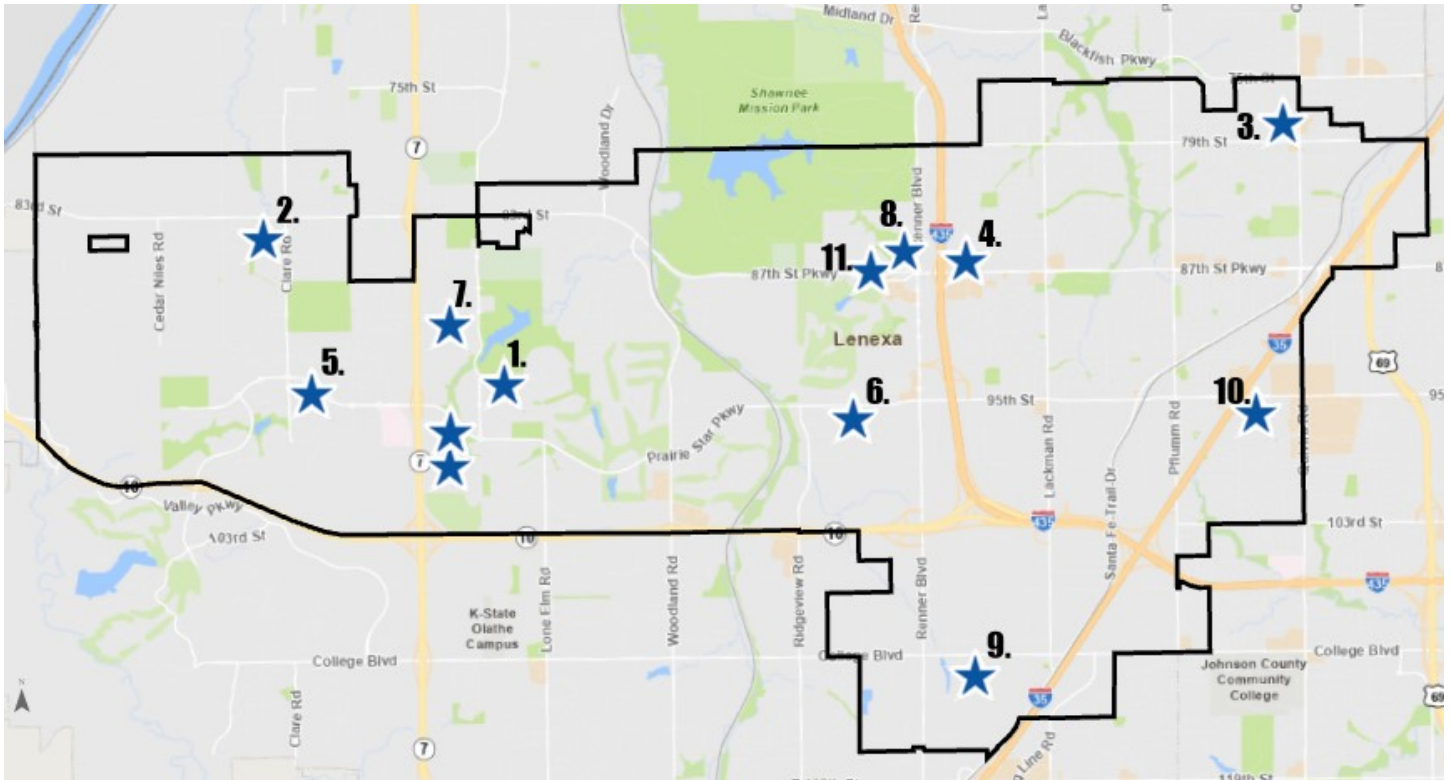


**AGENDA MAP**



**CALL TO ORDER**

**ROLL CALL**

**APPROVE MINUTES**

**CONSENT AGENDA**

*All matters listed within the consent agenda have been distributed to each member of the Planning Commission for review, are considered to be routine, and will be enacted by one motion with no separate discussion. If a member of the Planning Commission or audience desires separate discussion on an item, that item may be removed from the consent agenda and placed on the regular agenda.*

- 1. Brampton West Tracts - Consideration of a final plat for property located near the intersection of W. 95th Street & Aurora Street within the R-1, Single-Family Residential District. PT23-06F**
- 2. Clear Creek Landing - Consideration of a final plat for property located at the southwest corner of W. 83rd Street and Clare Road within the RP-2, Planned Residential (Intermediate Density) District. PT23-10F**

3. **Quivira Square - Consideration of a final plan for facade, site, and parking improvements for property located at 7820 Quivira Road within the CP-1, Planned Neighborhood Commercial District. PL22-24F**
4. **Lenexa Smart Storage - Consideration of a final plan a self-service storage use for property located at 8630 Maurer Road within the CP-2, Planned Community Commercial District. PL23-04F**
5. **St. James Academy Multi-Purpose Building - Consideration of a revised final plan for an accessory structure related to the existing private high school located at 24505 Prairie Star Parkway within the R-1, Single-Family Residential District. PL23-02FR**
6. **Vista Village, 2nd Plat - Consideration of a revised final plat for property located at the southwest corner of Prairie Star Parkway and Vahalla Street within the PUD, Planned Unit Development District. PT23-01FR**
7. **Watercrest Landing, Seventh Plat - Consideration of a final plat for property located at the northwest corner of W. 89th Street and Ginger Street within the RP-1, Planned Residential (Low Density) District. PT23-09F**
8. **Redevelopment Project Plan 3H for the City Center TIF District - Consideration of a resolution for property located at the northwest corner of 87th Street Parkway and Renner Boulevard within the CC, Planned City Center District.**

## REGULAR AGENDA

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9. **Auto Driveway Franchise Systems - Consideration of a special use permit for a commercial parking lot use for property located at 16105 W. 113th Street within BP-2, Planned Manufacturing District. SU23-02**
10. **StorTropolis - Consideration of a preliminary plan and special use permit for a self-service storage use for property located at the southwest corner of 96th Terrace and Rosehill Road within the CP-2, Planned Community Commercial District. PL23-01P & SU23-01**
11. **Midas Lenexa City Center - Consideration of a preliminary plan for a mixed-use development with hotel and retail uses for property located between Elmridge Street and Penrose Lane and the east and west lanes of W. 87th Street Parkway within the CC, Planned City Center District. PL23-02P**

**CONTINUED APPLICATIONS (NO DISCUSSION)**

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- > **Prairie Chase I (fka Blackhoof Reserve I) -**  
Consideration of a conceptual plan for a mixed-use development and rezoning property from the CP-3, Planned Regional Commercial, Zoning District and unzoned former public right-of-way to the CP-1, Planned Neighborhood Commercial, RP-2, Planned Residential (Intermediate-Density), and RP-4, Planned Residential (High-Density) Zoning Districts on property located near the southeast corner of Prairie Star Parkway and Monticello Terrace. RZ23-01 & PL23-01CP
  
- > **Prairie Chase II (fka Blackhoof Reserve II) -**  
Consideration of a conceptual plan for a multi-family residential development and rezoning property from the AG, Agricultural, Zoning District to the RP-3, Planned Residential (Medium High-Density) and RP-4, Planned Residential (High-Density), Zoning Districts on property located near the southeast corner of Prairie Star Parkway and Monticello Terrace. RZ23-02 & PL23-02CP

**STAFF REPORTS**

**ADJOURN**

**APPENDIX**

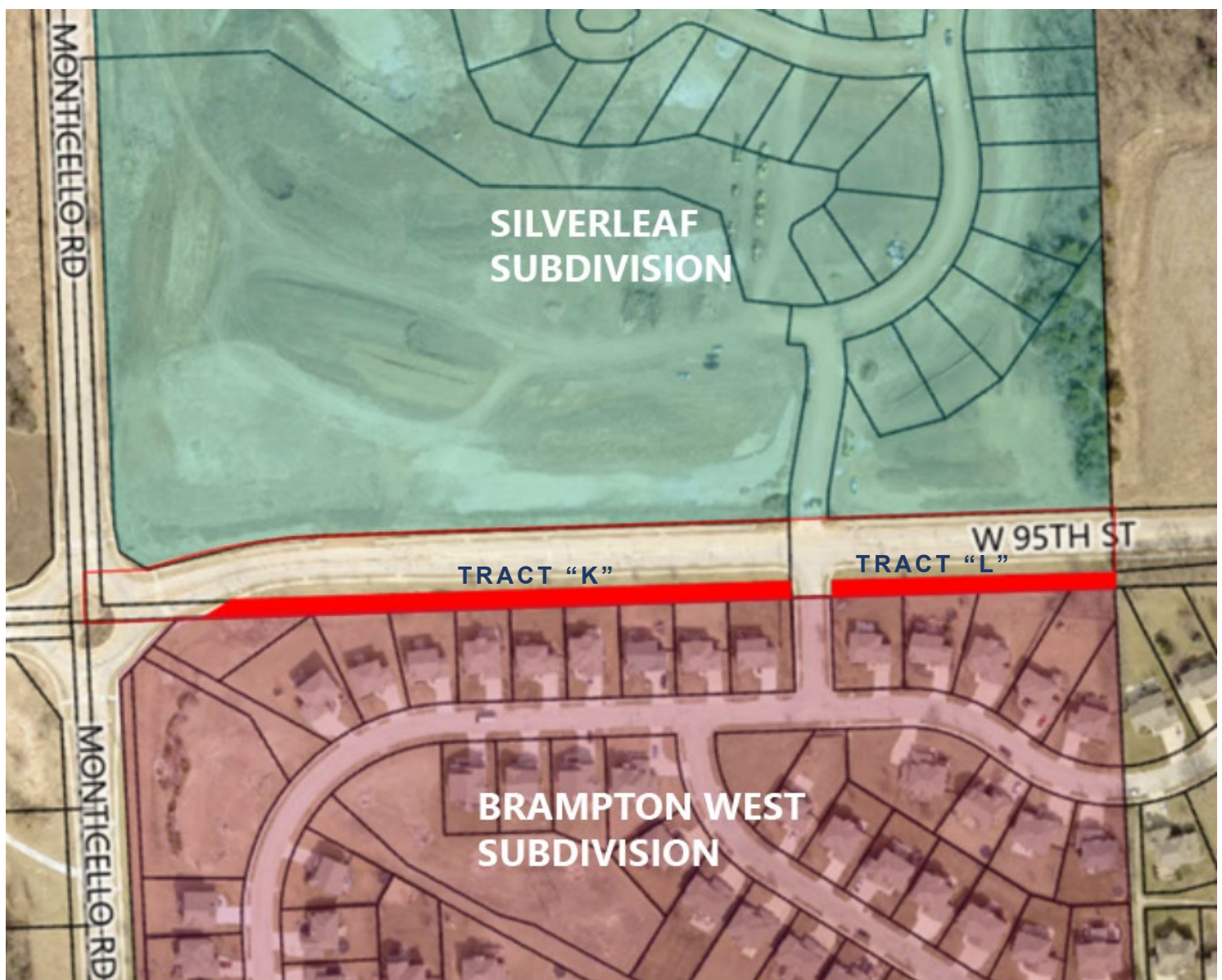
*If you have any questions about this agenda, please contact Stephanie Kisler, Planning and Development Administrator, at [skisler@lenexa.com](mailto:skisler@lenexa.com).*

*If you need any accommodations for the meeting, please contact the City ADA Coordinator at 913-477-7550 at least 48 hours prior to the meeting.  
Kansas Relay Service: 800-766-3777*

## SITE INFORMATION

Brampton West is a 50-lot single-family subdivision approved by the City in 2005. At the time of approval, the City did not require a landscape buffer to the north of the lots on 95<sup>th</sup> Terrace in anticipation of the potential future construction of 95<sup>th</sup> Street. Brampton West was not adjacent to the 95<sup>th</sup> Street right-of-way; a silver of privately-owned land remained between 95<sup>th</sup> Street and the lots along 95<sup>th</sup> Terrace in Brampton West (see Exhibit 1).

<b>LAND AREA (AC)</b> 2.895	<b>BUILDING AREA (SF)</b> N/A	<b>ZONING</b> R-1	<b>COMP. PLAN</b> N/A
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*Exhibit 1: Aerial Image of Subject Area.*

Silverleaf subdivision is located across 95<sup>th</sup> Street to the north of Brampton West. Silverleaf’s preliminary plat was approved in September 2020 for 55 lots and 5 tracts including a mix of single-family and two-family lots. Silverleaf, First Plat was approved in April 2021. This plat contained 62 lots, replacing the 12 two-family lots approved with the preliminary plan with 19 single-family lots along 94<sup>th</sup> Street.

Silverleaf, First Plat included the dedication of a 20-foot x 364-foot area (in red below) as a portion of 95<sup>th</sup> Street right-of-way, abutting the rear property lines of Brampton West to the south. Neither the City nor the utility companies needed the dedication of 95<sup>th</sup> Street right-of-way to extend this width; however, the plat was accepted by staff in error. The subject area should have instead been platted as a tract associated with Silverleaf, First Plat. On December 20, 2022, this area was vacated so that it could be platted as one of two tracts for the purpose of landscaping.

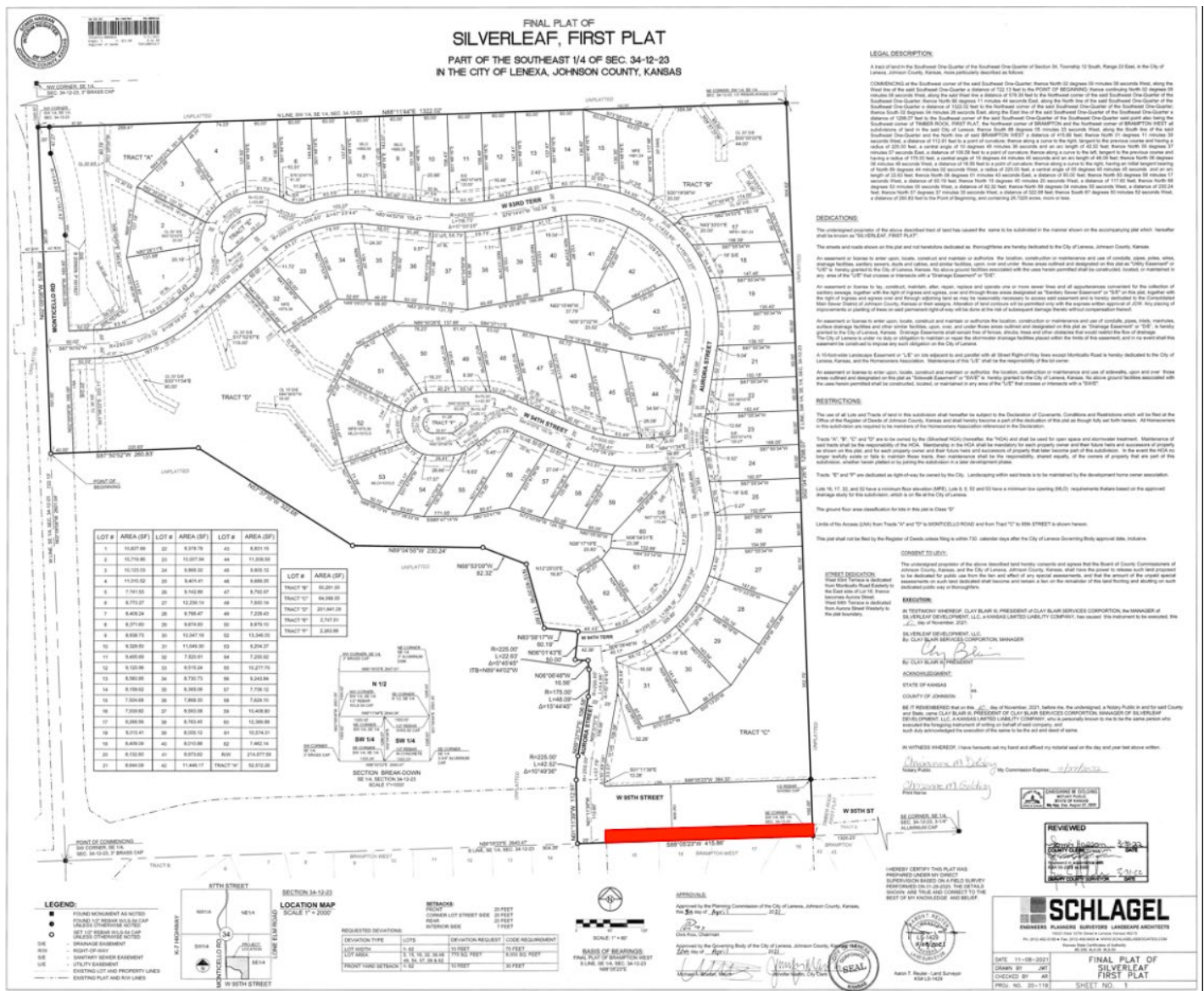
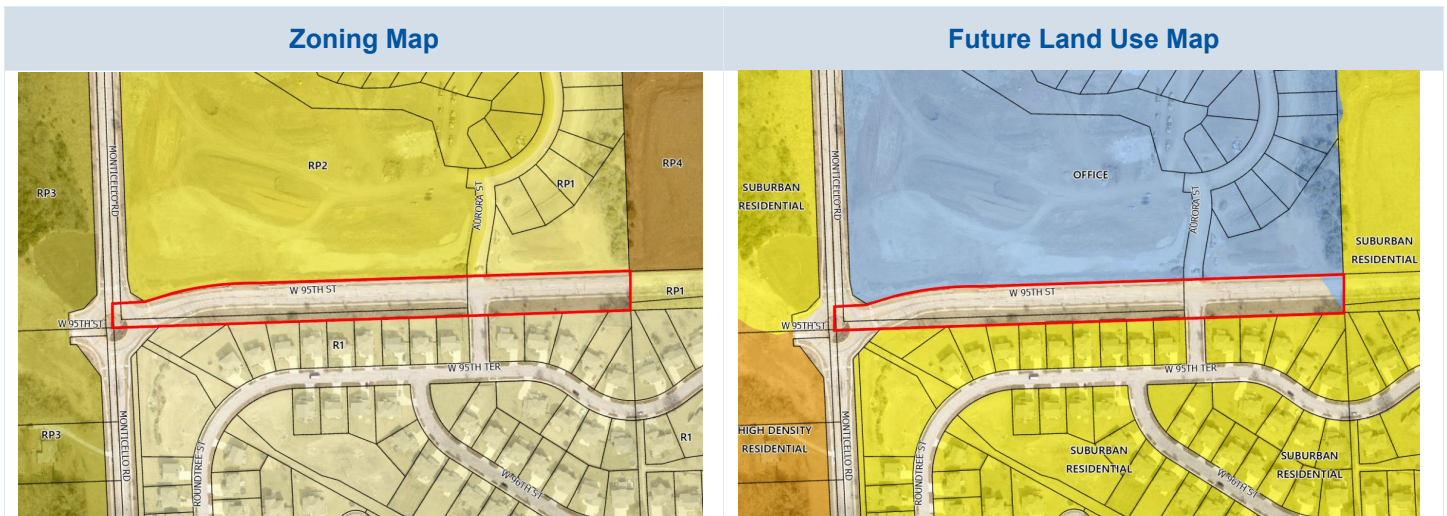


Exhibit 2: Silverleaf, First Plat and Right-of-Way Area (Vacated December 2022).

## LAND USE REVIEW

The tracts proposed with this plat will be landscape buffers for the Brampton West single-family residential subdivision and will be maintained by the Brampton West Homeowner’s Association. The use is compatible with surrounding residential lots and is consistent with the intent of the Comprehensive Plan.

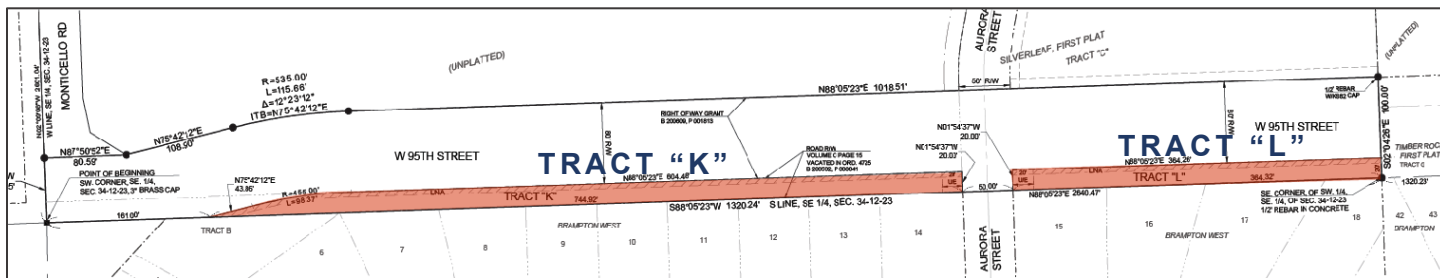


**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Suburban Residential	R-1 Residential Low Density	Right-of-Way & Unplatted Land
<b>North</b>	Office	RP-1 Planned Residential Low Density & RP-2 Planned Residential Intermediate Density	Undeveloped Future Single-Family Residential
<b>South</b>	Suburban Residential	R-1 Residential Low Density	Single-Family Residential
<b>East</b>	Suburban Residential	RP-1 Planned Residential Low Density	Right-of-Way
<b>West</b>	Suburban Residential	RP-3 Planned Residential Medium High Density	Undeveloped Future Townhomes

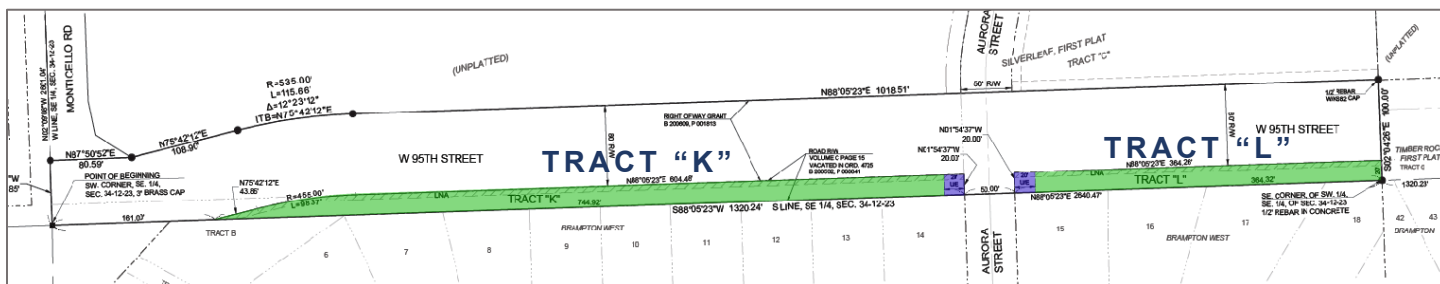
## FINAL PLAT REVIEW

This is a replat of the vacated portion of right-of-way from Silverleaf, First Plat together with right-of-way and unplatted land, containing 2.895 acres. The plat contains two tracts known as Tract “K” and Tract “L” to be dedicated as a landscape easement. Maintenance of the landscape easements shall be the responsibility of the Brampton West Homes Association. In the event the HOA no longer lawfully exists or fails to maintain these tracts, then maintenance shall be the responsibility, shared equally, of the owners of property that are part of the Brampton West subdivision. The purpose of the landscape easement is for Brampton West Subdivision to provide a landscape buffer between the subdivision and 95<sup>th</sup> Street.

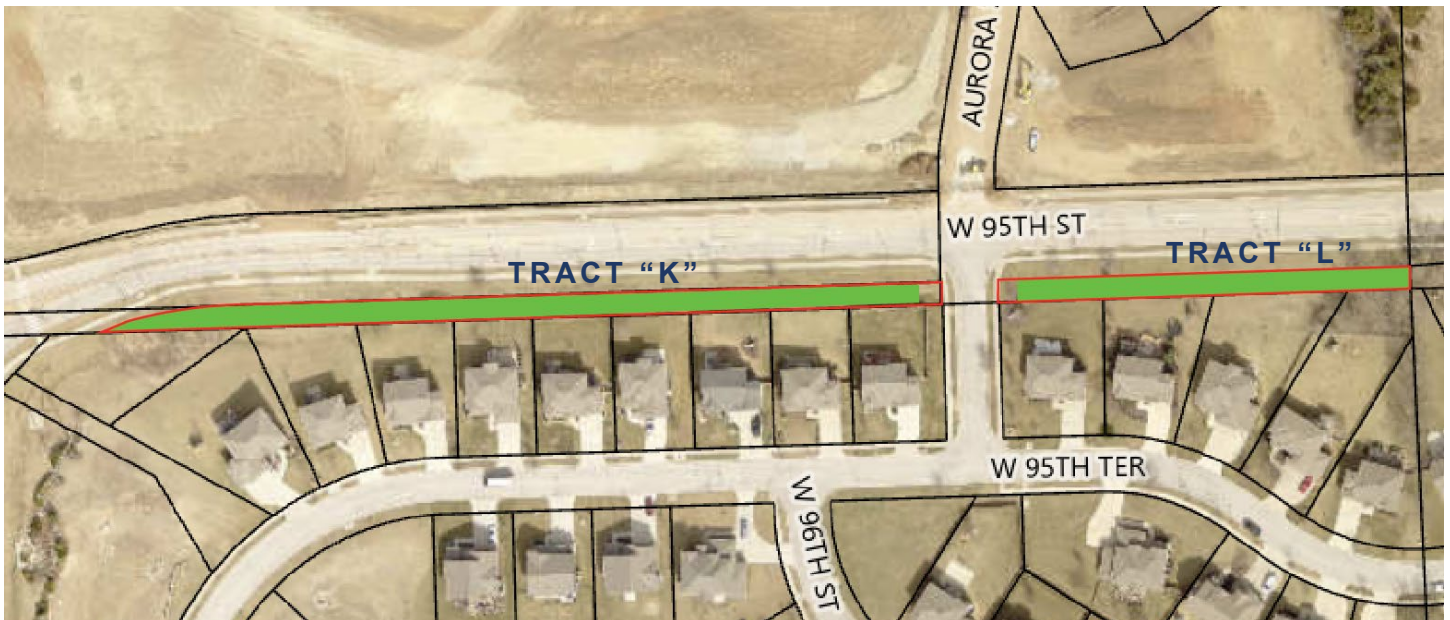


**Exhibit 3: Landscape Buffer Tracts: Tract K (left) and Tract L (right).**

The plat also contains Limits of No Access to prohibit access to the tracts from 95<sup>th</sup> Street and dedications for utility easements and right-of-way. Exhibit 4 shows two 20-foot utility easements (in blue) at the ends of Tracts K and L on either side of Aurora Street, to be dedicated to the City of Lenexa. Landscape plantings are not typically allowed within utility easements. Therefore, the remaining area of the tracts outside of the utility easements are where the landscape plantings will be planted (shown in green). This planting envelope area outside of any the utility easements is 20-feet wide.



**Exhibit 4: Utility Easements (blue) and Remaining Tract Areas Available for Landscape Plantings (green).**



*Exhibit 5: Tract Areas Available for Landscape Plantings (green).*

Exhibit 5 shows Tracts "K" and "L" outlined in red. The planting envelope area outside of any the utility easements is shown in green.

## DEVIATIONS

The applicant is not requesting any deviations from the Unified Development Code.

## REVIEW PROCESS

- The Planning Commission is the final authority for approval of this plat, pending Governing Body acceptance of right-of-way and easements for landscaping and utilities as shown on the final plat, which will be on the agenda for the March 21, 2023 City Council meeting.
- The applicant should inquire about additional City requirements, such as permits and development fees.



## RECOMMENDATION FROM PROFESSIONAL STAFF

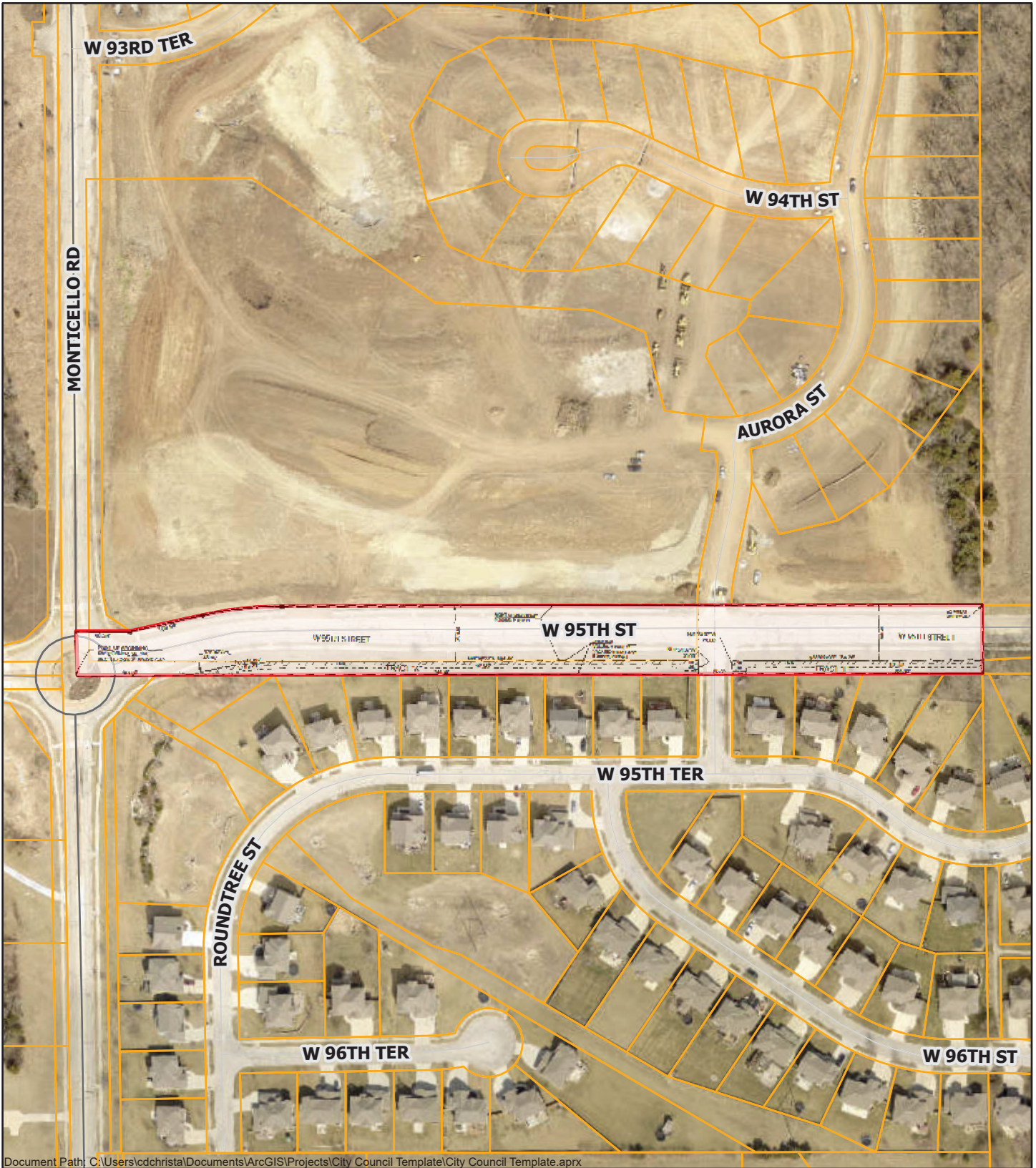
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★ **Staff recommends approval of the proposed Final Plat for Brampton West Tracts.**

- This is a replat of Silverleaf, First Plat and a portion of the vacated 95<sup>th</sup> Street resulting in two tracts for the purpose of buffering and landscaping between 95<sup>th</sup> Street and single-family residences.
- The project is consistent with Lenexa's goals through ***Strategic Community Investment*** and ***Responsible Economic Development*** to create ***Vibrant Neighborhoods***.

### FINAL PLAT

Staff recommends **approval** of the final plat for PT23-06F – **Brampton West Tracts** at 95<sup>th</sup> Street and Aurora Street, for two tracts.



Document Path: C:\Users\cdchrista\Documents\ArcGIS\Projects\City Council Template\City Council Template.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

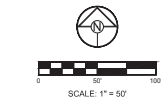
# Brampton West Tracts, Final Plat

## PT23-06F

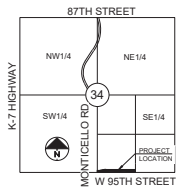


**FINAL PLAT OF  
BRAMPTON WEST TRACTS**  
A REPLAT OF ALL OF 95TH STREET RIGHT OF WAY AS RECORDED IN SILVERLEAF, FIRST  
PLAT AND PART OF THE SOUTHEAST 1/4 OF SEC. 34-12-23  
IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS

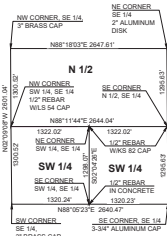
- LEGEND:**
- FOUND MONUMENT AS NOTED
  - FOUND 1/2" REBAR W/LS-54 CAP UNLESS OTHERWISE NOTED
  - SET 1/2" REBAR W/LS-54 CAP UNLESS OTHERWISE NOTED
  - D/E DRAINAGE EASEMENT
  - R/W RIGHT-OF-WAY
  - S/S SANITARY SEWER EASEMENT
  - U/E UTILITY EASEMENT
  - EXISTING LOT AND PROPERTY LINES
  - - - EXISTING PLAT AND ROW LINES
  - LNA LIMITS OF NO ACCESS



**BASIS OF BEARINGS:**  
FINAL PLAT OF BRAMPTON WEST  
S LINE, SE 1/4, SEC. 34-12-23  
N88°05'23"E



**SECTION 34-12-23  
LOCATION MAP**  
SCALE 1" = 2000'



**SECTION BREAK-DOWN**  
SE 1/4, SECTION 34-12-23  
SCALE 1" = 1000'

**DESCRIPTION:**

A replat of part of All of 95th Street right of way as platted in SILVERLEAF, FIRST PLAT, a subdivision of land as recorded in Book 202203 at Page 008810 and part of the Southwest One-Quarter of the Southeast One-Quarter of Section 34, Township 12 South, Range 23 East, All in the City of Lenexa, Johnson County, Kansas, and altogether being described as follows:

Beginning at the Southwest corner of the said Southeast One-Quarter; thence along the West line of said Southeast One-Quarter North 02 degrees 09 minutes 08 seconds West, a distance of 63.85 feet; thence North 87 degrees 50 minutes 52 seconds East a distance of 80.59 feet to a point on the Northern right of way line of West 95th Street as now established by the right of way grant as recorded in book 200909 at page 001813; thence along said Northern right of way line the following three courses, North 75 degrees 42 minutes 12 seconds East a distance of 108.50 feet to a point of curvature; thence North easterly on a curve to the right being tangent to the previous course, having a radius of 535.00 feet, a central angle of 12 degrees 23 minutes 12 seconds and an arc length of 115.66 feet; thence North 88 degrees 05 minutes 23 seconds East a distance of 1018.51 feet to the Northeast corner of said West 95th Street right of way as recorded in book 200909 at page 001813; said corner being the Southeast corner of Tract "C", of said SILVERLEAF, FIRST PLAT; and said corner being on the East line of the said Southwest One-Quarter of the Southeast One-Quarter of Section 34; thence along said East line South 02 degrees 04 minutes 26 seconds East a distance of 100.00 feet to the Southeast corner of the said Southwest One-Quarter of the Southeast One-Quarter of Section 34; said point also being the Southwest corner of TIMBER ROCK, FIRST PLAT; the Northwest corner of BRAMPTON and the Northeast corner of BRAMPTON WEST all subdivisions of land in the said City of Lenexa; thence along the South line of the said Southwest One-Quarter of the Southeast One-Quarter of Section 34 and the North line of said BRAMPTON WEST, South 88 degrees 05 minutes 23 seconds West a distance of 1320.24 feet to the Point of Beginning, and containing 2.885 acres, more or less.

**DEDICATIONS:**

The undersigned proprietor of the above-described tract of land has caused the same to be subdivided in the manner shown on the accompanying plat which hereafter shall be known as "BRAMPTON WEST TRACTS". The streets and roads shown on this plat and not heretofore dedicated as thoroughfares are hereby dedicated to the City of Lenexa, Johnson County, Kansas.

An easement or license to enter upon, locate, construct and maintain, or authorize the location, construction or maintenance and use of conduits, pipes, poles, wires, drainage facilities, sanitary sewers, ducts and cables, and similar facilities, upon, over and under those areas outlined and designated on this plat as "Utility Easement" or "UE" is hereby granted to the City of Lenexa, Kansas.

Tracts "K" and "L" will be owned and maintained by the Brampton West Homes Association. These Tracts are intended to be used for landscaping and utility easements.

A Landscape Easement or "LE" over Tracts "K" and "L" is hereby dedicated to the City of Lenexa, Johnson County, Kansas. Maintenance of said easement shall be the responsibility of the Brampton West Homes Association, with access granted to the City of Lenexa. In the event the HOA no longer lawfully exists or fails to maintain these tracts, then maintenance shall be the responsibility, shared equally, of the owners of property that are part of the Brampton West subdivision.

**RESTRICTIONS:**

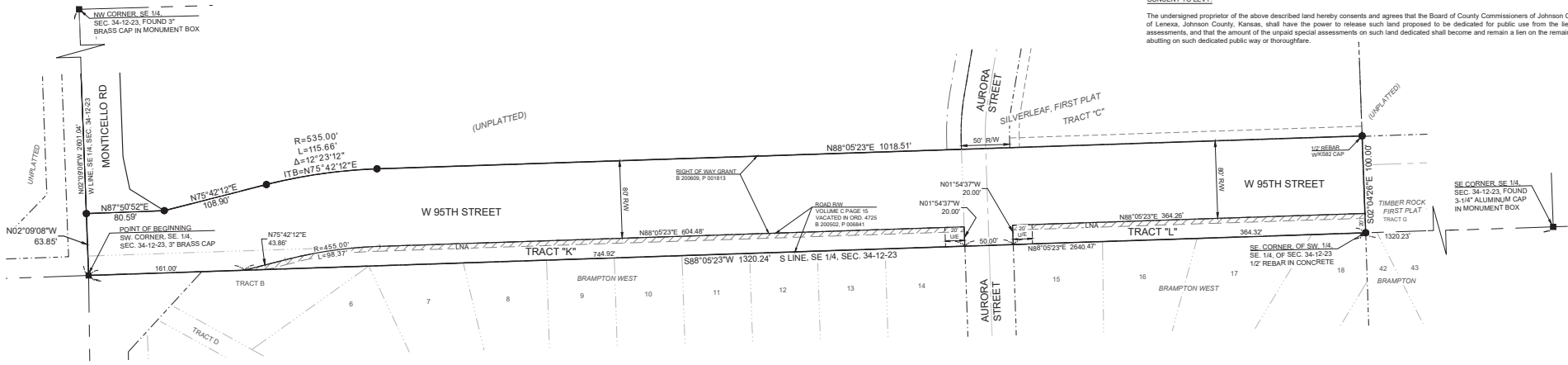
The use of all Tracts of land in this subdivision shall hereafter be subject to the Declaration of Brampton West Deed Restrictions filed at the Office of the Register of Deeds of Johnson County, Kansas on May 28, 2010 in book 201005 at page 008140 as instrument No. 7201002221 and shall hereby become a part of the dedication of this plat as though fully set forth herein. All property owners in this subdivision are required to be members of the Homeowners Association referenced in the Declaration.

Limits of No Access (LNA) from Tracts "K" and "L" to 95th STREET as shown hereon.

This plat shall not be filed by the Register of Deeds unless filing is within 730 calendar days after the City of Lenexa Governing Body approval date, inclusive.

**CONSENT TO LEVY:**

The undersigned proprietor of the above described land hereby consents and agrees that the Board of County Commissioners of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to release such land proposed to be dedicated for public use from the lien and effect of any special assessments, and that the amount of the unpaid special assessments on such land dedicated shall become and remain a lien on the remainder of this land fronting and abutting on such dedicated public way or thoroughfare.



LOT #	AREA (SF)
95TH ROW	104,901.17
TRACT "K"	13,900.17
TRACT "L"	7,285.77

**APPROVALS:**

Approved by the Planning Commission of the City of Lenexa, Johnson County, Kansas, this \_\_\_ day of \_\_\_, 2023.

Chris Pess, Chairman

Approved by the Governing Body of the City of Lenexa, Johnson County, Kansas, this \_\_\_ day of \_\_\_, 2023.

Michael A. Boehm, Mayor Jennifer Martin, City Clerk

**EXECUTION:**

IN TESTIMONY WHEREOF, CLAY BLAIR III, PRESIDENT OF CLAY BLAIR SERVICES CORPORATION, the MANAGER OF SILVERLEAF DEVELOPMENT, LLC, a KANSAS LIMITED LIABILITY COMPANY, has caused this instrument to be executed, this \_\_\_ day of \_\_\_, 2023.

SILVERLEAF DEVELOPMENT, LLC,  
By: CLAY BLAIR SERVICES CORPORATION, MANAGER

By: CLAY BLAIR III, PRESIDENT

**ACKNOWLEDGMENT:**

STATE OF KANSAS )  
COUNTY OF JOHNSON ) ss.

BE IT REMEMBERED that on this \_\_\_ day of \_\_\_, 2023, before me, the undersigned, a Notary Public in and for said County and State, came CLAY BLAIR III, PRESIDENT OF CLAY BLAIR SERVICES CORPORATION, MANAGER OF SILVERLEAF DEVELOPMENT, LLC, A KANSAS LIMITED LIABILITY COMPANY, who is personally known to me to be the same person who executed the foregoing instrument of writing on behalf of said company, and such duly acknowledged the execution of the same to be the act and deed of same.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my notarial seal on the day and year last above written.

Notary Public \_\_\_\_\_ My Commission Expires: \_\_\_\_\_

Print Name \_\_\_\_\_



Aaron T. Reuter - Land Surveyor  
KS#LS-1429

I HEREBY CERTIFY THIS PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION BASED ON A FIELD SURVEY PERFORMED ON 01-29-2020 AND 05-10-2022. THE DETAILS SHOWN ARE TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

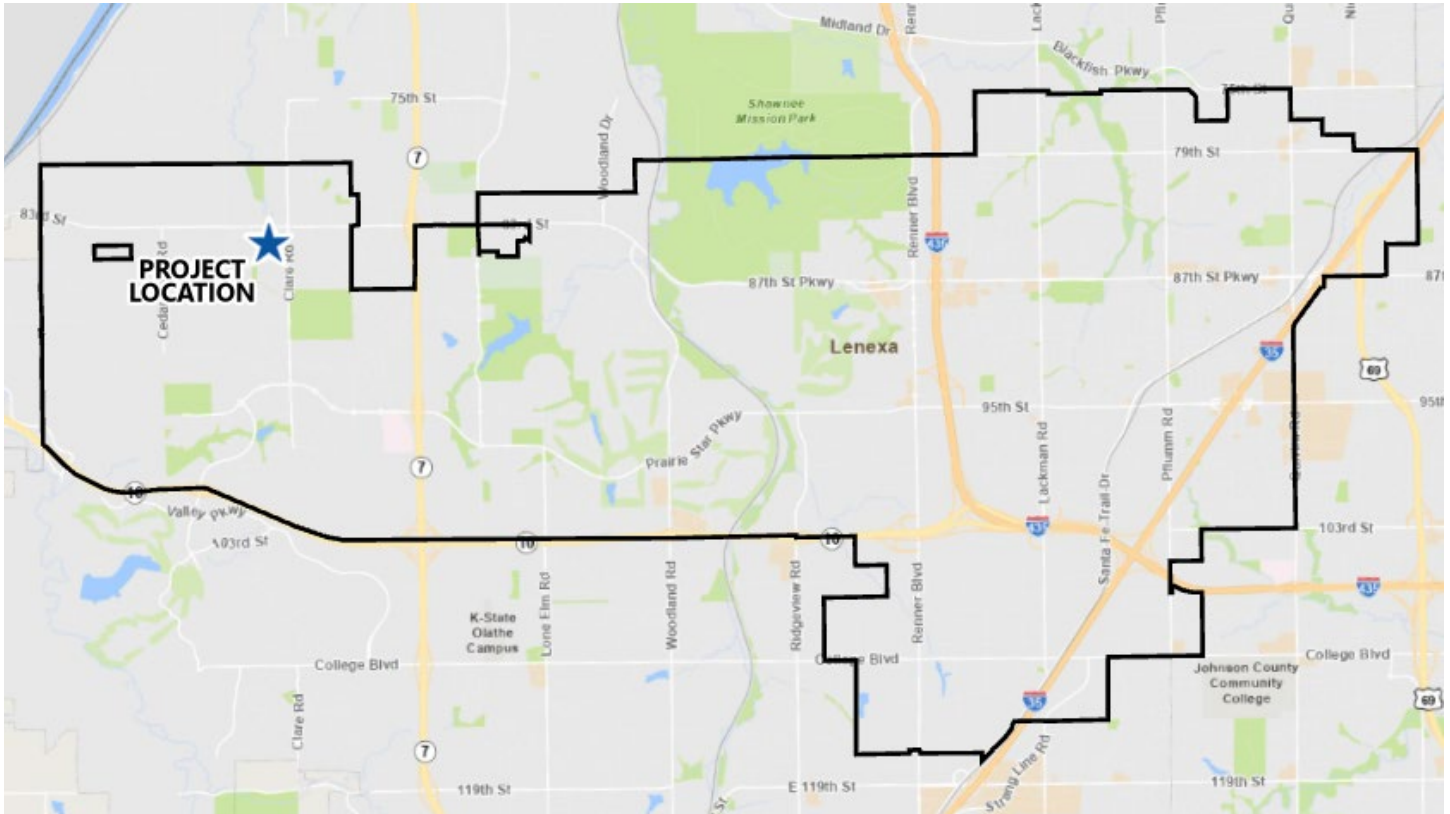
**SCHLAGEL**  
ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS  
1820 West 107th Street • Lenexa, Kansas 66215  
Ph: (913) 492-6158 • Fax: (913) 492-8465 • WWW.SCHLAGELASSOCIATES.COM  
Kansas State Certified of Authority  
REC-208-PL-22-RLS-54

DATE	12-15-2022	FINAL PLAT OF
DRAWN BY	SCH	BRAMPTON WEST TRACTS
CHECKED BY	AR	
PROJ. NO.	22-069	SHEET NO. 1

REV: 1-24-2023  
REV: 1-16-2023

# CLEAR CREEK LANDING, FINAL PLAT

<b>Project #:</b>	PT23-10F	<b>Location:</b>	SWC of 83 <sup>rd</sup> Street and Clare Road
<b>Applicant:</b>	Jeff Skidmore, Schlagel & Associates	<b>Project Type:</b>	Final Plat
<b>Staff Planner:</b>	Christa McGaha, AICP	<b>Proposed Use:</b>	Multi-Family Residential



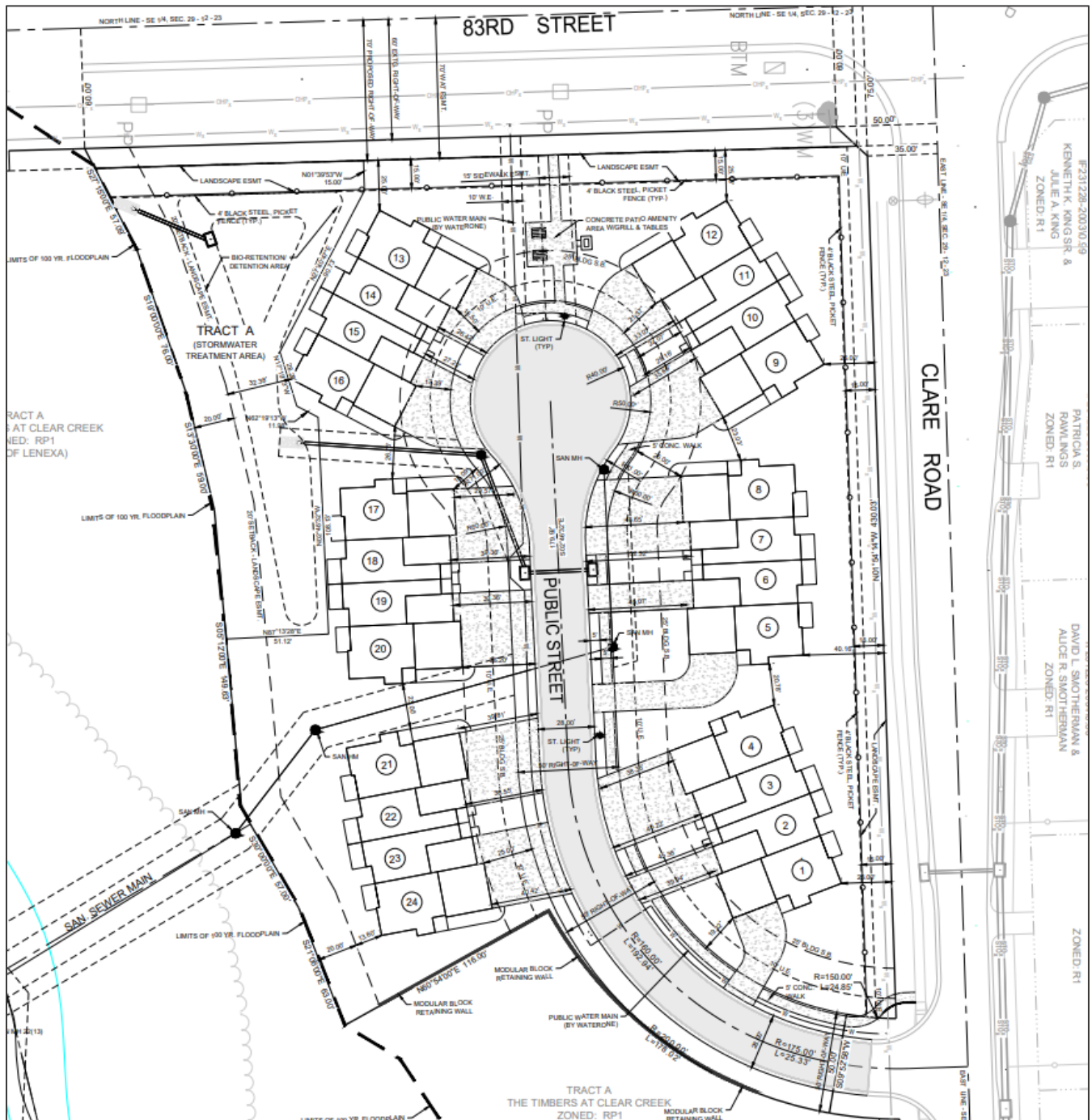
## PROJECT SUMMARY

The applicant proposes a final plat of 3.34 acres at the southwest corner of W. 83<sup>rd</sup> Street and Clare Road. The plat contains one lot and one tract known as Tract “A” to be dedicated as a stormwater management, sidewalk/trail access, and landscaping to be maintained by the Homeowners Association. The final plat includes construction of Greeley Street. Utility easements, landscape easements, sidewalk easements, and public rights-of-way are being dedicated to the City of Lenexa with this final plat. This project does not require a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL**

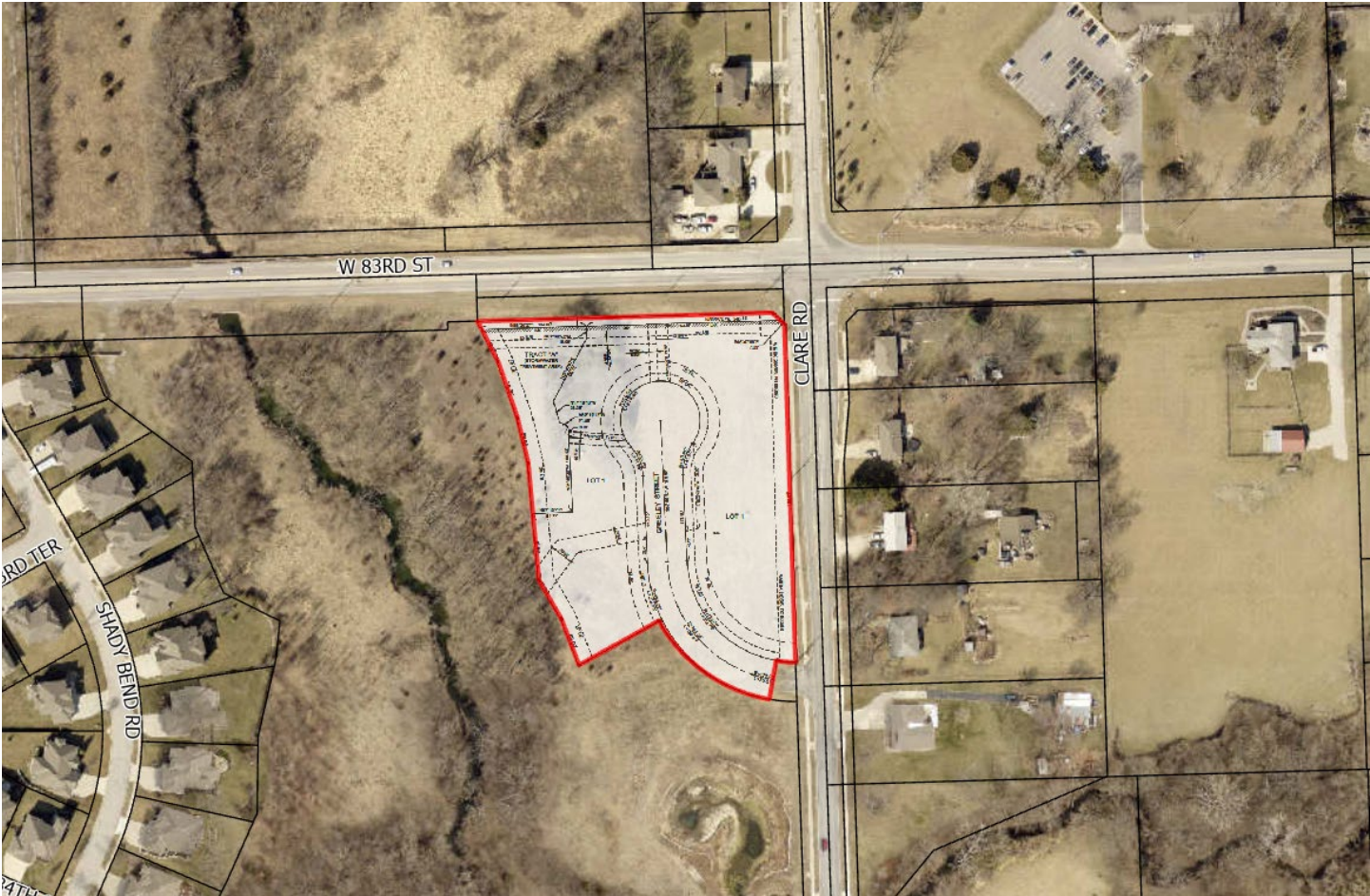
**SITE INFORMATION**

In November 2022, the applicant was granted approval of the preliminary plan and rezoning applications (RZ22-07 & PT22-13P) to rezone the 3.4-acre subject property from RP-1 to RP-2 to allow for a six-building, 24-unit townhome development at the southwest corner of 83<sup>rd</sup> Street and Clare Road.



**Exhibit 1: Approved Preliminary Plan and Plat, PT22-13P.**

<b>LAND AREA (AC)</b> 3.4	<b>DWELLING UNITS</b> 24	<b>ZONING</b> RP-2	<b>COMP. PLAN</b> Suburban Residential
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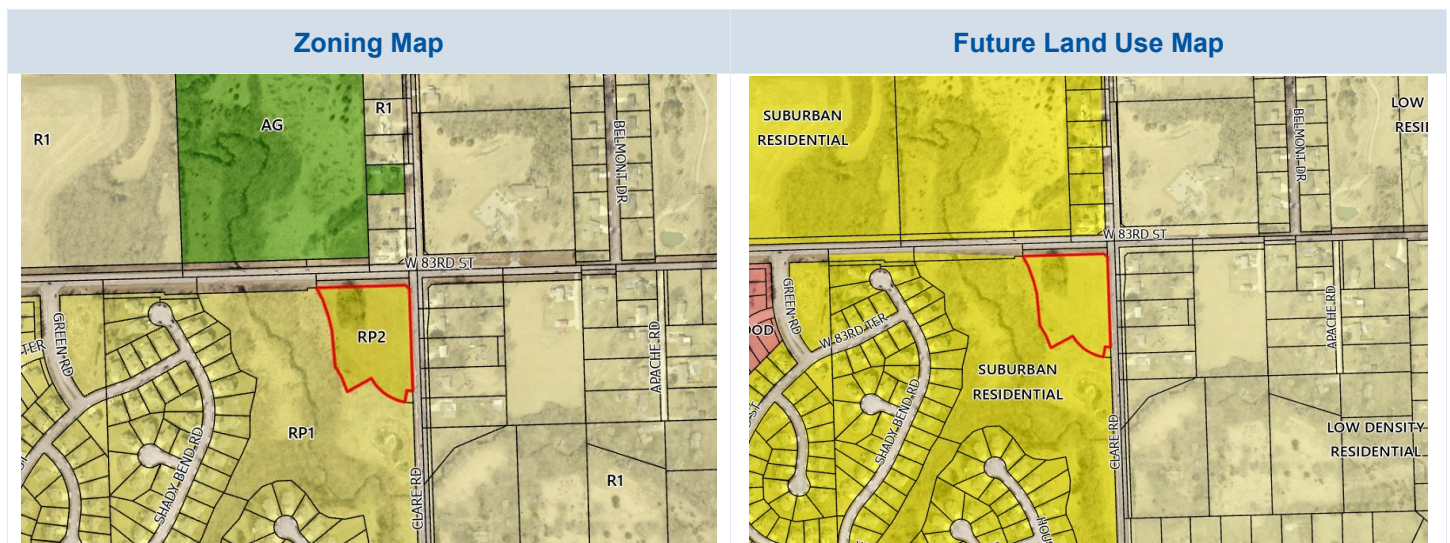


**Exhibit 2: Aerial Image of Subject Area.**

## LAND USE REVIEW

The one lot plat will contain six fourplex townhome buildings with 24 total units. The use is compatible with the RP-2 Zoning designation and will have a density calculation of 7.2 dwelling units per acre. The proposed development is not in conformance with the Comprehensive Plan’s Future Land Use Map (FLU). The Comprehensive Plan’s Future Land Use Map identifies the subject 3.4-acre parcel to be developed as Suburban Density Residential. This is defined as low density detached single-family housing not exceeding a gross density of 3.5 dwelling units per acre. The proposed development, at a density of 7.2 dwelling units per acre, is one level of intensity higher and proposes a multi-family townhome housing product.

While this is not in conformance with the FLU designation, Staff supported of the increase in the intensity of the zoning for this parcel due to the similar alignment of the proposed plan to the previously approved preliminary plan, and the existing buffer of this site from the existing single-family subdivision, as well as the proposed perimeter landscaping for the site. Staff pursued a reasonable balance of varied zoning districts by which to build community in this developing area of Lenexa taking into account specific locations and what each request can provide for the existing and future neighborhoods.

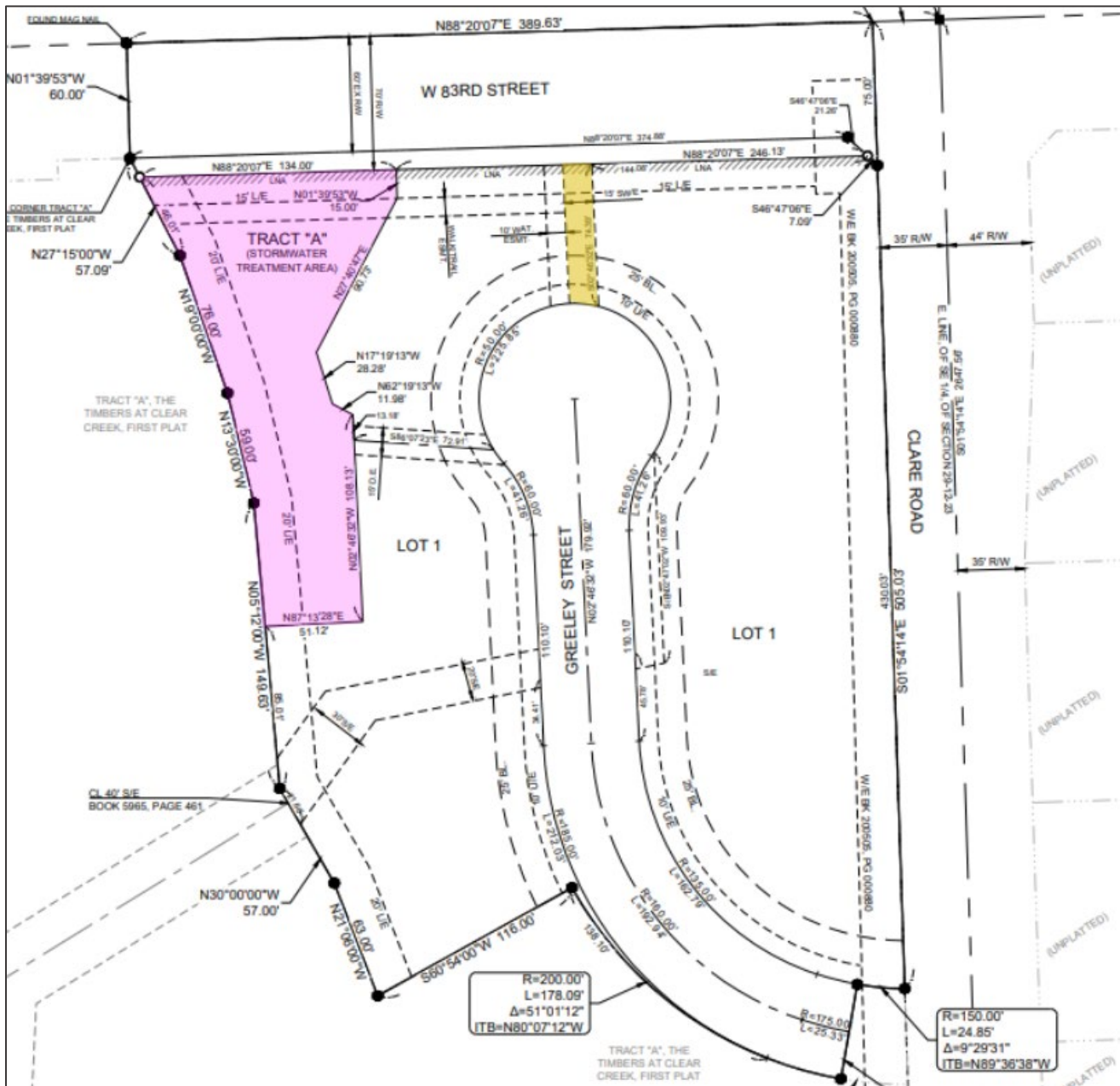


**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Suburban Residential	RP-2 Planned Residential Intermediate Density	Undeveloped Land
<b>North</b>	Suburban Residential	RP-1 Planned Residential Low Density & Agricultural	Single-Family Homes & Undeveloped Land
<b>South</b>	Suburban Residential	RP-1 Planned Residential Low Density	City Owned Land – Stream Buffer & Storm Detention BMP
<b>East</b>	Low Density Residential	R-1 Residential Low Density	Low Density Single-Family Homes
<b>West</b>	Suburban Residential	RP-1 Planned Residential Low Density	Stream Buffer & the Timbers at Clear Creek Single-Family Subdivision

## FINAL PLAT REVIEW

The proposed final plat consists of one lot and one tract, known as Tract “A”, for construction of a six-building, 24-unit townhome development on a 3.4-acre tract of land at the southwest corner of 83<sup>rd</sup> Street and Clare Road. Tract “A” is a stormwater treatment area.



**Exhibit 3: Final Plat with Tract “A” and Sidewalk Easement.**

The plat also contains Limits of No Access (LNA) to prohibit access to W. 83<sup>rd</sup> Street from both Lot 1 and Tract “A” and dedications for utility, drainage, sidewalk, and landscape easements and right-of-way. Exhibit 2 shows Tract “A” (magenta) a 15-foot sidewalk easement (yellow) connecting the cul-de-sac to W. 83<sup>rd</sup> Street to the north. Staff is working with the applicant to determine the appropriate location for a trail easement for a future walking trail connection to the future City trail to the west of the property.



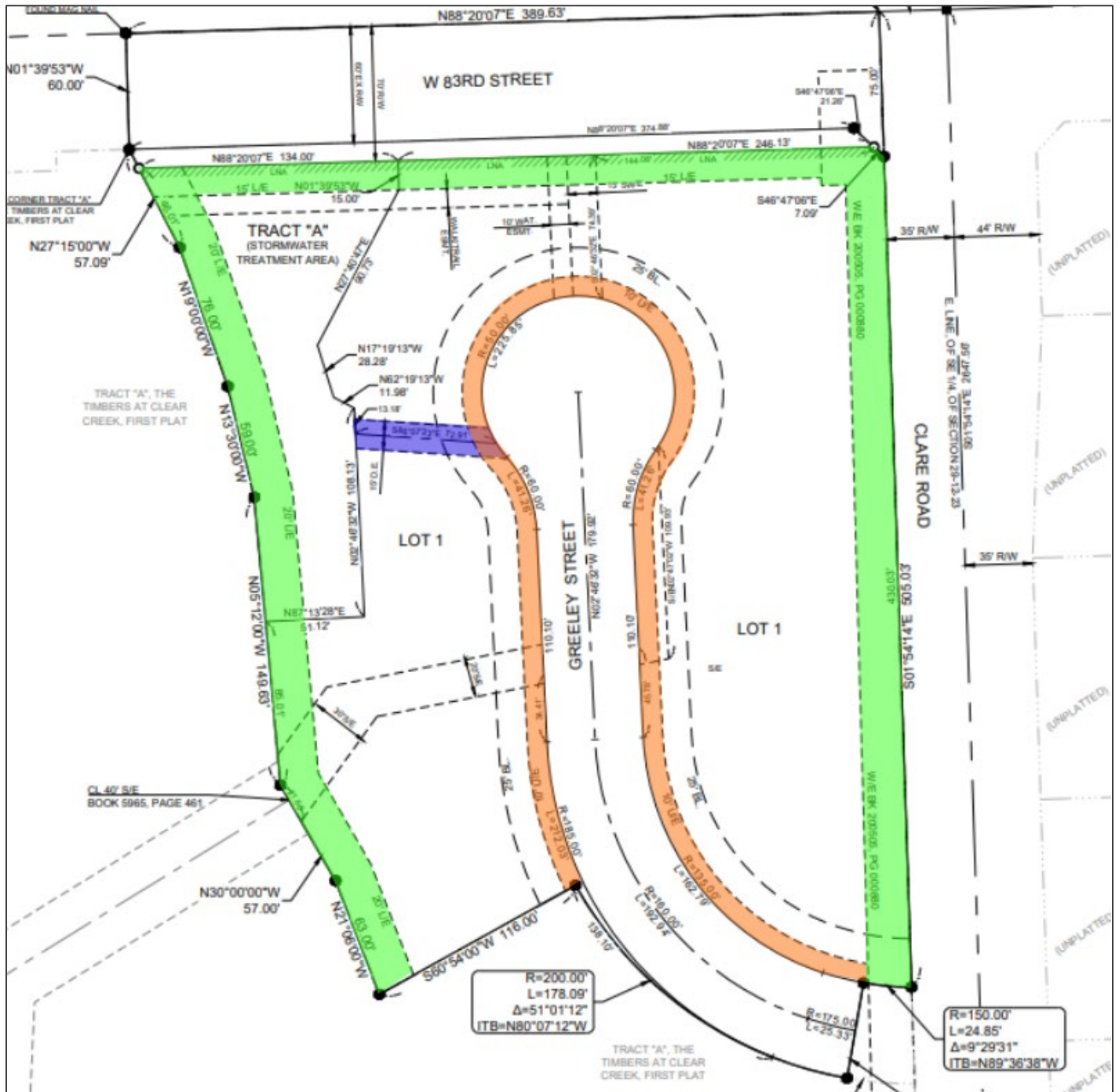


Exhibit 4: Final Plat with Utility and Landscape Easements.

Exhibit 3 shows proposed landscape easements (green) throughout the site. A 20-foot landscape easement is proposed along the north and east property lines to buffer from adjacent streets and along the west property line to provide a buffer from the single-family subdivision to the west. A 10-foot utility easement (orange) is to be dedicated to the City of Lenexa surrounding the Greeley Street right-of-way dedication. A 15-foot drainage easement (blue) is proposed to be dedicated to the City of Lenexa from the Greeley Street right-of-way to Tract "A".

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## DEVIATIONS

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The applicant is not requesting any additional deviations from the Unified Development Code with this application. A deviation was granted with the preliminary plan approval (PL22-13P) to reduce the 25-foot front yard setback by 10-feet to allow a front yard setback of 15-feet in a few instances. The majority of the structures will meet the 25-foot front yard setback requirement.

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## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this plat, pending Governing Body acceptance of right-of-way and easements for landscaping and utilities as shown on the final plat. This project is tentatively scheduled for the March 21, 2023 City Council meeting.
- The applicant should inquire about additional City requirements, such as permits and development fees.

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## RECOMMENDATION FROM PROFESSIONAL STAFF

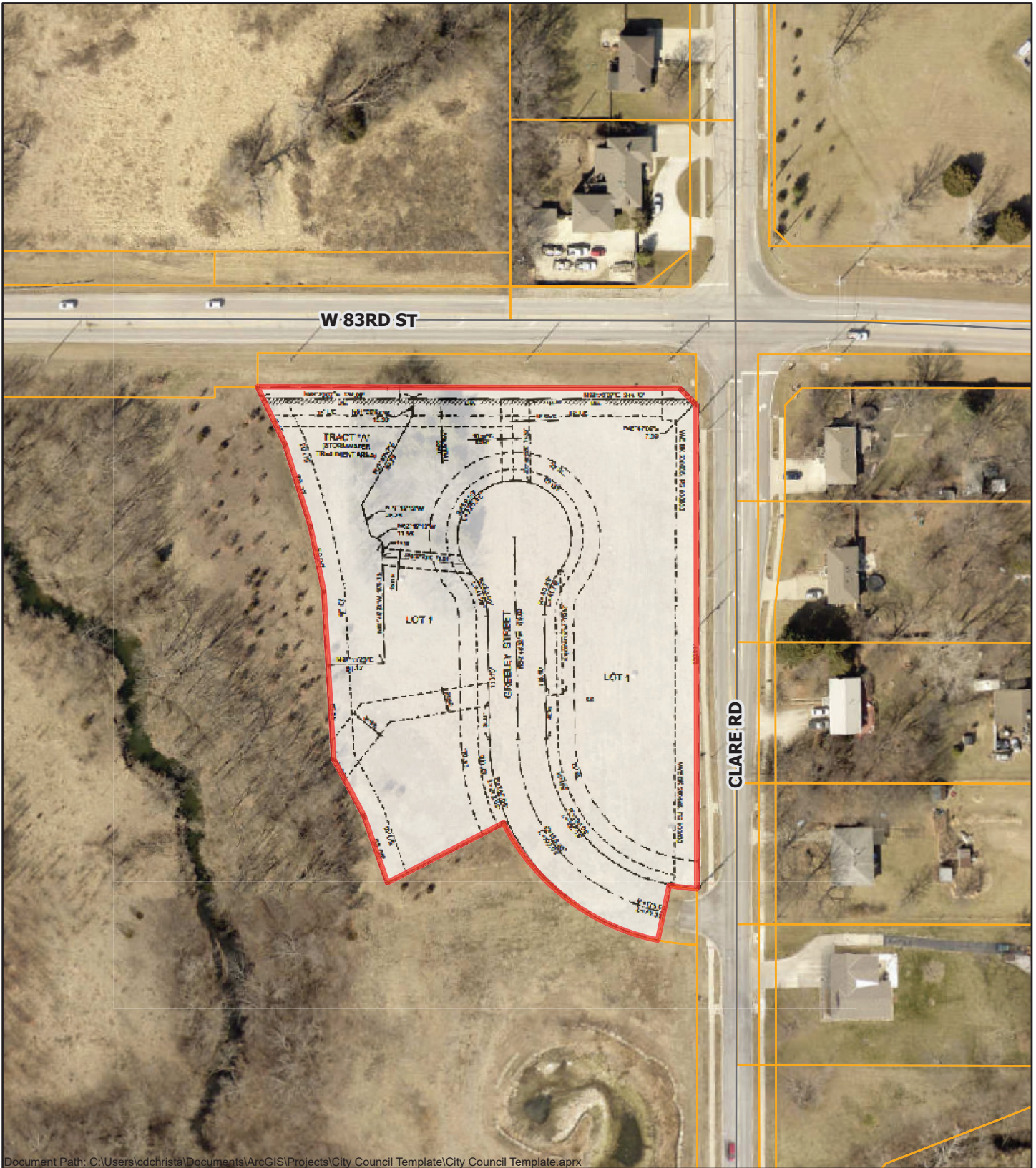
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★ **Staff recommends approval of the proposed Final Plat for Clear Creek Landing.**

- This is a one-lot final with one tract for the purpose of stormwater management.
- The project is consistent with Lenexa's goals through ***Strategic Community Investment*** and ***Responsible Economic Development*** to create ***Vibrant Neighborhoods***.

## FINAL PLAT

Staff recommends **approval** of the final plat for PT23-10F – **Clear Creek Landing** at the southwest corner of W. 83<sup>rd</sup> Street and Clare Road, for one-lot and one tract.

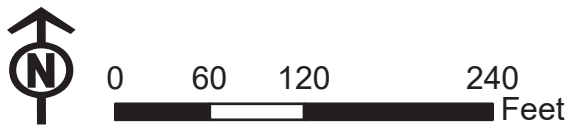


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Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Clear Creek Landing, Final Plat

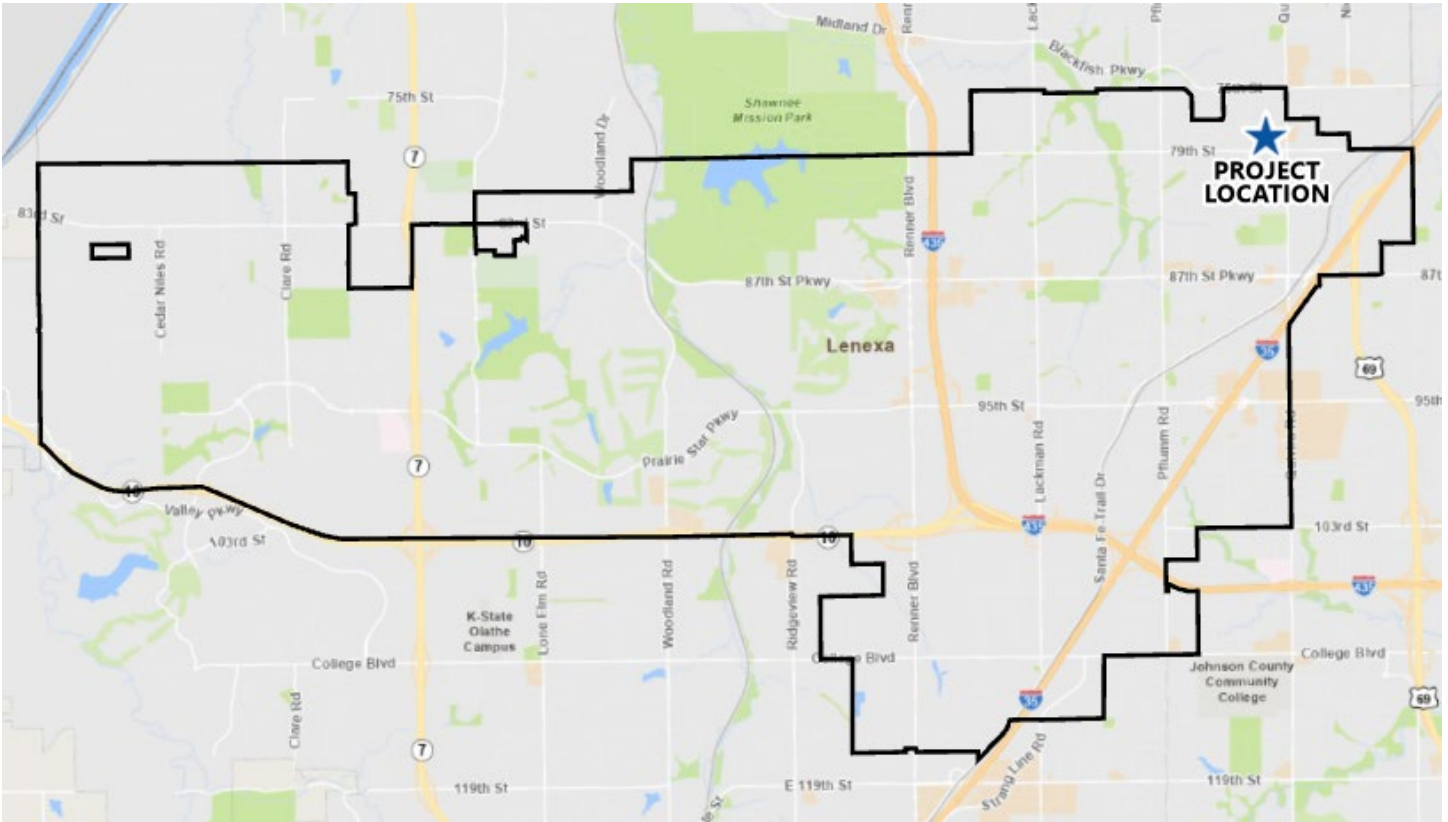
## PT23-10F





# QUIVIRA SQUARE SHOPPING CENTER

<b>Project #:</b>	PL22-24F	<b>Location:</b>	7820 Quivira Road
<b>Applicant:</b>	Daniel Umscheid, Clockwork	<b>Project Type:</b>	Final Plan
<b>Staff Planner:</b>	Christa McGaha, AICP	<b>Proposed Use:</b>	Commercial Shopping Center



## PROJECT SUMMARY

The applicant proposes site, façade and parking lot improvements for the Quivira Square shopping center at the northwest corner of 79<sup>th</sup> Street and Quivira Road. The applicant requests approval of a final plan. This project does not require a Public Hearing.

### STAFF RECOMMENDATION: APPROVAL

## SITE INFORMATION

The subject property is located at the northwest corner of 79<sup>th</sup> Street and Quivira Road. There are three existing shopping center buildings that were built in 1976. The site does not include the restaurant outlot at the hard corner of the intersection.

<b>LAND AREA (AC)</b> 5.87	<b>BUILDING AREA (SF)</b> 53,625	<b>CURRENT ZONING</b> CP-1	<b>COMP. PLAN</b> Neighborhood Retail
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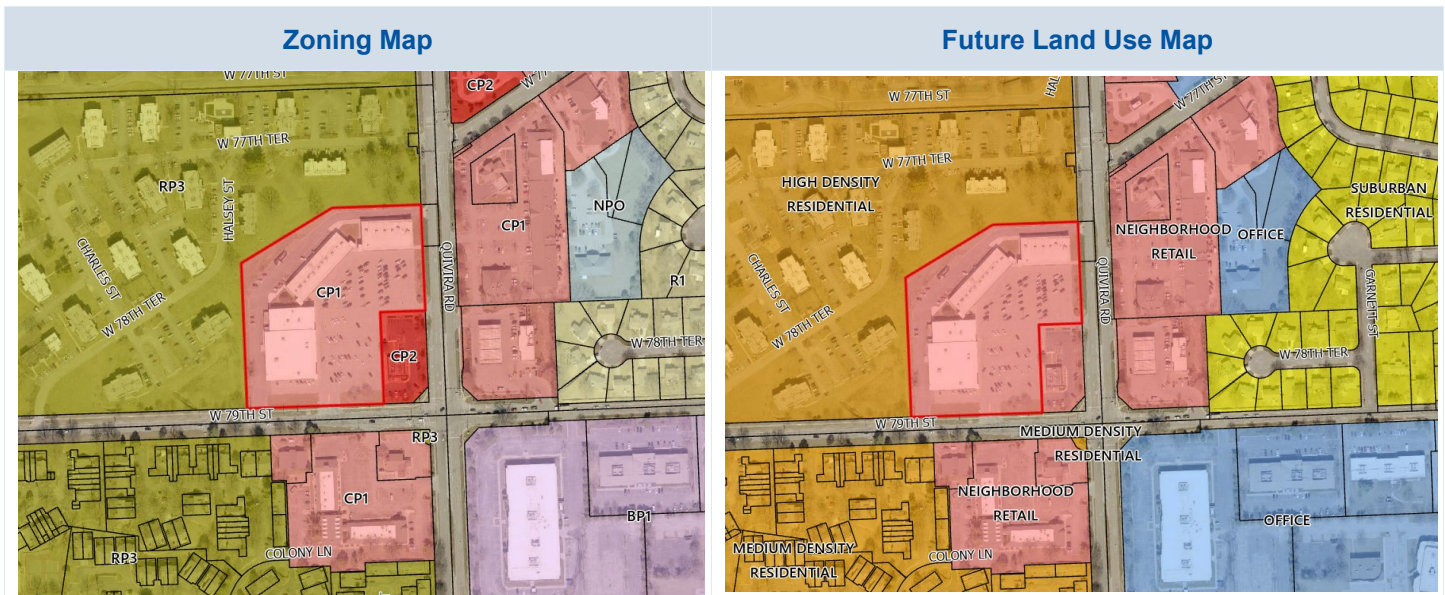
**Exhibit 1: Vicinity Aerial Image of Subject Site**



Exhibit 2: Georeferenced Aerial Image with Site Plan.

## LAND USE REVIEW

The existing three buildings include multiple tenant spaces, including restaurant, retail, and fitness center land use categories in the Unified Development Code (UDC). These are permitted land uses within the CP-1, Planned Neighborhood Commercial Zoning District.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

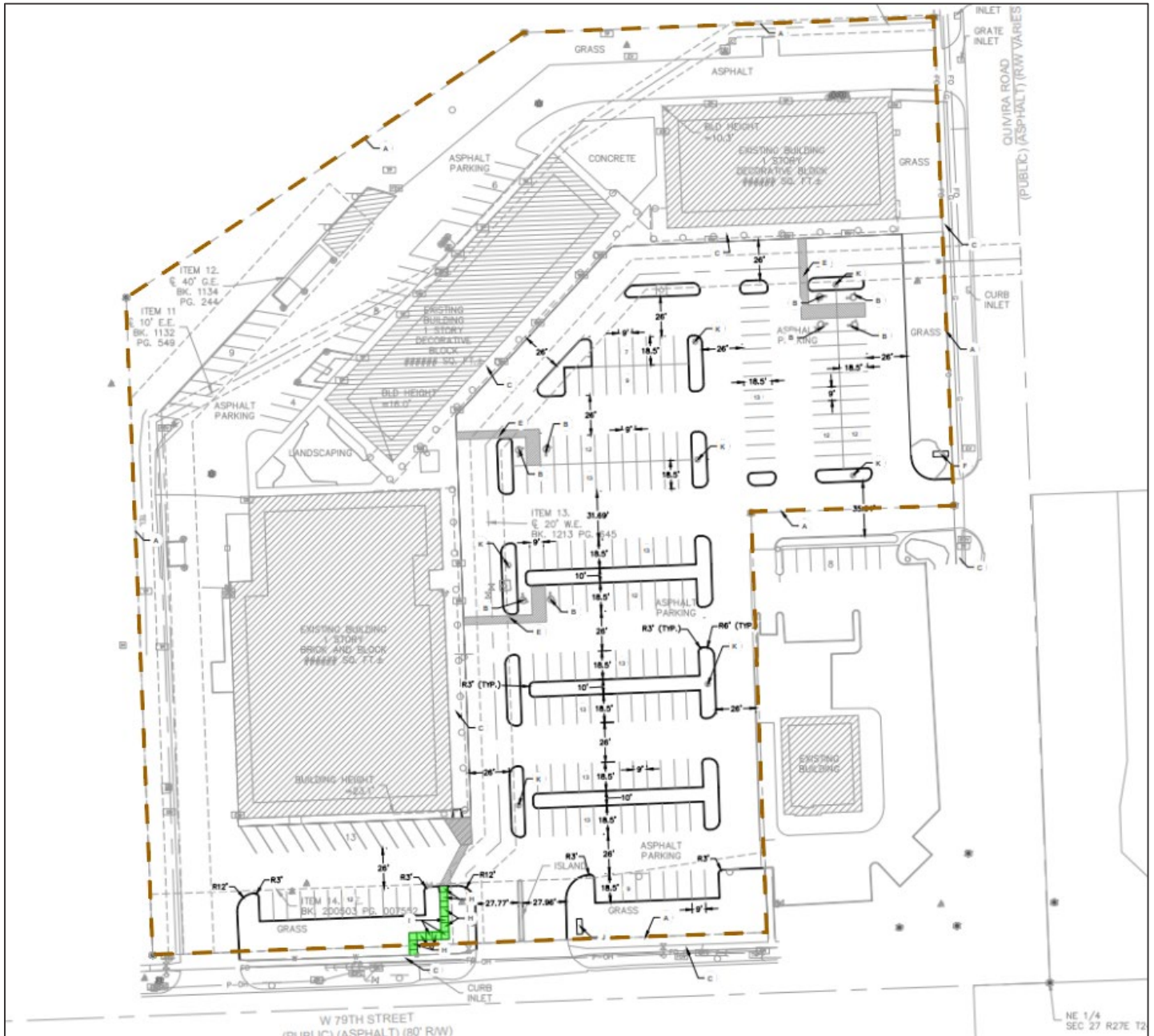
Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Neighborhood Retail	CP-1, Planned Neighborhood Commercial	Commercial, Shopping Center
<b>North</b>	High Density Residential	RP-3, Residential Medium-High Density District	Apartments, The Reserve at 77
<b>South</b>	Neighborhood Retail	CP-1, Planned Neighborhood Commercial	Commercial, Shopping Center & Bank
<b>East</b>	Neighborhood Retail	CP-1, Planned Neighborhood Commercial	Commercial, Shopping Center, Bank, Gasoline Sales & Restaurant
<b>West</b>	High Density Residential	RP-3, Residential Medium-High Density District	Apartments, The Reserve at 77

There are no proposed changes to the uses in the shopping center with this final plan application. All existing land uses are compatible with the surrounding uses in the area. The Four Colonies shopping center is located across 79<sup>th</sup> Street to the south and includes restaurant, bank, personal instruction, and religious institutional uses. The Lenexa Plaza shopping center is located across Quivira Road to the east and has similar banking restaurant and retail uses. The Reserve at 77 Apartments is located directly adjacent to the site to the north and west.



## FINAL PLAN REVIEW

The proposed final plan includes site, façade, and parking lot improvements for the existing three building Quivira Square Shopping Center located at the northwest corner of W. 79<sup>th</sup> Street and Quivira Road.



**Exhibit 3: Site Plan.**

Exhibit 3 shows the proposed site plan with improvements. The dashed brown line represents the property line. The building and parking lot area outside of the dashed line includes a Margaritas restaurant and parking lot, which is not included within the scope of the proposed improvements.

Proposed site improvements include:

- Parking lot redesign:
  - Remove existing diagonal parking layout.
  - Resurface and restripe parking lot adding striped crosswalks for pedestrian access from proposed ADA stalls and from exterior sidewalk network.
  - Add 15 new curbed and landscaped parking lot islands.
  - Add 27 new internal parking lot tree plantings.
  - Add 6 new parking lot light poles.
- New sidewalk connection (shown in green on Exhibit 3) is to be added from the sidewalk network on the north side of W. 79<sup>th</sup> Street to connect north to the buildings on the site.
- Landscape improvements throughout site include 19 additional perimeter trees, 8 of which are used to provide screening of the trash enclosure area at the northwest of the site from the apartments to the west.
- Architectural improvements to building facades.

## PUBLIC IMPROVEMENTS

There are only private site improvements proposed with this project. No public improvements are proposed.

## TRAFFIC AND ACCESS

- Quivira Road is classified as a Major Arterial roadway.
- 79<sup>th</sup> Street is classified as a Minor Arterial roadway.
- There are two points of access to the site from Quivira Road and two points of access from 79<sup>th</sup> Street.
- Public sidewalk exists along W. 79<sup>th</sup> Street and Quivira Road. The proposed sidewalk connection to W. 79<sup>th</sup> Street will improve access for pedestrians.

## FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

## LIGHTING

Proposed lighting types include architectural building lighting and internal parking lot lighting. Poles shall be Square Straight Steel Poles, 25 feet in height measured from top of foundation to fixture, with the base of the pole not to exceed 2 feet in height. The photometric plan is in compliance with the performance standards of [Section 4-1-C-4-I](#) of the UDC.

## LANDSCAPING

Proposed landscape improvements to the site include the addition of street trees, internal parking lot landscape islands, and monument sign landscaping for two separate monument signs.

Exhibit 4 shows the proposed landscape plan. Street trees are proposed along the W. 79<sup>th</sup> Street and Quivira Road frontages. Eight trees are proposed along the northwest property line as a buffer to screen the trash enclosure area from the apartments adjacent to the site. Twenty-seven trees are proposed in new internal landscape island areas. New landscaping is proposed for new monument signs on both street frontages at the entrances from W. 79<sup>th</sup> Street and Quivira Road.

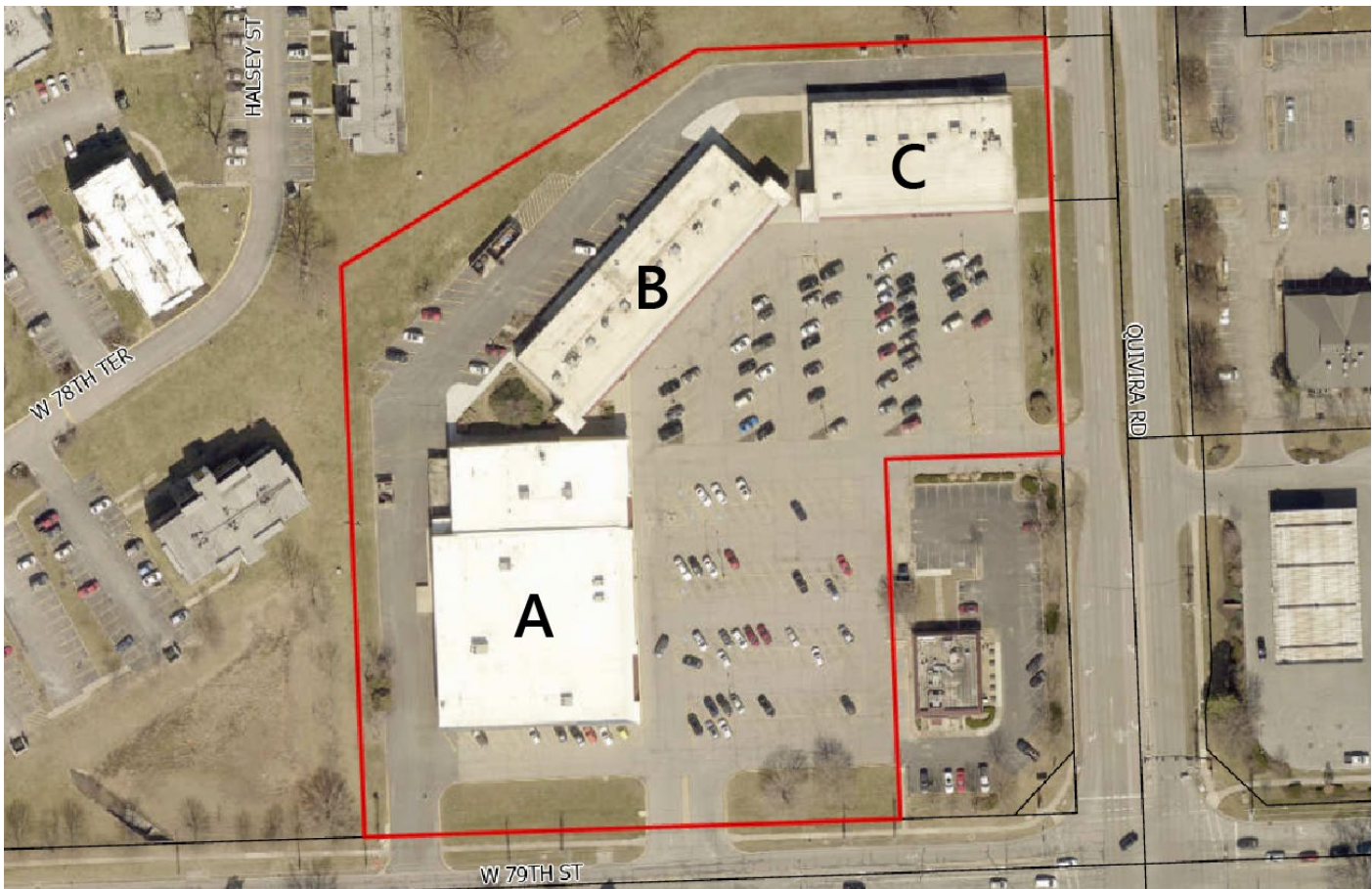


**Exhibit 4: Landscape Plan.**

**TABLE 2: PROPOSED LANDSCAPE IMPROVEMENTS**

	Code Requirement	Required	Proposed
<b>Street Trees</b>	<i>1 street tree per 40 feet of street frontage</i>	W. 79 <sup>th</sup> Street (404 ft frontage): <b>11 trees</b>	5 trees along south property line 6 trees along NW property line
		Quivira Road (320 ft frontage): <b>8 trees</b>	2 existing trees to remain 3 trees along NW property line
<b>Parking Lot</b>	<i>1 parking lot tree per 300 feet of internal landscaped area</i>	7,897 internal landscaped area: <b>27 trees</b>	27 parking lot trees
	<i>1 shade tree per 500 sf landscape area</i>	240 sf landscape area: <b>1 shade tree</b>	1 shade tree
<b>Monument Signs</b>	<i>1 ornamental tree per 200 sf</i>	240 sf landscape area: <b>2 ornamental trees</b>	2 ornamental trees
	<i>1 shrub per 20 sf</i>	240 sf landscape area: <b>12 shrubs</b>	12 shrubs

**ARCHITECTURE**



**Exhibit 5: Existing Buildings A, B, & C in Quivira Square Shopping Center.**

The existing shopping center has three separate buildings identified in Exhibit 5 as buildings “A” “B” and “C”. Architectural modifications to the existing building façade include painting and replacing existing wood cladding and trim with EIFS and synthetic wood and stone materials.

<b>PT1</b>	<b>PAINT COLOR: SHERWIN WILLIAMS ANONYMOUS / SW 7046</b>	<b>WOOD POSTS AND BEAMS</b>	<b>ULTRASHIELD NATURALE COLOR: IPE</b>
<b>PT2</b>	<b>PAINT COLOR: SHERWIN WILLIAMS INTELLECTUAL GRAY / SW 7045</b>	<b>STONE COLUMN BASE</b>	<b>GRAPHITE LAVASTONE COLOR NORSTONE PLANC</b>
<b>PT3</b>	<b>PAINT COLOR: SHERWIN WILLIAMS URBANE BRONZE / SW 7048</b>		

**Exhibit 6: Proposed Architectural Materials and Finishes.**

The new façade materials are consistent in character with surrounding buildings in the area. The existing concrete and fluted CMU block wall are to be cleaned, patched, and repaired prior to painting. Existing columns shall have current decorative wood trim removed and replaced with new synthetic Ultrashield Naturale “IPE” wood and stone cladding column base. Exhibits 7, 8, and 9 show the proposed architectural façade changes to buildings “A”, “B”, and “C” from the east, southeast, and south perspectives.



**Exhibit 7: Existing and Proposed Perspectives for Building A.**

The applicant is proposing the replacement of the existing exterior building lighting. Soffit lights and wall packs shall be exchanged for new LED fixtures. Emergency egress lighting shall be replaced at all egress doors where required.



**Exhibit 8: Existing and Proposed Perspectives for Building B.**

The existing wood-wrapped façade cladding will be removed and replaced with new water-managed EIFS cladding system. The existing standing seam mansard roof remain and shall be cleaned and painted to match building façade.



**Exhibit 9: Existing and Proposed Perspectives for Building C.**

The proposed building meets the architectural standards required in [Section 4-1-C-5](#) of the UDC. Staff is supportive of the proposed architectural improvements.

## DEVIATIONS

The site plan does not meet Lenexa’s parking requirements. The applicant is requesting a 32-stall parking deviation to allow for 216 parking stalls when 248 parking stalls are required by code.

**TABLE 3: PARKING DEVIATION ANALYSIS**

Land Use	Parking Formula	Required Parking	Total Required	Total Proposed	Difference
Restaurant	1 space per 75 square feet	32	248	216	<b>-32</b>
Retail	1 space per 250 square feet	114			
Fitness Center	1 space per 200 square feet	102			

Staff is supportive of the proposed parking deviation in exchange for the site improvements to the parking lot including the removal of existing angled parking layout and the addition of curbed, landscape islands and parking lot trees which will meet the internal parking lot landscape requirements and provide for a more pleasant parking experience for the shopping center overall. The shopping center may change tenants from time to time, so the calculation of parking fluctuates depending on the land uses. Staff studied the site and the information provided by the applicant and believes that the site will not encounter a parking problem by having a deficit of 32 parking stalls.

## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

## RECOMMENDATION FROM PROFESSIONAL STAFF

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- ★ **Staff recommends approval of the proposed Final Plan for Quivira Square Shopping Center.**
  - This project is consistent with Lenexa’s goals through *Responsible Economic Development* to promote *Vibrant Neighborhoods* and *Healthy People*.

## FINAL PLAN

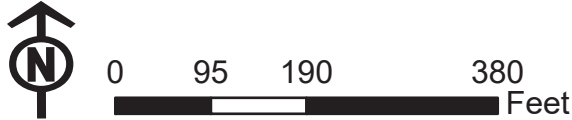
Staff recommends **approval** of the final plan for PL22-24F – **Quivira Square Shopping Center** at 7820 Quivira Road, for site, façade, and parking lot improvements with a 32-stall parking deviation to allow for a total of 216 parking stalls.



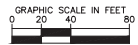
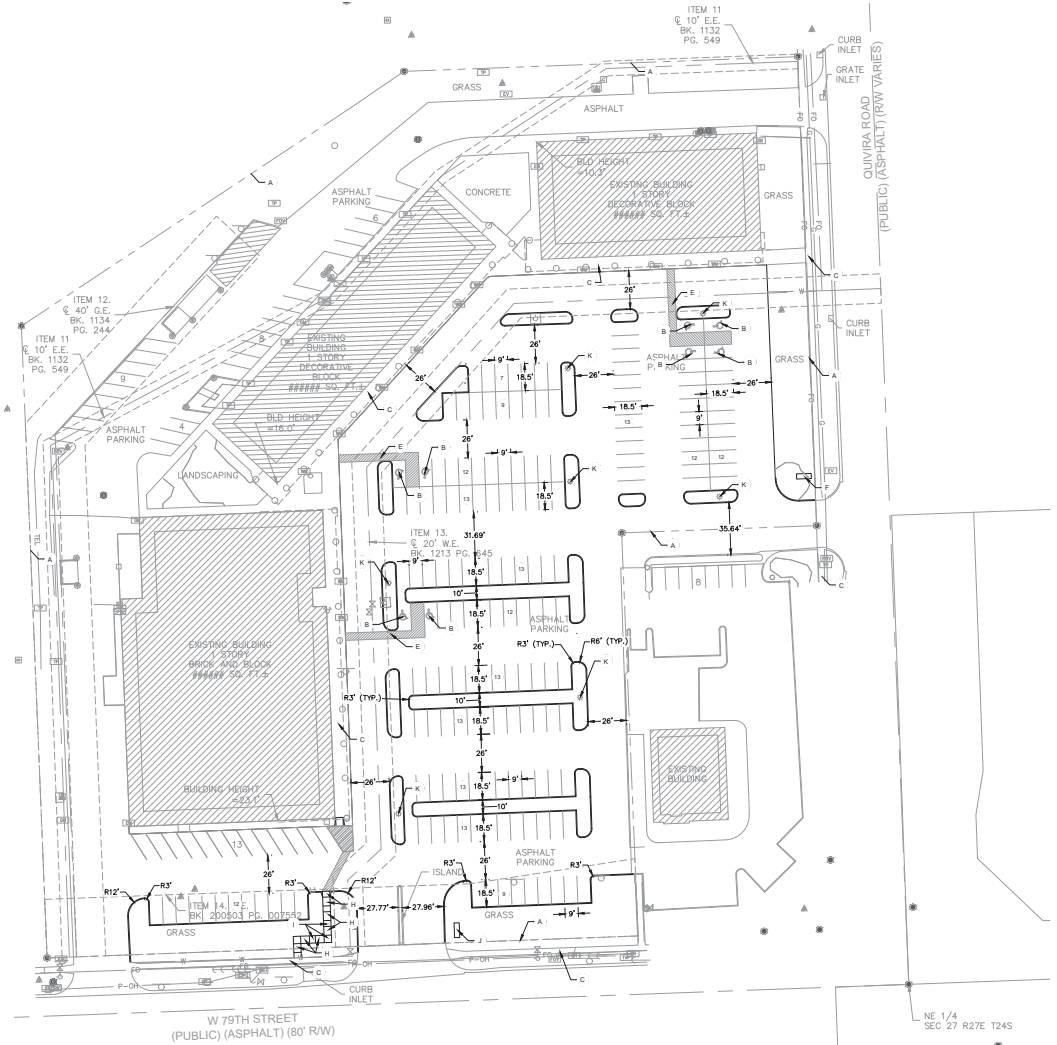
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Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Quivira Square, Final Plan PL22-24F







- KEYNOTE INDEX**
- ① PROPERTY LINE
  - ② ACCESSIBLE PARKING SPACE AND SYMBOL
  - ③ EXISTING SIDEWALK
  - ④ PROPOSED LIGHT POLE. REFER TO ELECTRICAL PLANS (TYP.)
  - ⑤ DRIVEWAY
  - ⑥ PRIMARY MONUMENT SIGN
  - ⑦ ADA RAMP
  - ⑧ PROPOSED ADA RAMP
  - ⑨ PROPOSED SIDEWALK
  - ⑩ SECONDARY MONUMENT SIGN
  - ⑪ PROPOSED LIGHT POLES

- SIGN LEGEND**
- TP-21-N ACCESSIBLE PARKING
  - TP-21-P ACCESSIBLE PENALTY
  - R-1 STOP SIGN
  - R-1-1 DO NOT ENTER SIGN

- SYMBOL LEGEND**
- ACCESSIBLE PARKING SPACE
  - XX PARKING STALL COUNT
  - PROPERTY LINE
  - LIGHT POLE

**SITE DATA**

LOT AREA	5.98 AC	
ZONING	CP-1	

**PARKING SUMMARY**

	REQUIRED	PROVIDED
STANDARD	215	209
ACCESSIBLE	7	7
TOTAL	222	216

**PARKING CALCULATION:**  
 1 SPACE PER 200 SF OF BUILDING AREA  
 53,726 SF OF BUILDING AREA  
 (53,726 / 200) = 214.9 = 215 SPACES REQUIRED

**LOT COVERAGE:**

AREA	AC	SF	%
BUILDING AREA	1.23	53,726 SF	20.63%
IMPERVIOUS AREA	3.80	165,837 SF	61.65%
LANDSCAPE AREA	1.05	45,168 SF	17.72%
TOTAL LOT AREA	5.98	264,731 SF	100%

**PARKING CALCULATIONS:**

REQUIRED PARKING PER SCHEDULE: 1 SPACE/250 SF  
 FOR RETAIL AND 1 SPACE/75 SF FOR RESTAURANT  
 EXISTING RETAIL BUILDINGS: 49,833 SF  
 EXISTING RESTAURANT BUILDINGS: 2400 SF  
 REQUIRED RETAIL PARKING: 195 SPACES  
 REQUIRED RESTAURANT PARKING: 32 SPACES  
 REQUIRED TOTAL PARKING: 227 SPACES

PREVIOUS PARKING IN MAIN LOT: 191 SPACES  
 PREVIOUS PARKING IN BACK LOT: 27 SPACES  
 PREVIOUS ADA STALLS IN MAIN LOT: 14 SPACES  
 TOTAL SPACES: 216 SPACES

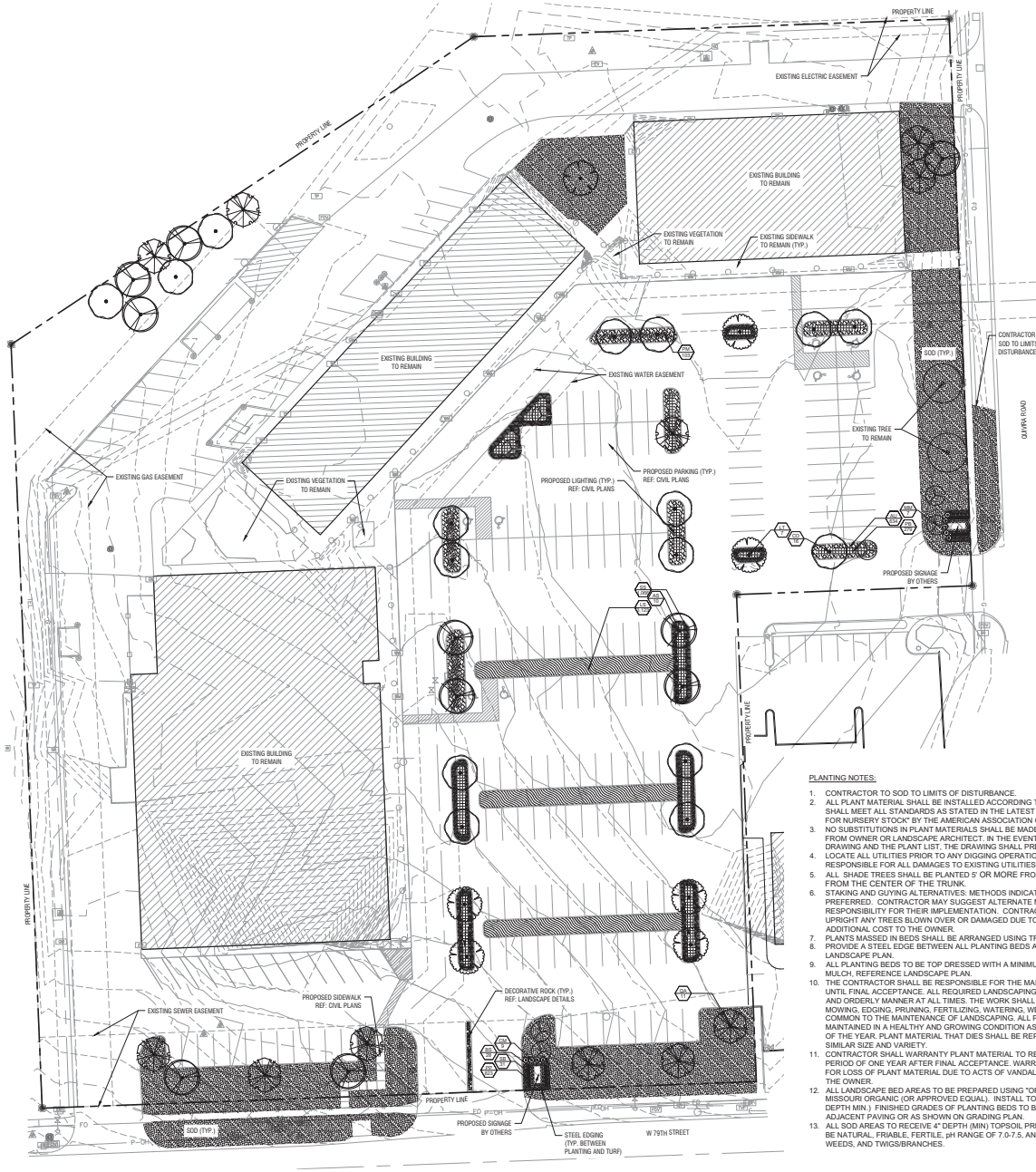
PROPOSED PARKING IN MAIN LOT: 192 SPACES  
 PROPOSED ADA STALLS IN MAIN LOT: 7 SPACES  
 TOTAL SPACES INCLUDING BACK LOT: 216 SPACES

**QUIVIRA SQUARE  
 SHOPPING CENTER**  
 7820 Quivira Road  
 Lenexa, KS 66216

REV	ISSUE	DATE
1	FINAL DEV PLAN	01/30/23

22.09 QUIVIRA SQUARE

**SITE PLAN**



- PLANTING NOTES.**
- CONTRACTOR TO SOD TO LIMITS OF DISTURBANCE.
  - ALL PLANT MATERIAL SHALL BE INSTALLED ACCORDING TO SOUND NURSERY PRACTICES AND SHALL MEET ALL STANDARDS AS STATED IN THE LATEST EDITION OF "AMERICAN STANDARD FOR NURSERY STOCK" BY THE AMERICAN ASSOCIATION OF NURSERMEN.
  - NO SUBSTITUTIONS IN PLANT MATERIALS SHALL BE MADE WITHOUT WRITTEN AUTHORIZATION FROM OWNER OR LANDSCAPE ARCHITECT. IN THE EVENT OF DISCREPANCIES BETWEEN THE DRAWING AND THE PLANT LIST, THE DRAWING SHALL PREVAIL.
  - LOCATE ALL UTILITIES PRIOR TO ANY DIGGING OPERATIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGES TO EXISTING UTILITIES INCURRED BY HIS WORK.
  - ALL SHADE TREES SHALL BE PLANTED 4' OR MORE FROM ANY UTILITY LINE—MEASURED FROM THE CENTER OF THE TRUNK.
  - STAKING AND GUYING ALTERNATIVES: METHODS INDICATED IN DRAWING DETAILS ARE PREFERRED. CONTRACTOR MAY SUGGEST ALTERNATE METHODS, ASSUMING FULL RESPONSIBILITY FOR THEIR IMPLEMENTATION. CONTRACTOR SHALL REPLACE, PLANT, OR UPRIGHT ANY TREES BLOWN OVER OR DAMAGED DUE TO INADEQUATE STAKING AT NO ADDITIONAL COST TO THE OWNER.
  - PLANTS MASSSED IN BEDS SHALL BE ARRANGED USING TRIANGULAR SPACING.
  - PROVIDE A STEEL EDGE BETWEEN ALL PLANTING BEDS AND LAWN AREAS. REFERENCE LANDSCAPE PLAN.
  - ALL PLANTING BEDS TO BE TOP DRESSED WITH A MINIMUM OF 3" SHREDDED HARDWOOD MULCH, REFERENCE LANDSCAPE PLAN.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPING UNTIL FINAL ACCEPTANCE. ALL REQUIRED LANDSCAPING SHALL BE MAINTAINED IN A NEAT AND ORDERLY MANNER AT ALL TIMES. THE WORK SHALL INCLUDE, BUT NOT BE LIMITED TO, MOWING, EDGING, PRUNING, FERTILIZING, WATERING, WEEDING, AND OTHER SUCH ACTIVITIES COMMON TO THE MAINTENANCE OF LANDSCAPING. ALL PLANT MATERIALS SHALL BE MAINTAINED IN A HEALTHY AND GROWING CONDITION AS IS APPROPRIATE FOR THE SEASON OF THE YEAR. PLANT MATERIAL THAT DIES SHALL BE REPLACED WITH THE PLANT MATERIAL OF SIMILAR SIZE AND VARIETY.
  - CONTRACTOR SHALL WARRANTY PLANT MATERIAL TO REMAIN ALIVE AND HEALTHY FOR A PERIOD OF ONE YEAR AFTER FINAL ACCEPTANCE. WARRANTY SHALL NOT INCLUDE DAMAGE FOR LOSS OF PLANT MATERIAL DUE TO ACTS OF VANDALISM OR NEGLIGENCE ON THE PART OF THE OWNER.
  - ALL LANDSCAPE BED AREAS TO BE PREPARED USING "ORGANICALLY ENRICHED TOP SOIL" BY MISSOURI ORGANIC (OR APPROVED EQUAL). INSTALL TO DEPTHS, PER PLANTING DETAILS (12" DEPTH MIN.). FINISHED GRADES OF PLANTING BEDS TO BE 2" BELOW FINISHED GRADE OF ADJACENT PAVING OR AS SHOWN ON GRADING PLAN.
  - ALL SOD AREAS TO RECEIVE 2" DEPTH (MIN) TOPSOIL PRIOR TO INSTALLATION. TOPSOIL SHALL BE NATURAL, FRYABLE, FERTILE, pH RANGE OF 7.0-7.5, AND FREE OF TRASH, DEBRIS, STONES, WEEDS, AND TWIGS/BRANCHES.

**City of Lenexa, Kansas - Landscape Data Table**  
 Site Area: 7.72 Acres (74,924 sqft) - Zoning (UR) Urban Redevelopment

Section 4-3-2: LANDSCAPING, BUFFERING AND SITE DESIGN	REQUIRED	PROVIDED
<b>2. Street Trees</b>		
It is the intent of this Section that one (1) street tree shall be planted for every 40 linear feet of public or private street frontage.		
W 79th Street - 404' HW Street Frontage / 40' = 11 Street Trees Quivira Road - 320' HW Street Frontage / 40' = 8 Street Trees	W 79th Street - 11 Trees Quivira Road - 8 Trees	W 79th Street - 5 Trees Along S Property Line 6 Trees Along NW Property Line Quivira Road - 2 Existing Trees 3 Trees Along E Property Line 3 Trees Along NW Property Line
Required street trees shall be installed within a landscape easement or within the right-of-way abutting the street.		
<b>M. Internal Parking Lot, Landscaped Areas</b>		
Every opening 10' or over 10' spaces and 3,500 square feet shall contain internal landscaped areas based on the following table criteria.	Yes	Yes
1. Amount: The following percentage of the total parking lot area shall be dedicated to internal landscaped areas. Where there is more than 1 distinct parking area on a site, this amount should be provided for each lot. Where multiple lots are provided for a multi-use site, the site may elect the predominant use to which the spaces serve. For multi-family projects - 10 percent of total parking lot area. For office and commercial sites - 7.5 percent of total parking lot area. For industrial or warehouse sites - 5 percent of total parking lot area.	7,897 sqft Interior Landscape Area	7,897 sqft Interior Landscape Area
The total parking lot area shall be measured along the perimeter of the lot including all parking spaces, access drives, curbs lanes, and landscaped islands within the perimeter and at the corners of the lot. The total parking lot area shall not include loading and service areas, but shall include drive-up areas for customer service and drives and maneuvering areas that are jointly used by parking lot and service bays.		
Interior Landscape Area: 195,290 sqft Parking Lot Area x 7.5% = 7,897 sqft Interior Landscape Area		
2. Design: Internal landscape islands and Stormwater Best Management Practices (BMP), such as infiltration areas, satisfying this internal parking lot landscape requirement shall be at a minimum 4' feet in width and 125 square feet in area. A minimum of 1' trees shall be planted per 500' linear of overall internal landscaped area, with at least 1 tree planted in each island. The remainder of the island shall be landscaped with additional trees, shrubs, ground cover or turf. Plantings should anticipate hot traffic patterns, be used to discourage hot traffic where dictated by safety concerns, and shall provide adequate visibility for the safety of pedestrians and vehicles.	Yes	Yes
Parking Lot Trees: 7,897 sqft Interior Landscape Area / 300 = 27 Trees	27 Parking Lot Trees	27 Parking Lot Trees
<b>D. Landscaping of Monument Signs</b>		
1. Minimum Landscaping Area: An area equal to 3 times the combined total area of the face of the sign and monument base (only one side of a two-sided sign is used in calculating landscape area) shall be landscaped using ornamental trees and shrubs, and ground covers, rock beds or other decorative ground treatments contrasting with the base or other predominant ground cover on the site.	Yes	Yes
2. Minimum Landscape Material: The monument sign landscaped area shall include, as a minimum: a. One shade tree per 200 square feet of required landscaped area or additional major portion thereof. b. One ornamental tree per 200 square feet of required landscaped area or additional major portion thereof. c. Two shrubs per 200 square feet of required landscaped area or additional major portion thereof.	Yes	Yes
<b>Sign 1</b> Shade Tree: 240 sqft Landscape Area / 500 sqft = 1 Shade Tree Ornamental Tree: 240 sqft Landscape Area / 200 sqft = 2 Ornamental Trees Shade Tree: 240 sqft Landscape Area / 200 sqft = 2 Shrubs	1 Shade Tree 2 Ornamental Trees 12 Shrubs	1 Shade Tree 2 Ornamental Trees 12 Shrubs
<b>Sign 2</b> Shade Tree: 240 sqft Landscape Area / 500 sqft = 1 Shade Tree Ornamental Tree: 240 sqft Landscape Area / 200 sqft = 2 Ornamental Trees Shade Tree: 240 sqft Landscape Area / 200 sqft = 2 Shrubs	1 Shade Tree 2 Ornamental Trees 12 Shrubs	1 Shade Tree 2 Ornamental Trees 12 Shrubs

**PLANT SCHEDULE**

NO.	SYMBOL	PLANT NAME	DEPT.	QUANTITY	DATE	STATUS
01	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
02	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
03	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
04	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
05	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
06	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
07	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
08	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
09	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
10	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
11	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
12	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
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14	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
15	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
16	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
17	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
18	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
19	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE
20	(Symbol)	AMERICAN WOOD VINE	01.0	12	12-18	CALL FOR QUOTE

NOTE: PLANT QUANTITIES ARE PROVIDED FOR CONVENIENCE ONLY. IN THE CASE OF A DISCREPANCY, THE DRAWING SHALL TAKE PRECEDENCE.

**clockwork**  
 423 Delaware St Ste 102  
 Kansas City MO 64105  
 www.clockwork-ad.com

**Kimley-Horn**  
 805 PENNINGMAN AVE SUITE 150  
 KANSAS CITY, MO 64105  
 816-652-0352

FOR REVIEW ONLY  
 Not for construction or permit purposes.

**Kimley-Horn**

R.L. JASON D. KANAK  
 No. 0688 Date: 01/20/2023

**QUIVIRA SQUARE  
 SHOPPING CENTER**  
 7820 Quivira Road  
 Lenexa, KS 66216

rev	issue	date
FINAL DEV PLAN		01/30/2023

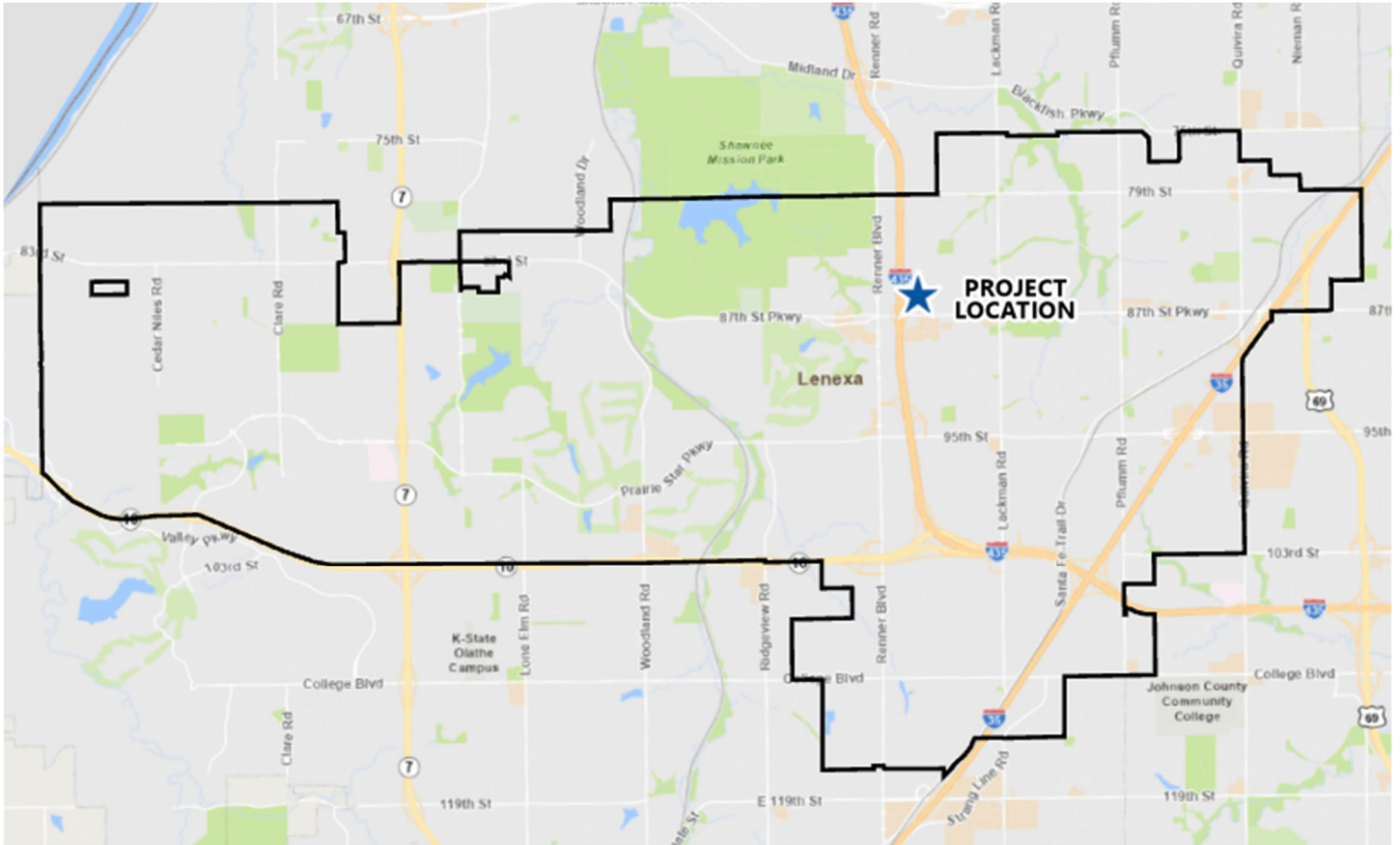
22.09 QUIVIRA SQUARE

**LANDSCAPE PLAN**

L-100

## LENEXA SMART STORAGE

<b>Project #:</b>	PL23-04F	<b>Location:</b>	8630 Maurer Road
<b>Applicant:</b>	Phelps Engineering	<b>Project Type:</b>	Final Plan
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	Self-storage



### PROJECT SUMMARY

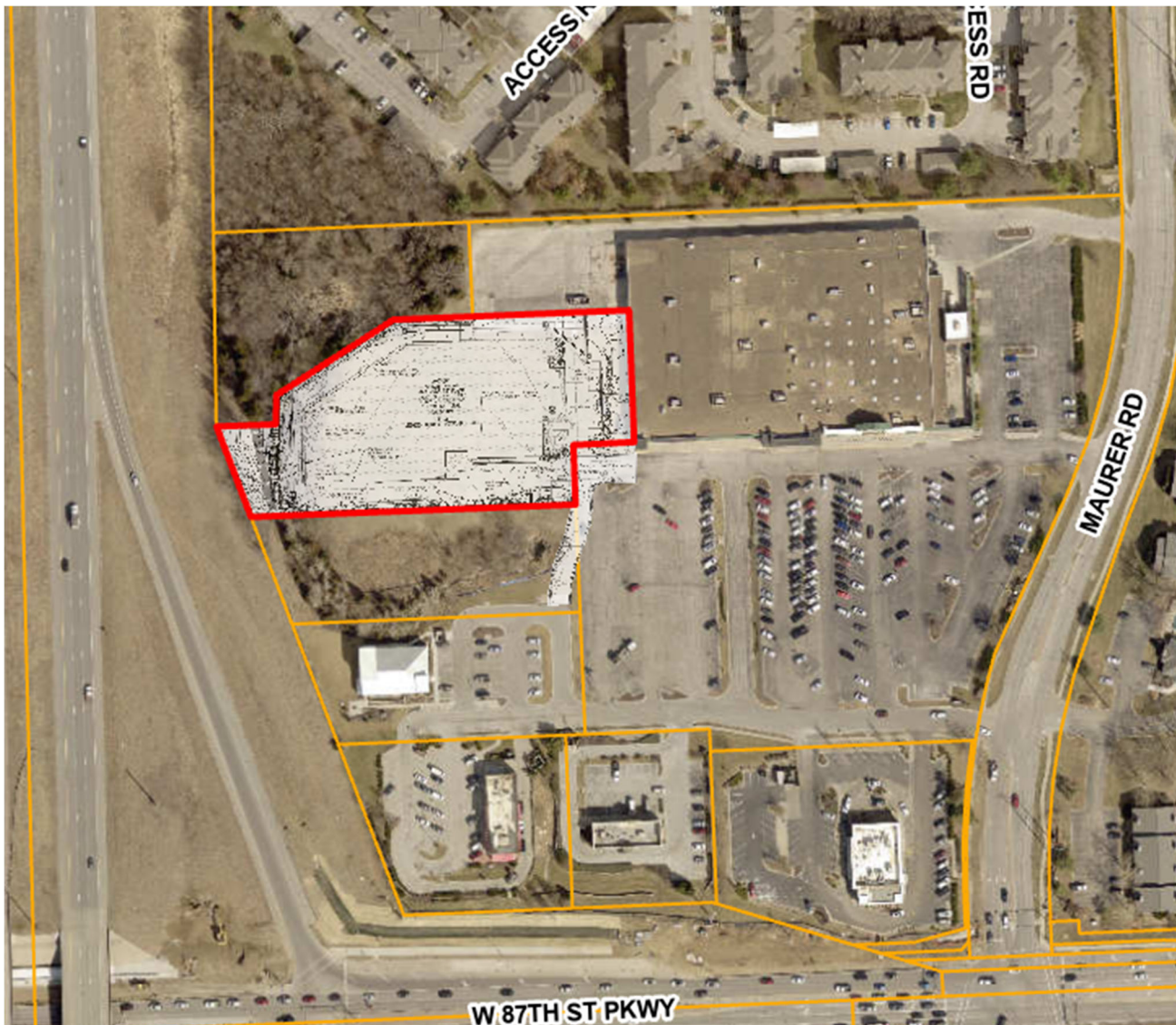
The applicant proposes to construct a three-story building in the Lenexa Pointe commercial center for a self-storage facility. The building is at the northwesterly corner of the development. Access into the site is from Maurer Road, along the easterly street frontage. The completion of this building will leave one outparcel remaining undeveloped in the commercial center. The building is 118,560 square feet and will have 697 storage units.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

Lenexa Pointe is a commercial center at the northeast quadrant of the 87<sup>th</sup> Street Parkway and I-435 intersection. Access into the center is from three drives onto Maurer Road. The center began developing in the later 1980's. A large (big-box) retail building was built and was originally a K-mart store. The building is now divided into multiple tenants. A second building for a dentist's office was built in 2020 on the westerly side of the center. Three fast food restaurants are to the south of the center. Although the fast-food restaurants share an access drive from Maurer Road, these restaurants are not part of Lenexa Pointe commercial center.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
2.17	118,560	CP-2	Community Retail

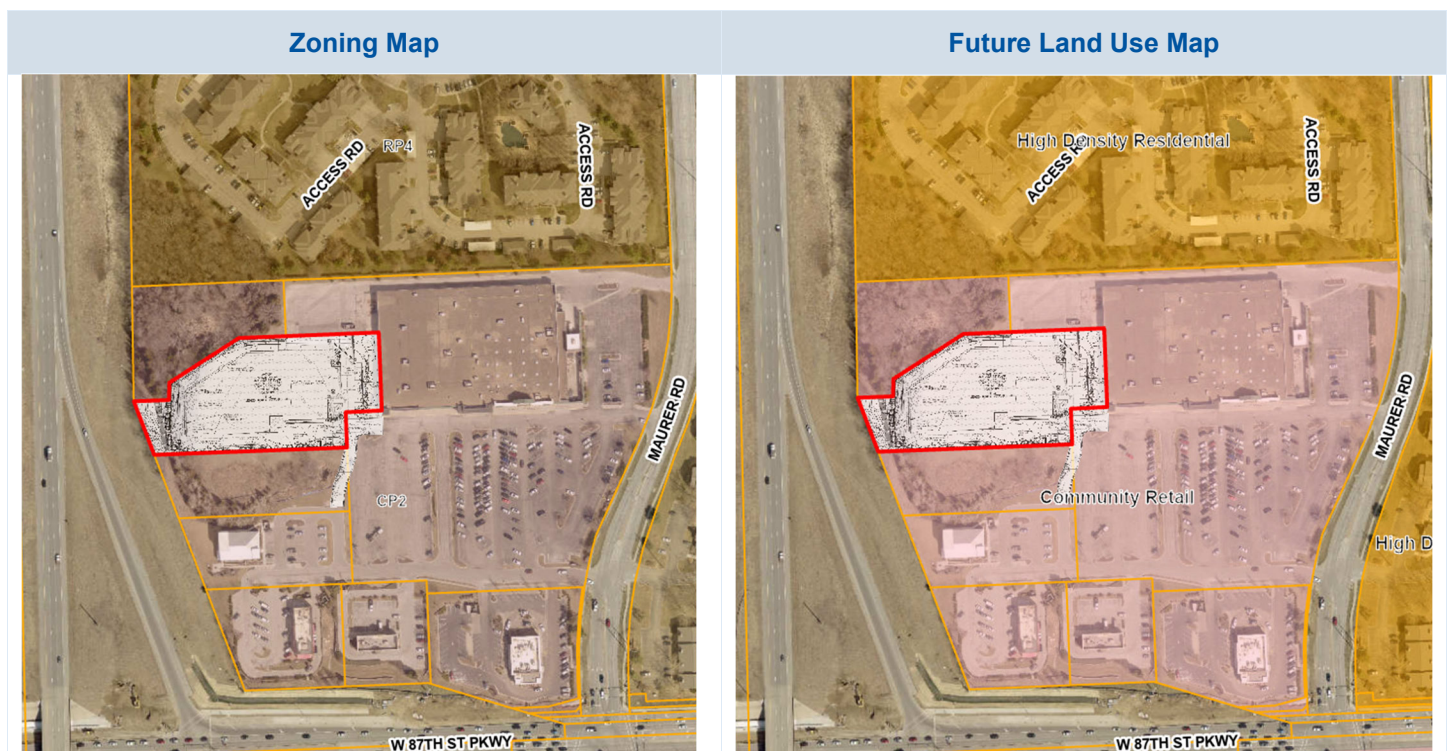


*Exhibit 1: Vicinity Aerial Image of Subject Site*

## LAND USE REVIEW

Self-service storage is permitted in the CP-2 Zoning District by special use permit (SUP). A special use permit and preliminary plan for Lenexa Smart Storage was approved in the fall of 2022 (SU22-11 and PL22-21P). The SUP is approved for an indefinite period of time. This site in the Lenexa Pointe commercial center has remained undeveloped.

The north line of the center is adjacent to the Crescent Apartments. Immediately to the north of the site is a truck maneuver area and detention basin for the overall center. The parcel line for the self-storage facility and the north line of the commercial center is 113 feet.

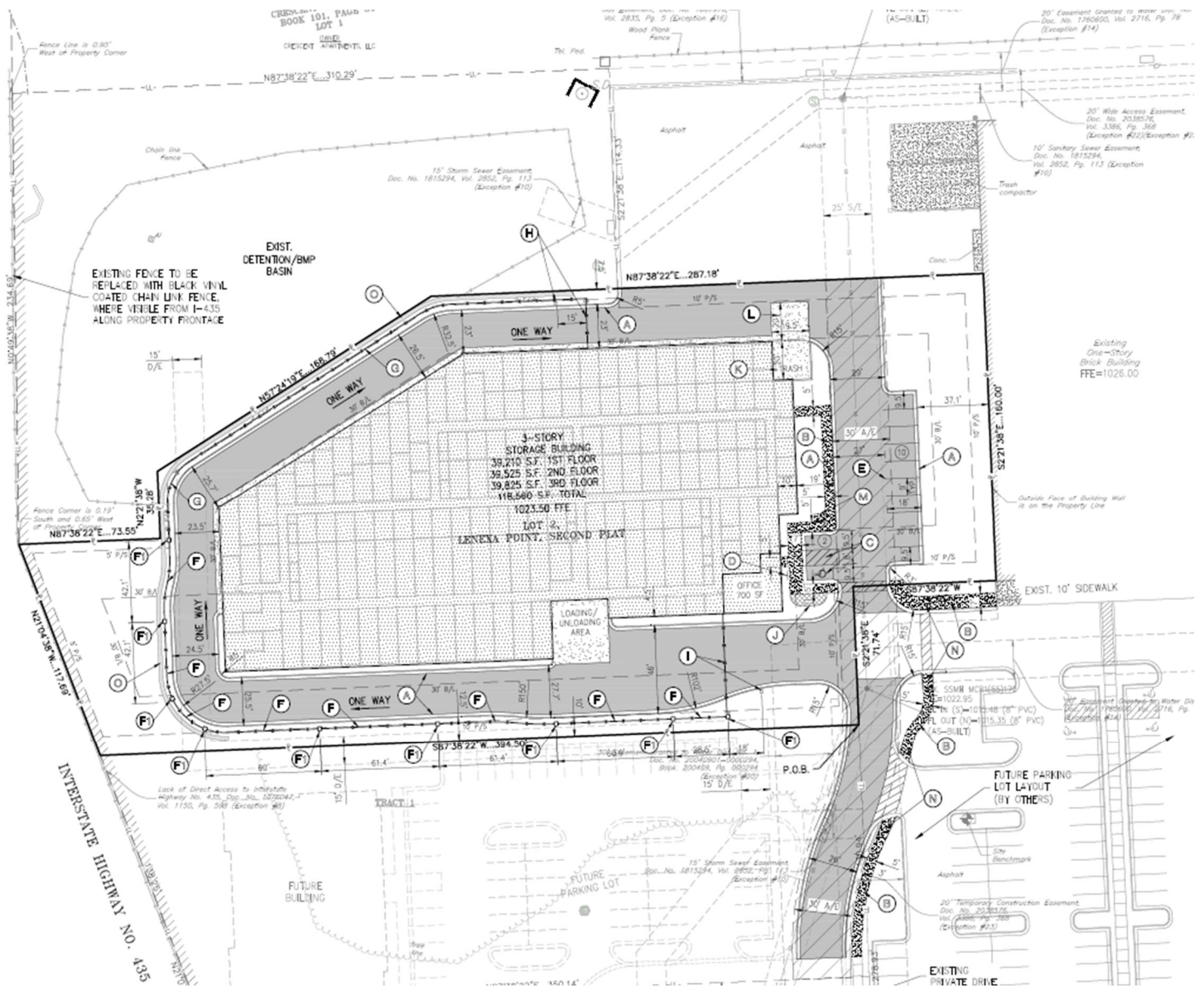


**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Future Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Community Retail	CP-2, Planned Community Commercial District	Commercial Center
<b>North</b>	High-Density Residential	RP-4, Residential Planned (High-Density) District	High-Density Residential
<b>South</b>	Community Retail	PUD, Planned Unit Development	Regional Commercial Center
<b>East</b>	High-Density Residential	RP-3, Residential Planned (Medium High-Density) District	High-Density Residential
<b>West</b>	City Center	CC, Planned City Center District	Undeveloped land

## FINAL PLAN REVIEW

The proposed building is three stories tall and 118,560 square feet in area, which is approximately 39,000 square feet per floor. A 12-stall parking area is located on the east side of the site. The building will have a wrought iron-style metal fence to secure access into and out of the facility. The facility will be accessible to customers 24 hours a day. A gate with security code controls will allow access to current customers of the facility. A 700 square-foot office is located on the southeasterly corner of the building. The office will have administrative activity where new customers can lease a bay and purchase packaging materials.



**Exhibit 2: Site Plan.**

## TRAFFIC, ACCESS, AND PARKING

- Internal site circulation is accomplished with a one-way paved drive aisle.
- Sidewalks are provided from the parking lot to the entrance into the building.
- Access to the site is from private drives within the Lenexa Pointe that have access out to Maurer Road.
- The plan includes reconstructing a drive aisle to align an internal intersection of the two main drive aisles of the site. The owner of the part of the center with the existing multi-tenant building and large parking lot will undergo a site renovation to reconstruct the parking lot converting the stalls from angled parking to 90-degree parking. This change will add more landscape islands to the parking lot and will greatly improve circulation. The existing parking lot is a mix of one-way and two-way circulation which creates confusion for visitors to the center. The parking lot renovation is anticipated to happen in a parallel track with the construction of the self-storage building.
- The site plan meets parking requirements.

**TABLE 2: PARKING ANALYSIS**

Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Self-service storage	1 space for every 75 bays	12	12	0

Self-service storage uses are considered a low trip generating use; therefore, the parking requirement is low relative to the overall size of the building. Parking for visitors and customers to this business is provide on the east side of the building. A total of 12 stalls are provided. The commercial center has a large parking field that serves the large multi-tenant building. Each lot will be under individual ownership, but it is anticipated that overflow parking may park in the large parking field if needed.

## STORMWATER

The overall stormwater management for this site has been previously constructed; however, some minor upgrades (basin grading/resizing) are currently proposed to appropriately accommodate this project.

## FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

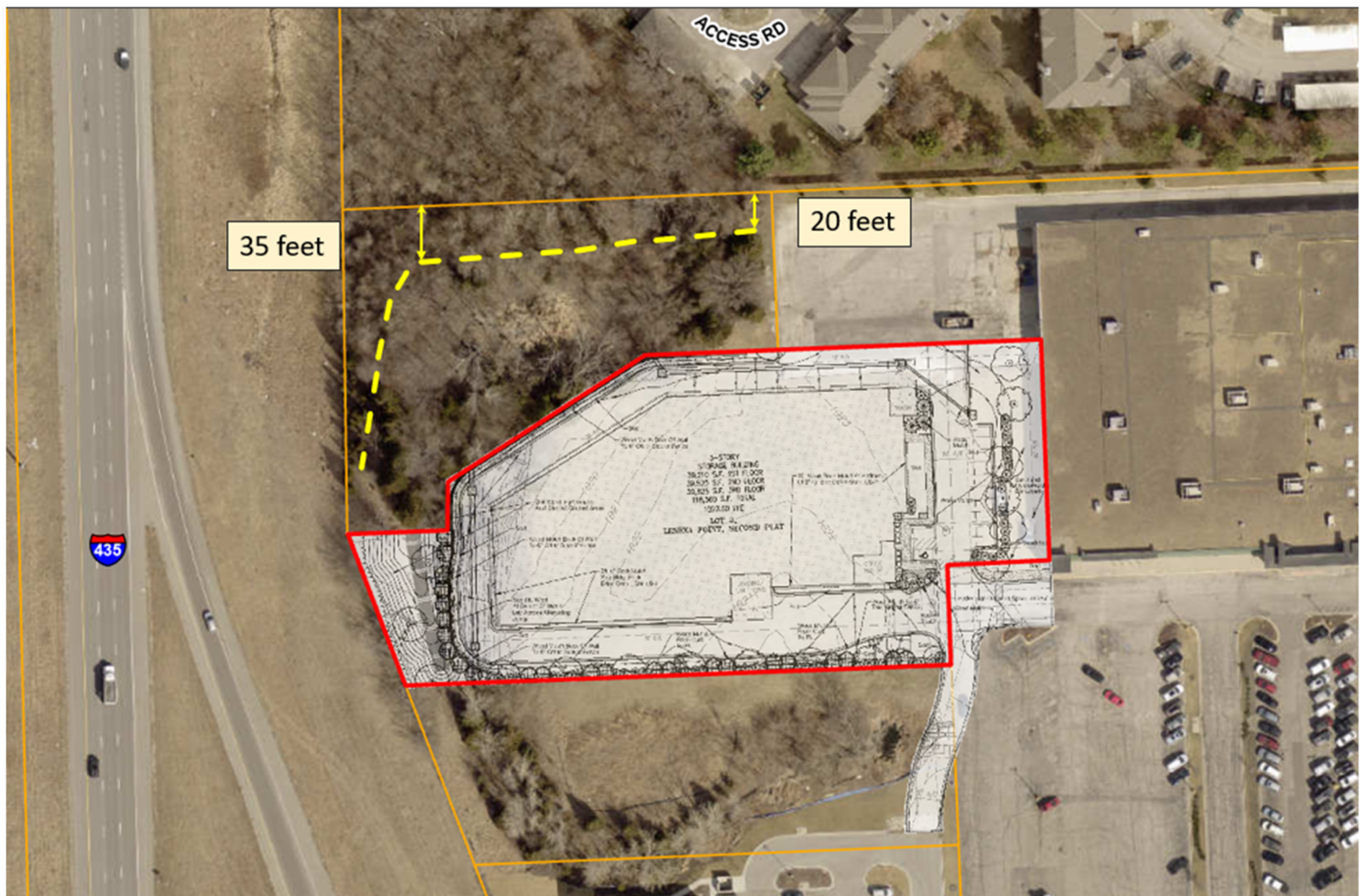
## LIGHTING

No site lighting is shown with the final plan. The regulations do not require that parking lot lighting be provided. The parking area for the site is small and the circulation drive around the building is a one-way paved drive aisle. Illuminating the parking and the areas around the building may be achieved with wall mounted light fixtures. All exterior lighting is subject to [Section 4-1-C-4-I](#) of the UDC. The requirements include fixture type and mounting height standards. Staff will review the building mounted light fixture details with the building permit submittal for compliance with the regulations.

**LANDSCAPING**

Landscaping will be installed around the perimeter of the lot. A line of trees and shrubs will be installed between the curb of the drive and the property line. Groupings of landscaping will be installed on the edge of the parking stalls on the east side of the building. The perimeter and parking lot landscaping complies with the requirements of [Section 4-1-D-2-L](#) of the UDC.

Grading will be done along the west and north part of the site to make modifications to the existing detention basin. The grading activity will remove some of the existing tree cover. Additional trees will be installed in areas where appropriate to install around a detention basin. A group of deciduous and evergreen trees will be installed on the north side of the building to offset the removal of some of the existing trees to provide additional screening. The trees along the northerly property line of the site will remain, this space ranges in width from 20 feet at the narrowest point to over 35 feet. The landscape plan complies with the site feature buffering and screening requirement of [Section 4-1-D-2-P](#) of the UDC.



**Exhibit 3: Tree Line to Remain.**



## ARCHITECTURE

The building is a self-storage use in a commercial center. This use is a nontraditional use in a commercial development but can be a complimentary use to the other retail and commercial businesses. The appearance of the building, looking more like an office or commercial building, is an important consideration for the building to harmonize with the commercial activity in the center. The building has been revised to include additional details and features commonly used for office and commercial buildings.

The building has multiple plane changes and articulations to reduce long facades of the same materials. Multiple tower elements are incorporated into the façade at key locations such as the office entrance at the corner of the southeast building, as well as the other corners of the building and at a main loading canopy where customers will access the building. The tower elements include banks of windows that help break up the façade and will add light into the corridor spaces of the building. Several windows are added on the south facing façade and used more strategically on the north and west façade. Windows are added to the tower elements on the east façade. Trim details where materials change, lintels above several window openings and geometric patterning of the stucco panels are used to break up the larger panels of materials. It is staff's opinion the building is designed appropriately to fit within the context of the Lenexa Pointe commercial center. The proposed building meets the architectural standards required in [Section 4-1-C-5](#) of the UDC. Staff is supportive of the proposed architecture.

The building will use brick, stucco, architectural metal siding, and composite wood siding as the primary exterior finish materials. The brick material is used primarily along the ground floor of the building and at tower features.



*Exhibit 4: Southeast Perspective Rendering.*



*Exhibit 5: Southwest Perspective Rendering.*

## DEVIATIONS

The applicant is not requesting any deviations.

## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

## RECOMMENDATION FROM PROFESSIONAL STAFF

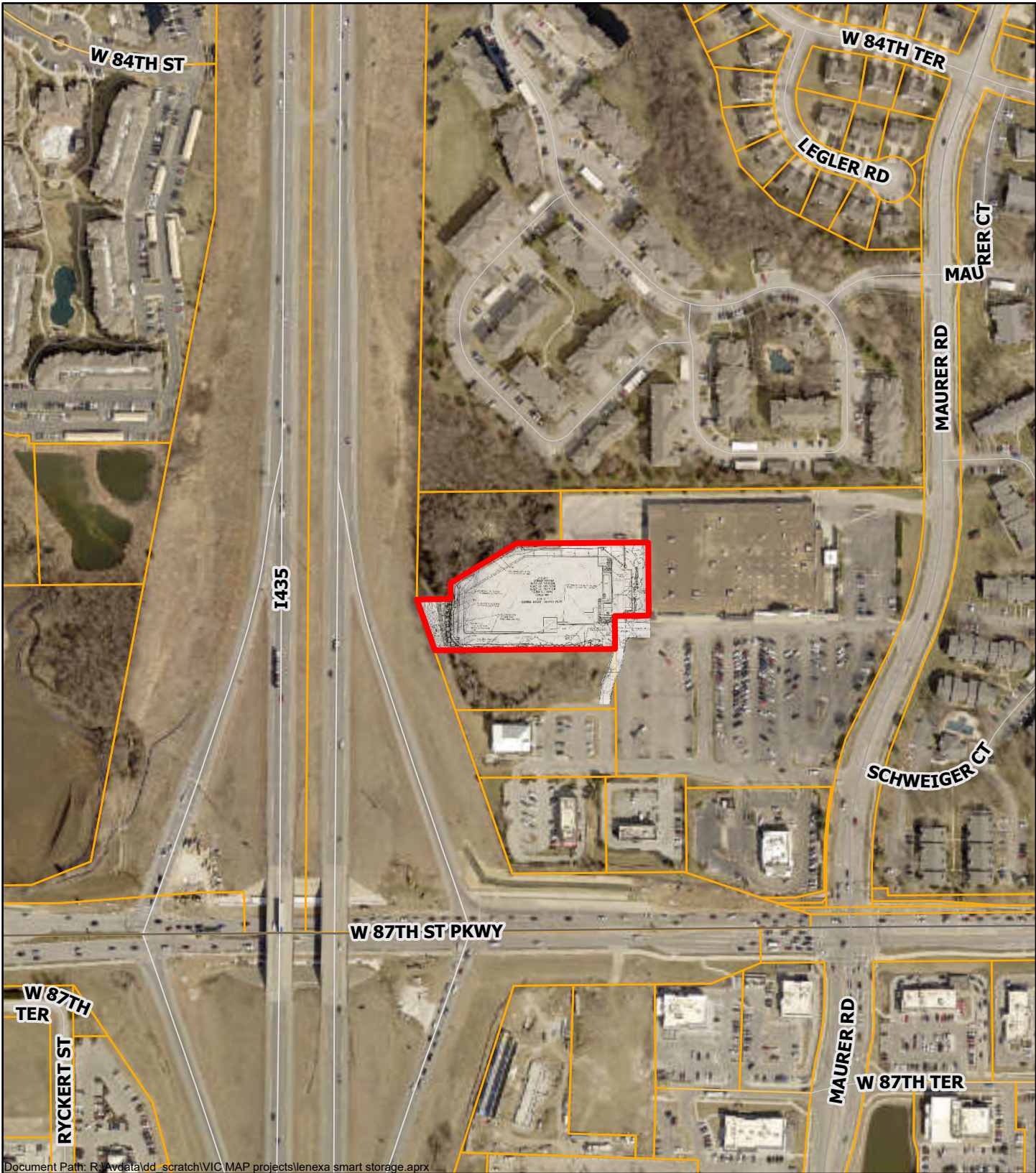
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★ **Staff recommends approval of the proposed Final Plan for Lenexa Smart Storage.**

- This project is consistent with Lenexa’s goals through *Responsible Economic Development* to promote *Vibrant Neighborhoods* and *Healthy People*.

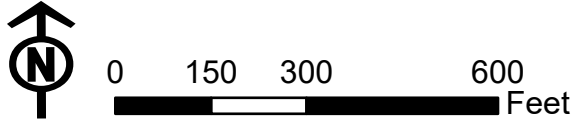
## FINAL PLAN

Staff recommends **approval** of the final plan for PL23-04F – **Lenexa Smart Storage** at 8630 Maurer Road, for a self-service storage use.

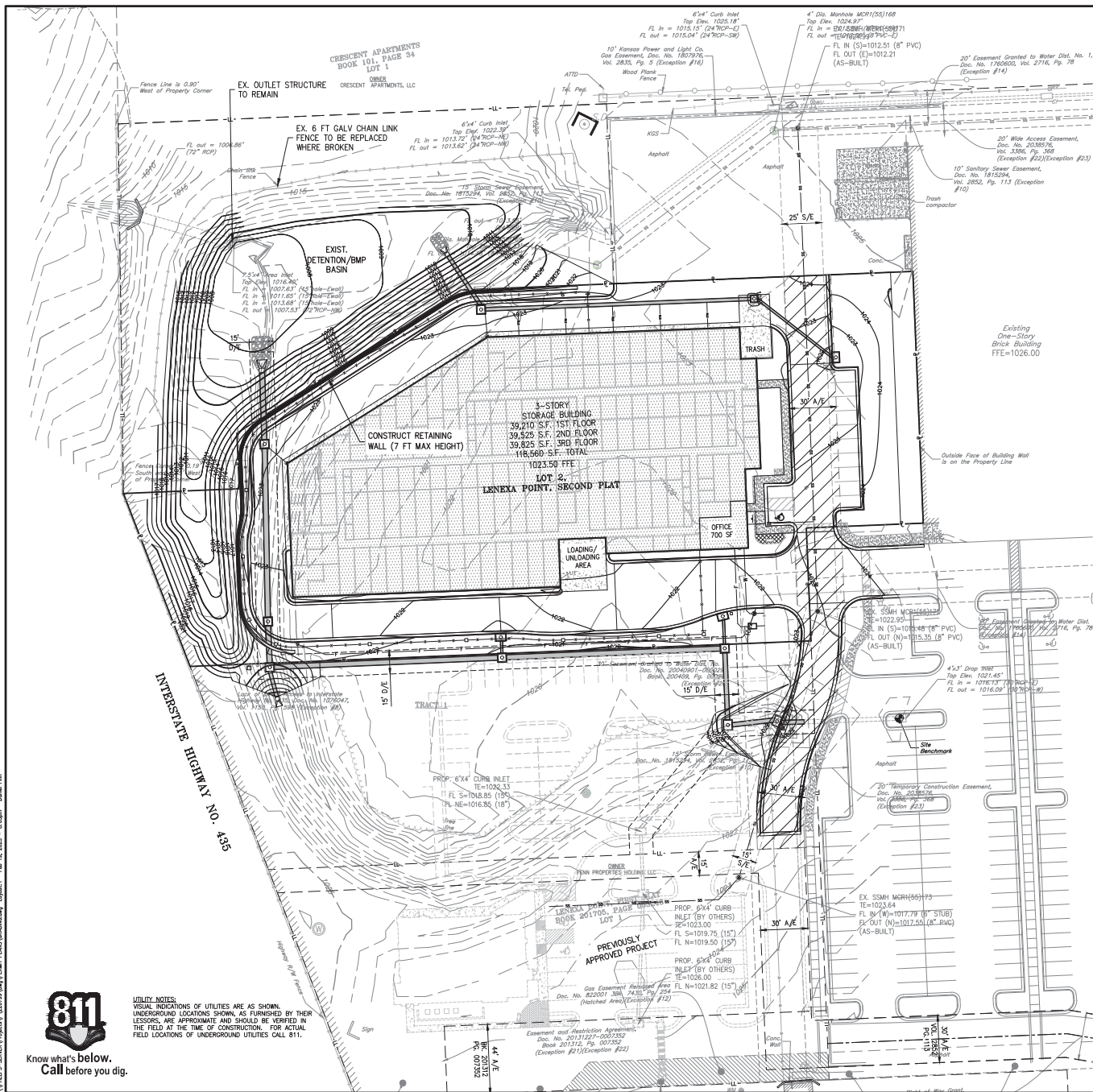


Data Source: City of Lenexa and Johnson County Kansas  
 For further information, please call 913-477-7500

# Lenexa Smart Storage PL23-04F







**SITE GRADING NOTES:**

- CONTOURS AND ELEVATIONS:** Existing and proposed contours are shown on plans at one foot (1') contour intervals, unless otherwise noted. Proposed contours and elevations show required approximate finish grade. Contractor shall hold down subgrade to allow for placement and sub-rose thicknesses.
- If the contractor does not accept existing topography as shown on the plans, without exception, he shall have made at his expense, a topographic survey by a registered land surveyor and submit it to the owner for review.
- CLEARING AND GRUBBING:** Prior to beginning preparation of subgrade, all areas under pavements or buildings shall be stripped of all topsoil, vegetation, large rock fragments (greater than 6 inches in any dimension) and any other deleterious material. The actual stripping depth should be based on most excavation during construction and the results of proof-rilling operations. The root systems of all trees (not designed to remain) shall be removed in their entirety. Stripping materials shall not be incorporated into structural fill.
- TOPSOIL STRIPPING:** Prior to the start of site grading, the contractor shall strip all topsoil from areas to be graded, and stockpiled at a location on or adjacent to the site as directed by the owner. All completion of grading operations and related construction, the contractor shall be responsible for redistribution of topsoil over all areas disturbed by the construction activities. Topsoil shall be placed to a minimum depth of six inches (6") and in accordance with specifications for landscaping. At that time, and prior to the initiation of landscaping or irrigation, all topsoil grade work shall be closely inspected and accepted by the owner and ILL.
- Contractor shall adjust and/or cut existing pavement as necessary to assure a smooth fit and continuous grade. Contractor shall ensure positive drainage away from building for all outdoor and paved areas.
- SUBGRADE PREPARATION:** Prior to placement of new fill material, the existing subgrade shall be protected and approved under the direction of the Geotechnical Engineer or his representative.
- PROOF-RILLING:** Subsequent to completion of stripping and over-excavation, all building and pavement areas to receive engineering fill should be proof-rilled using a tandem roller along truck tracks to approximately 20,000 pounds per inch. Also, any finished subgrade areas to receive paving shall be proof-rilled within 48 hours of paving. Unsuitable soils that are detected and that can not be recompacted should be over-excavated and replaced with controlled structural fill.
- EARTHWORK:**
  - GEOTECHNICAL:** All earthwork shall conform to the recommendations of the Geotechnical Engineer. Soil report and its recommendations are herein incorporated into the project requirements by reference. Prior to beginning construction, the contractor shall obtain a copy of and become familiar with the geotechnical report. Unless specifically noted on the plans, the recommendations in the geotechnical report are hereby incorporated into the project requirements and specifications.
  - SURFACE WATER:** Surface water shall be intercepted and diverted during the placement of fill.
  - FILLS:** All fills shall be considered controlled or structural fill and shall be free of vegetation, organic matter, topsoil and debris. In areas where the thickness of the engineered fill is greater than the total building and pavement construction should not commence until so authorized by the on-site geotechnical engineer to allow for consolidation.
  - BUILDING SUBGRADE:** As specified in the Geotechnical Engineering Report, the upper section of building subgrade shall consist of Low Volume Change (LVC) material defined as approved, compacted granular fill or low to moderate plasticity cohesive soil materials stabilized with Class C Flyash. Granular fill shall consist of compacted granular materials with a maximum particle size of two (2) inches or less, such as limestone screenings. Refer to geotechnical report for complete requirements.
  - EXISTING SLOPES:** Where fill material is to be placed on existing slopes greater than 5:1 (horizontal to vertical), existing slopes shall be bench graded providing a minimum vertical face of twelve inches (12"). The benches should be cut wide enough to accommodate the compacting equipment. Fill material shall be placed and compacted in horizontal lifts not exceeding nine inches (9") (loose fill measurement), unless otherwise approved by the Geotechnical Engineer.
  - COMPACTION REQUIREMENTS:** The upper 9" inches of pavement subgrade areas shall be compacted to a minimum density of ninety percent (90%) of the material's maximum dry density as determined by ASTM D698 (standard proctor compaction). The moisture content of the fill at placement and compaction shall be within a range of 0.5% below to 4% above optimum moisture content, as defined by the standard proctor compaction procedure. The moisture contents shall be maintained within this range until completion of the work. Where compaction of earth by a large roller is impractical or uneconomical, the earth fill shall be hand compacted with small vibrating rollers or mechanical tampers.
- All cut or fill slopes shall be 3:1 or flatter. All asphalt parking areas shall be a minimum of 1% slope but not more than 5% slope unless otherwise noted. All pavements within ADA parking areas shall not exceed 2% total slope. All grass around buildings shall be laid down 6" from finish floor and slope away another 6" in 10 feet. Contractor shall notify engineer prior to final subgrade construction of any areas not within this slope requirement.
- TESTING AND INSPECTION:** Owner's Independent Testing Laboratory (ITL) shall make tests of earthwork during construction and observe the placement of fill and other work performed on this project to verify that work has been completed in accordance with Geotechnical Engineering Report, Project Specifications and within industry standards. The ITL will be selected by the owner and the cost of testing will be the owner's responsibility.
- CLASSIFICATION:** All excavation shall be considered unclassified. No separate or additional payments shall be made for rock excavation.
- PERMANENT RESTORATION:** All areas disturbed by earthwork operations shall be sodded, unless shown otherwise by the landscaping plan or erosion control plan.
- UTILITIES:** The contractor is specifically cautioned that the location and/or elevation of existing utilities as shown on these plans is based on records of the various utility companies, and where possible, measurements taken in the field. The information is not to be relied on as being exact or complete. The contractor must call the appropriate utility companies at least 48 hours before any excavation to request exact field location of utilities. It shall be the responsibility of the contractor to relocate all existing utilities which conflict with the proposed improvements shown on the plans.
- LAND DISTURBANCE:** The contractor shall adhere to all terms & conditions as outlined in the EPA or applicable state N.P.D.E.s permit for storm water discharge associated with construction activities. Refer to project S.B.P.P.P. requirements.

**FLOOD NOTE:**

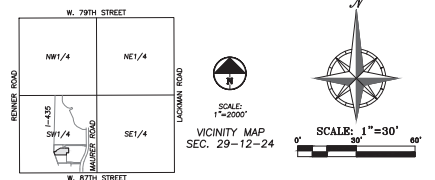
THIS PROPERTY IS WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF LENEXA, COMMUNITY NO. 200408 003 G, JOHNSON COUNTY, KANSAS, MAP NO. 20090100050G, AND DATED AUGUST 3, 2009.

**BENCHMARK:**

SITE BENCHMARK - "U" CUT ON WEST SIDE OF DROP INLET AT LOW POINT AT WEST SIDE OF SEARS GRAND PARKING LOT.  
 COORDINATES:  
 N: 24916.82  
 E: 222859.02  
 EL: 1021.50'

**LEGEND**

— PL	PROPERTY LINE
—	LOT LINE
— R/W —	RIGHT-OF-WAY
—	2" CURB & GUTTER
—	EXISTING CONTOURS
—	PROPOSED CONTOURS
PROPOSED SPOT ELEVATION	
LD	LP OF GUTTER
TC	TOP OF CURB
SE	SEWER
ME	MATCH EXISTING
HE	HEAD OF PAVEMENT
LP	LOW POINT
TP	TOP OF PAVEMENT
TE	TOP OF STRUCTURE
SE	GROUND ELEVATION
BS	BOTTOM OF STEPS
TS	TOP OF STEPS
SW	BOTTOM OF WALL
TM	TOP OF WALL



**GRADING PLAN**  
**LENEXA POINT STORAGE**  
**ADDRESS:**  
**LENEXA, JOHNSON COUNTY, KANSAS**

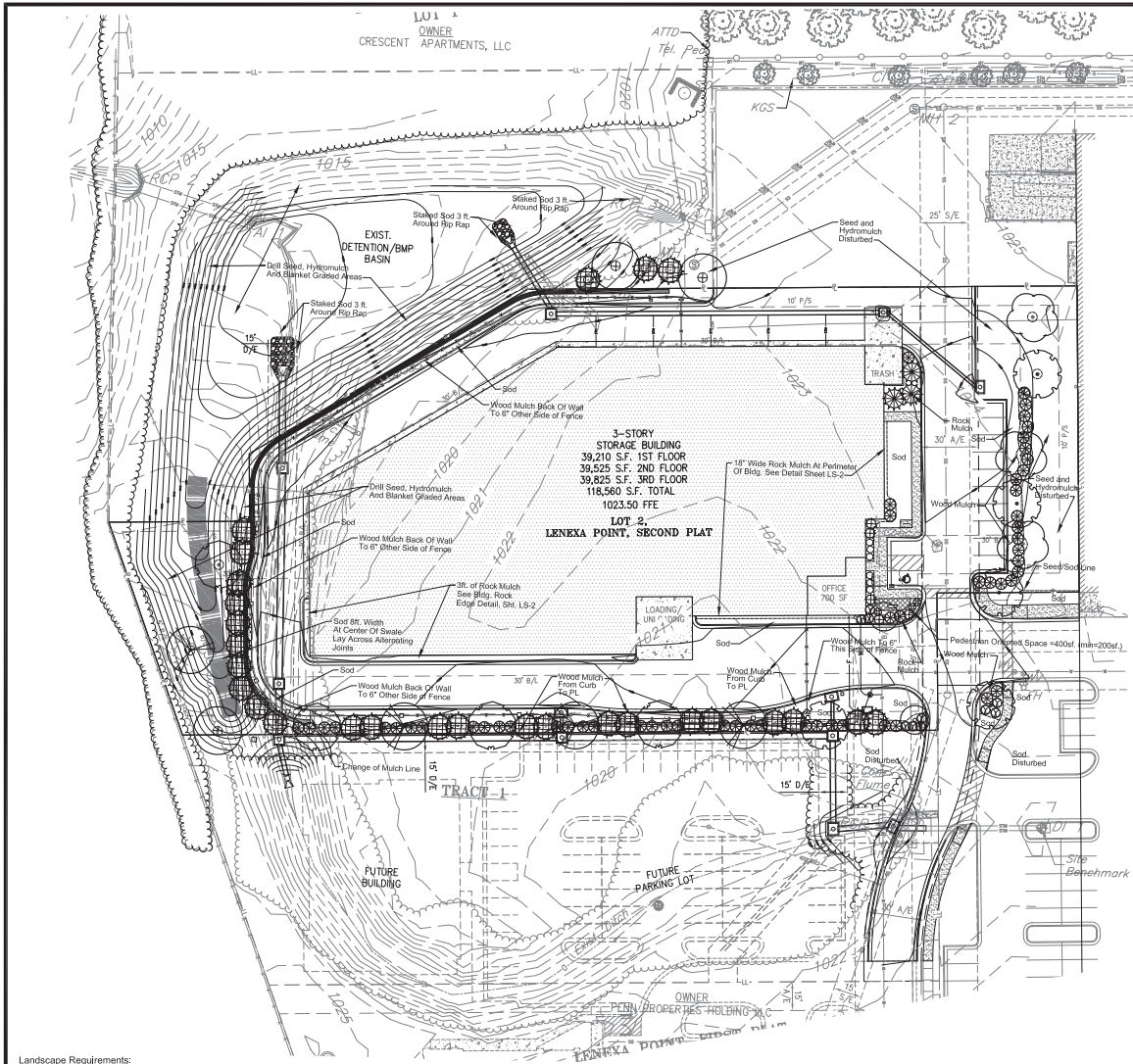
PROJECT NO. 220799  
DATE: 11-10-2013  
DRAWN BY: JAC  
CHECKED BY: JAC  
SCALE: 1"=30'

REVISIONS:

NO.	DATE	REVISIONS
1	11-10-2013	RELEASED FOR STATE COMMENTS

BY: JAC  
DATE: 11-10-2013

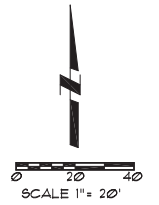
**SHEET**  
**C2**



**PLANT SCHEDULE**

TREES	QTY	BOTANICAL / COMMON NAME	CONT	CAL	SIZE
	5	Acer rubrum 'Red Pointe' / Red Pointe Red Maple	B & B		2" Cal
	2	Cercis canadensis / Oklahoma Redbud	B & B		1.5" Cal
	3	Gleditsia triacanthos 'Skyline' / 'Skyline' Honey Locust Thornless	B & B		2" Cal
	3	Gymnodium dioicus 'Espresso' / Kentucky Coffee Tree Seedless/Male Only	B & B		2" Cal
	3	Juniperus virginiana 'CanaertII' / CanaertII Juniper	B & B		6' hgt.
	27	Juniperus virginiana 'Hillspre' / Hillspre Juniper	B & B		6' hgt.
	4	Quercus bicolor / Swamp White Oak	B & B		2" Cal
	4	Quercus phellos / Willow Oak	B & B		2" Cal
SHRUBS	QTY	BOTANICAL / COMMON NAME	CONT		
	3	Calliopsis dichotoma 'Early Amethyst' / Beautyberry 18"-24" hgt. & sp.	3 gal		
	6	Hypericum frondosum 'Sunburst' / Sunburst Hypericum 18"-24" hgt. & sp.	2 gal		
	57	Juniperus chinensis 'Sea Green' / Sea Green Juniper 24"-30" hgt. & sp.	5 gal		
	5	Juniperus virginiana 'Grey Owl' / Grey Owl Juniper 24" sp.	3 gal		
	10	Physocarpus opulifolius 'Center Glow' / Center Glow Ninebark 24"-30" hgt. & sp.	3 gal		

NOTE: SEE SHEET LS-2 FOR CONSTRUCTION DETAILS AND SPECIFICATION NOTES.



Landscape Requirements:

Perimeter Planting Along I-435 / 100' (75')	1.5 (2)	Provided 2
2 shade		
Continuous Shrubs @ Parking	Evergreen Trees at Top of Slope In Lieu of Shrubs	
Evergreen Trees 75% screen		9
South Property Line / 100' (243')		
1 Shade	2.43	Provided 6
9 shrubs	21.87(23)	32
Evergreens 50% screen		12

Buffer  
 Commercial (LUI 8) vs. Apartments 16+ ulac. (LUI 7)  
 LUI difference of 1  
 Existing trees and wooded area along the westerly 2/3rds of the property boundary and easterly 1/3 has existing paved truck maneuvering space between site and the north line of the existing shopping center.

Utility Note:  
 Utilities shown on plan are diagrammatic and some may be missing. Before starting any construction call appropriate locating services. In Kansas call 1-800-DIG-SAFE (344-7233) to have utilities located.

Notes:  
 Existing trees along the north and west sides of the site will be removed as needed to accommodate construction but landscape requirements will be met.

Existing Wooded Areas  
 Proposed Wooded Areas

Note On Existing Wooded Area:  
 The majority of trees in the wooded area are small species such as Gray Dogwood, Smooth Sumac, some Redbud and a few other species. There are a lot less larger tree species such as Hackberry, Elms, Walnut, Mulberry, Honeylocust and Cottonwoods. The Cottonwoods are pretty much in the lower portions of the swale leading to the larger detention basin with most of the other larger tree species on the side slopes or edges of the wooded area.



**Landscape Plan  
 Lenexa Smart  
 Storage**  
 Lenexa, Kansas

**Oppermann LandDesign, LLC**  
 Land Planning • Landscape Architecture  
 99 Datas Lane, Lenexa, Kansas 66157  
 New Windsor, New York 12553 • 913.592.5598  
 brett@oppermans.com

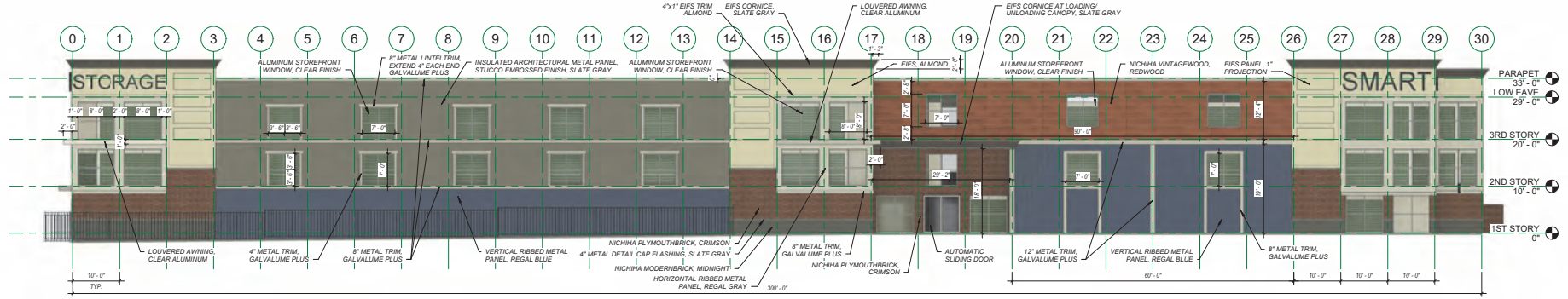
02/20/2023

LS-1

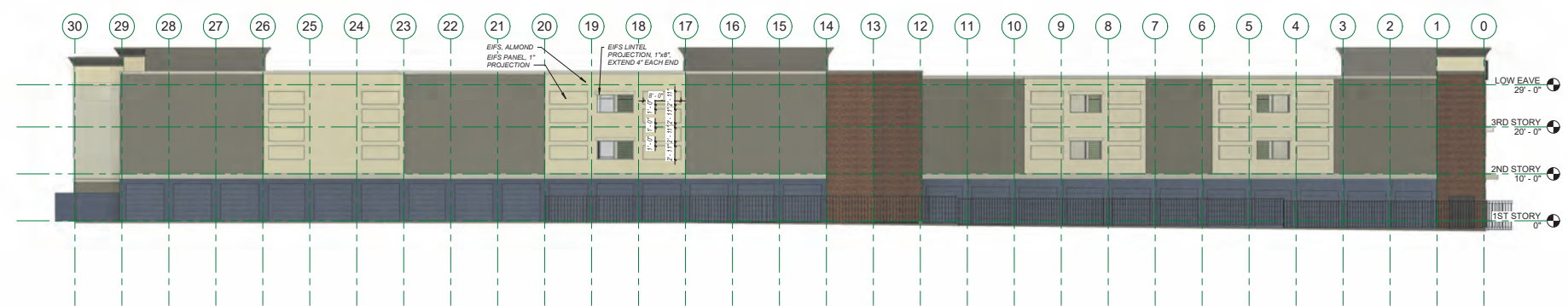


**1 EAST ELEVATION**  
3/32" = 1'-0"

**2 WEST ELEVATION**  
3/32" = 1'-0"



**3 SOUTH ELEVATION**  
3/32" = 1'-0"



**4 NORTH ELEVATION**  
3/32" = 1'-0"

**LENEXA SMART STORAGE**  
 87TH & I-435  
 LENEXA, KS



1100 Rhode Island St.  
Lenexa, Kansas 66344  
785-749-5806

ELEVATIONS	
Date:	2023-02-19
Drawn by:	Author
Checked by:	
Revisions:	

**A2.0**



1 SW VIEW



2 SE VIEW



3 NW VIEW

LENEXA SMART STORAGE  
 87TH & I-435  
 LENEXA, KS

**Herny**  
ARCHITECTURE  
 ARCHITECTS  
 1100 Rhode Island St.  
 Lawrence, Kansas 66044  
 785 - 749 - 5806

PERSPECTIVES

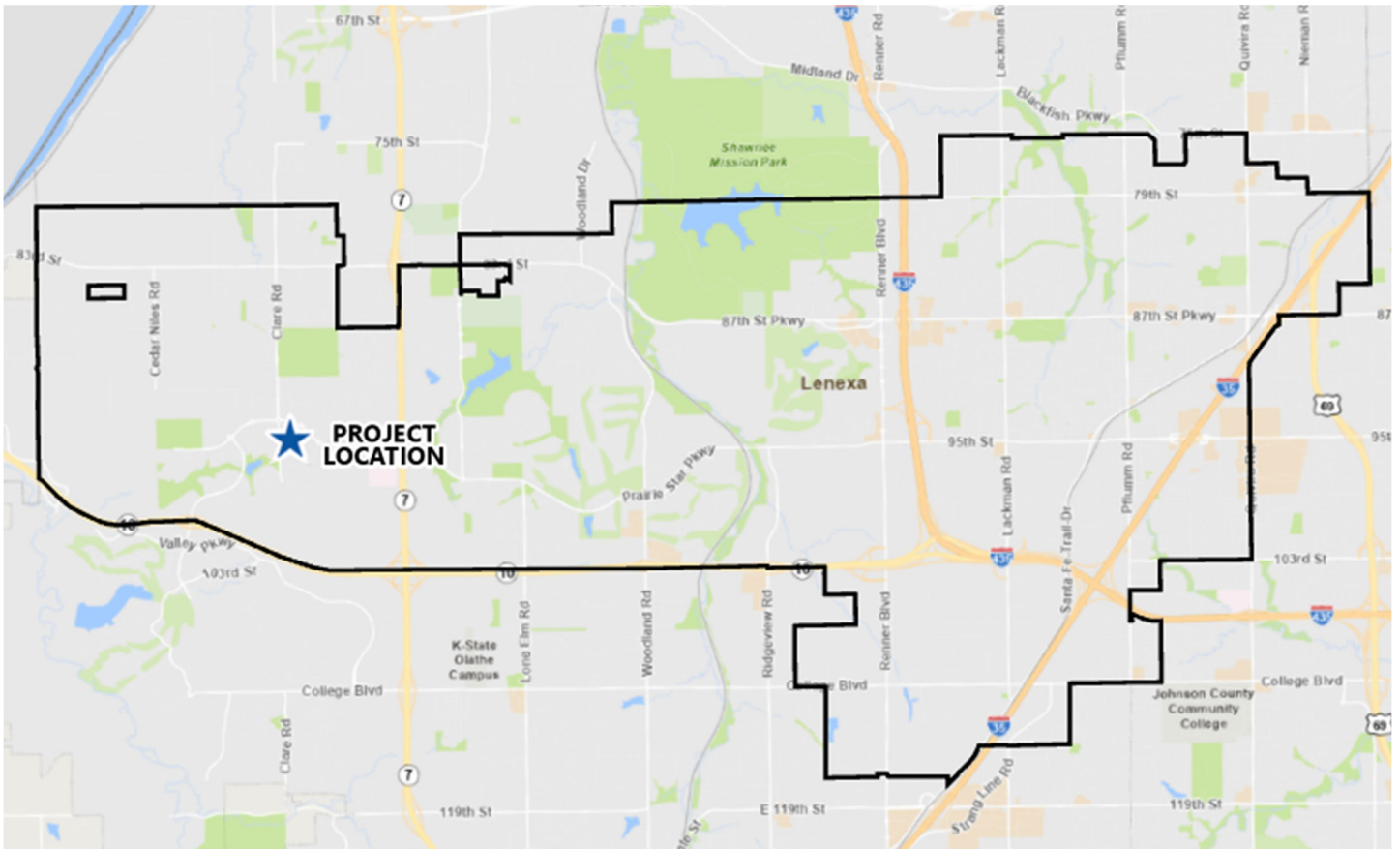
Date: 2023-02-19  
 Drawn by: Author  
 Checked by:  
 Revision:

A7.0



# ST. JAMES ACADEMY MULTI-PURPOSE BUILDING

<b>Project #:</b>	PL23-02FR	<b>Location:</b>	24505 Prairie Star Parkway
<b>Applicant:</b>	Andy Tylicki, St. James Academy	<b>Project Type:</b>	Revised Final Plan
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	High School Multi-Purpose Building



## PROJECT SUMMARY

The applicant proposes to construct a one-story multi-purpose building at the St. James Academy high school campus. The campus is located at the southeast quadrant of the intersection of Prairie Star Parkway and Clare Road. The building will primarily be used as an auxiliary gymnasium. The building is 12,300 square feet. This final plan is the first phase of building. A future phase will approximately double the size of the building for a total of 23,922 square feet.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

The St. James high school campus is a 44-acre site located at the southeast corner of Prairie Star Parkway and Clare Road. The campus started construction in 2009 with the main high school building. Incremental additions have been made to the campus since the school was originally built. The site has access from Clare Road and from Prairie Star Parkway.

<b>LAND AREA (AC)</b> 43.8	<b>BUILDING AREA (SF)</b> 13,800 (new bldg.)	<b>CURRENT ZONING</b> R-1	<b>COMP. PLAN</b> Public and Open Space
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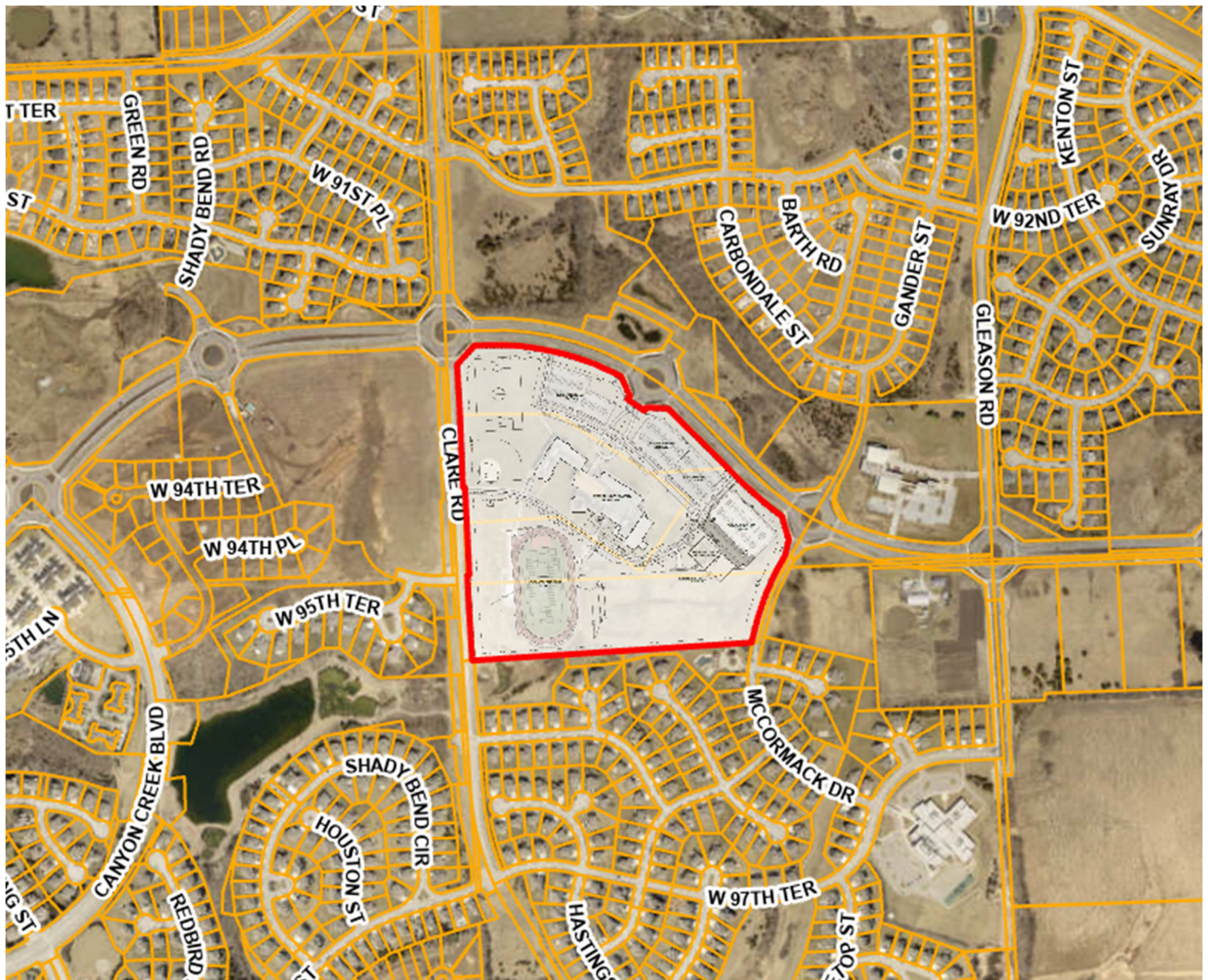
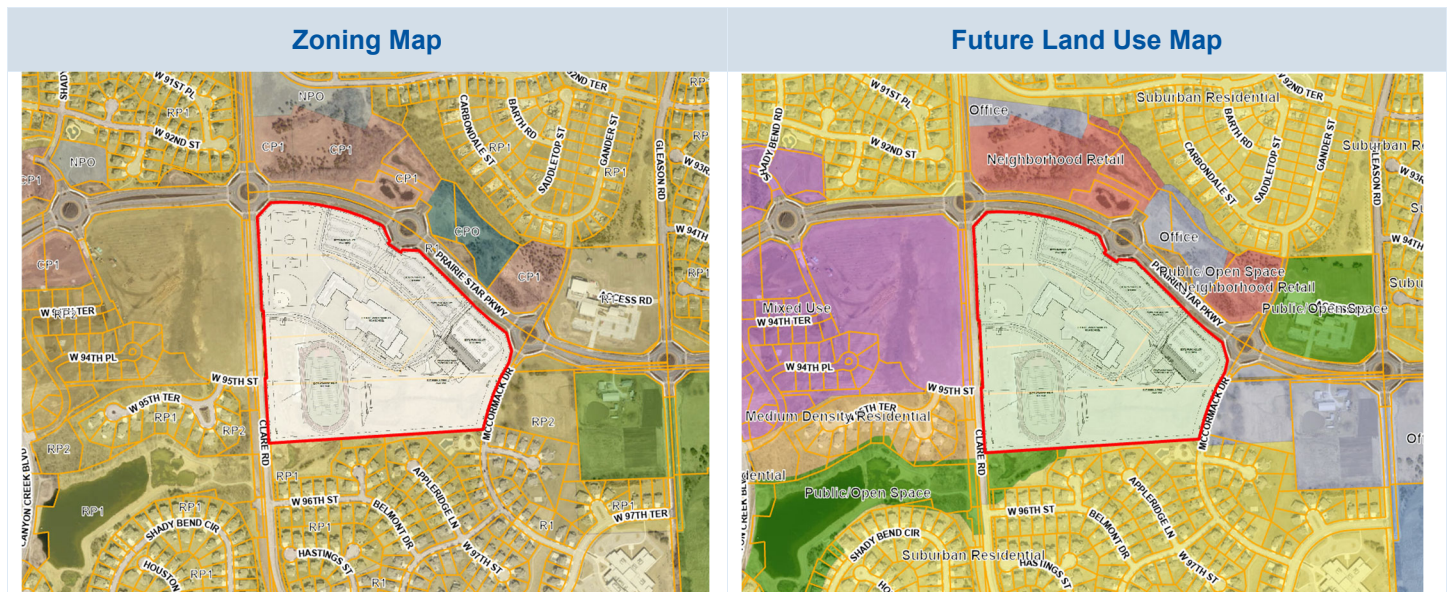


Exhibit 1: Vicinity Aerial Image of Subject Site

## LAND USE REVIEW

The proposed multi-purpose building is an accessory building for the St. James Academy high school campus. A public or private school is permitted in the R-1 Zoning District by special use permit (SUP). The initial preliminary plan for the campus was approved in the spring of 2003. A permanent SUP and preliminary plan were approved in the spring of 2003 (SU03-02, PL03-01). A revised preliminary plan was approved in 2009 (PL09-01PR) and the school begun development the same year. The development of the campus has proceeded in phases. Several final plans have been approved for the campus since the 2009, subsequent final plan submittals are anticipated until such time the campus is completed.



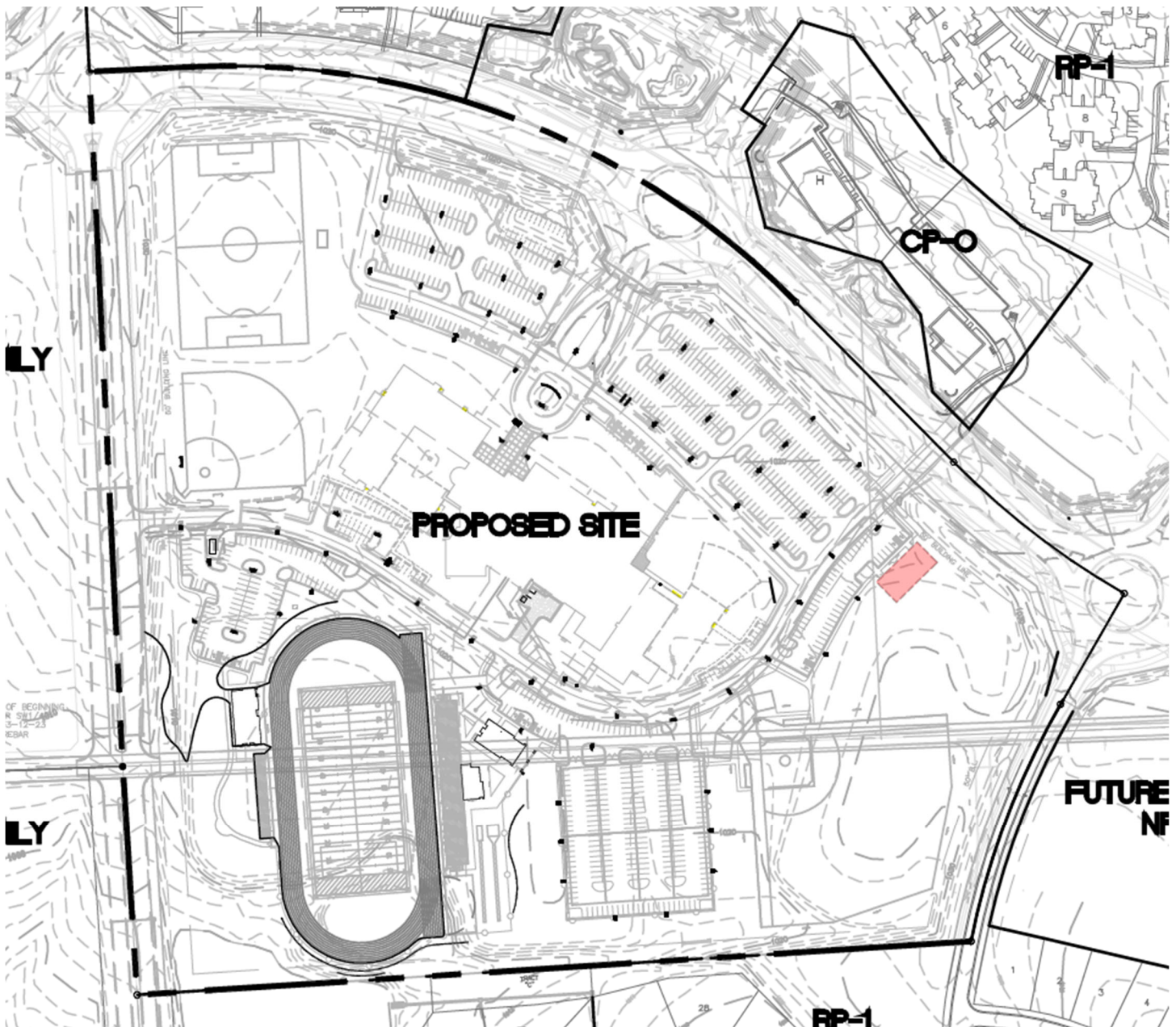
**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Future Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Public and Open Space	R-1, Residential Single-Family District	Private school
<b>North</b>	Neighborhood Retail, and Office/Employment Center	CP-1, Planned Neighborhood Commercial District, and CP-O, Planned General Office District	Undeveloped land
<b>South</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low-Density) District	Single-Family Residential subdivision
<b>East</b>	Office/Employment Center	RP-2, Residential Planned (Intermediate-Density) District	Undeveloped land
<b>West</b>	Mixed Use	RP-2, Residential Planned (Intermediate-Density) District	Two-Family Residential subdivision (under construction)

## FINAL PLAN REVIEW

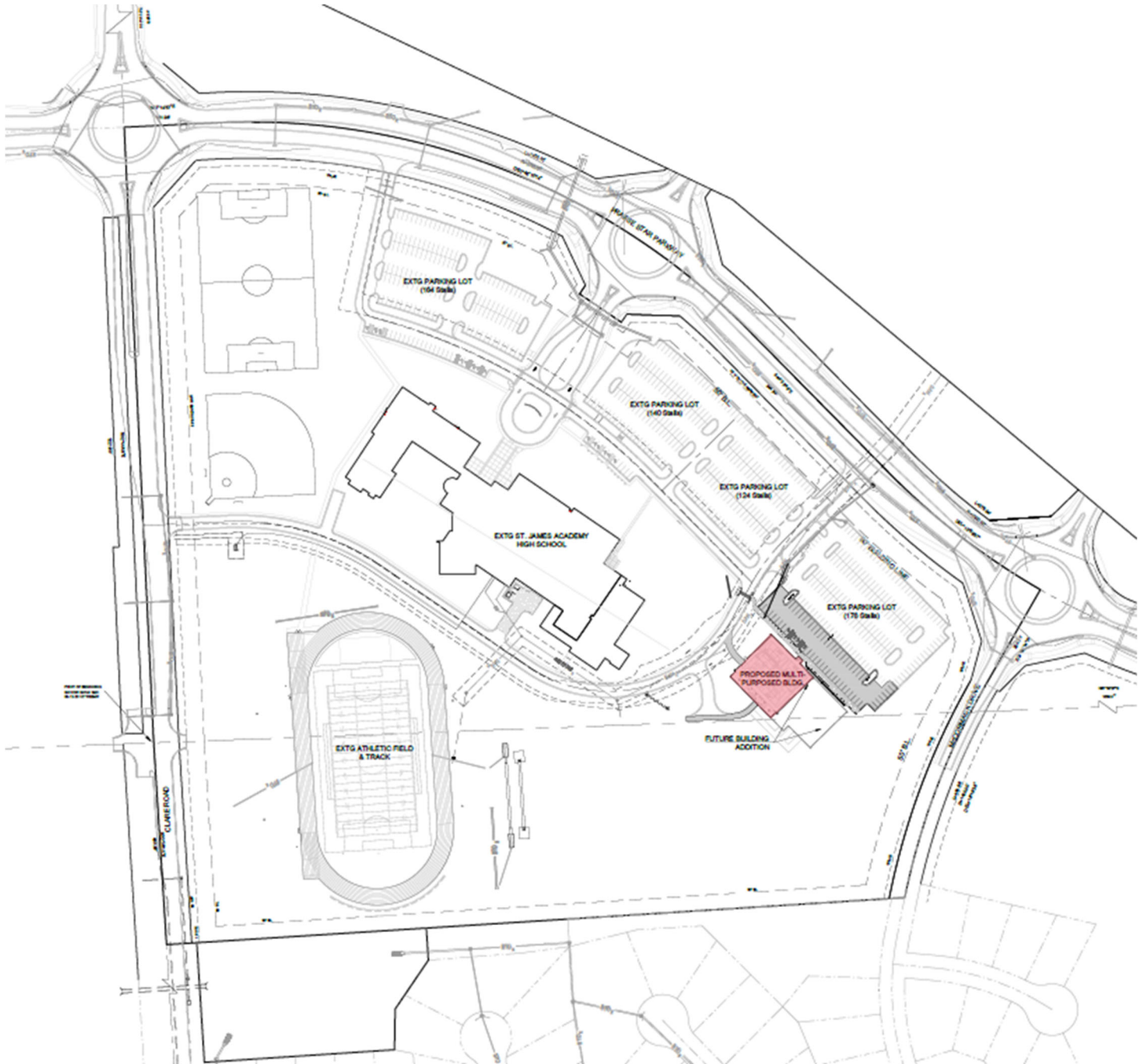
The applicant is requesting final plan approval for a new multi-purpose building at the St. James Academy high school campus. The campus includes a main high school building, parking areas, a football and track stadium, and sports fields. The proposed building is located at the northeasterly corner of the site. The building will be located at the southerly edge of a parking area. The parking area will be expanded to provide additional parking as the number of students has gradually increased in the years since the school has opened.

The location of the proposed building is consistent with the approved preliminary plan for the campus, while not shown the exact location and is proposed to be larger, the building is in the general vicinity.



**Exhibit 2: 2009 Approved Preliminary Plan**

The proposed location of multi-purpose building will result in modifications to the placement of future sports fields, future parking areas, and stormwater treatment features. Some variation in the exact locations of these features is expected as the campus continues to develop. The variations of the plan do not exceed the threshold of a substantial change that will require a new preliminary plan. Each phase of any development is analyzed to determine if a new preliminary plan is required.



**Exhibit 3: Proposed Site Plan.**

**TRAFFIC, ACCESS, AND PARKING**

- Prairie Star Parkway and Clare Road are classified as Major Arterial roadways.
- The site plan meets parking requirements.

<b>TABLE 3: PARKING ANALYSIS</b>				
<b>Land Use</b>	<b>Parking Formula</b>	<b>Required Parking</b>	<b>Proposed Parking</b>	<b>Difference</b>
School, high	0.50 spaces for every student and 1 space for every employee	576	667	91 Additional spaces

Access into the site is from Prairie Star Parkway and from Clare Road. A primary drive crosses the site on the south side of the building that allows traffic to circulate out to Clare Road from the large parking fields on the north site of the site. This drive is a fire lane and shall remain unobstructed. Vehicles are not allowed to park along the drive.

The final plan includes an expansion to the parking lot on the easterly side of the campus. This additional parking will provide spaces to accommodate the vehicles currently parking along the drive aisles and on gravel spaces. As the campus continues to develop, more parking areas will be constructed to accommodate the expected vehicles from students and visitors.

Parking for all students as the number of students gradually increases over time is a challenge for the school. Staff has observed several vehicles, approximately 20 to 30, parking in areas that are not designed for parking. The areas include parallel parking along the drives around the main parking fields on the north side of the building and in gravel areas along the drive aisle on the south side of the building. Parallel parking in the drive aisles and parking on gravel are not allowed. The parking issues on the St. James campus is a separate issue from the consideration of plan the final plan for this multi-purpose building and will be resolved by coordination with the appropriate parties that are responsible for the management of the school campus.

**STORMWATER**

The shared wetland facility on the south side of the property must be completed/constructed prior to any additional permits for the St. James campus being issued. Staff notes that the applicant is currently in the permitting process for the shared wetland construction to complete this requirement. Although there remain a few small stormwater facilities to be constructed with other future campus improvements, the necessary stormwater facilities for this proposed component of the campus will be in place with the completion of the shared wetland.

**FIRE PREVENTION**

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

## LIGHTING

The parking lot addition will include parking lot light fixtures to be installed on the existing poles. The photometric plan is in compliance with the performance standards of [Section 4-1-C-4-I](#) of the UDC.

## LANDSCAPING

As this campus has developed, landscaping has been installed in phases along the streets and throughout the site. The undeveloped areas of the campus are not landscaped. These areas are along the southeasterly part of the site.

This phase of the campus includes landscaping installed along McCormack Street and landscaping installed on the west side of the building. The landscaping along McCormack Street is perimeter landscaping. The groupings of trees and shrubs will be installed on berms that are consistent with the landscaping berms around the campus.

The landscaping meets the perimeter planting requirements of [Section 4-1-D-2-L](#) of the UDC. The proposed landscape plan also meets the standards for street trees, internal parking lot landscaping, and conforms to the internal parking lot requirements.

## ARCHITECTURE

The exterior finish materials for the building are brick, cast stone, and metal siding. The brick is the same palette of colors used for the high school. The high school also uses architectural metal panels as an accent material on multiple facades of the building. The metal materials on the multi-purpose building will reflect this mix of materials. The metal materials on the east and west facades show a random application of the St. James shield logo applied onto the wall. The graphic adds an interesting pattern and provides texture to the two façades of the building. Staff is supportive of the insertion of this type of detail used sparingly on the building. The applicant does not have a timeline for the future phase to expand the building. The exterior finishes are consistent on all sides of the building. The east façade will match the west façade.

The main entrance to the building is a vestibule entrance on the south side of the building, facing the parking lot. The building will have a glass overhead door on the west façade to allow larger equipment to be moved into and out of the building. A screened mechanical court is on the south side of the building that will house air handling equipment. The mechanical court is a brick wall space to match the wall of the building. The proposed building meets the architectural standards required in [Section 4-1-C-5](#) of the UDC. Staff is supportive of the proposed architecture.



Exhibit 4: Southwest Perspective Rendering



Exhibit 5: Northwest Perspective Rendering.

## DEVIATIONS

The applicant is not requesting any deviations.



## REVIEW PROCESS

---

- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

## RECOMMENDATION FROM PROFESSIONAL STAFF

---

- ★ **Staff recommends approval of the proposed Final Plan for St. James Academy Multi-purpose Building.**
  - This project is consistent with Lenexa’s goals to promote *Vibrant Neighborhoods* and *Healthy People*.

## FINAL PLAN

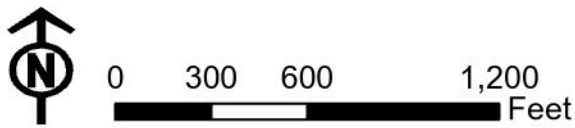
Staff recommends **approval** of the final plan for PL23-02FR – **St. James Academy Multi-Purpose Building** at 24505 Prairie Star Parkway, for an accessory building for a private school.



Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# St. James Academy Multipurpose Building

## PL23-02FR



SITE DATA TABLE (LOT 1)		
LOT AREA:	1,907,217 SQ. FT. (43.78 AC.)	
	EXISTING	PROPOSED
BUILDING AREA*	106,500 S.F. (5.58%)	120,300 S.F. (6.31%)
PAVEMENT / DRIVE AREA	406,766 S.F. (21.33%)	429,537 S.F. (22.52%)
OPEN / LANDSCAPE AREA	1,393,951 S.F. (73.09%)	1,357,380 S.F. (71.17%)

**\* Gross Building Footprint Area**

**Site Information:**

**Legal Description:**

Lot 1, Saint James Academy, a subdivision in the City of Lenexa, Johnson County, Kansas

Lot Area: 1,907,217 Square Feet (43.78 Ac.)

Existing Floor Area Ratio (F.A.R.) = 0.056

Proposed Floor Area Ratio (F.A.R.) = 0.063

Current Zoning: R-1(Planned Unit Development)

Proposed Zoning: R-1 (Planned Unit Development) - No Change

Current Use: Education - High School

Proposed Use: Education - High School

**Parking Summary:**

Current No. of Students: 840 Students

Required Parking Rate: 0.5 Spaces per Student

Current required student parking: 420 Stalls

Maximum No. of Students: 950 Students

Required Parking Rate: 0.5 Space per Student

Maximum required student parking: 475 Stalls

No. of Staff/Administration: 101 Full/Part Time

Required Parking Rate: 1 Space per Ea. Staff

Required staff parking: 101 Stalls

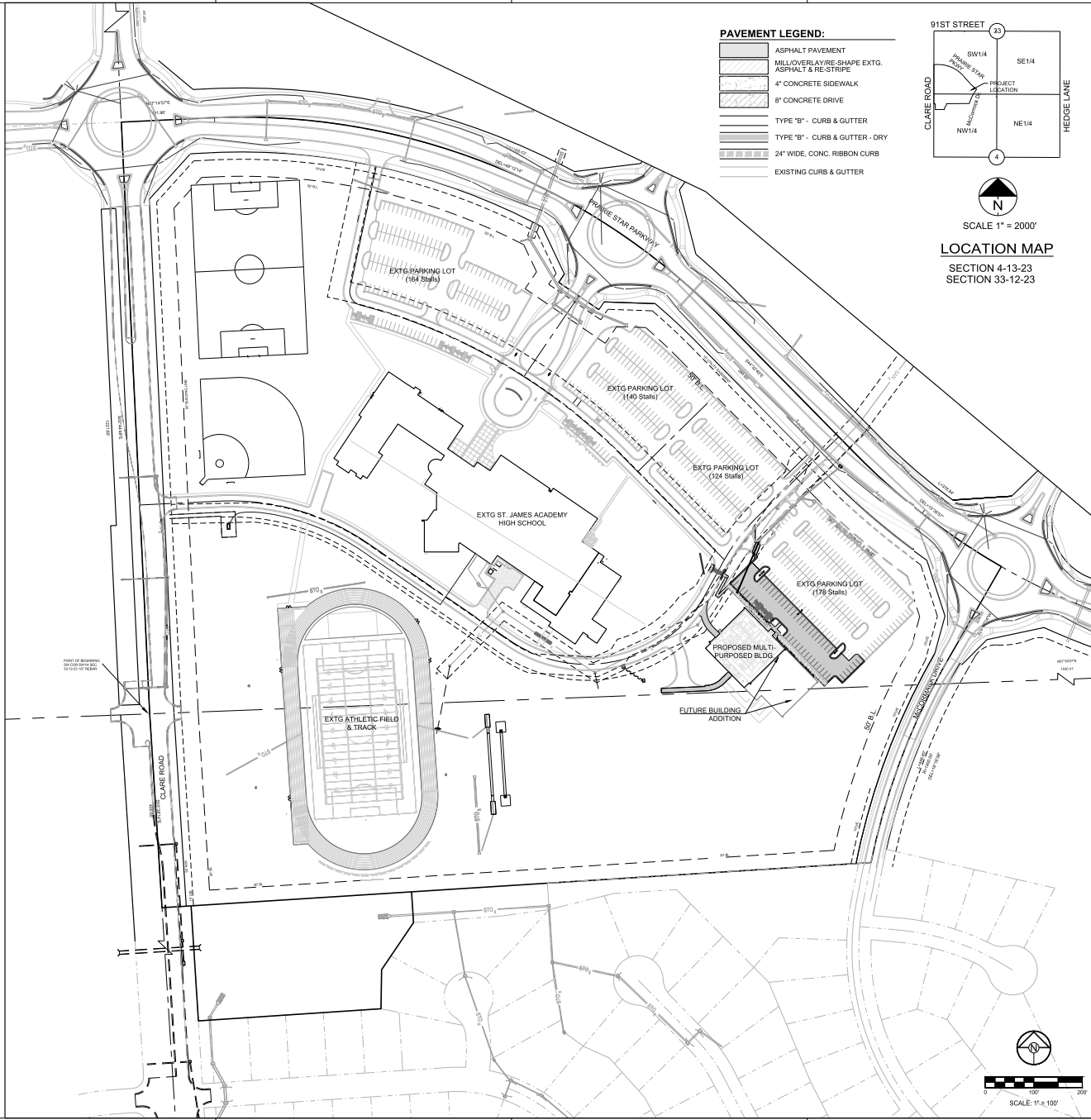
Current Required Parking Total: 521 Stalls

Maximum Required Parking total (Future): 575 Stalls

Existing Number of Parking Stalls: 608 Stalls

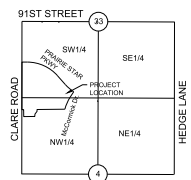
Proposed Number of Parking Stalls: 57 Stalls

Total Number of Parking Stalls: 663 Stalls



**PAVEMENT LEGEND:**

[Symbol]	ASPHALT PAVEMENT
[Symbol]	MIL/OVERLAY/RE-SHAPE EXTG. ASPHALT & RE-STRIPED
[Symbol]	4" CONCRETE SIDEWALK
[Symbol]	8" CONCRETE DRIVE
[Symbol]	TYPE "B" - CURB & GUTTER
[Symbol]	TYPE "B" - CURB & GUTTER - DRY
[Symbol]	24" WIDE, CONC. RIBBON CURB
[Symbol]	EXISTING CURB & GUTTER

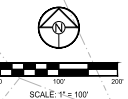


SCALE 1" = 2000'

**LOCATION MAP**

SECTION 4-13-23

SECTION 33-12-23



**ibs incite Design Studio**  
Architect  
INCITE DESIGN STUDIO  
110 WEST 18TH STREET  
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Civil  
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14020 W 107TH STREET  
LENEXA, KS 66215  
913.492.5158

Structural  
BOB D. CAMPBELL & CO.  
4338 BELLEVUE AVENUE  
KANSAS CITY, MO 64111  
816.231.1444

Mech, Plumb, Elec, Telecom.  
SMITH AND BEUCHER, INC.  
29610 W 103RD STREET  
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Cert. of Authority  
Kansas #E-2959L-S-54  
Schlagel Associates, P.A.

**ST. JAMES MULTIPURPOSE BUILDING**

24505 PRAIRIE STAR PKWY  
LENEXA, KS 66227

Project Phase  
**FINAL DEVELOPMENT PLAN**

Project Number  
21-115

Issue Date  
01/30/2023

Revision No.	Description	Date Issued

**Key Plan**

Sheet Name  
**OVERALL MASTER SITE PLAN**

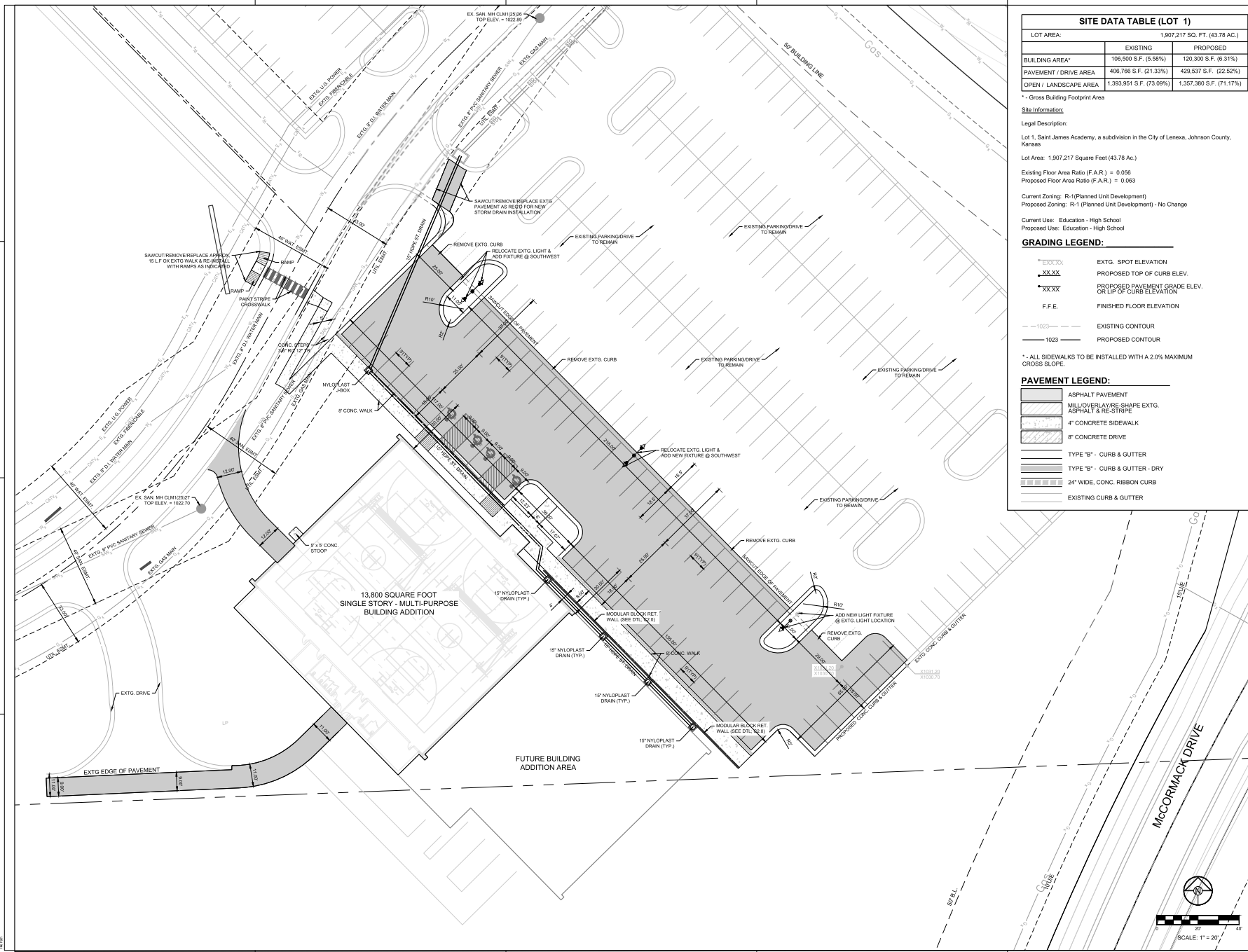
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SITE DATA TABLE (LOT 1)		
LOT AREA:	1,907,217 SQ. FT. (43.78 AC.)	
	EXISTING	PROPOSED
BUILDING AREA*	106,500 S.F. (5.58%)	120,300 S.F. (6.31%)
PAVEMENT / DRIVE AREA	406,766 S.F. (21.33%)	429,537 S.F. (22.52%)
OPEN / LANDSCAPE AREA	1,393,851 S.F. (73.09%)	1,357,380 S.F. (71.17%)

\* - Gross Building Footprint Area

**Site Information:**

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Proposed Floor Area Ratio (F.A.R.) = 0.063

**Current Zoning:** R-1 (Planned Unit Development) - No Change

**Proposed Zoning:** R-1 (Planned Unit Development) - No Change

**Current Use:** Education - High School

**Proposed Use:** Education - High School

- GRADING LEGEND:**
- XXXXXX EXGT. SPOT ELEVATION
  - XX XX PROPOSED TOP OF CURB ELEV.
  - XX XX PROPOSED PAVEMENT GRADE ELEV. OR LIP OF CURB ELEVATION
  - F.F.E. FINISHED FLOOR ELEVATION
  - 1023--- EXISTING CONTOUR
  - 1023— PROPOSED CONTOUR
- \* - ALL SIDEWALKS TO BE INSTALLED WITH A 2.0% MAXIMUM CROSS SLOPE.

- PAVEMENT LEGEND:**
- ASPHALT PAVEMENT
  - MILL COVERLAY/RE-SHAPE EXGT. ASPHALT & RE-STRIPPE
  - 4" CONCRETE SIDEWALK
  - 8" CONCRETE DRIVE
  - TYPE "B" - CURB & GUTTER
  - TYPE "B" - CURB & GUTTER - DRY
  - 24" WIDE, CONC. RIBBON CURB
  - EXISTING CURB & GUTTER

**bs incite Design Studio**  
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 KANSAS CITY, MO 64108  
 816.379.3500

Architect  
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Civil  
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Mech., Plumb., Elec., Telecom.  
 SMITH AND REUCHER, INC.  
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**SCHLAGEL**

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 LICENSE KS-16516

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 Kansas #E-2959L-S-54  
 Schlagel Associates, P.A.

**ST. JAMES MULTIPURPOSE BUILDING**

24505 PRAIRIE STAR PKWY  
 LENEXA, KS 66227

Project Phase  
**FINAL DEVELOPMENT PLAN**

Project Number  
**21-115**

Issue Date  
**01/30/2023**

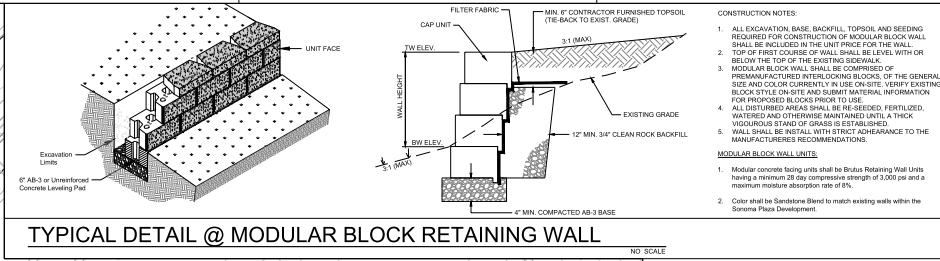
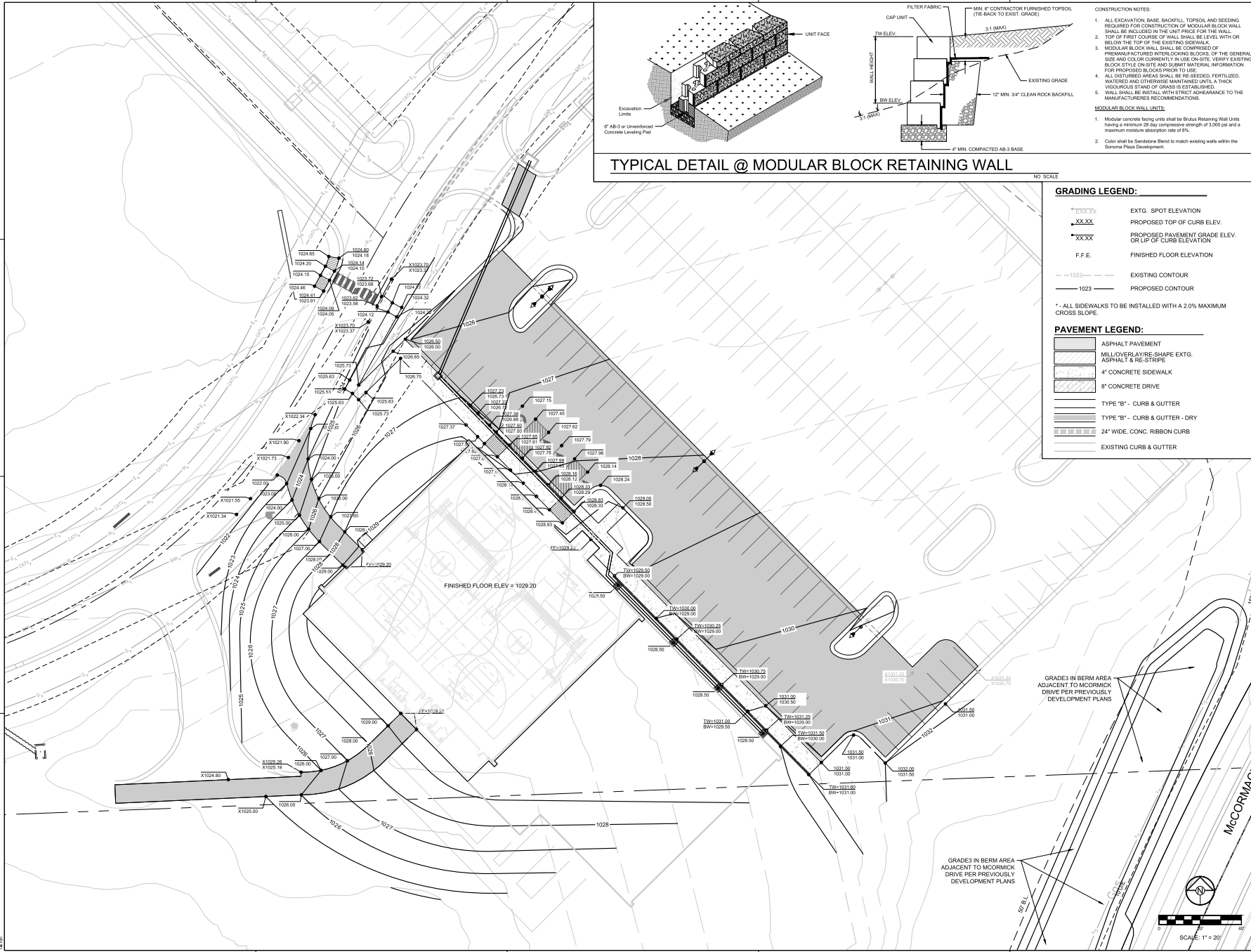
Revision No.	Description	Date Issued

**Key Plan**

ENLARGED SCALE SITE PLAN

Sheet Number  
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**GRADING LEGEND:**

- EXTG. SPOT ELEVATION
- PROPOSED TOP OF CURB ELEV.
- PROPOSED PAVEMENT GRADE ELEV. OR LIP OF CURB ELEVATION
- FINISHED FLOOR ELEVATION
- EXISTING CONTOUR
- PROPOSED CONTOUR

\* ALL SIDEWALKS TO BE INSTALLED WITH A 2.0% MAXIMUM CROSS SLOPE.

**PAVEMENT LEGEND:**

- ASPHALT PAVEMENT
- MILL LAYER AT THE SHAPE EXTG. ASPHALT & RE-STRIPE
- 4" CONCRETE SIDEWALK
- 8" CONCRETE DRIVE
- TYPE "B" - CURB & GUTTER
- TYPE "S" - CURB & GUTTER - DRY
- 24" WIDE CONC. RIBBON CURB
- EXISTING CURB & GUTTER

**ibs incite Design Studio**  
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 816.231.1144

**Mech., Plumb., Elec., Telecom.**  
 SMITH AND REUCHER, INC.  
 2816 W 103RD STREET  
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 LICENSE KS-16516

**Cert. of Authority**  
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 Schlager Associates, P.A.

**ST. JAMES MULTIPURPOSE BUILDING**

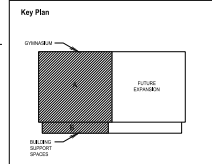
24505 PRAIRIE STAR PKWY  
 LENEXA, KS 66227

**Project Phase**  
 FINAL DEVELOPMENT PLAN

**Project Number**  
 21-115

**Issue Date**  
 01/30/2023

Revision No.	Description	Date Issued

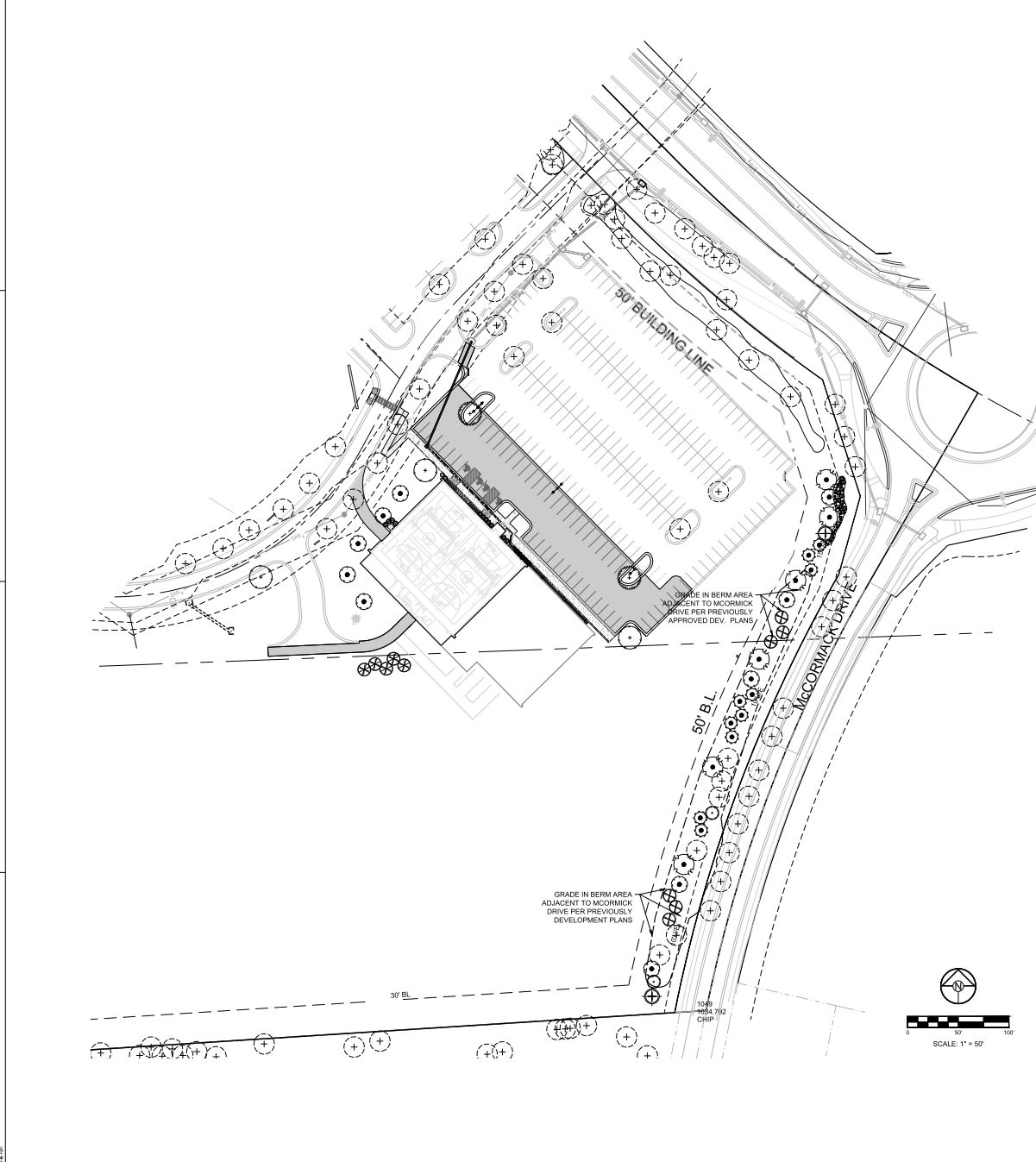


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 SITE GRADING PLAN

**Sheet Number**  
**C2.0**

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SHADE TREES					
	3 EA.	Acer rubrum 'Sun Valley'	Sun Valley Red Maple	2.5' Cal.	B&B
	6 EA.	Acer truncatum	Shantung Maple	2.5' Cal.	B&B
	10 EA.	Quercus robur 'Crimson Spire'	Crimson Spire Oak	2.5' Cal.	B&B
	2 EA.	Zelkova serrata 'Green Vase'	Japanese Zelkova	2.5' Cal.	B&B
ORNAMENTAL TREES					
	3 EA.	Malus sp. 'Royal Raindrops'	Royal Raindrops Crabapple	2' Cal. & 6' ht.	B&B
EVERGREEN TREES					
	4 EA.	Juniperus chinensis 'Spartan'	Spartan Juniper	6' ht.	B&B
	10 EA.	Juniperus virginiana 'Canaertii'	Canaert Juniper	6' ht.	B&B
	5 EA.	Pinus fixilla 'Vanderwolf Pyramis'	Vanderwolf Limber Pine	6' ht.	B&B
	6 EA.	Pinus strobus	White Pine	6' ht.	B&B
SHRUBS					
	6 EA.	Juniperus horizontalis 'Hughes'	Hughes Juniper	5 gal.	Cont.
	9 EA.	Ilex verticillata 'Nana' Red Sprite	Red Sprite Winterberry	5 gal.	Cont.
	10 EA.	Juniperus horizontalis 'Hughes'	Hughes Juniper	5 gal.	Cont.
	5 EA.	Juniperus virginiana 'Grey Owl'	Grey Owl Juniper	5 gal.	Cont.
GRASSES					
	24 EA.	Calamagrostis x acutifolia 'Karl Foerster'	Karl Foerster Grass	2 gal.	Cont.

**LANDSCAPE DATA:**

**PERIMETER PLANTINGS ALONG STREET FRONTAGES (Section 4-1-D-2.L Table 1)**

MCCORMICK DRIVE (COLLECTOR)(544.51 FT.)  
 SHADE TREES REQUIRED(1) PER 100'(544.51/100 X2) 11 TREES  
 ORNAMENTAL TREES REQUIRED(1) PER 100'(544.51/100 X1) 6 TREES  
 SHRUBS REQUIRED(2) PER 100'(544.51/100 X12) 65 SHRUBS  
 LANDSCAPE BE REQUIRED(75 S.F. PER 100'(544.51/100X75) 408 S.F.

SHADE TREES PROVIDED 11 TREES(1 EXIST, 10 NEW)  
 ORNAMENTAL TREES PROVIDED 7 TREES(5 EXIST, 2 NEW)  
 EVERGREEN TREES PROVIDED INSTEAD OF SHRUBS(1 TREE PER 3 SHRUBS)18 TREES(INSTEAD OF 54 SHRUBS)  
 SHRUBS PROVIDED 11 SHRUBS  
 LANDSCAPE BED PROVIDED 408 S.F. (PLANTED WITH GRASSES)

**PARKING LOT LANDSCAPE(Section 4-1-D-2.M.1&2)**

INTERNAL PARKING LOT AREA (7.5% OF PARKING LOT AREA)  
 TREES REQUIRED (1 PER ISLAND)

**ibs incite Design Studio**  
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 110 WEST 18TH STREET  
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 816.279.2500

Architect  
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License  
 LICENSE KS-#523

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 Kansas #E-2959L-S-54  
 Schlagel Associates, P.A.

**ST. JAMES  
 MULTIPURPOSE  
 BUILDING**

24505 PRAIRIE STAR PKWY  
 LENEXA, KS 66227

Project Phase  
**FINAL DEVELOPMENT PLAN**

Project Number  
**21-115**

Issue Date  
**01/30/2023**

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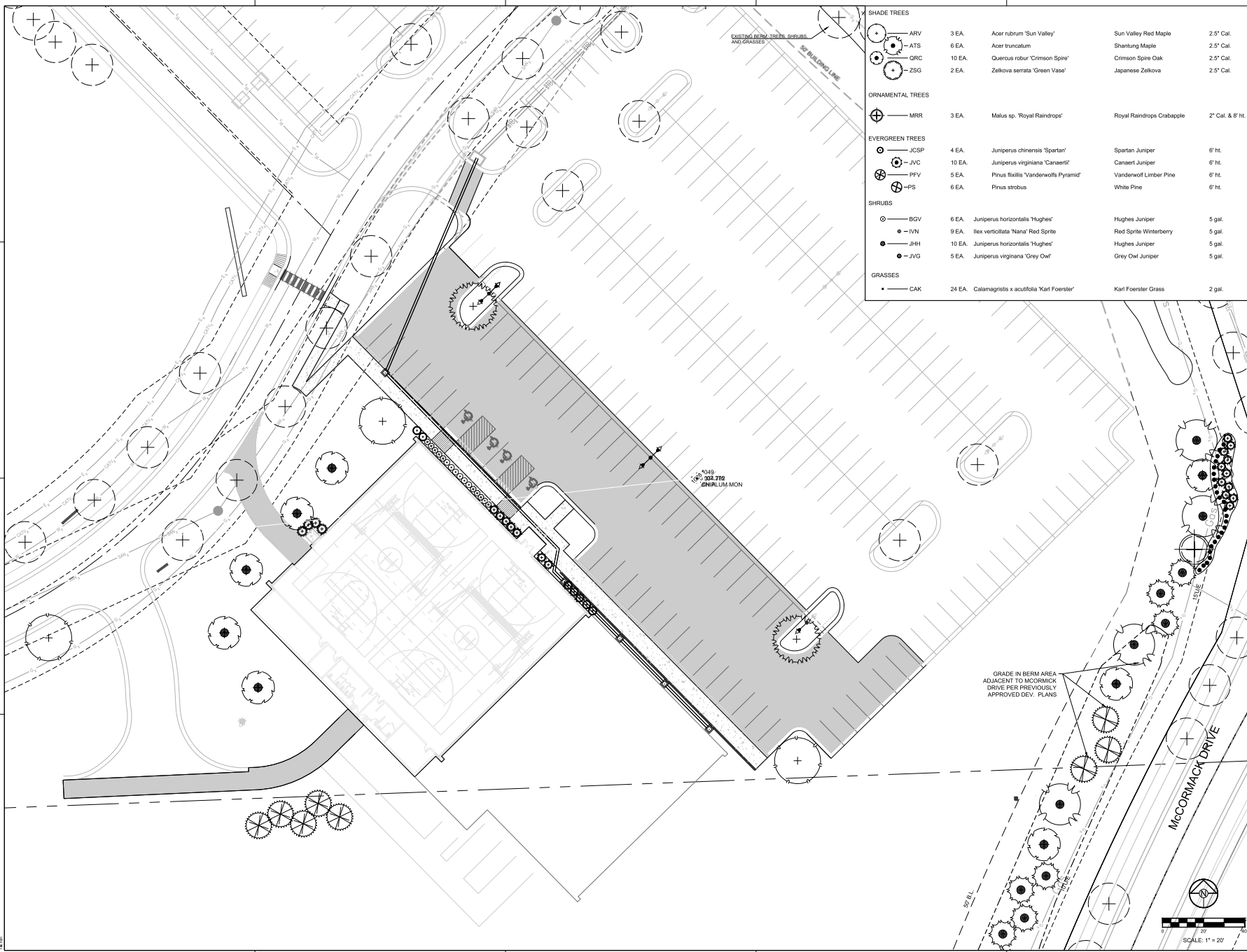
Key Plan

Sheet Name  
**OVERALL LANDSCAPE PLAN**

Sheet Number  
**L1.0**

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Symbol	Quantity	Plant Name	Common Name	Size	Notes
ARV	3 EA.	Acer rubrum 'Sun Valley'	Sun Valley Red Maple	2.5' Cal.	8&B
ATS	6 EA.	Acer truncatum	Shantung Maple	2.5' Cal.	8&B
QRC	10 EA.	Quercus robur 'Crimson Spire'	Crimson Spire Oak	2.5' Cal.	8&B
ZSG	2 EA.	Zelkova serrata 'Green Vase'	Japanese Zelkova	2.5' Cal.	8&B
MRR	3 EA.	Malus sp. 'Royal Raindrops'	Royal Raindrops Crabapple	2' Cal. & 8' Ht.	8&B
JCP	4 EA.	Juniperus chinensis 'Spartan'	Spartan Juniper	6' Ht.	8&B
JVC	10 EA.	Juniperus virginiana 'Canaertii'	Canaert Juniper	6' Ht.	8&B
PFV	5 EA.	Pinus flexilis 'Vanderwolf's Pyramid'	Vanderwolf Limber Pine	6' Ht.	8&B
PS	6 EA.	Pinus strobus	White Pine	6' Ht.	8&B
HGH	6 EA.	Juniperus horizontalis 'Hughes'	Hughes Juniper	5 gal.	Cont.
IVN	9 EA.	Ilex verticillata 'Nana' Red Spruce	Red Spruce Winterberry	5 gal.	Cont.
JHH	10 EA.	Juniperus horizontalis 'Hughes'	Hughes Juniper	5 gal.	Cont.
JVG	5 EA.	Juniperus virginiana 'Grey Owl'	Grey Owl Juniper	5 gal.	Cont.
CAK	24 EA.	Calamagrostis x acutifolia 'Karl Foerster'	Karl Foerster Grass	2 gal.	Cont.

**ibs incite Design Studio**  
 ARCHITECT  
 INCITE DESIGN STUDIO  
 110 WEST 18TH STREET  
 KANSAS CITY, MO 64108  
 816.379.3500

**SCHLAGEL AND ASSOCIATES**  
 CIVIL  
 SCHLAGEL AND ASSOCIATES  
 1402 W 107TH STREET  
 LENEXA, KS 66215  
 913.492.5158

**Structural**  
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 816.231.1144

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**SCHLAGEL**

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 License  
 LICENSE KS - #523

Cont. of Authority  
 Kansas #E-29581, S-54  
 Schlagel Associates, P.A.

**ST. JAMES  
 MULTIPURPOSE  
 BUILDING**

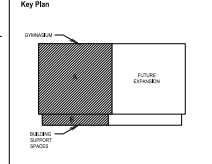
24505 PRAIRIE STAR PKWY  
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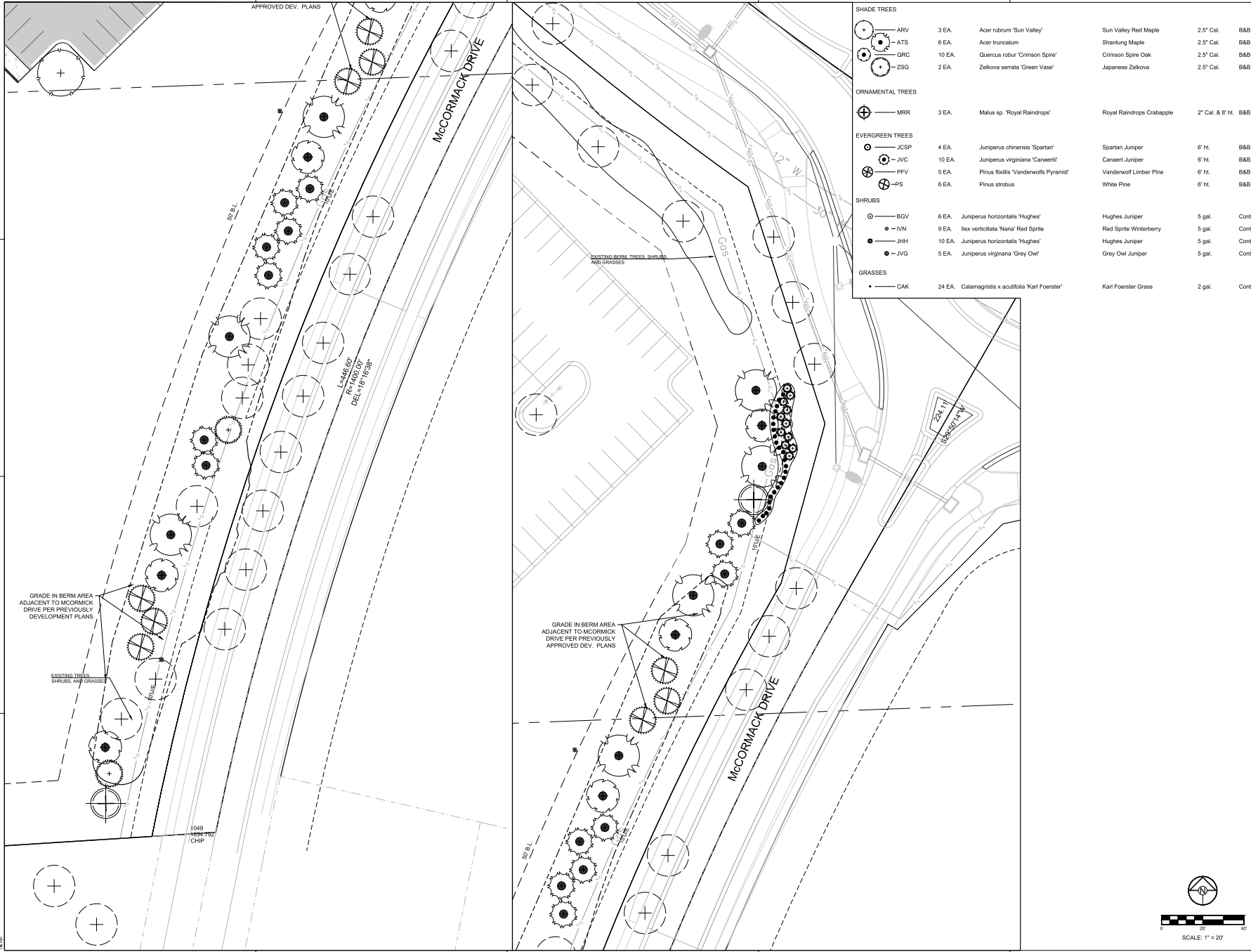


Sheet Name  
**LANDSCAPE PLAN**

Sheet Number  
**L1.1**

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I:\PROJECT\2020\2025-1003\01 Design\01 DWG Plans\01 FDP\2025-1003 FDP\_LP.dwg LANDSCAPE PLAN, 2/27/2023 9:41:58 AM  
 2/27/2023 9:41:58 AM



SHADE TREES			
ARV	3 EA.	Acer rubrum 'Sun Valley'	Sun Valley Red Maple 2.5' Cal. B&B
ATS	6 EA.	Acer truncatum	Shantung Maple 2.5' Cal. B&B
QRC	10 EA.	Quercus robur 'Crimson Spire'	Crimson Spire Oak 2.5' Cal. B&B
ZSG	2 EA.	Zelkova serrata 'Green Vase'	Japanese Zelkova 2.5' Cal. B&B
ORNAMENTAL TREES			
MRR	3 EA.	Malus sp. 'Royal Raindrops'	Royal Raindrops Crabapple 2' Cal. & 6' Ht. B&B
EVERGREEN TREES			
JCSF	4 EA.	Juniperus chinensis 'Spartan'	Spartan Juniper 6' Ht. B&B
JVC	10 EA.	Juniperus virginiana 'Canaertii'	Canaert Juniper 6' Ht. B&B
PFV	5 EA.	Pinus fixilla 'Vanderwolf's Pyramid'	Vanderwolf Limber Pine 6' Ht. B&B
PS	6 EA.	Pinus strobus	White Pine 6' Ht. B&B
SHRUBS			
BGV	6 EA.	Juniperus horizontalis 'Hughes'	Hughes Juniper 5 gal. Cont.
IVN	9 EA.	Ilex verticillata 'Nana' Red Sprite	Red Sprite Winterberry 5 gal. Cont.
JHH	10 EA.	Juniperus horizontalis 'Hughes'	Hughes Juniper 5 gal. Cont.
JVG	5 EA.	Juniperus virginiana 'Grey Owl'	Grey Owl Juniper 5 gal. Cont.
GRASSES			
CAK	24 EA.	Calamagrostis x acutifolia 'Karl Foerster'	Karl Foerster Grass 2 gal. Cont.

**ibs incite Design Studio**  
 ARCHITECT  
 INCITE DESIGN STUDIO  
 110 WEST 18TH STREET  
 KANSAS CITY, MO 64108  
 816.379.3500

**Civil**  
 SCHLAGEL AND ASSOCIATES  
 14020 W 107TH STREET  
 LENEXA, KS 66215  
 913.462.5158

**Structural**  
 BOB G. CAMPBELL & CO.  
 4330 BELLEVUE AVENUE  
 KANSAS CITY, MO 64111  
 816.231.4144

**Mech., Plumb., Elec., Telecom.**  
 SMITH AND REICHER, INC.  
 29610 W 103RD STREET  
 OLATHE, KS 66061  
 913.346.2127

**SCHLAGEL**

Licensed LA Architect  
 Daniel G. Foster, L.S.  
 License  
 LICENSE KS-4523

Cont. of Authority  
 Kansas #S-2959LS-54  
 Schlagel Associates, P.A.

**ST. JAMES  
 MULTIPURPOSE  
 BUILDING**

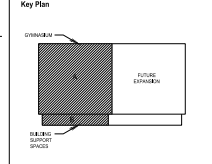
24505 PRAIRIE STAR PKWY  
 LENEXA, KS 66227

Project Phase  
**FINAL DEVELOPMENT PLAN**

Project Number  
**21-115**

Issue Date  
**01/30/2023**

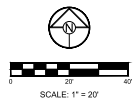
Revision No.	Description	Date Issued



Sheet Name  
**LANDSCAPE PLAN**

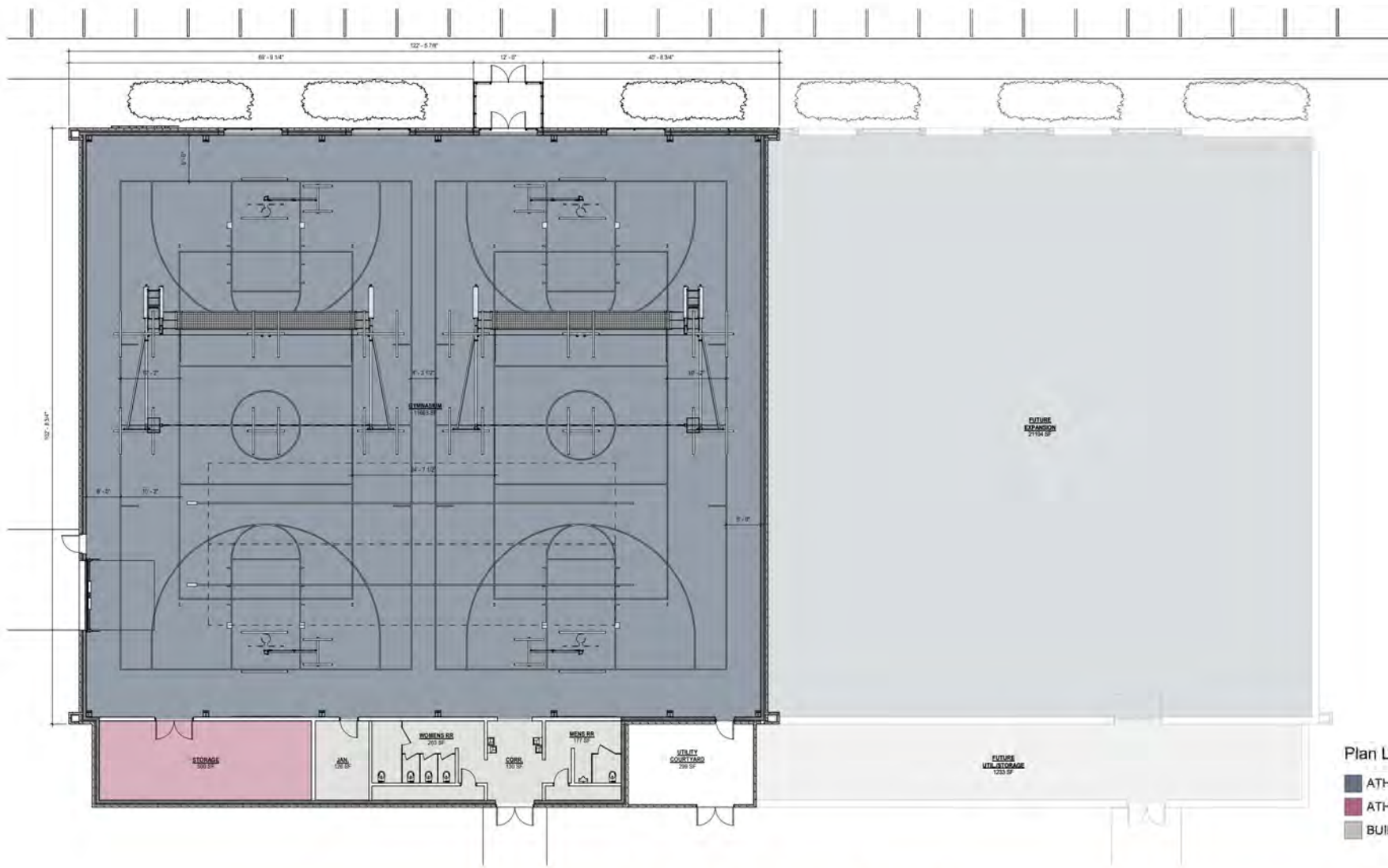
Sheet Number  
**L1.2**

in • cite v. to provoke thought



I:\PROJECTS\2023\21-115\03\03 DWG Plans\11 Final\11 Final Landscape Plan (2) - 2/7/2023 9:42:10 AM





**incite Design Studio**  
Project Name:  
**ST. JAMES ACADEMY  
MULTI-PURPOSE BLDG.**  
Project Number:  
**21-115**  
Address:  
**24505 PRAIRIE STAR  
PKWY, LENEXA, KS,  
66227**



# ARCHITECTURAL FLOOR PLAN

SCALE: 1/8" = 1'-0"

# 01



FUTURE EXPANSION

SJA MULTI-PURPOSE BUILDING



incite Design Studio  
Project Name:  
**ST. JAMES ACADEMY  
MULTI-PURPOSE BLDG.**  
Project Number:  
**21-115**  
Address:  
**24505 PRAIRIE STAR  
PKWY, LENEXA, KS,  
66227**



**EXTERIOR  
RENDERING**  
SCALE: NTS

**02**



FUTURE  
EXPANSION



incite Design Studio  
Project Name:  
**ST. JAMES ACADEMY  
MULTI-PURPOSE BLDG.**  
Project Number:  
**21-115**  
Address:  
**24505 PRAIRIE STAR  
PKWY, LENEXA, KS,  
66227**



**EXTERIOR  
RENDERING**  
SCALE: NTS

**03**



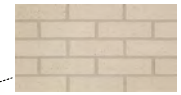
DRYVIT EIFS BRICK



CAST STONE



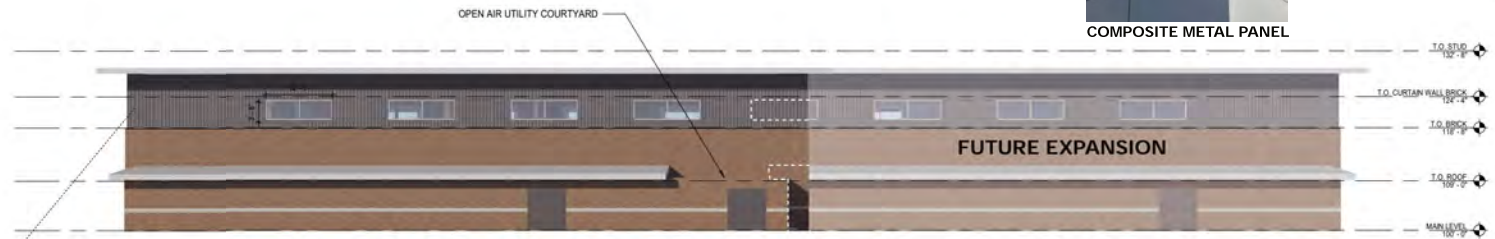
**NORTH ELEVATION**  
SCALE: 3/32" = 1' - 0"



INTERSTATE BRICK:  
"TUMBLEWEED"



INTERSTATE BRICK:  
"MIDNIGHT BLACK"



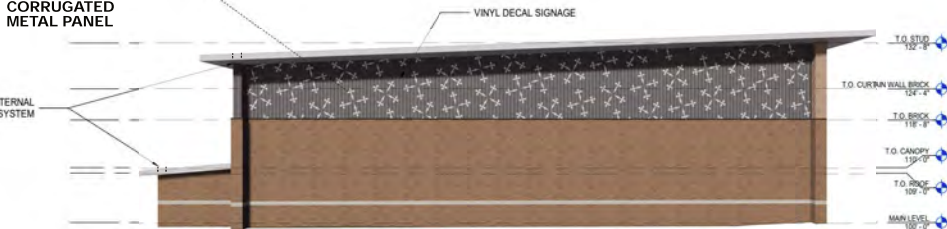
**SOUTH ELEVATION**  
SCALE: 3/32" = 1' - 0"



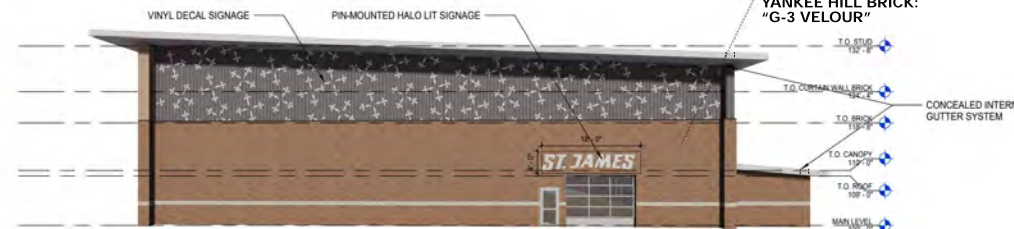
CORRUGATED  
METAL PANEL



YANKEE HILL BRICK:  
"G-3 VELOUR"



**EAST ELEVATION**  
SCALE: 3/32" = 1' - 0"



**WEST ELEVATION**  
SCALE: 3/32" = 1' - 0"



incite Design Studio  
Project Name:  
**ST. JAMES ACADEMY  
MULTI-PURPOSE BLDG.**  
Project Number:  
21-115  
Address:  
24505 PRAIRIE STAR  
PKWY, LENEXA, KS,  
66227



**EXTERIOR  
ELEVATIONS**  
SCALE: SEE ELEVATION

**04**



**ids** incite  
Design  
Studio  
architects building relationships

incite Design Studio  
Project Name:  
**ST. JAMES ACADEMY  
MULTI-PURPOSE BLDG.**  
Project Number:  
**21-115**  
Address:  
**24505 PRAIRIE STAR  
PKWY, LENEXA, KS,  
66227**

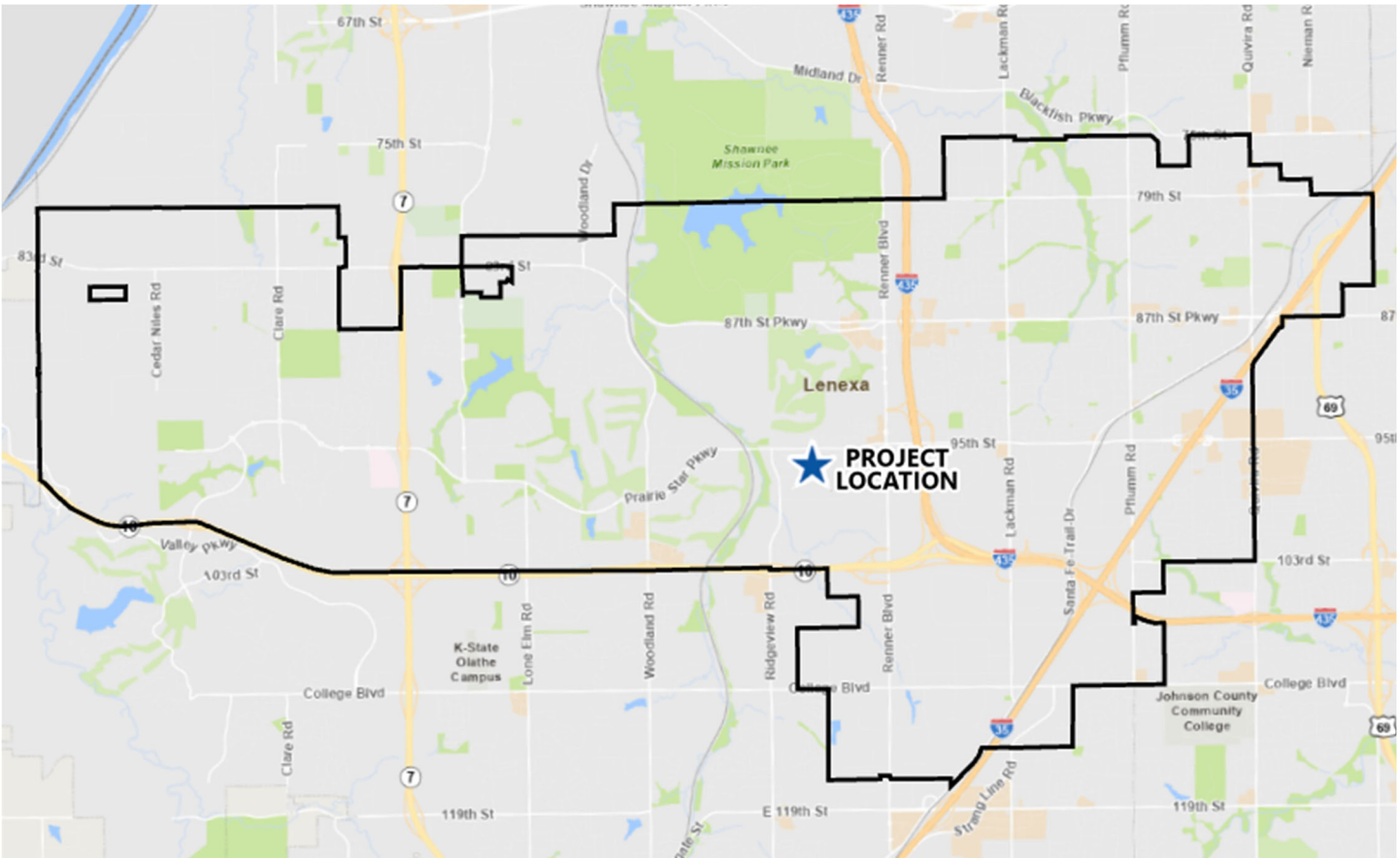


**INTERIOR  
RENDERING**  
SCALE: NTS

**05**

## VISTA VILLAGE, 2<sup>ND</sup> PLAT, FINAL PLAT

<b>Project #:</b>	PT23-01FR	<b>Location:</b>	SWC of Prairie Star Parkway and Vahalla Street
<b>Applicant:</b>	Chad Porter, Payne and Brockway	<b>Project Type:</b>	Final Plat
<b>Staff Planner:</b>	Dave Dalecky	<b>Proposed Use:</b>	Multi-Family Residential



### PROJECT SUMMARY

The applicant is requesting approval of a final plat for the townhome portion of the Vista Village development. This replat of Vista Village, 1<sup>st</sup> Plat (PT22-09F) will change the development pattern from a mix of two-, four-, five-, and six-unit buildings to all two-unit (duplex) buildings. The change will result in a net reduction of 11 units. The change to the development, from multi-unit buildings to all two-unit buildings is sufficiently consistent with the approved plan to not warrant a revised final plan. [Section 4-1-H-4-D](#) of the Unified Development Code (UDC) lists the threshold for when such a revision is required. The proposed plan shows the two-unit buildings and internal private drives in a nearly identical development pattern to what is shown on the approved plan. The development is less dense with the reduction of 11 units.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

The site is zoned PUD, Planned Unit Development (RZ15-06). This zoning category allows for multiple uses developed as a unified development. Vista Village includes the townhome component, now all two-unit buildings, commercial buildings, and an apartment component. Several preceding versions of a preliminary plan were approved prior to the current plan for Vista Village. The most recent preliminary plan approval for Vista Village development is PL21-05PR.

LAND AREA (AC)	DWELLING UNITS	ZONING	COMP. PLAN
23.6	108	PUD	Business Park

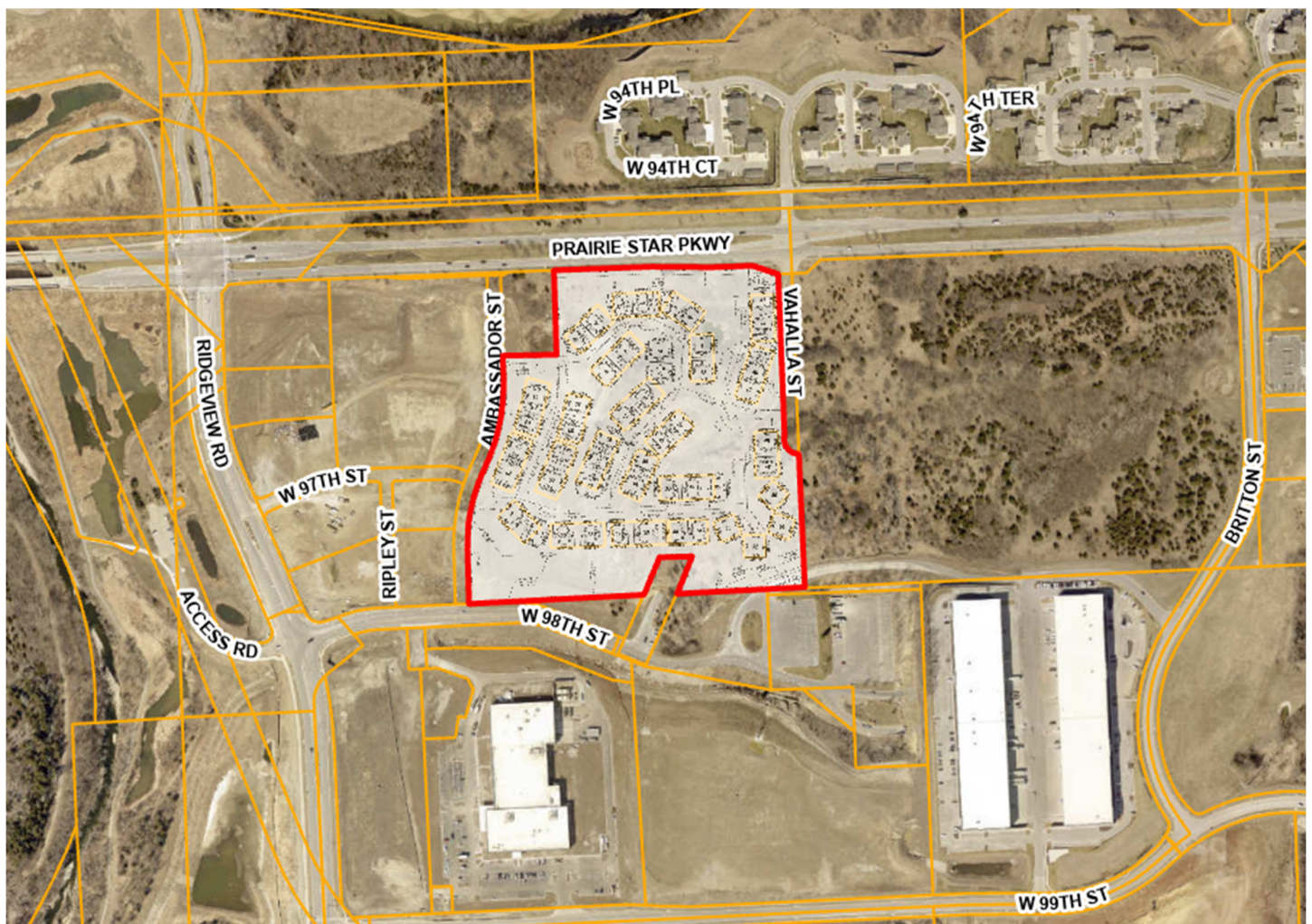
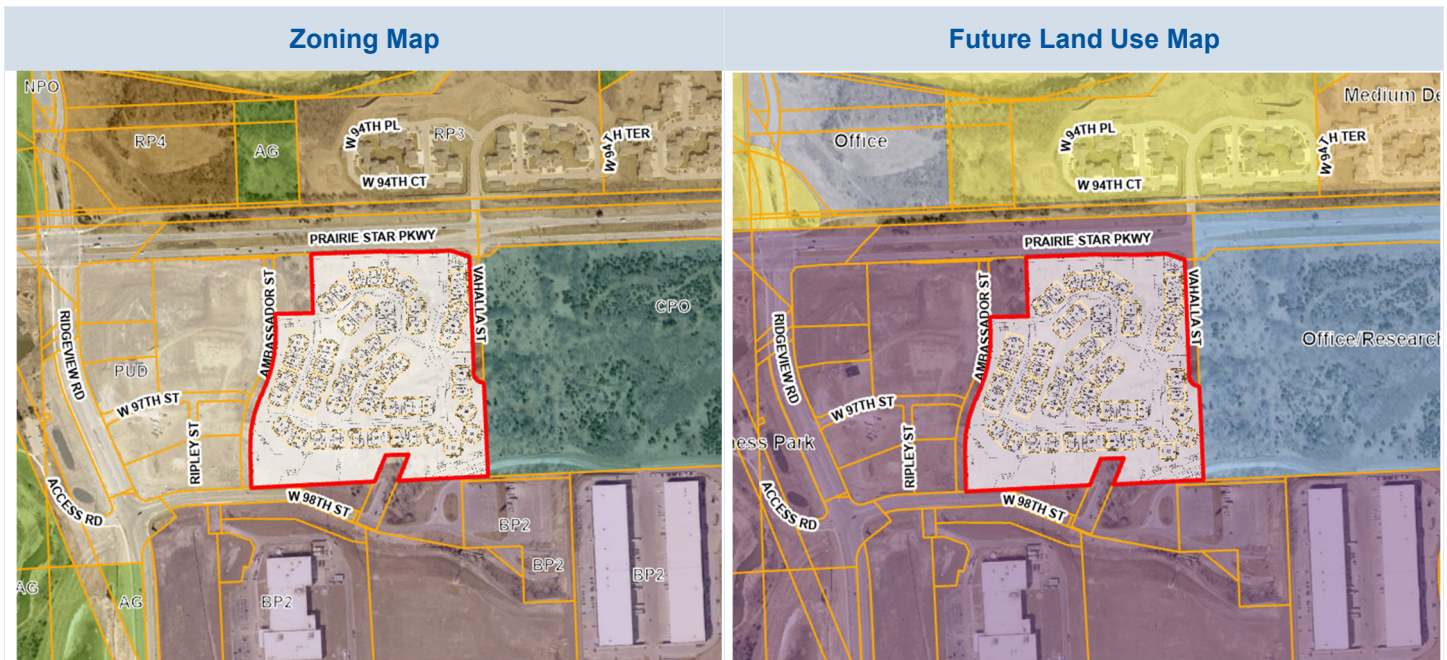


Exhibit 1: Aerial Image of Subject Area.

## LAND USE REVIEW

The plat will establish individual lots for each two-unit building. The placement of the lots are in a nearly identical pattern as the recorded plat and will have the same private street alignment. The plat contains 54 lots and one tract. Utility easements shown on the plat are the same locations as are shown on Vista Village, 1<sup>st</sup> Plat (now recorded) with the exception of minor changes to sections of the sanitary sewer easement.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Future Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Business Park	PUD, Planned Unit Development District	Undeveloped land
<b>North</b>	Suburban Density Residential	RP-3, Residential Planned (Medium High-Density) Residential District	Medium Density
<b>South</b>	Business Park	BP-2, Planned Manufacturing District	Laboratory, manufacturing, office, and distribution
<b>East</b>	Office, Research and Development	CP-O, Planned General Office District	Undeveloped land
<b>West</b>	Business Park	PUD, Planned Unit Development District	Undeveloped land



## FINAL PLAT REVIEW

This final plat is a replat of part of Vista Village, 1<sup>st</sup> Plat. The original plat is for the entire 50-acre site. The replat is for the easterly part of the site that contains the townhomes. The applicant is now proposing all the buildings be two-unit buildings. The previous concept was for two-, four-, five, and six-unit buildings.

The plat includes separate lots to encapsulate each building. The “common” space of the site is within a single tract. The general layout of the buildings on the site is effectively identical to the previous layout. The change to the development is the actual footprints of each of the buildings. The plat contains 54 lots and one tract. The plat contains utility, drainage, and sewer easements. All easements remain exactly as shown on Vista Village, 1<sup>st</sup> Plat except for minor adjustments to the sewer easement. This easement is dedicated to Johnson County Wastewater and is subject to the provisions of Johnson County Wastewater. The applicant is coordinating the minor changes of this easement with Johnson County Wastewater.

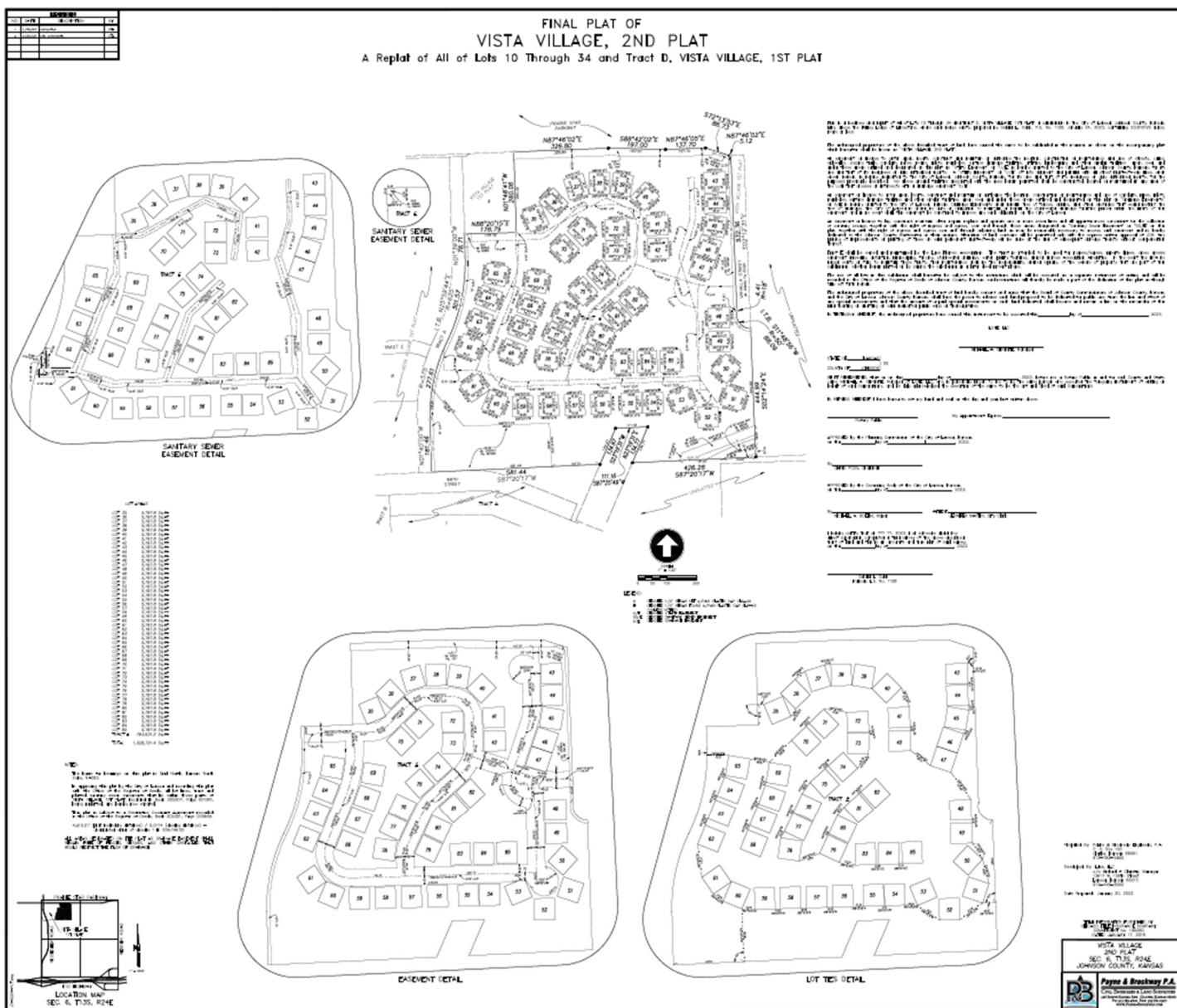


Exhibit 3: Final Plat with Tracts and Easements to be Dedicated.

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## DEVIATIONS

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The applicant is not requesting any deviations from the Unified Development Code.

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## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this plat, pending Governing Body acceptance of easements for utilities as shown on the final plat, which will be on the agenda for the March 21, 2023, City Council meeting.
- The applicant should inquire about additional City requirements, such as permits and development fees.

---

## RECOMMENDATION FROM PROFESSIONAL STAFF

---

★ **Staff recommends approval of the proposed Final Plat for Vista Village, 2<sup>nd</sup> Plat.**

- This is a 54-lot final plat with one tract for common open space.
- The project is consistent with Lenexa's goals through *Strategic Community Investment* and *Responsible Economic Development* to create *Vibrant Neighborhoods*.

### FINAL PLAT

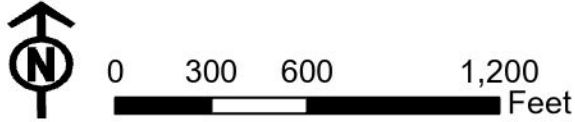
Staff recommends **approval** of the final plat for PT23-01FR – **Vista Village, 2<sup>nd</sup> Plat** at the southwest corner of Prairie Star Parkway and Vahalla Street, for 54 lots and one tract.



Document Path: R:\A\data\dd\_scratch\VIC MAP projects\lenexa smart storage.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Vista Village, 2nd Plat PT23-01FR



NO.	DATE	REVISIONS	BY
1.	1/26/23	Issued	CS
2.	1/25/23	City comments	CS

# FINAL PLAT OF VISTA VILLAGE, 2ND PLAT

A Replat of All of Lots 10 Through 34 and Tract D, VISTA VILLAGE, 1ST PLAT

This is a replat and replat of all of Lots 10 through 34 and Tract D, VISTA VILLAGE, 1ST PLAT, a subdivision in the City of Lenexa, Johnson County, Kansas, lying above the Farley Lodge L18000, where such ledge exists, prepared by Gerald L. Coon, P.S. No. 1128, January 24, 2023, containing 23816/95 acres, more or less.

The undersigned proprietors of the above described tract of land have caused the same to be subdivided in the manner as shown on the accompanying plat, which hereafter shall be known as VISTA VILLAGE, 2ND PLAT.

An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of streets, curbs, sidewalks, gutters, drains, canals, pipes, mains, water, manholes, surface drainage facilities, utilities, landscaping and other similar facilities, sewers, ditches, and other structures shall remain the property of the City of Lenexa, Johnson County, Kansas, for the use and benefit of its citizens or any authorized agency. A "Utility Easement" or "U/E" is hereby granted to the City of Lenexa, Johnson County, Kansas, for the use and benefit of its citizens or any authorized agency. A "Utility Easement" or "U/E" is hereby granted to the City of Lenexa, Johnson County, Kansas, for the use and benefit of its citizens or any authorized agency for the purposes previously described. No above ground facilities associated with the uses herein permitted shall be constructed, located or maintained in any area of the U/E that crosses or intersects with a drainage easement "D/E".

An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of canals, pipes, mains, manholes, surface drainage facilities and other similar facilities, upon, over and under those areas outlined and designated on this plat as "Drainage Easements" or "D/E" is hereby granted to the City of Lenexa, Kansas. Drainage Easements shall remain the property of the City of Lenexa, Johnson County, Kansas, for the use and benefit of its citizens or any authorized agency. A "Drainage Easement" or "D/E" is hereby granted to the City of Lenexa, Johnson County, Kansas, for the use and benefit of its citizens or any authorized agency for the purposes previously described. No above ground facilities associated with the uses herein permitted shall be constructed, located or maintained in any area of the U/E that crosses or intersects with a drainage easement "D/E".

An easement or license to lay, construct, maintain, alter, repair, replace and operate one or more sewer lines and all appurtenances convenient for the collection of sanitary sewage, together with the right of ingress and egress, over and through those areas designated as "Sanitary Sewer Easement" or "SSE" on this plat, together with the right of ingress and egress over and through adjoining lands as may be reasonably necessary to access said easement and to hereby dedicated to the Johnson County Board of Health and their designees. Invention of this easement will be permitted only with the express written approval of J.C.W. Any planting of improvements or planting of trees on said easement right-of-way will be done at the risk of subsequent damage thereto without compensation therefor.

Tract E shall be owned and maintained by the Lenexa Home Association. The tract is intended to be used for ingress/egress, private drives, cross access easement sidewalks, detention, landscaping, fencing, stormwater drainage, water quality features, and/or Home Association Amenities. In the event the HOA no longer exists or fails to maintain these tracts, then maintenance shall be the responsibility, shared equally, of the owners of projects that are part of this subdivision, whether herein platted or by joining the subdivision in a later development phase.

The use of all lots in this subdivision shall hereafter be subject to the restrictions which will be executed as a separate instrument of writing and will be recorded in the Office of the Registrar of Deeds of Johnson County, Kansas, said restrictions will thereby be made a part of the dedication of this plat as though fully set forth herein.

The undersigned proprietors of the above described tract of land hereby consent and agree that the Board of County Commissioners of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to release such land proposed to be dedicated for public use, from the lien and effect of any special assessments and that the amount of unpaid special assessments on such land dedicated, and secured for public use, from the lien and effect of such lien and effect, or defaulting on said dedicated public ways or thoroughfares.

IN TESTIMONY WHEREOF, the undersigned proprietors have caused this instrument to be executed this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

LUXE, LLC

MICHAEL A. CHRISTE, Manager

STATE OF KANSAS  
COUNTY OF JOHNSON SS

BE IT REMEMBERED, that on this \_\_\_\_\_ day of \_\_\_\_\_, 2023, before me a Notary Public in and for said County and State, came MICHAEL A. CHRISTE, Manager of LUXE, LLC, who is personally known to me to be the same person who executed the foregoing instrument of writing on behalf of said Corporation, and he duly acknowledged the execution of the same to be the act and deed of said Corporation.

IN WITNESS WHEREOF, I have hereunto set my hand and seal on the day and year last written above.

My Appointment Expires: \_\_\_\_\_

Notary Public

APPROVED by the Planning Commission of the City of Lenexa, Kansas, on this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

CHRIS FOSS, Chairman

APPROVED by the Governing Body of the City of Lenexa, Kansas, on this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

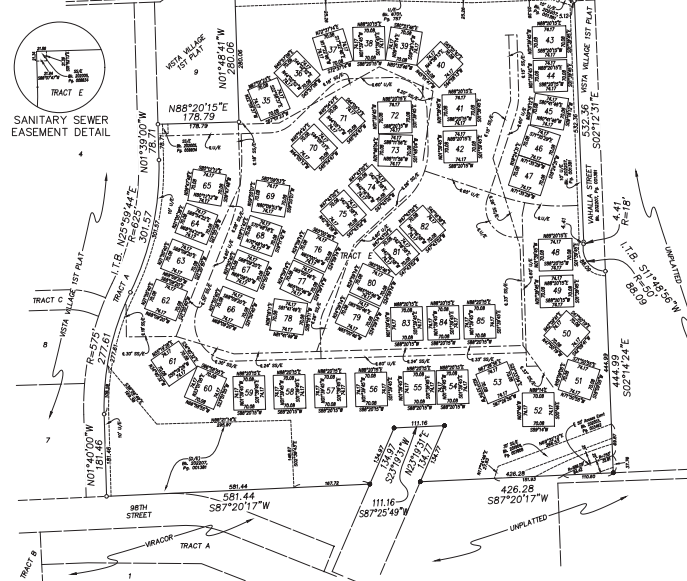
MICHAEL A. BOEHL, Mayor

ATTEST: DENNER MARTIN, City Clerk

I hereby certify that on 1/27/23, 2023, I or someone under my direct supervision completed a field survey of the above described tract of land and this is an accurate and true plat of said survey on this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

Gerald L. Coon  
Kansas L.S. No. 1128

- LEGEND:
- SOURCES 1/2" REBAR SET w/PAB PLASTIC CAP GLS-89
  - SOURCES 1/2" REBAR FOUND w/PAB PLASTIC CAP GLS-89
  - UNDEVELOPED LOTS
  - SOURCES SANITARY SEWER EASEMENT
  - SOURCES DRAINAGE EASEMENT



LOT AREAS

LOT 35	5,197.8	Sq.Ft.
LOT 36	5,197.8	Sq.Ft.
LOT 37	5,197.8	Sq.Ft.
LOT 38	5,197.8	Sq.Ft.
LOT 39	5,197.8	Sq.Ft.
LOT 40	5,197.8	Sq.Ft.
LOT 41	5,197.8	Sq.Ft.
LOT 42	5,197.8	Sq.Ft.
LOT 43	5,197.8	Sq.Ft.
LOT 44	5,197.8	Sq.Ft.
LOT 45	5,197.8	Sq.Ft.
LOT 46	5,197.8	Sq.Ft.
LOT 47	5,197.8	Sq.Ft.
LOT 48	5,197.8	Sq.Ft.
LOT 49	5,197.8	Sq.Ft.
LOT 50	5,197.8	Sq.Ft.
LOT 51	5,197.8	Sq.Ft.
LOT 52	5,197.8	Sq.Ft.
LOT 53	5,197.8	Sq.Ft.
LOT 54	5,197.8	Sq.Ft.
LOT 55	5,197.8	Sq.Ft.
LOT 56	5,197.8	Sq.Ft.
LOT 57	5,197.8	Sq.Ft.
LOT 58	5,197.8	Sq.Ft.
LOT 59	5,197.8	Sq.Ft.
LOT 60	5,197.8	Sq.Ft.
LOT 61	5,197.8	Sq.Ft.
LOT 62	5,197.8	Sq.Ft.
LOT 63	5,197.8	Sq.Ft.
LOT 64	5,197.8	Sq.Ft.
LOT 65	5,197.8	Sq.Ft.
LOT 66	5,197.8	Sq.Ft.
LOT 67	5,197.8	Sq.Ft.
LOT 68	5,197.8	Sq.Ft.
LOT 69	5,197.8	Sq.Ft.
LOT 70	5,197.8	Sq.Ft.
LOT 71	5,197.8	Sq.Ft.
LOT 72	5,197.8	Sq.Ft.
LOT 73	5,197.8	Sq.Ft.
LOT 74	5,197.8	Sq.Ft.
LOT 75	5,197.8	Sq.Ft.
LOT 76	5,197.8	Sq.Ft.
LOT 77	5,197.8	Sq.Ft.
LOT 78	5,197.8	Sq.Ft.
LOT 79	5,197.8	Sq.Ft.
LOT 80	5,197.8	Sq.Ft.
LOT 81	5,197.8	Sq.Ft.
LOT 82	5,197.8	Sq.Ft.
LOT 83	5,197.8	Sq.Ft.
LOT 84	5,197.8	Sq.Ft.
LOT 85	5,197.8	Sq.Ft.
TRACT E	263,821.4	Sq.Ft.
TOTAL	1,028,721.4	Sq.Ft.

NOTES:

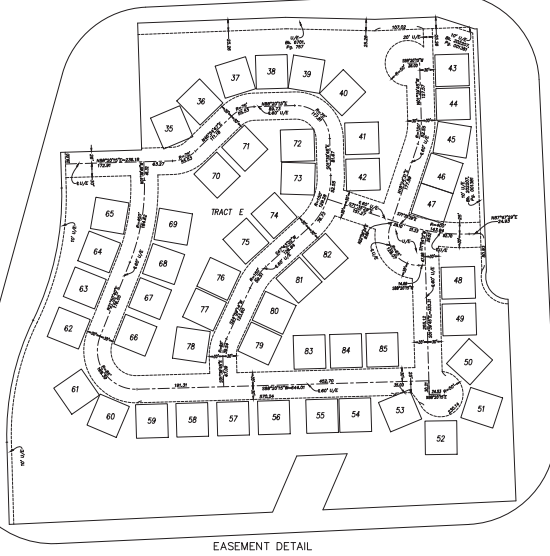
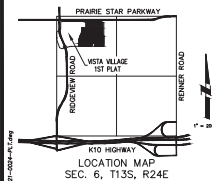
The basis for bearings on this plat is Grid North, Kansas North Zone, NAD83.

In executing this plat by the City of Lenexa and recording this plat with the Office of the Registrar of Deeds, all lot lines, tract and portion boundaries, sewer easements that fall within those parts of VISTA VILLAGE, 1ST PLAT, recorded in Book 20207, Page 00381, being replatted, are hereby now voided.

This plat is subject to a Restrictive Covenant Agreement recorded in the Office of the Registrar of Deeds, Book 20202, Page 00066.

4,510.07' (plot boundary distance) / 0.0174' (draining distance) = undulating error of closure 1 in 259196.55

ALL AREAS DESIGNATED ON THE PLAT AS DRAINAGE EASEMENT SHALL REMAIN FREE OF FENCES, SHEDS, AND OTHER OBSTACLES THAT WOULD RESTRICT THE FLOW OF DRAINAGE.



Prepared By: Payne & Brackway Engineers, P.A.  
P. O. Box 128  
Guthrie, Kansas 66501  
913-751-4600

Developed By: Luxe, LLC  
c/o Michael A. Christe, Manager  
3817 W. 120th Street  
Lenexa, KS 66151  
913-448-8000

Date Prepared: January 30, 2023

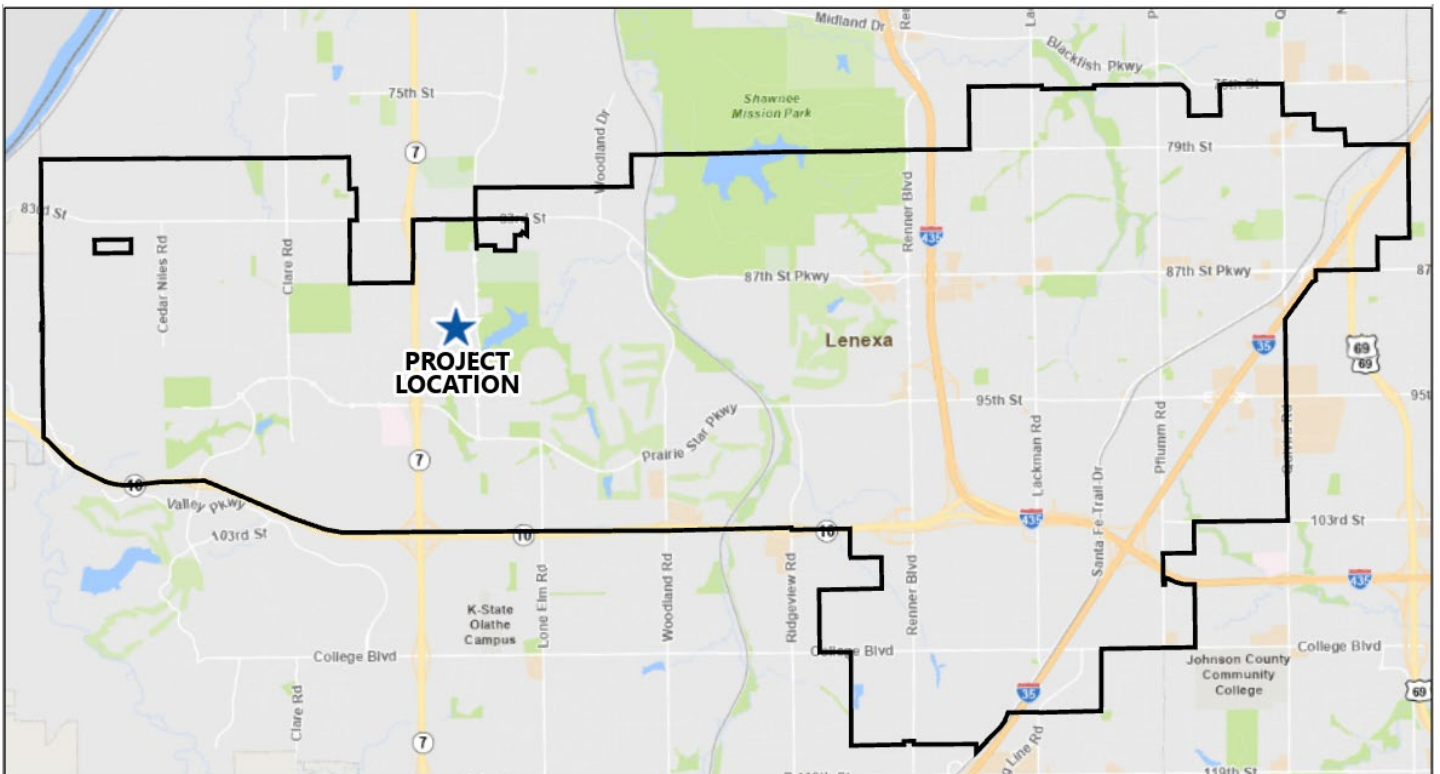
TITLE INFORMATION FURNISHED BY  
CHICAGO TITLE INSURANCE COMPANY  
COMBINATION No. 090206  
DATED JANUARY 17, 2019

VISTA VILLAGE  
2ND PLAT  
SEC. 6, T13S, R24E  
JOHNSON COUNTY, KANSAS



# WATERCREST LANDING, SEVENTH PLAT

<b>Project #:</b>	PT23-09F	<b>Location:</b>	NW Corner of W. 89 <sup>th</sup> Street and Ginger Street
<b>Applicant:</b>	Judd Claussen, Phelps Engineering	<b>Project Type:</b>	Final Plat
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Single-Family Residential



## PROJECT SUMMARY

The applicant proposes to replat Tract H of Watercrest Landing Replat, into one single-family residential lot. The property is located at the northwest corner of W. 89<sup>th</sup> Street and Ginger Street in the Watercrest Landing Subdivision. The proposed lot will be known as Lot 124 and is 16,189.5 square feet. This lot will have a housing classification of "D". The final plat does not include construction of any new public street or related infrastructure. A 10' utility easement dedicated to the City of Lenexa will remain within the property, but there are no new dedications proposed. The tract was originally approved as a subdivision amenity tract for a community pool under PT16-14F, which was recorded on November 30, 2017. Subsequent phases of the subdivision have led to a relocation of the pool area and as such this tract is no longer necessary to serve that purpose. This project does not require a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL**

## SITE INFORMATION

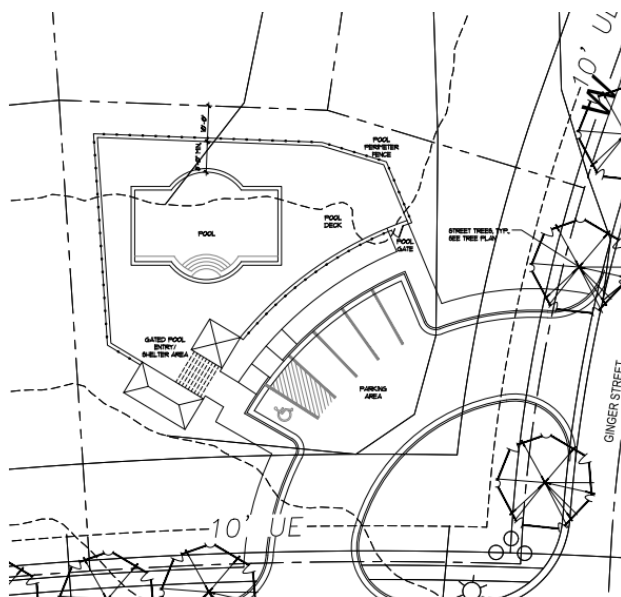
The subject site was rezoned from the AG, Agricultural Zoning District to the RP-1, Planned Residential Single Family (Low-Density Zoning District) for Watercrest Landing single-family subdivision in 2016 under RZ16-04. A companion preliminary plan and plat, PL16-01P, were concurrently filed and approved at that time.

Watercrest Landing received final plat and final plan approval under PL16-09F (final plan) and PT16-14F (final plat). The final plan and plat designated Tract H as an amenity area intended to be the location of the community pool. Watercrest Landing Plat was recorded November 30, 2017 and encompassed 120 lots and 11 tracts on 46.45 acres, with a housing classification “C”. In 2018, Watercrest Landing Replat was approved to change the designated housing classification on the plat from “C” to “D”.

LAND AREA (AC)	DWELLINGS	CURRENT ZONING	COMP. PLAN
.37	1	RP-1	Office/Employment Center & Suburban Residential



**Exhibit 1: Aerial Image of Subject Site**



**Exhibit 2: Watercrest Landing Replat; Showing the subject property as the proposed amenity area known as Tract H (PT16-14F, Final Plat)**

## LAND USE REVIEW

The proposed use is a single-family residential lot. This is compatible with surrounding uses, which are also single-family lots located within a subdivision. The Comprehensive Plan’s Future Land Use Map designates the eastern portion of the parcel as suburban residential and the western as office/ employment center. This location is developed as a single-family subdivision and is compatible with the surrounding uses and the suburban residential land use designation. Given the approval of the Watercrest Landing Subdivision at this location, it is not anticipated that an office use would develop as indicated by the office/employment center designation.



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Suburban Residential & Office/Employment Center	RP-1, Planned Residential Single-Family (Low Density) District	Vacant Lot
<b>North</b>	Suburban Residential & Office/ Employment Center	RP-1, Planned Residential Single-Family (Low Density) District	Single-family
<b>South</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low Density) District	Single-family
<b>East</b>	Suburban Residential	RP-1, Planned Residential Single-Family (Low Density) District	Single-family
<b>West</b>	Office/ Employment Center	RP-1, Planned Residential Single-Family (Low Density) District	Single-family

## FINAL PLAT REVIEW

The Watercrest Amenity Center has been relocated to the middle of the overall Watercrest Landing development and is now twice as large as originally planned, leaving this lot, which was originally planned to be the Amenity Center, available to develop as a single-family lot without creating a deficiency in the required amount of amenity space.

The final plat is for one, .37-acre single-family lot in the Watercrest Landing Subdivision. The single-family lot is a corner lot with frontage along 89<sup>th</sup> Street and Ginger Street. The lot will be addressed off 89<sup>th</sup> Street. The lot meets dimensional standards of the zoning district and is in compliance with the subdivision regulations. Street landscaping requirements, as approved with the Watercrest Landing Subdivision, are not changing with the approval of this final plat. Required utilities are existing and available to the site. The final plat shows an existing 10-foot utility easement along the east and south street frontages. The utility easement will remain and does not require re-acceptance by the Governing Body. A front yard setback of 30 feet is delineated on 89<sup>th</sup> Street. A side yard setback of 20 feet is delineated on Ginger Street.

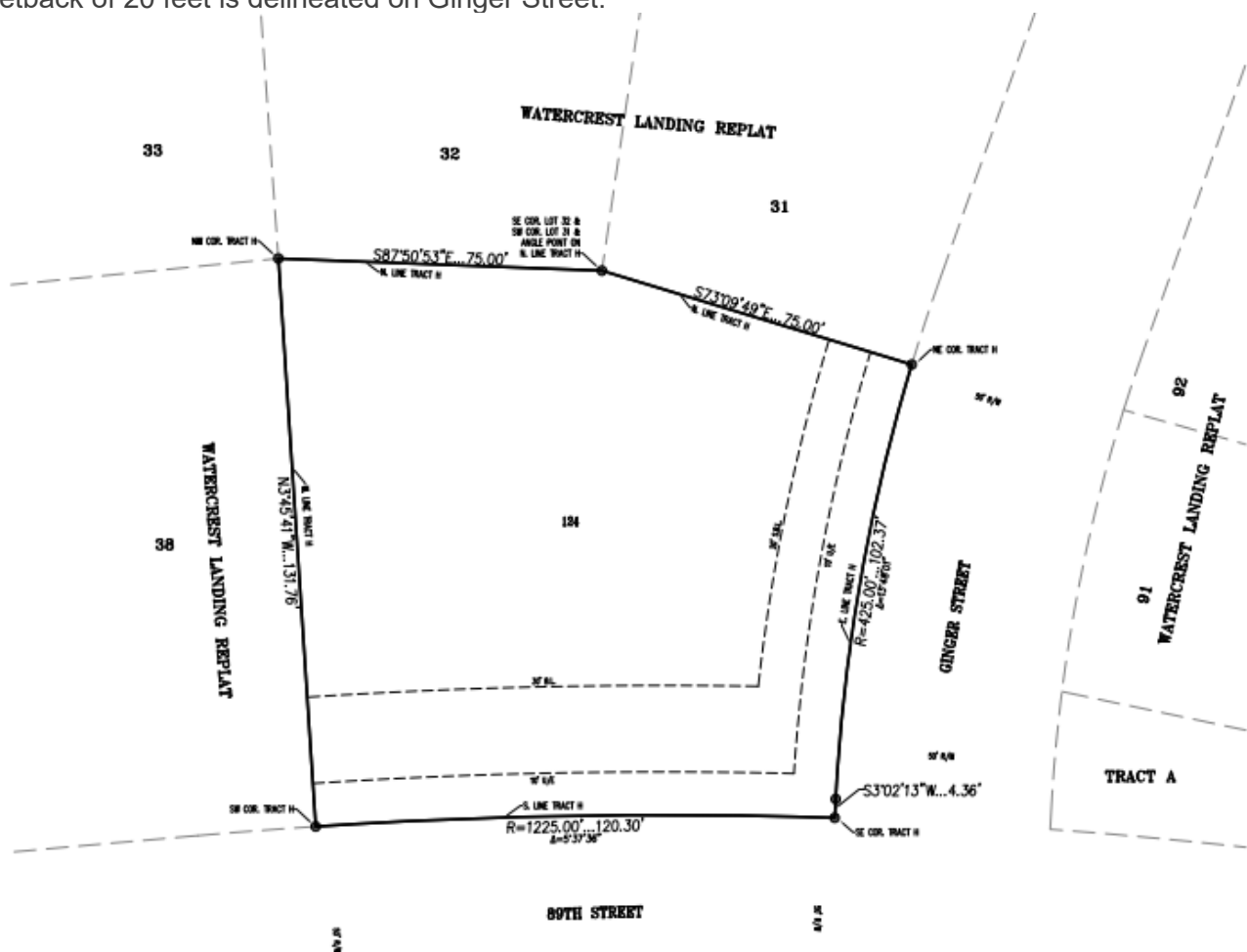


Exhibit 3: Final Plat

## DEVIATIONS

The applicant is not requesting any deviations from the Unified Development Code.



## REVIEW PROCESS

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- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

## RECOMMENDATION FROM PROFESSIONAL STAFF

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★ **Staff recommends approval of the proposed Final Plat for Watercrest Landing, Seventh Plat.**

- This is a replat of one tract from a community amenity space to a single-family lot. There are no new dedications to the City.
- The project is consistent with Lenexa's goals through ***Strategic Community Development*** to create ***Vibrant Neighborhoods***.

## FINAL PLAT

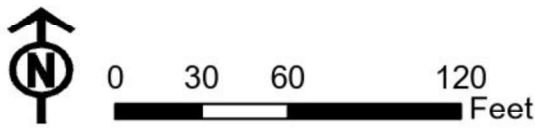
Staff recommends **approval** of the final plat for PT23-09F – **Watercrest Landing, Seventh Plat** at the northwest corner of Ginger Street and 89<sup>th</sup> Street, for one single-family residential lot.



Document Path: C:\Users\cdkimberly\Documents\ArcGIS\Projects\City Council Packet Template\City Council Packet Template.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Watercrest Landing Seventh Plat PT23-09F



# FINAL PLAT OF WATERCREST LANDING, SEVENTH PLAT

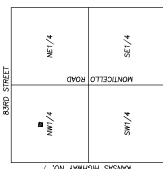
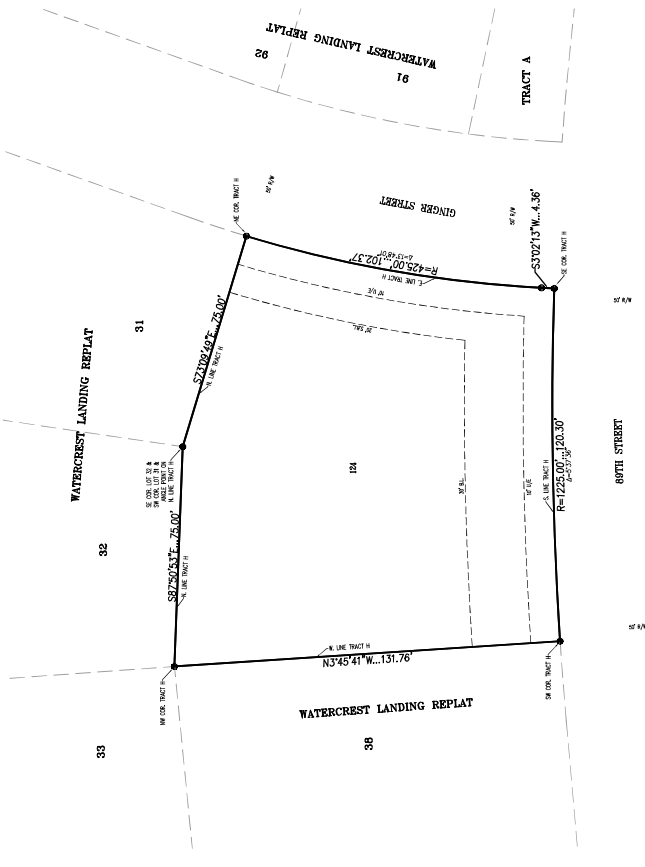
RESURVEY AND REPLAT OF TRACT H, WATERCREST LANDING REPLAT,  
A PLATTED SUBDIVISIONS OF LAND IN THE NORTHWEST QUARTER OF SECTION 34, TOWNSHIP 12  
SOUTH, RANGE 23 EAST, IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS



SCALE: 1"=50'  
BEARING BASIS: RECORDED PLAT OF  
WATERCREST LANDING REPLAT

**LEGEND**

- QUARTER SECTION "A" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "B" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "C" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "D" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "E" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "F" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "G" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "H" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "I" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "J" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "K" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "L" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "M" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "N" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "O" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "P" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "Q" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "R" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "S" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "T" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "U" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "V" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "W" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "X" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "Y" - SEVEN WINDMILLS COOP.
- QUARTER SECTION "Z" - SEVEN WINDMILLS COOP.



NEIGHBORHOOD MAP  
SEC. 34-12-23

PARCEL	AREA (SQ. FT.)	AREA (AC)
124	10888.8	0.2477
125	10888.8	0.2477

**DESCRIPTION**  
This description was prepared by Scott C. Chismann, K.S. #13106 on January 16, 2024, for project no. 20019. Resurvey and replat of Tract H, Watercrest Landing Replat, a Platted Subdivision of Land in the Northwest Quarter of Section 34, Township 12 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, containing 16,198 square feet or 0.377 acres, more or less, registered and.

The undersigned proprietors of the above described tract of land have caused the same to be subdivided in the manner shown on the accompanying plat, which subdivision and plat shall hereafter be known as "WATERCREST LANDING, SEVENTH PLAT". An easement or license is hereby granted to the City of Lenexa, Johnson County, Kansas, to locate, construct and maintain or repair, and to use, for utility lines, conduits, pipes, cables, etc., upon, over and under these areas outlined hereon and depicted on this plat as "U/T" or "Utility Easement" to any ground facilities associated with the utility lines, pipes, conduits, cables, etc., which are permitted shall be constructed, located or maintained in any area of the utility easement that causes no damage to the utility easement.

**RESTRICTIONS**  
Classification Note: Lots 124 shall carry a housing classification of "U".

The undersigned proprietors of the property hereinabove described hereby certify that the Board of Public Safety of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to accept and record this instrument for public use and that the undersigned proprietors have no objection to the same being so recorded and that the undersigned proprietors have no objection to the same being so recorded and that the undersigned proprietors have no objection to the same being so recorded.

**CONSENT TO LEVY**  
The undersigned proprietors of the property hereinabove described hereby certify that the Board of Public Safety of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to accept and record this instrument for public use and that the undersigned proprietors have no objection to the same being so recorded and that the undersigned proprietors have no objection to the same being so recorded.

**EXECUTION**  
IN WITNESS WHEREOF, I, S. Development, LLC, a Kansas Limited Liability Company, owner of the above described tract, have caused this instrument to be executed by its Member this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

Member this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

Mickey M. Venn, Managing Member  
STATE OF KANSAS }  
COUNTY OF JOHNSON }

BE IT REMEMBERED, that on this \_\_\_\_\_ day of \_\_\_\_\_, before me, the undersigned, a Notary Public, in and for the said County and State, came Mickey M. Venn, Managing Member of S. Development, LLC, a Kansas Limited Liability Company, who is personally known to me to be the same person who executed as such member and agent of said company, and such person duly acknowledged the execution of the above written.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year last above written.

Notary Public \_\_\_\_\_ My Appointment Expires \_\_\_\_\_

Printed Name \_\_\_\_\_

APPROVED by the Planning Commission of the City of Lenexa, Johnson County, Kansas, this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

Chairman, Chris Paas \_\_\_\_\_

Mayor, Michael A. Boehm \_\_\_\_\_

ATTEST: City Clerk, Jennifer Martin \_\_\_\_\_

SCOTT C. CHISMANN, KANSAS SURVEYOR #13106  
2024-01-16 10:15 AM  
LENEXA, KS  
ACCEPTED BY THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS, THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.



GOOD NOTE:  
ALL LINES WITHIN THIS PLAT REFERRED TO AS BEING REFERRED TO BE OUTSIDE THE 0.2% ANGLE CHANGE TOLERANCE AS SHOWN ON THE LEGAL RESURVEY PLAT MAP PREPARED BY THE SURVEYOR, JOHNSON COUNTY, KANSAS, PANEL NO. 208F000476, AND DATED AUGUST 8, 2018.

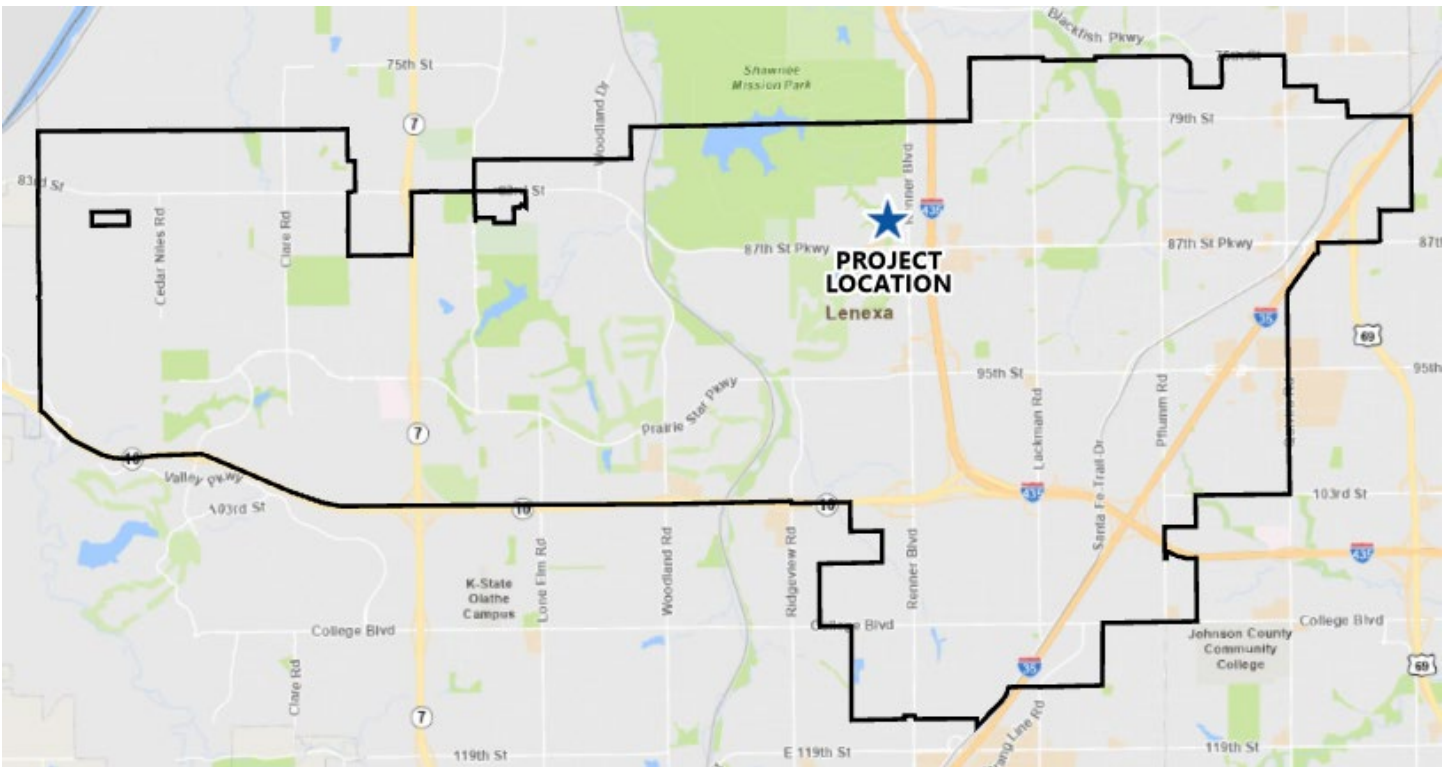
**PH**

PLANNING  
COMMISSION  
CITY OF LENEXA, KANSAS

OFFICE OF THE  
CITY CLERK  
CITY OF LENEXA, KANSAS

(913) 799-1100  
FAX (913) 799-1101

# REDEVELOPMENT PROJECT PLAN 3H FOR THE CITY CENTER TIF DISTRICT (ADVENT HEALTH PHASE 1 – HOSPITAL PROJECT)



## PROJECT SUMMARY

On March 7, 2023, the City Council will consider a resolution giving notice of its intent to hold a public hearing on April 18, 2023 to consider approving Redevelopment (TIF) Project Plan 3H (“Project Plan 3H”). Project Plan 3H covers approximately 13.5 acres located on the northwest corner of Renner Boulevard and the 86<sup>th</sup> Street (the “Project Plan 3H Area”). The Developer for Project Plan 3H is Shawnee Mission Medical Center, Inc. d/b/a Advent Health (the “Developer”).

Pursuant to state law, before a redevelopment project plan may be considered by the City Council at a public hearing and formally adopted, the Planning Commission must first examine the proposed project plan in light of the City’s Comprehensive Plan and make a finding that the two plans are consistent. Attached for review is a copy of the proposed Project Plan 3H.

Project Plan 3H contemplates construction of an approximately 240,000 sq. ft. medical center and an approximately 54,000 sq. ft. medical office building, including associated site work, utilities, and parking facilities and reimbursement with TIF increment generated from the Project Plan 3H Area for Private TIF Reimbursable Costs consisting of associated parking, landscaping, hardscaping, sidewalks, related site amenities, signage, infrastructure improvements, and land acquisition within the Project Plan 3H Area.

A description of all the TIF reimbursable costs, the amount, priority and duration of reimbursements are set forth in a separate Disposition & Development Agreement between the City and the Developer.

Staff believes Project Plan 3H, as proposed, is consistent with the City's Comprehensive Plan for the reasons noted below:

- The Planning Commission recommended approval of the Final Plan for the Project Plan 3H development (PL22-07F) at its June 6, 2022 Planning Commission meeting.
- A principle articulated in the Comprehensive Plan is that the City will create neighborhoods that include varying developments that relate to each other, as well as to parks, schools and other shared amenities. These neighborhoods, larger than just subdivisions, will include or directly relate to the commercial areas that serve their daily needs. They will include these different residential and commercial uses by employing logical and attractively designed transitions. The mixing of uses both vertically and horizontally within City Center helps achieve this purpose
- The Project is located within the City Center North which is intended to be a unique mixed-use development in the geographic center of Lenexa. Development should be tailored to an urban environment, and thus be of a greater intensity or density, with public open space and pedestrian-friendly streets and include a mix of employment, entertainment, office, retail, residential, recreation and civic uses. Parking should primarily be provided on street or in structures, carefully integrated into the building architecture and site layout. While mixing of uses within each building is preferred, the mixing of uses on a street is also desirable.
- The construction of a hospital and office will drive the need for retail and restaurant uses, helping incorporate the mixing of uses, which is an important component of the City Center concept. The mixing of uses may be vertical or horizontal and will provide the opportunity for reduced travel distances, thereby creating greater efficiency in the infrastructure.
- Project Plan 3H includes the construction of sidewalks, landscaping and amenity zone along 86<sup>th</sup> Street and Scarborough St. with the intent of providing a pedestrian friendly area.

## **RECOMMENDATION FROM PROFESSIONAL STAFF**

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Staff recommends adoption of the attached resolution, making findings that Project Plan 3H for the City Center (TIF) District is consistent with the Comprehensive Plan for the City of Lenexa, Kansas.

**RESOLUTION NO. 2023 - \_\_\_\_\_**

**A RESOLUTION FINDING CITY CENTER REDEVELOPMENT (TIF) PROJECT PLAN 3H IS CONSISTENT WITH THE CITY OF LENEXA'S COMPREHENSIVE PLAN FOR DEVELOPMENT (ADVENT HEALTH PHASE 1 – HOSPITAL PROJECT)**

WHEREAS, the Lenexa City Council on September 11, 2001 adopted Ordinance No. 4427 establishing a Redevelopment District pursuant to K.S.A. 12-1770 *et seq.*, as amended (“Original District”); and

WHEREAS, the Original District was amended on December 20, 2005, by Ordinance No. 4824 to include a total of approximately 424 acres (the “District”, also referred to as the “City Center TIF District”); and

WHEREAS, the District is generally described as an area of approximately 424 acres located on all four corners of 87<sup>th</sup> Street and Renner Blvd., as well as a tract of land located east of I-435 at 87<sup>th</sup> St Pkwy; and

WHEREAS, pursuant to K.S.A. 12-1772, as amended, the City prepared Redevelopment Project Plan 3H for the City Center TIF District (“Project Plan 3H”); and

WHEREAS, Project Plan 3H covers an area of approximately 13.5 acres located at the northwest corner of Renner Blvd. and the 86<sup>th</sup> Street in the area referred to as City Center North; and

WHEREAS, K.S.A. 12-1772(b), as amended, requires the Planning Commission to make a finding that Redevelopment Project Plan 3H is consistent with the City of Lenexa Comprehensive Plan before said Plan may be considered at a public hearing and officially adopted by the Lenexa City Council; and

WHEREAS, as required by law, the Planning Commission has reviewed Project Plan 3H in light of the City’s Comprehensive Plan for development; and

WHEREAS, the Planning Commission finds that the proposed development for the District as a mixed use, urban development is a valuable resource for the City; and

WHEREAS, the Planning Commission finds that the development of the property within Project Plan 3H of an approximately 240,000 sq. ft. medical center and an approximately 54,000 sq. ft. medical office building, parking, related site amenities, and associated infrastructure improvements is consistent with the City’s Comprehensive Plan for development.

NOW THEREFORE, BE IT RESOLVED that the Planning Commission:

SECTION ONE: The Lenexa Planning Commission pursuant to K.S.A. 12-1772 hereby finds that Redevelopment Project Plan 3H is consistent with the City of Lenexa Comprehensive Plan for development for the reasons discussed at its meeting on March 6, 2023 and makes the following specific findings:

- The Planning Commission recommended approval of the Final Plan for the Project Plan 3H development (PL22-07F) at its June 6, 2022 Planning Commission meeting.
- A principle articulated in the Comprehensive Plan is that the City will create neighborhoods that include varying developments that relate to each other, as well as to parks, schools and other shared amenities. These neighborhoods, larger than just subdivisions, will include or directly relate to the commercial areas that serve their daily needs. They will include these different residential and commercial uses by employing logical and attractively designed transitions. The mixing of uses both vertically and horizontally within City Center helps achieve this purpose
- The Project is located within the City Center North which is intended to be a unique mixed-use development in the geographic center of Lenexa. Development should be tailored to an urban environment, and thus be of a greater intensity or density, with public open space and pedestrian-friendly streets and include a mix of employment, entertainment, office, retail, residential, recreation and civic uses. Parking should primarily be provided on street or in structures, carefully integrated into the building architecture and site layout. While mixing of uses within each building is preferred, the mixing of uses on a street is also desirable.
- The construction of a hospital and office will drive the need for retail and restaurant uses, helping incorporate the mixing of uses, which is an important component of the City Center concept. The mixing of uses may be vertical or horizontal and will provide the opportunity for reduced travel distances, thereby creating greater efficiency in the infrastructure.
- Project Plan 3H includes the construction of sidewalks, landscaping and amenity zone along 86<sup>th</sup> Street and Scarborough St. with the intent of providing a pedestrian friendly area.

SECTION TWO: This resolution shall become effective immediately upon adoption.

ADOPTED by the City of Lenexa Planning Commission this 6th day of March, 2023.

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Chris Poss, Chairman

**ATTEST:**

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Scott McCullough, Secretary

**APPROVED AS TO FORM:**

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Steven Shrout, Assistant City Attorney



# CITY CENTER REDEVELOPMENT (TIF) PROJECT PLAN 3H CITY CENTER REDEVELOPMENT (TIF) DISTRICT AS AMENDED

(Advent Health Phase 1 – Hospital and Retail)

In accordance with K.S.A. 12-1770 *et seq.*, as amended (the “**Act**”), to promote, stimulate and develop the general and economic welfare of the city of Lenexa, Kansas (“**City**”), the Lenexa City Council adopted Ordinance No. 4427 on September 11, 2001, establishing a Redevelopment (TIF) District (the “**Original District**”). The Original District was amended on December 20, 2005 by Ordinance No. 4824 to include a total of approximately 424 acres (the “**District**,” also referred to as the “**City Center TIF District**”) and legally described in attached **Exhibit A**.

The City has identified multiple City Center development projects located within the City Center TIF District. The area consisting of approximately 67 acres and located on the northwest corner of 87<sup>th</sup> Street Parkway and Renner Boulevard is commonly referred to as “**City Center North Village**” or “**City Center North**”). The Act allows one or more TIF projects to be undertaken by a city within an established district and any such project plan may be implemented in separate development stages. There are currently multiple approved TIF Project Plans within City Center North.

Project Plan 3H (the “**Project Plan 3H**”, also referred to as the “**Project Plan**”) will include approximately 13.5 acres located at the northwest corner of the intersection of Renner Boulevard and 86<sup>th</sup> Street and is legally described on **Exhibit B** (the “**Project Plan 3H Area**”). The improvements anticipated within the Project Plan 3H Area consist of an approximately 240,000 sq. ft. medical center and an approximately 54,000 sq. ft. medical office building including associated site work, utilities and parking facilities and are described in more detail in **Section 5** herein (the “**Project**”). Project Plan 3H shall extend for a period of twenty (20) years from the date the Project Plan is approved by the City (the “**Project Plan Term**”). The Tax Increment (as defined by the Act) generated from the real property in the Project Plan 3H Area during the Project Plan Term is referred to herein as the TIF Revenues (the “**TIF Revenues**”).

## 1. **Comprehensive Feasibility Study.**

City Staff prepared a Comprehensive Financial Feasibility Study (“**Feasibility Study**”) for Project Plan 3H. Projections on development in the Project Plan 3H Area were provided by Shawnee Mission Medical Center, Inc., a Kansas corporation (the “**Developer**”). The Feasibility Study incorporates a number of conservative assumptions, including a constant mill levy of 90.733, which excludes 20 mills from the school levy and the 1.5 State mill levy and it assumes a one (1%) annual increase in appraised valuation after substantial completion of each project phase. The Feasibility Study also assumes the Project is substantially complete by December 2025 with an assessed value of \$2,362,500.

It is expected that the Developer will advance funds necessary to construct the improvements described in **Section 5** herein and to pay the estimated TIF eligible private project reimbursable costs set forth on **Exhibit C** attached hereto (the “**Private TIF**”).

**Reimbursable Costs**”), and that Developer will subsequently be reimbursed with TIF Revenues received by the City from the Project Plan 3H Area on a “pay-as-you-go” basis, in accordance with the terms of a Disposition & Development Agreement (the “**DDA**”). The City will not issue full faith and credit tax increment bonds for this Project. Further, the City does not anticipate issuing special obligation tax increment bonds (“**TIF Bonds**”) for the Project, however, upon the future request of Developer, the City shall reasonably consider any such request to issue TIF Bonds if the market can feasibly support such a bond issue and if the TIF Revenues and any other collateral provided for such TIF Bonds provide reasonable assurance that the principal of and interest on the TIF Bonds will be paid on a timely basis. A decision on whether or not a TIF Bond issue is feasible and adequately secured will be the City’s final decision and within the City’s sole discretion. The City is under no obligation to issue TIF Bonds. The Private TIF Reimbursable Costs are generally described on **Exhibit C**, attached hereto.

Developer will enter into a DDA with the City which will describe the Private TIF Reimbursable Costs in more detail and set forth the amount, priority, process and terms for reimbursement. Reimbursement of Private TIF Reimbursable Costs is dependent upon the amount of TIF Revenues and shall be paid in accordance with the amounts and priority set forth in the DDA. In no event will any Private TIF Reimbursable Costs be reimbursed in an amount that exceeds the amount of TIF Revenues available.

Private TIF Reimbursable Costs must (1) be approved by the City; (2) meet the definition of “redevelopment project cost” set out in K.S.A. 12-1770a(o), as amended; (3) be an eligible expense under the City’s adopted TIF Policy and/or Procedures, unless otherwise permitted in the DDA; (4) be authorized in this Project Plan 3H and in the City Center TIF District Plan; (5) be in compliance with the terms for reimbursement and prioritization described with particularity in a subsequent DDA; and (6) not previously reimbursed by any other public source of revenue. The City approval of Private TIF Reimbursable Costs is subject to available TIF Revenues and further subject to the terms and conditions for reimbursement set forth in a DDA.

Based on the current projections and cash flow analysis contained in the Feasibility Study, it is determined that the Project benefits, TIF Revenues and other available sources, including private revenues, exceed the Private TIF Reimbursable Costs, and that the TIF Revenues and other available revenue sources, including private debt and Developer equity, should be sufficient to reimburse Developer for a portion of the Private TIF Reimbursable Costs. For any improvements constructed by Developer in Project Plan 3H, the Developer is responsible for all expenses, including but not limited to Private TIF Reimbursable Costs. The City is under no obligation to provide financial assistance to supplement TIF Revenues actually received, nor to increase the duration or allocation of TIF Revenues for reimbursement.

The City has authorized a maximum reimbursement of \$9,000,000 to the Developer for eligible **Private TIF Reimbursable Costs** as generally described on **Exhibit C** subject to available TIF Revenues and the terms and conditions set forth in a DDA.

The City reserves the right to amend this Project Plan 3H in accordance with state law provided that such amendments shall not, without the consent of Developer, alter or affect the financial terms of this Project Plan 3H benefitting Developer.

In summary, the City conservatively anticipates the ad valorem property tax increment from Project Plan 3H will generate approximately \$4,451,574 over the Project Plan 3H Term. However, changes in mill levies, the phasing of the Private Project, assessed valuations and legislation, in addition to other factors outside the Parties' control may change the amount of available property tax increment. The Developer will be responsible for all expenses of Developer, including costs associated with constructing the Project as well as the Private TIF Reimbursable Costs even if they exceed the TIF Revenue generated from Project Plan 3H. If the TIF Revenues do not meet the estimate, the City shall be under no obligation to provide financial assistance to Developer beyond the TIF Revenues actually generated from the Project Plan 3H Area in accordance with the distribution formula set out in the approved DDA. A summary of the assumptions and anticipated TIF Revenue is included in **Appendix I**.

## **2.     Redevelopment District Plan and Redevelopment (TIF) Project Plan 3H.**

### Redevelopment District Plan (City Center TIF District Plan)

The City Center TIF District area includes land within the City of Lenexa, Kansas as legally described on **Exhibit A**, but generally described as an area of approximately 424 acres located on all four corners of 87<sup>th</sup> Street and Renner Blvd., as well as a tract of land located east of I-435 at 87<sup>th</sup> St Pkwy. The Redevelopment (TIF) District Plan for the City Center TIF District contemplates development of multiple mixed use urban development projects to include office, retail, residential and public uses to be located on all four corners of Renner Boulevard and 87<sup>th</sup> St Pkwy, as well as on a tract located east of I-435 at 87<sup>th</sup> St Pkwy (the "**City Center Project**").

City Center North Village incorporates approximately 67 acres and is located on the northwest corner of Renner Boulevard and 87<sup>th</sup> Street Parkway. The initial project constructed in City Center North Village includes multiple developments such as a multi-family residential development, single family villas, and a public central green/storm water amenity. A hotel has also been constructed and a multi-tenant building is scheduled to commence construction adjacent to the central green soon. City Center Lenexa is one of the largest developments in the City Center Project and is located on the southwest corner of 87<sup>th</sup> Street Parkway and Renner Boulevard. City Center Lenexa is proposed to be developed as a multi-phase, mixed use project pursuant to a development agreement entered into between the City and its master developer. The City constructed a new civic campus in City Center Lenexa which includes a recreation center, structured parking and a City Hall building which incorporates a public market and an area leased to a university tenant. The civic campus is located south of City Center North Village. City Center East is located east of Renner Boulevard and south of 87<sup>th</sup> Street Parkway and includes a four story commercial office/retail building, multi-level parking structure and a hotel/conference center. The remaining undeveloped City Center East property will include additional commercial properties. City Center Northeast, located north of 87<sup>th</sup> Street Parkway and east of Renner Boulevard contains a multi-phase upscale multi-family

residential and commercial/retail project. Additional City Center projects of a smaller scale are anticipated on the other remaining tracts of land in the City Center District.

In accordance with the City Center District Plan, tax increment may be used to pay for eligible project expenses within specific project plan areas for such items including but not limited to public infrastructure; land acquisition; site preparation; street improvements and their appurtenances; sidewalks; storm and sanitary sewers; utility improvements as permitted in the Act; parks; surface and structured parking facilities; landscaping; water mains; storm water detention; sculptures, public art and similar amenities; plazas and open space; reimbursement for special assessments levied pursuant to KSA 12-6a01 *et seq.* for eligible public infrastructure authorized in the District Plan; and other authorized uses set forth in the District Plan and permitted by the Act and the City TIF Policy & Procedures.

### Redevelopment (TIF) Project Plan 3H

Project Plan 3H incorporates approximately 13.5 acres of improvements located in City Center North Village within the City Center TIF District. The Project Plan 3H Area is will include site improvements described in **Section 5** herein (the “**Project**”) and is legally described in Exhibit B.

### **3. Map of Redevelopment Project Plan 3H Area.**

A map of the Project Plan 3H Area is attached as **Exhibit D**.

### **4. Relocation Assistance Plan.**

No relocation will occur as a result of Project Plan 3H and therefore no relocation assistance plan is provided.

### **5. Description of the Buildings and Facilities Proposed to be Constructed or Improved.**

Developer intends to construct an approximately 240,000 s.f., five-story medical facility and an approximately 54,000 s.f. medical office building as well a associated site work, utilities, private streets, and structured and surface parking facilities. The Private TIF Reimbursable Costs incurred as a result of the Project constructed as part of Project Plan 3H include, but are not limited to: land acquisition, architectural and engineering costs not associated with vertical improvements; site development; surface and structured park parking; lighting; landscaping; hardscape; associated utilities in the right of way; amenities; temporary construction interest associated with the Project financing; public improvements; and other eligible costs permitted by the Act, the City TIF Policy & Procedures and the DDA. The Private TIF Reimbursable Costs are described in more detail in **Exhibit C** and in the DDA.

### **6. Other Relevant Information.**

- a. Reimbursement of TIF Reimbursable Costs shall be made with TIF Revenues actually received by the City from the Project Plan 3H Area and deposited into the special fund established by the City in accordance with K.S.A. 12-1778 (the “**City Center Project Plan 3H Fund**”).
- b. If sufficient TIF Revenues are not available to pay all of the Private TIF Reimbursable Costs, the City is under no obligation to reimburse Private TIF Reimbursable Costs from any other public source.
- c. Prior to any reimbursement of Private TIF Reimbursable Costs, each entity receiving reimbursement with TIF Revenues (excluding the City) shall enter into a separate, valid and enforceable DDA with the City. The procedure for distribution, reimbursement and priority of payment of the Private TIF Reimbursable Costs shall be set out in the DDA and consistent with this Project Plan 3H.

## EXHIBIT A

### LEGAL DESCRIPTION OF CITY CENTER REDEVELOPMENT DISTRICT

Beginning at the Northwest corner of the Northeast Quarter of Section 31, Township 12 South, Range 24 East; thence South along the West line of the Northeast Quarter of said Section 31 to the Southwest corner of the Northeast Quarter of said Section 31; thence East along the South line of the Northeast Quarter of said Section 31 to the Southeast corner of the Northeast Quarter of said Section 31, and continuing East along the South line of the Northwest Quarter of Section 32, Township 12 South, Range 24 East to the Easterly right-of-way line of Renner Boulevard as it now exist; thence North along the Easterly right-of-way line of said Renner Boulevard to the intersection with the South line of the Northwest Quarter of the Northwest Quarter of Said Section 32; thence East along the South line of the Northwest Quarter of the Northwest Quarter of said Section 32, to the intersection with the centerline of Interstate Route 435, as it now exists; thence South along the centerline of said Interstate Route 435 to the South line of the Northwest Quarter of said Section 32; thence East along the South line of the Northwest Quarter of said Section 32, to the Southeast corner of the Northwest Quarter of said Section 32; thence North along the East line of the Northwest Quarter of said Section 32, to the Northeast corner of the Northwest Quarter of said Section 32; thence West along the North line of the Northwest Quarter of said Section 32, to the centerline of Interstate Route 435, as it now exists; thence North along the centerline of said Interstate Route 435 to the North line of the Southwest Quarter of Section 29, Township 12 South, Range 24 East; thence West along the North line of the Southwest Quarter of said Section 29, to the Northwest corner of the Southwest Quarter of said Section 29: thence South along the West line of the Southwest Quarter of said Section 29, to the Northeast corner of the Southeast Quarter of the Southeast Quarter of Section 30, Township 12 South, Range 24 East; thence West along the South line of Stonecreek of Parkhurst 1<sup>st</sup> Plat, Stonecreek of Parkhurst 2<sup>nd</sup> Plat, and Estates of Parkhurst 1<sup>st</sup> Plat, all subdivisions of land in Johnson County, Kansas, to the Northeast corner of Horizons West Re-Plat 3<sup>rd</sup> Plat, a subdivision of land in Johnson County, Kansas; thence South along the East line of said Horizons West Replat 3<sup>rd</sup> Plat, and its extension South, to the South line of the Southeast Quarter of said Section 30; thence West along the South line of the Southeast Quarter of said Section 30 to the Point of Beginning, containing approximately 424 acres.

## EXHIBIT B

### TIF PROJECT PLAN 3H AREA

All that part of the Southeast Quarter of Section 30, Township 12 South, Range 24 East of the 6th P.M., in the City of Lenexa, Johnson County, Kansas, more particularly described by Roger B. Dill, Kansas PS# 1408, on June 21, 2022, as follows:

Commencing at the Southeast corner of the Southeast Quarter of said Section 30; thence North 02°12'39" West, along the East line of said Southeast Quarter, a distance of 1327.04 feet to the Southeast corner of STONECREEK OF PARKHURST, 1ST PLAT, a platted subdivision in said City; thence South 87°29'00" West, along the South line of said STONECREEK OF PARKHURST, 1ST PLAT, a distance of 256.44 feet to a point on the Westerly right-of-way line of Renner Boulevard, as it now exists, said point being the Point of Beginning; thence along said Westerly right-of-way line, the following six courses; thence South 72°53'48" East a distance of 100.39 feet; thence South 15°53'39" East a distance of 303.02 feet; thence South 02°12'39" East a distance of 137.64 feet; thence South 38°41'54" West a distance of 43.09 feet; thence South 59°13'27" West a distance of 43.70 feet; thence South 01°15'24" West a distance of 77.26 feet to a point of intersection with the Southerly right-of-way line of 86th/ Street, as it now exists; thence North 90°00'00" West, along said Southerly right-of-way line, a distance of 792.19 feet; thence westerly, continuing along said Southerly right-of-way line, on a curve to the left having a radius of 168.00 feet, and a central angle of 16°28'47", an arc length of 48.32 feet to a point on the Easterly right-of-way line of Elmridge Street, as it now exists; thence along said Easterly right-of-way line and Southerly right-of-way line, for the following three courses: thence South 47°27'11" East a distance of 0.04 feet; thence South 42°32'49" West a distance of 60.00 feet; thence North 47°27'11" West a distance of 112.00 feet, to a point on the Northerly right-of-way line of said Elmridge Street; thence North 42°32'49" East, along said Northerly right-of-way line, a distance of 19.07 feet; thence North 49°00'00" West a distance of 70.20; thence North 00°00'00" East a distance of 468.13 feet to a point on the South line of Tract "B", STONECREEK OF PARKHURST, 2ND PLAT, a platted subdivision in said City of Lenexa; thence North 87°29'00" East along the South line of said Tract "B", STONECREEK OF PARKHURST, 2ND PLAT, and Tract "A", said STONECREEK OF PARKHURST, 1<sup>ST</sup> PLAT, a distance of 885.78 to the Point of Beginning, containing 590,425 square feet, or 13.554 acres, more or less.

**EXHIBIT C**  
**TIF Reimbursable Costs**

The following items are estimated costs eligible for reimbursement with TIF Revenues generated from Project Plan 3H. The items are described in more detail in the DDA.

Description of Expenditure	Reimbursement to:	Maximum Reimbursement
<b>TIF Fee</b>	Developer <sup>1</sup>	\$50,000
<p><b>Itemized Private TIF Reimbursable Costs paid by Developer, including:</b></p> <p>Land acquisition within the Project Plan area; site development; A/E (excluding vertical buildings owned or leased by the Developer other than parking structures); water mains; surface and structured parking; landscaping, lighting, sidewalks, benches and similar amenities; utilities; and temporary construction interest.</p>	Developer	\$9,000,000
<b>Annual Administrative TIF Fee:</b> 0.5% of the annual TIF Revenues reimbursed to Developer	City	TBD
<b>Total Maximum Aggregate of Eligible Private TIF Reimbursable Costs</b>	Developer	\$9,050,000
<b>Total Maximum TIF Reimbursable Costs</b>		<b>\$9,050,000</b> <sup>1,2,3,4</sup>

<sup>1</sup> This sum shall be reimbursed to Developer if paid to City by Developer, and if not, it shall be deducted from the first TIF Reimbursable Cost payment (and thereafter until paid in full) and paid to the City. This sum excludes the Annual Administrative TIF Fee as it is TBD based upon eligible TIF Revenues disbursed.

<sup>2</sup> This amount includes sums which may be included in a subsequent Community Improvement District. At the time of certifying its costs to the City, Developer must elect the Project Plan and the incentive source for which it desires reimbursement and it shall not submit those same costs for reimbursement from any other reimbursement source. Further, the total Private TIF Reimbursable Costs may be less depending upon the priority and duration of reimbursement as set forth in a separate DDA.

<sup>3</sup> The Total TIF Reimbursable Costs do not include interest carry costs which are not an eligible expense in this Project Plan. Interest during construction is an eligible cost to the extent permitted by the TIF Act and shall be reimbursed in accordance with the provisions of the DDA. The interest shall be simple interest and shall not be compounded.

<sup>4</sup> The City has only authorized a maximum reimbursement to the Developer of \$9,050,000 for Private TIF Reimbursable Costs. The Developer will be responsible for all expenses of Developer, including costs associated with constructing the Project as well as the Private TIF Reimbursable Costs even if they exceed the TIF Revenue generated from Project Plan 3H.

Notwithstanding any other provision of this Plan to the contrary, reimbursable expenditures shall at all times be consistent with the Act, including judicial interpretation of the Act.

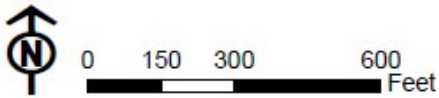


Exhibit D



Data Source: City of Lenexa and Johnson County, Kansas  
For further information, please call 913-477-7030

**Advent Health Lenexa City Center**  
City Center Project Plan 3H



## APPENDIX 1 – Feasibility Study

Jan 2023

### City Center TIF District TIF Revenue Projections Advent Health Project Phase 1

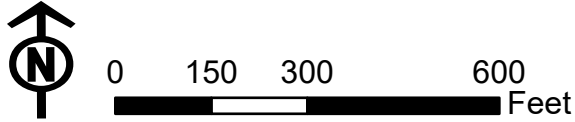
Year of TIF (1)	Distribution Year (2)	Total Assessed Value (3)	Base Year Assessed Value (4)	Captured Assessed Value (Column 3 - Column 4) (5)	Projected Property Tax Increment (6)
1	2023	\$784,423	\$13,194	\$771,229	\$34,988
2	2024	\$1,181,250	\$13,194	\$1,168,056	\$105,981
3	2025	\$2,362,500	\$13,194	\$2,349,306	\$213,160
4	2026	\$2,386,125	\$13,194	\$2,372,931	\$215,303
5	2027	\$2,409,986	\$13,194	\$2,396,792	\$217,468
6	2028	\$2,434,086	\$13,194	\$2,420,892	\$219,655
7	2029	\$2,458,427	\$13,194	\$2,445,233	\$221,863
8	2030	\$2,483,011	\$13,194	\$2,469,817	\$224,094
9	2031	\$2,507,841	\$13,194	\$2,494,647	\$226,347
10	2032	\$2,532,919	\$13,194	\$2,519,725	\$228,622
11	2033	\$2,558,248	\$13,194	\$2,545,054	\$230,920
12	2034	\$2,583,830	\$13,194	\$2,570,636	\$233,242
13	2035	\$2,609,668	\$13,194	\$2,596,474	\$235,586
14	2036	\$2,635,765	\$13,194	\$2,622,571	\$237,954
15	2037	\$2,662,123	\$13,194	\$2,648,929	\$240,345
16	2038	\$2,688,744	\$13,194	\$2,675,550	\$242,761
17	2039	\$2,715,631	\$13,194	\$2,702,437	\$245,200
18	2040	\$2,742,787	\$13,194	\$2,729,593	\$247,664
19	2041	\$2,770,215	\$13,194	\$2,757,021	\$250,153
20	2042	\$2,797,917	\$13,194	\$2,784,723	\$252,666
21	2043	\$2,825,896	\$13,194	\$2,812,702	\$127,602
<b>Total Projected Property Tax Increment</b>					<b>\$4,451,574</b>
<b>Net Mill Levy</b>					<b>90.733</b>
<u>Assumptions:</u>					
a) TIF Mill Levy is 90.733 mills in all years.					
b) Estimated assessed value upon completion (1/1/2024) is \$2,362,500;					
c) Base year assessed valuation is \$13,194 for parcels IF241230-4001.					
c) Assessed value increases by 1% annually after completion.					
d) Property tax collection rate will be 100%.					
e) Property tax increment is distributed twice each year.					
f) Year 1 and Year 21 are half-year distributions					



Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

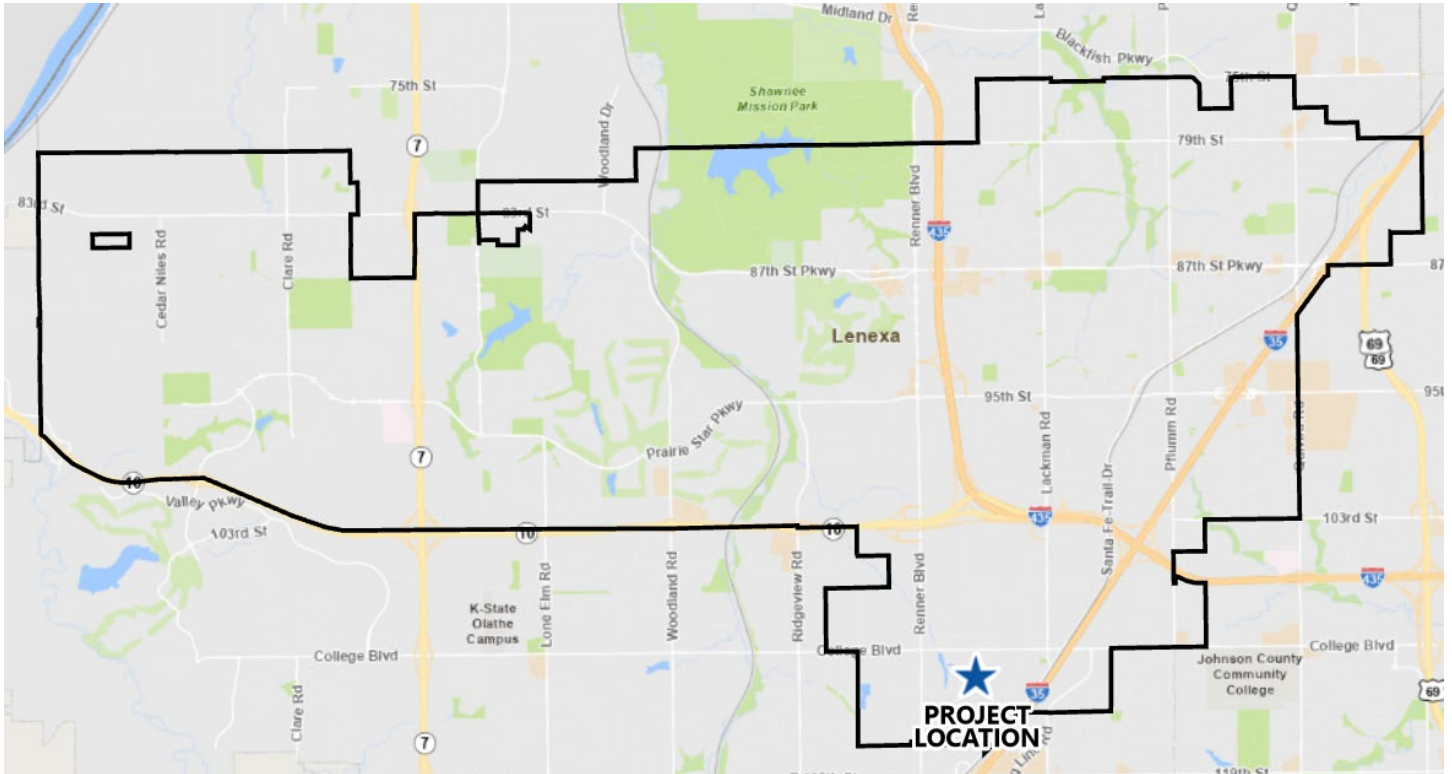
# Advent Health Lenexa City Center

## City Center Project Plan 3H



## AUTO DRIVEAWAY

<b>Project #:</b>	SU23-02	<b>Location:</b>	South of 16105 W. 113 <sup>th</sup> Street
<b>Applicant:</b>	Kendall Harrell	<b>Project Type:</b>	Special Use Permit
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Parking Lot, Commercial



### PROJECT SUMMARY

The applicant proposes to enclose a portion of an existing parking lot to use as a secured parking area, in a corner of the parking lot south of 16105 W. 113<sup>th</sup> Street. Up to 75 vehicles in functioning mechanical condition will be kept in the secured lot. The development includes construction of fencing around the secured parking area but does not propose any other site development. This application is being reviewed in conjunction with a staff-level final plan review (PL23-01FSR). The special use permit requires approval by the by the Governing Body. This project requires a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL FOR FIVE YEARS**

## SITE INFORMATION

The parking lot was constructed in 2006-2007 and was reviewed under staff review final plan PL06-08FSR. At the time of construction, the parking lot was intended to serve as additional office parking for Ecoworks at Southlake Technology Park.

In 2021, building permit B21-2051 was approved for a salt storage tent and screening fence in the southwest corner of the parking lot. The salt tent serves the Ecoworks parking lots.

<b>LAND AREA (AC)</b> 10.37	<b>BUILDING AREA (SF)</b> N/A	<b>CURRENT ZONING</b> BP-2	<b>COMP. PLAN</b> Business Park
--------------------------------	----------------------------------	-------------------------------	------------------------------------



**Exhibit 1: Aerial Image of Subject Site**



**Exhibit 2, 3: Secured area as outlined in red.**

## LAND USE REVIEW

Auto Driveaway is a vehicle relocation company that serves as a third party between buyer and seller of corporate vehicle fleets. Auto Driveaway receives the vehicle from the seller and stores it on their lot while licensing, title and registration is transferred to the new owner. The company is relocating their office from Overland Park to 16011 College Boulevard. The Lenexa office location will house 15 to 20 full-time employees. The office use is allowed by-right in the zoning district.



**Exhibit 4: Location of the proposed office space for Auto Driveaway in relation to the secured parking lot.**

The use of a commercial parking lot requires a special use permit in the BP-2, Planned Manufacturing District. The commercial parking lot will be used for the intermittent storage of vehicles while paperwork is processed. Vehicles will be stored in the secured parking area for two to six weeks before being delivered to the final owner. The secured lot is proposed to accommodate an estimated 75 vehicles which will be parked in standard parking spots. Approximately four to six vehicle trips will be made daily. Vehicles will be driven in and out of the lot by Auto Driveaway staff only. Clients will not have access to the lot or secured vehicles and there will be no on-site sales to private individuals.



Exhibit 5: Example image of a comparable Auto Driveaway parking lot in Vienna, Virginia.

Zoning Map

Future Land Use Map



**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Business Park	BP-2	Parking Lot
<b>North</b>	Business Park	BP-2	Office
<b>South</b>	Business Park	BP-2	Warehouse
<b>East</b>	Business Park	BP-2	Undeveloped Lot
<b>West</b>	Business Park	BP-1	Office

The proposed use is compatible with existing surrounding uses and land use designations. The secured portion of the parking lot will be located in the furthestmost southeast corner of the parking lot where it will not be visible from the public right-of-way.

## **SPECIAL USE PERMIT REVIEW**

Staff recommends approval of a special use permit for a commercial parking lot for five years, which is consistent with the approval of a previous commercial parking lot for Reed Automotive in 2020.

Staff provides the following analysis to the review criteria within [Section 4-1-G-5](#) of the UDC.

### **1. The character of the neighborhood.**

The secured parking lot is in a secluded area, located at the intersection of the dead-end of Corporate Avenue and a private drive. Nearby office buildings have large parking lots to the north and west and there is an abundance of parking in the general area. Industrial uses to the south are buffered from this site by an existing grove of trees.

### **2. The zoning and use of properties nearby.**

Surrounding zoning and uses are described in Table 1. The proposed use is compatible with surrounding zoning and uses. The use of the lot for commercial storage will not be significantly different than the existing use as a standard parking lot.

### **3. The suitability of the subject property for the uses to which it has been restricted.**

The subject property is a good location for the proposed use. The parking lot is in a secluded area with existing perimeter landscaping. The lot is not used to full capacity to serve the office building for which it was built. The lot is underutilized for the existing Ecoworks building and provides an amount of parking beyond the code required standard.



**TABLE 2: PARKING STANDARDS**

Use	Standard	Required	Provided
Office (129,484 SF)	1 space per 250 SF	518	743
Salt Tent			Occupies 7 stalls
Secured Parking			Occupies 82 stalls
<b>Total Available for Office Use</b>		<b>518</b>	<b>654 (Surplus 136)</b>

**4. The extent to which the proposed use will detrimentally affect nearby property.**

The proposed use will not detrimentally affect nearby property. The site itself is in a secluded location with existing screening between the parking lot and the adjacent building to the south.

**5. The length of time the subject property has remained vacant as zoned.**

The property is not vacant. The lot serves the existing Ecoworks building, although it is underutilized because the building tenants do not need all of the provided parking at this time. The parking lot also houses a salt storage area that takes up a small corner of the parking lot.

**6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.**

There will be no gain to the public health, safety, and welfare if the application is denied.

**7. Recommendation of City's permanent professional staff.**

See staff's recommendation at the end of this report.

**8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.**

The requested special use permit will not change the zoning of the property or the Master Plan designation.

**9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.**

Required utilities to serve the use are existing and adequate.

**10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.**

The proposed use would generate a low number of vehicle trips. The projected number of vehicle trips are much lower than the potential vehicle trips if the parking lot was fully utilized as a daily parking lot for

nearby employers. No adverse impacts to the capacity of the street network are anticipated due to the overall reduction in the potential trips to the parking lot and the fencing of a portion of the parking lot.

**11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.**

All vehicles to be kept in the secured lot will be in working order. There will be no on-site mechanical work done on any of the vehicles. There will not be any negative environmental impacts from keeping the vehicles in the secured lot. The storage lot will not be significantly different than the existing use of an office parking lot and would likely improve overall security of the site.

**12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.**

There are no proposed modifications to the stormwater system or increases to the overall amount of impervious area. All vehicles will be kept in working order and will not be leaking fluids. For these reasons the proposed development would not adversely affect the capacity or water quality of the stormwater system.

**13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.**

The applicant demonstrated in the plan set an ability to satisfy applicable zoning regulations and other ordinances.

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## DEVIATIONS

The applicant is not requesting any deviations

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## REVIEW PROCESS

- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on March 21, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

---

## RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Conduct a Public Hearing.**

★ **Staff recommends approval of the proposed Special Use Permit for Auto Driveway.**

- This is a special use permit for a commercial parking lot. The special use permit is being reviewed in conjunction with a staff-review only final plan.
- The project is consistent with Lenexa's goals through *Strategic Community Investment* to create *Thriving Economy*.

## **SPECIAL USE PERMIT**

Staff recommends **approval** of SU23-02 - a special use permit for a commercial parking lot with a maximum of 75 parking spaces for **Auto Driveway** on property directly south of 16105 W. 113<sup>th</sup> Street for a period of five years.



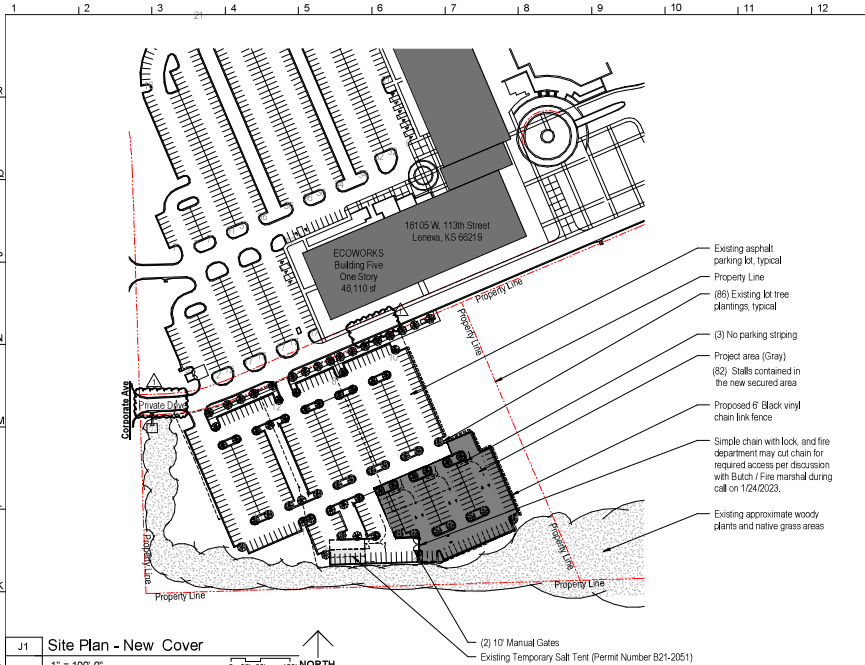
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Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Auto Driveway Special Use Permit

## SU23-02





**General Project Information:**  
 Project Name: Auto Driveway Franchise Systems, LLC  
 Project Address: 16105 W 113th St Lenexa, KS 66219  
 Proposed Use: Chain link fencing enclosure for a section of an existing paved surface lot.  
 Lot originally approved 06/03/2006 and platted under PL 05-08 FSR

NO CHANGE - Impermeable surface area  
 NO CHANGE - Existing curb cuts  
 NO CHANGE - Existing paved parking area  
 NO CHANGE - Curb, gutter and storm inlets  
 NO CHANGE - Bio Retention and Swales  
 NO CHANGE - In landscape  
 (8) Existing lot tree plantings along with existing perimeter woody plantings and grasses.

**Applicable Building Codes:**  
 2018 International Building Code  
 2018 International Fire Code  
 2018 International Plumbing Code  
 2018 International Energy Code  
 2018 Mechanical Code  
 2018 Fuel Gas Code  
 2017 National Electric Code  
 2006 Accessibility Code - (ICCANSI A117.1-2003)

**Accessibility:**  
 Areas of construction has been designed to meet the minimum requirements of the Americans with Disabilities Act (ADA) of 2010 and ANSI 117.1-2009.

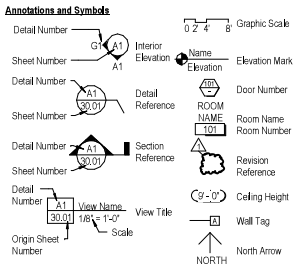
**Site Data:**

Address: NA - City of Lenexa, Johnson County, KS  
 Associated building address: 16105 West 113 St

Parcel ID	IP2601000/004
Vicinity Area	Section 17, Township 13, Range 24 - Lot 4 Block 6
Zoning	BP2 Industrial
Site Area	5.76 Acres approximately
Lot Coverage (Building + Pavement)	126,502 sf (approximately 2.95 Acres)
Total Open Space	122,404 sf (approximately 2.81 Acres) = 49%
Fencing	Proposed 6' Black Vinyl coated chain link with swing door access
Fenced Area	35,648 sf

Parking Requirement				
Designation	Parking Design Standard	Building sf	Required Parking	Total Parking Provided
Office	1/123 sf	129,484	516	743 Existing with 7 reduced by Temp. Salt Tent 736 Existing w/ 82 Secured

**J13 Project Information**  
Not to Scale



**Abbreviations**

OFICI - Owner Furnished/ Contractor Installed  
 OFIOI - Owner Furnished/ Owner Installed  
 CFICI - Contractor Furnished/ Contractor Installed  
 TPICI - Tenant Provided/ Contractor Installed  
 TPFOI - Tenant Provided/ Owner Installed  
 A.F.F. - Above Finished Floor  
 N.I.C. - Not in Contact  
 V.I.F. - Verify In Field  
 O.C. - On Center  
 F.R.T. - Fire Retardant Treated

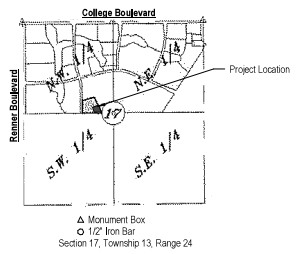
**A19 Symbols Legend**  
Not to Scale

**Drawing Index**  
 Architectural  
 A0 Cover  
 A1 Plans and Elevations

**G20 Sheet Index**  
Not to Scale



**L20 Location Map**  
Not to Scale



**G20 Vicinity Map**  
Not to Scale

- General Notes**
- Do not scale these drawings for construction purposes. In the event of omission of necessary dimensions the contractor shall notify the construction manager.
  - Refer to elevations or details for dimensions not indicated on plan drawings.
  - Verify size, location, and characteristics of all work and equipment to be installed (whether furnished by tenant/owner or by contractor(s)) before any construction pertaining to same is started.
  - Coordinate with all trades in order to maintain scheduled ceiling heights. Verify that required operations and maintenance clearances are provided for all equipment items.
  - All items noted as by Others' or by Tenant/Owner' are to be coordinated with new construction.
  - Install all items in accordance with the manufacturer's written instructions except where the drawings are more stringent. Notify the construction manager in writing of any conflicts.
  - Perform all work in accordance with applicable local codes and ordinances and under requirements established by general conditions of the contract.
  - Secure all permits and approvals as required by authorities having jurisdiction and utility companies prior to beginning work.
  - Remove rubbish from premises as often as necessary or as directed in order to keep premises clean and exit ways clear. Provide complete and thorough cleaning periodically as the work progresses and upon completion of construction.
  - Install work readily accessible for operation, maintenance, and repairs.
  - Drawings are prepared to scale unless noted NTS (not to scale)
  - All notes and details marked "typ." apply to similar conditions throughout the project whether specifically noted or not.
  - The location and dimension of the existing architectural, structural, and MEP elements are taken from pre-site surveys. Field verification is required by the contractor prior to any construction involving these items.
  - All shop drawings shall be submitted to the architect for review prior to fabrication or purchasing.
  - Coordinate any detail color questions or clarifications with architect.



**A1 General Construction Notes**  
Not to Scale

**GastingerWalker &**  
 Architects  
 817 Hyattsville  
 Kansas City, Missouri 64110  
 Phone: 816.452.1230  
 Fax: 816.452.1231  
 Email: info@gastingerwalker.com  
 Construction Managers  
 817 Hyattsville  
 Kansas City, Missouri 64110  
 Phone: 816.452.1230  
 Fax: 816.452.1231  
 Email: info@gastingerwalker.com

**Auto Driveway Franchise Systems, LLC Fence**  
 16105 W 113th St  
 Lenexa, KS 66219

**ISSUED FOR:**

Preliminary Review	20 January 2023
Permit	27 January 2023
Response to Comments	14 February 2023

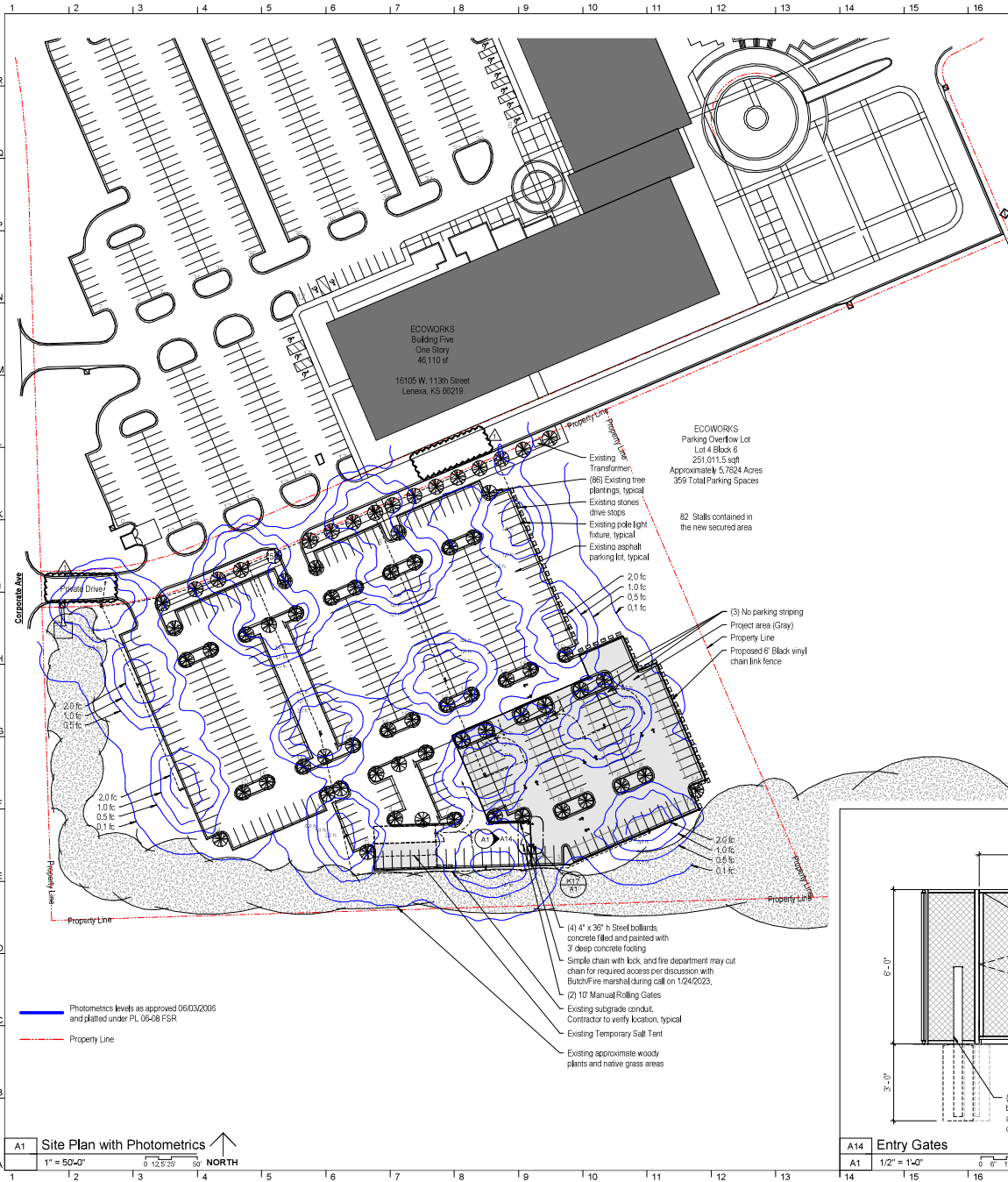


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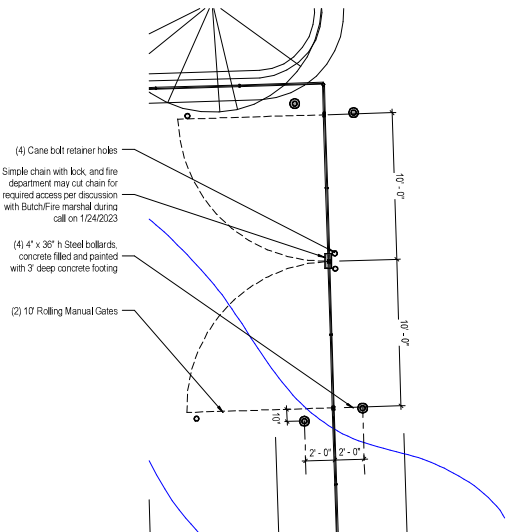
Cover

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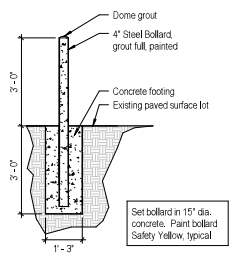
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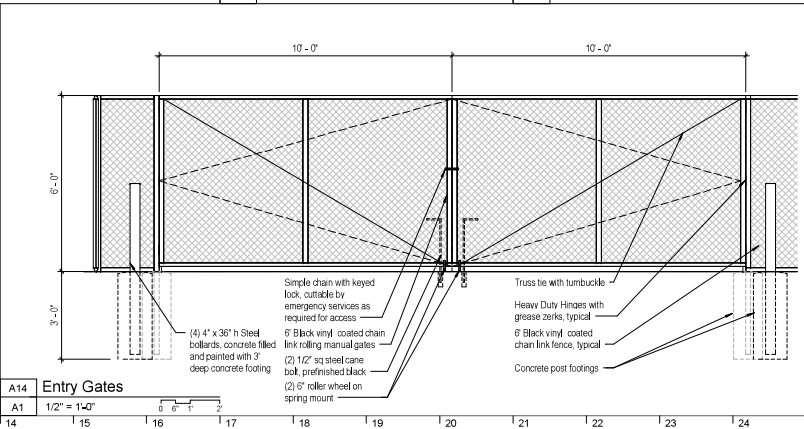
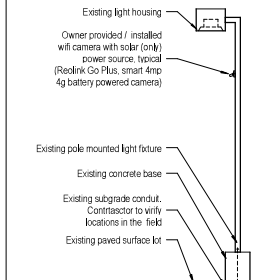
**K17 Entry Gate Plan**  
 A1 1/4" = 1'-0"



**F17 Typical Bollard**  
 1/2" = 1'-0"



**F21 Typical Pole Light**  
 1/4" = 1'-0"



**A14 Entry Gates**  
 A1 1/2" = 1'-0"

**GastingerWalker &**  
 Architects Interior Designers Construction Managers  
 617 Hyattville Kansas City, Missouri 64110  
 816.452.6200 | gawalk.com

**Auto Driveway Franchise Systems, LLC Fence**  
 16105 W 113th St  
 Lenexa, KS 66219

**ISSUED FOR:**  
 Preliminary Review 20 January 2023  
 Permit 27 January 2023  
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Drawn By: MS  
 Checked By: KR

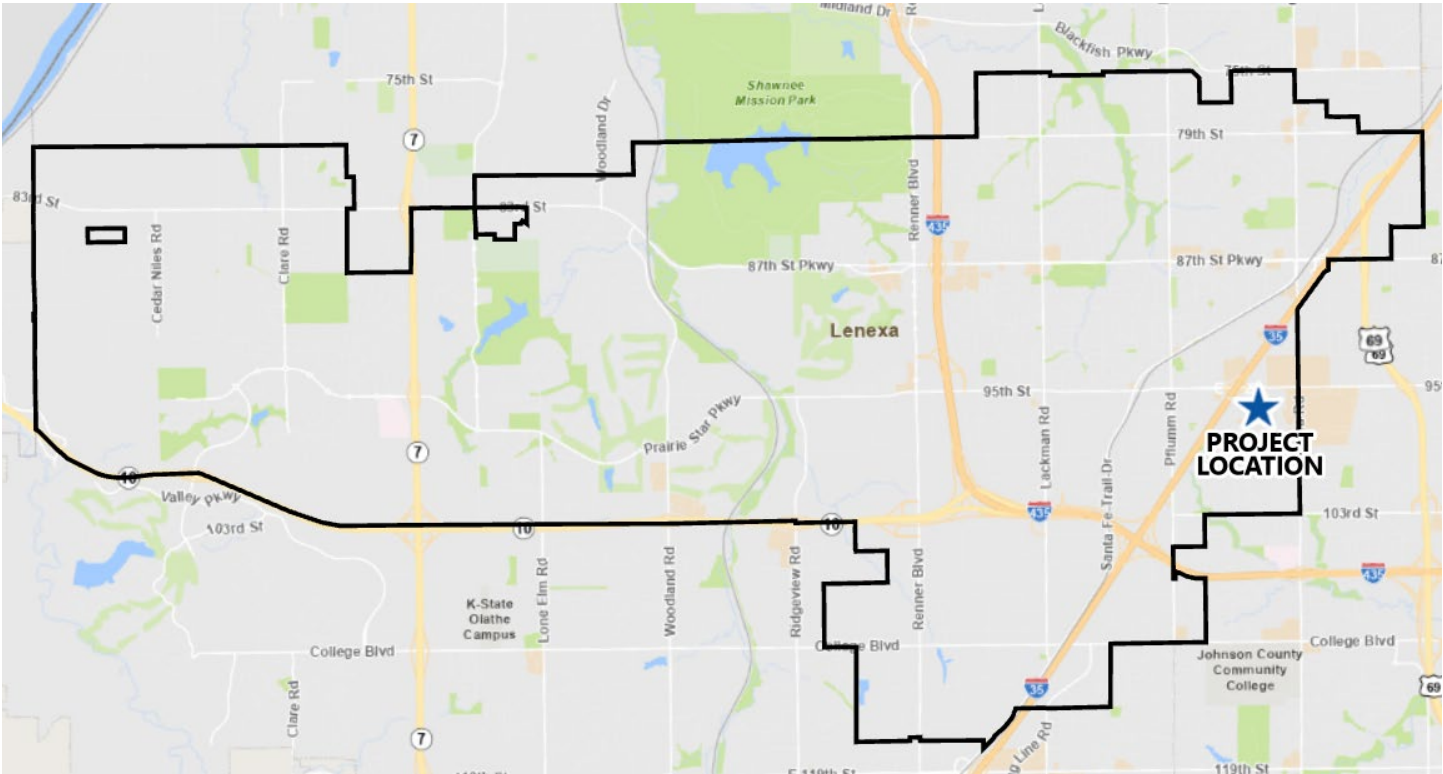
Plans and Elevations

**A1**

Project Number: 2022.023 Copyright 2023

# STORTROPOLIS

<b>Project #:</b>	SU23-01 & PL23-01P	<b>Location:</b>	SWC of 96 <sup>th</sup> Terrace & Rosehill Road
<b>Applicant:</b>	Judd Claussen, Phelps Engineering	<b>Project Type:</b>	Special Use Permit & Preliminary Plan
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Self-Storage



## PROJECT SUMMARY

The applicant proposes to construct a self-storage facility at the southwest corner of 96<sup>th</sup> Terrace and Rosehill Road. The development will include five buildings with a total floor area of 170,640 SF. The primary four-story structure designated as “Building A” is proposed to house 944 units with 156,300 SF of floor area with a maximum building height of 45 feet. Buildings B, C, D, and E include a combined total of 16 standard units and 16 units for recreational vehicle (RV) storage.

The development includes construction of a new public sidewalk along Rosehill Road and a connection to the existing public sidewalk along 96<sup>th</sup> Terrace and related infrastructure improvements. The applicant requests approval of a special use permit and preliminary plan for the development with one deviation from the Unified Development Code (UDC) related to required bicycle parking. Staff supports the proposed deviation. The preliminary plan is intended to serve as a preliminary plat whereas two unplatted parcels will be combined into one, 3.3 acre lot. This project requires a Public Hearing.

**STAFF RECOMMENDATION: APPROVAL WITH CONDITIONS**

## SITE INFORMATION

The subject site includes two separate parcels. The northern parcel is not platted and has not been developed. The southern parcel is not platted and has only been partially developed as an auxiliary parking lot to serve the lot to the north. The parking lot was constructed in the late 1980's.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
3.3	170,640	CP-2	Community Retail



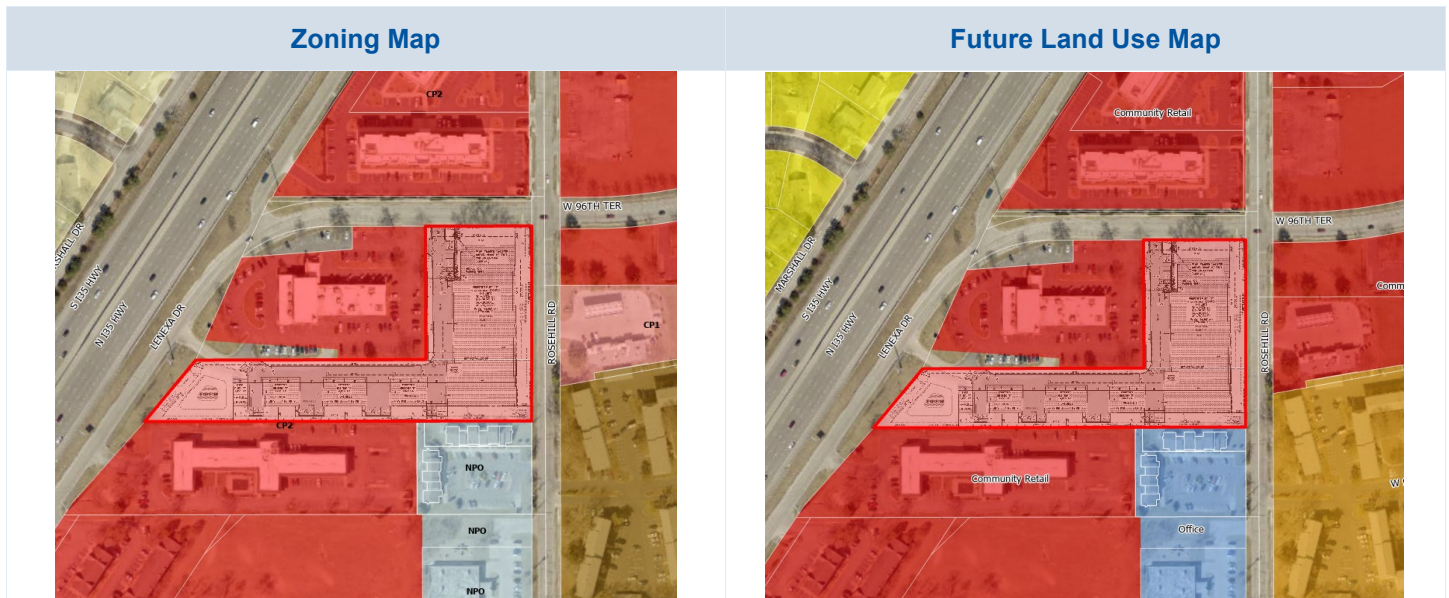
*Exhibit 1: Aerial image of subject site.*

## LAND USE REVIEW

This proposal is for a self-storage facility, StorTropolis, with standard units and units for RV or vehicle storage. The 3.3-acre site will consist of approximately 170,000 SF and 976 units. The use of a self-storage facility is allowed in the CP-2 Zoning District with approval of a special use permit and is subject to the supplementary use regulations as outlined in [Section 4-1-B-23-Z](#) of the UDC.

StorTropolis will operate Monday through Friday from 9:30am to 5:30pm and Saturday from 9:00am to 4:00pm. Customers will have access to the facility via a secured gate and unique pin code from 6:00am to 10:00pm. During standard office hours there will be one to two employees on-site. The perimeter of the site will be secured by a fence with gated entries with keypads. Each customer will have a unique pin to enter the secured facility. Additional security will be provided through the use of security cameras.





**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	Community Retail	CP-2	Vacant Land
<b>North</b>	Community Retail	CP-2	Hotel and Office
<b>South</b>	Community Retail and Office	CP-2 and NP-O	Hotel and Office
<b>East</b>	Community Retail and High Density Residential	CP-2, CP-1 and RP-4	Hotel, Convenience Retail and Apartments
<b>West</b>	I-35 Highway	Right-of-way	Highway

The location is an appropriate fit for the proposed use. Surrounding uses include hotel, office and a strip retail complex. A small corner of the southeast portion of the site is across Rosehill Road from an apartment complex. Other than this, there is not nearby residential.

## SPECIAL USE PERMIT REVIEW

The subject site is zoned CP-2, Planned Community Commercial District. Self-service storage uses are subject to the supplementary use regulations found in [Section 4-1-B-23-Z](#) of the UDC, which has the following standards:

1. Rental unit door openings shall face toward the interior of the development, except that outward orientation may be allowed if it can be demonstrated that the adjoining land use is such that it will not be impacted.
2. Buildings shall have architectural design treatment on all sides. The architectural style shall be compatible with the predominant area land uses.
3. When proposed, fencing and walls shall be constructed with materials and design elements that are compatible with the predominant area land uses.

4. When proposed, fencing and walls shall be constructed with materials and design elements that are compatible with the neighborhood character. Fencing and walls shall be opaque, except that limited use of materials such as wrought iron may be permitted where berming and or/ landscape planting provide effecting screening of the storage facilities.

The UDC does not specify a duration for special use permits for self-service storage facilities. In these instances, Staff will recommend a time limit to the Planning Commission and Governing Body. Staff recommends that the special use permit be approved for a period of twenty (20) years.

Staff provides the following analysis for the review criteria within [Section 4-1-G-5](#) of the UDC.

- 1. The character of the neighborhood.**

The neighborhood is commercial in nature with a mix of uses. The area has direct access onto the I-35 frontage road.

- 2. The zoning and use of properties nearby.**

Zoning and uses of property are outlined in Table 1 and discussed in the *Land Use Review* section.

- 3. The suitability of the subject property for the uses to which it has been restricted.**

The site is currently made up of two separate parcels, which combined, make a reverse L shape. The south parcel is a long and narrow strip, which could potentially make it difficult for some uses to develop on. The general location of site is appropriate for commercial uses as designated by the zoning district.

- 4. The extent to which the proposed use will detrimentally affect nearby property.**

A self-storage facility has the potential to generate traffic from larger vehicles, such as a moving van, but is not likely to be a high-volume traffic use. Traffic will have close access to the adjacent highway frontage road and will not detrimentally affect nearby property. Self-storage can also be unsightly when exterior overhead doors are proposed or there are large expanses of blank walls due to the internal layout of the storage units. Staff worked with the applicant to provide upgraded screening and will continue to work on overall architecture with the applicant at the final plan stage to ensure there are no negative visual impacts to nearby properties.

- 5. The length of time the subject property has remained vacant as zoned.**

Historic aerial imagery shows that the site has never been fully developed. There is a parking lot at the southwestern tip of the site that was constructed sometime in the late 1980's as additional parking for the office use to the north. The parking lot is now in poor condition.

- 6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.**

Denial of the application would not lead to any relative gain to the public health, safety, or welfare. The proposed use will not be a nuisance and would clean up a site that is regularly used for illegal dumping.

- 7. Recommendation of City's permanent professional staff.**

See *Staff's Recommendation* section.

**8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.**

It is the opinion of Staff that the proposed use is in compliance with the adopted Comprehensive Plan for the City. The area serves a mix of commercial and office uses.

**9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.**

All required utilities and public services are existing to the site.

**10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.**

A self-storage facility is a relatively low impact use for traffic generation. All parking, loading, and unloading will occur within the property boundaries and will not create any adverse effects to the capacity or safety of the street network.

**11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.**

It is not anticipated that the proposed use will generate excessive environmental harms. The proposed development shall meet code requirements related to the environmental impacts listed above.

**12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.**

The applicant demonstrated the intent to meet the City's stormwater requirements with the use of an underground infiltration trench and a bioretention basin for stormwater management.

**13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.**

The applicant is requesting one deviation to the zoning regulations as outlined in the Unified Development Code, to allow reduced bicycle parking. With the exception of this deviation, the plans submitted show an ability to meet the applicable regulations and ordinances.

## PRELIMINARY PLAN REVIEW

The proposed site is 3.3 acres and includes two unplatted parcels. The StorTropolis development includes five buildings with a total of 976 storage units. A small office area is included in Building A. Employees will be on-site during office hours.

Building A, located adjacent to Rosehill Road, is four stories tall and includes outside access units along the west façade facing the interior of the site. These units would be visible to the adjacent office building. The building is proposed to have a setback of 30 feet along Rosehill Road and approximately 37 feet from 96<sup>th</sup> Terrace. The proposed setbacks are within the allowable range for the CP-2 Zoning District. Building A includes the front office entrance and 944 self-storage units, which are accessed internally with the exception of the west-facing ground units. Buildings B, C, D, and E are located along the southern portion of the site. The buildings are set back 30 feet from the adjacent property line. Buildings B, C, D and E have exterior overhead doors that face internally towards each other. Buildings B-E include the following types of units:

- Building B: Four RV units, five standard units
- Building C: Eight RV units
- Building D: Four RV units, five standard units
- Building E: Six standard units

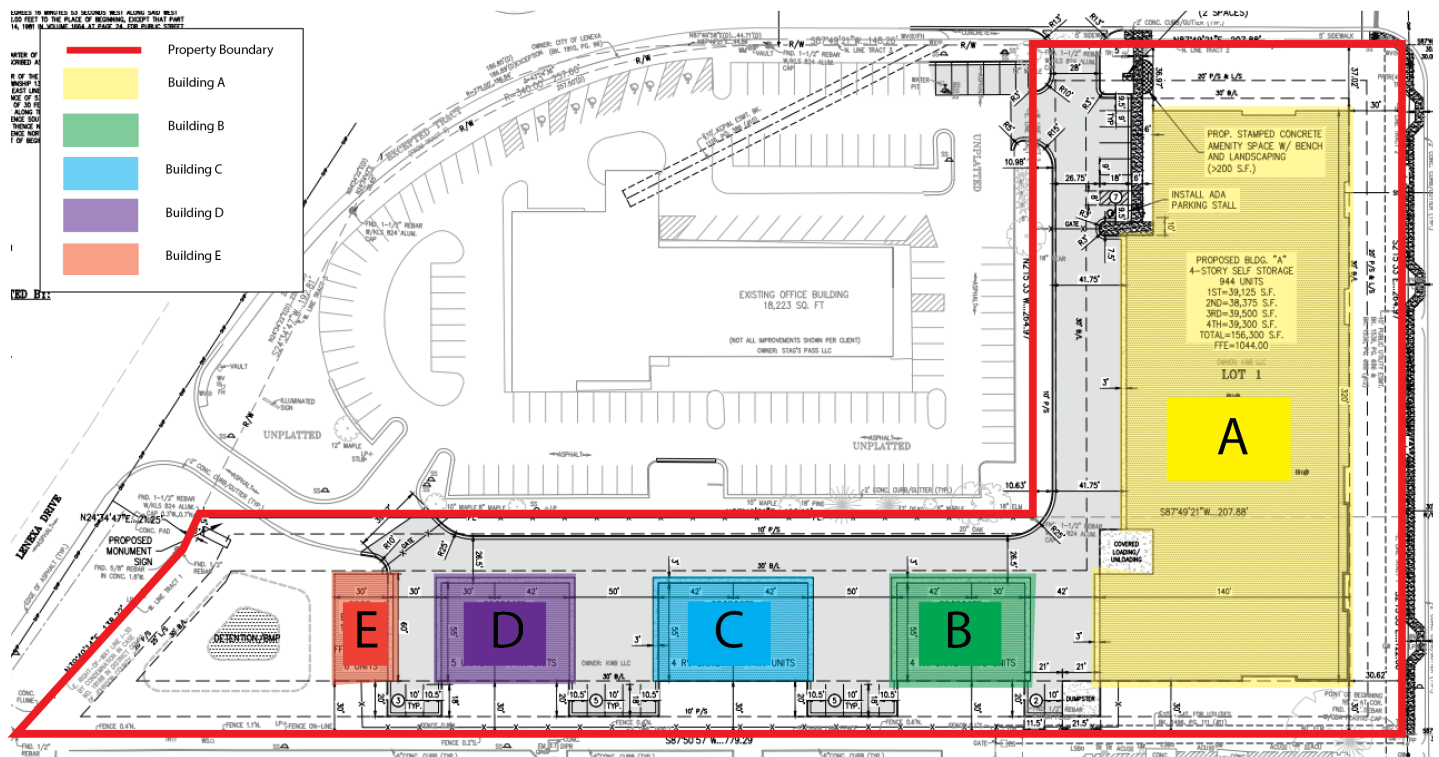


Exhibit 2: Layout of buildings A through E.

Landscaping shall be provided along perimeter lot lines and street frontages and screening is required adjacent to the existing office use. A pedestrian amenity area is proposed north of Building A by the primary office entrance. The plans include two bicycle parking spaces; however, the requirement is 15 spaces. The applicant requests a deviation from this requirement, which is discussed in detail within the *Deviations* section of the Staff Report.

## DIMENSIONAL STANDARDS

The CP-2, Planned Community Commercial District has a maximum allowable height of 45 feet. Building A, the tallest of the proposed buildings, is 45 feet at its maximum height. A minimum of 25 percent of the lot area is required for open space. The plans propose 36% open space. Setbacks are outlined in Table 2. This proposal meets required dimensional standards.

TABLE 2: SETBACK ANALYSIS			
	Required Minimum Setback	Proposed Setback	Difference
Streets	30 ft	30	0
Other	30 ft	30+	0

## PUBLIC IMPROVEMENTS

A five-foot-wide public sidewalk is proposed on the west side of Rosehill Road. The sidewalk includes three bump-outs toward Rosehill Road where light poles are within the path of the sidewalk. Staff recommends the bump-outs be modified to diverge to the west side of the power poles rather than towards the right-of-way, or for the sidewalk to be moved slightly west onto the subject property. A sidewalk easement is required for either option. For accessibility, the sidewalk design should also be modified to be curvilinear rather than take sharp angles around the poles.

## TRAFFIC, ACCESS, AND PARKING

This site is located at the southwest corner of Rosehill Road and 96<sup>th</sup> Terrace. At the northwest corner of the site 96<sup>th</sup> Terrace becomes Lenexa Drive. Rosehill Road and 96<sup>th</sup> Terrace are both local roads. Lenexa Drive is classified as a Collector.

Access on the site is available from a private drive that connects to Lenexa Drive on the west. Lenexa Drive serves as a highway frontage road and will provide access to the Interstate 35 Highway. A second access point is at the north of the site along 96<sup>th</sup> Terrace, with a branched-off shared drive to the adjacent parcel, 9705 Lenexa Drive. Prior to approval of a final plan the applicant must provide an access agreement with the adjacent property. A 5-foot public sidewalk is proposed on the west side of Rosehill Road, which will improve pedestrian connectivity where there is a gap in sidewalk network between the adjacent north block and blocks to the south. A connection from the public sidewalk network to the internal sidewalk is provided from 96<sup>th</sup> Terrace to the main office entrance.

Parking, to include accessible spaces, is provided at the office entrance. Additional parking is provided near each of the smaller buildings. Enhancements to the pedestrian connectivity network include construction of a new public sidewalk along Rosehill Road and a separate sidewalk connection from the existing sidewalk along 96<sup>th</sup> Terrace to the front entrance.

TABLE 3: PARKING ANALYSIS				
Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Self-Storage	1 per 75 units (976 Units proposed)	13	22 standard 2 unloading spaces	+ 11

**STORMWATER**

Stormwater management for this site will be provided with a combination of an underground infiltration trench and a bioretention basin to be constructed with this project. The applicant demonstrated the intent to meet the City’s stormwater requirements with the addition of these two new facilities. Staff is working with the applicant to provide some minor tweaks to the stormwater management report in order to provide additional clarity; however, Staff is comfortable recommending approval of the proposed project, anticipating the revised preliminary report will be received prior to moving to the final plan/plat stage.

**FIRE PREVENTION**

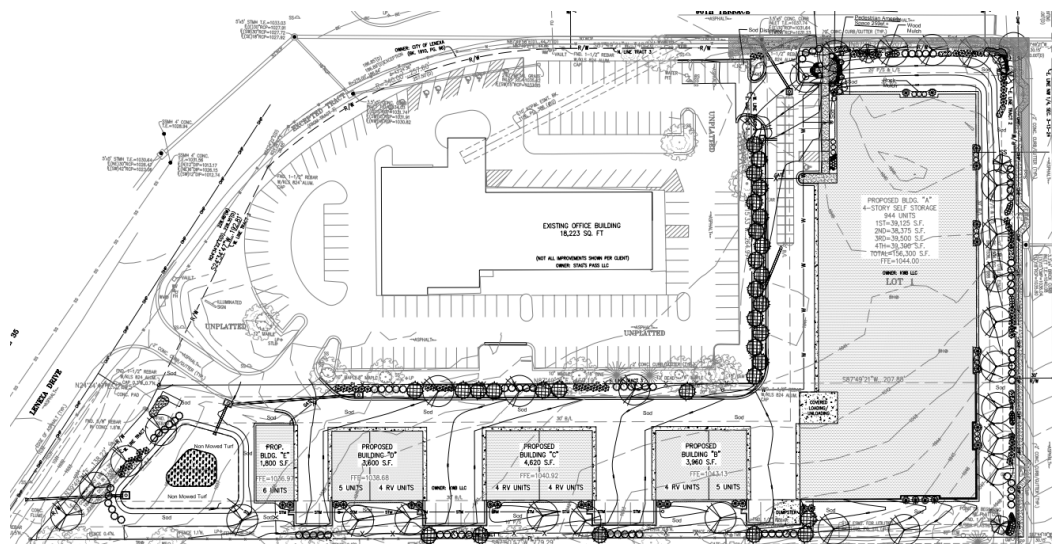
The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

**LIGHTING**

Wall-mounted lighting is proposed. A photometric plan will be reviewed at the time of final plan for full compliance.

**LANDSCAPING**

A Land Use Intensity Buffer Design Type 1 is required between the proposed development and adjacent office uses. This requires 1 shade tree, 1 evergreen and 1 shrub per 100 feet of lot line adjacent to parking areas. Overhead doors visible from off-site require additional screening. Overhead doors face the office building to the northwest of the site. Due to this additional screening requirement, staff recommended the applicant provide a double row of evergreens for greater screening along the western property line between the parking lot of the proposed site and the parking lot of the adjacent office building. The applicant complied with this request. A standard LUI Type 1 Buffer is provided on the north property line fronting the office property as there are no proposed overhead doors facing the office. Perimeter plantings along lot lines are required and provided. Street trees are provided along Rosehill Road, 96<sup>th</sup> Terrace and Lenexa Drive. A pedestrian amenity area is provided off the internal sidewalk near the front door with a stamped concrete patio, bench, landscaping, and bike racks.



**Exhibit 3: Landscape Plan**

## ARCHITECTURE

Building A, along Rosehill Road, encounters a grade increase of approximately five feet from the north to the south of the building. The design includes a foundation band of stone that is more visible at the northern end and becomes smaller with the grade change to the south. Above the foundation band, primary materials include architectural block, fiber cement siding, and metal panels. Staff encouraged the applicant to consider a more neutral color palette than was originally proposed, which included blue as the primary color of the building. The applicant was amenable to this change and incorporated a neutral off-white color as the primary wall color. Metal panels are an allowable material in the UDC as an accent material but should not be used extensively. Aluminum storefront windows provide fenestration.

The westward facing side of building A, which faces internal to the site and toward the existing office development, uses a similar material pattern but does not have the masonry foundation band. Overhead bay doors are located at the ground level. To reduce visual impact on the adjacent office use, the bay doors will be colored to match the surrounding façade. Additional landscaping is also required to serve as a buffer.

A trash enclosure is proposed at the southwest corner of building A. The applicant has not provided a rendering of the trash enclosure. This will be required at the final plan stage and shall include the use of materials consistent with the approved building materials.

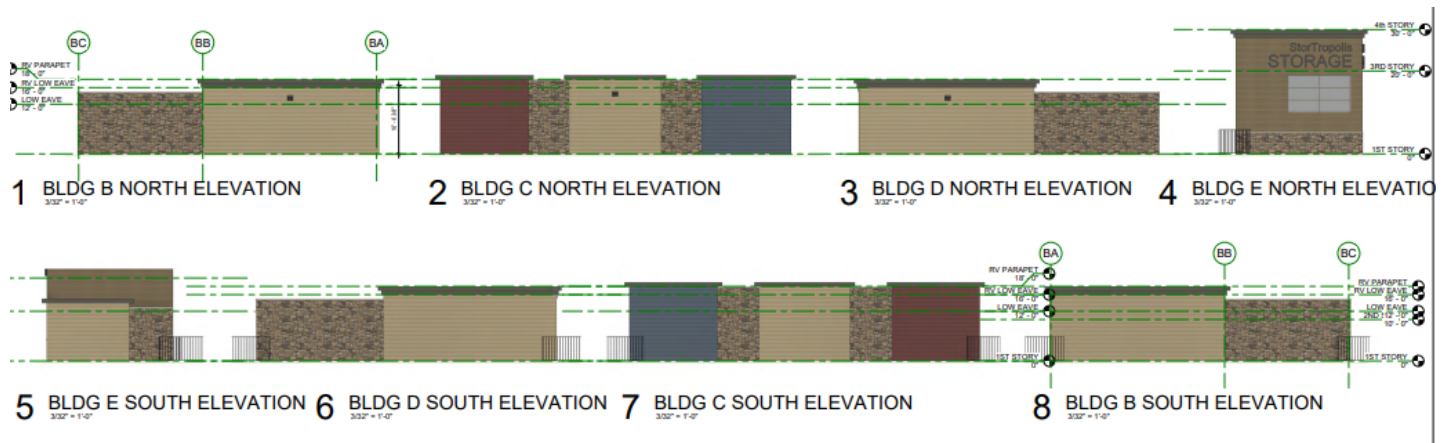
Buildings B, C, D and E are single-story buildings with additional height for RV storage. Building E is the tallest of the four at 30 feet high. These buildings use a similar material palette to Building A, with greater use of stone. Staff would like to see additional detailing on the future final plan, such as faux windows on the south facades where the buildings face adjacent properties. Staff will continue working with the applicant on the material palette at the final plan stage.



*Exhibit 4: Building A, view from northeast.*



**Exhibit 5: Building A, view from northwest.**



**Exhibit 6: Buildings B, C, D and E north and south elevations.**

## DEVIATIONS

Bicycle parking is required at a ratio of 1 space per 10,000 square feet of building area. The building area of 170,640 square feet requires 17 bicycle parking spaces, whereas the applicant has proposed to provide one rack with two spaces. The majority of the building area will be used for storage rather than space that customers can gather. The use is not anticipated to draw many customers on bike, considering vehicles will be required to carry storage loads in and out; however, an employee may bike to the site a customer may occasionally come to the facility by bike to pick up smaller items. Staff supports this deviation request given the nature of the use.

**Discussion Item #1: The Planning Commission must consider the requested deviation from the bicycle parking requirement.**



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## REVIEW PROCESS

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- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on March 21, 2023.

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## RECOMMENDATION FROM PROFESSIONAL STAFF

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- **Conduct a Public Hearing.**
- **Staff recommends approval of the proposed Special Use Permit and Preliminary Plan for StorTropolis.**
  - This is a request for a special use permit and preliminary plan/plat for a self-service storage facility in the CP-2, Planned Community Commercial District.
  - The project is consistent with Lenexa's goals through **Responsible Economic Development** and **Strategic Community Investment** to create **Thriving Economy**.

## SPECIAL USE PERMIT

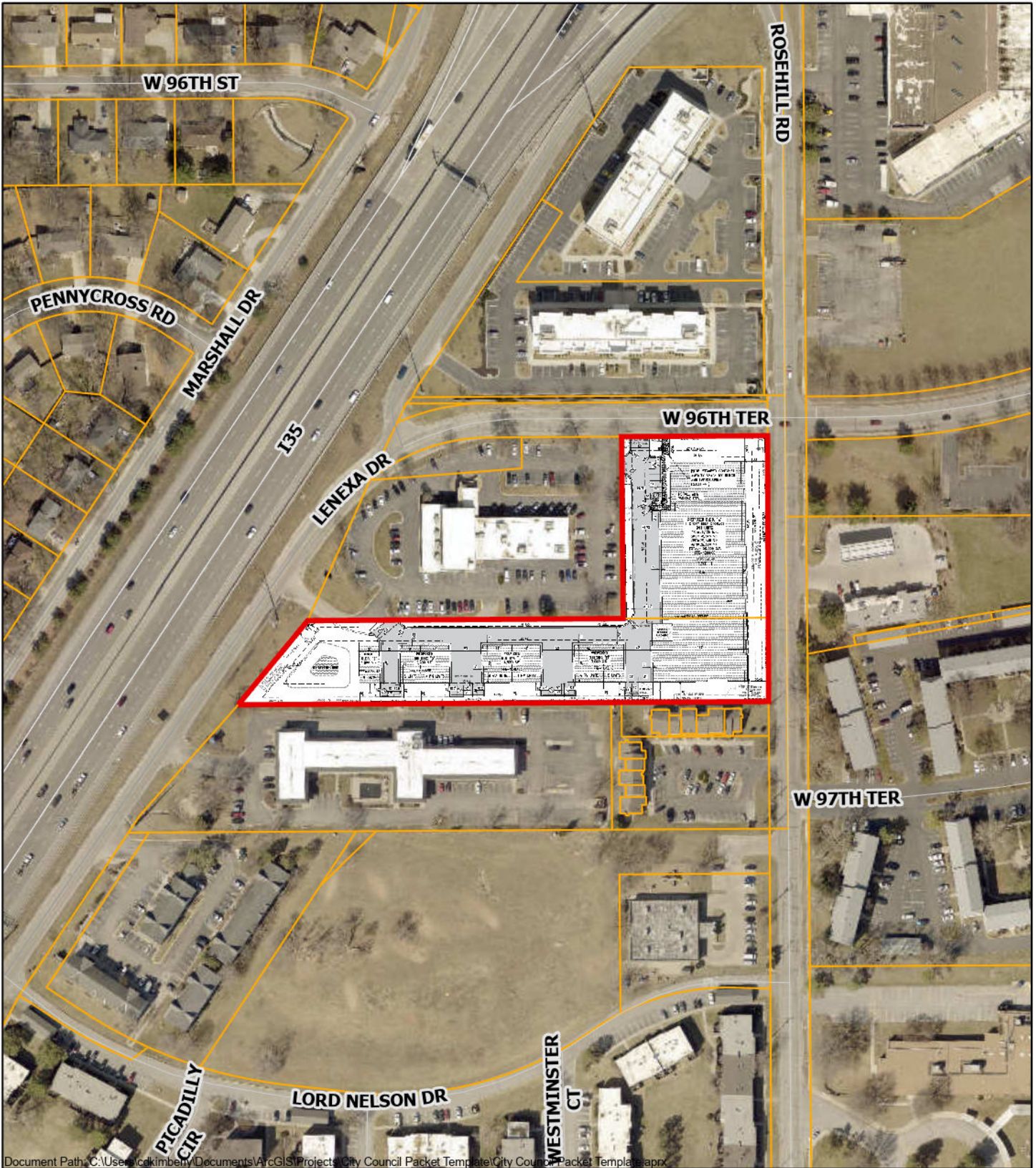
Staff recommends **approval** of SU23-01 - a special use permit for self-service storage for **StorTropolis** at the southwest corner of Rosehill Road and 96<sup>th</sup> Terrace, with the following condition:

1. The special use permit is limited to a period of twenty (20 years).

## PRELIMINARY PLAN

Staff recommends **approval** of the preliminary plan/plat for PL23-01P - **StorTropolis** at the southwest corner of Rosehill Road and 96<sup>th</sup> Terrace, for a self-service storage development, with the following condition and deviation:

1. Prior to approval of a final plan the applicant must provide an access agreement with the adjacent property.
2. A deviation be granted for the bicycle parking requirement to allow two (2) bicycle parking spaces where seventeen (17) are required.



Document Path: C:\Users\jckimberly\Documents\ArcGIS\Projects\City Council Packet Template\City Council Packet Template.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# StorTropolis Preliminary Plan & Special Use Permit

## PL23-01P & SU23-01



**LEGAL:**

TRACT 1:  
ALL THAT PART OF THE NORTHWEST QUARTER OF SECTION 3, TOWNSHIP 13 SOUTH, RANGE 24 EAST, LENEXA, JOHNSON COUNTY, KANSAS DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF THE SOUTH HALF OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 3, TOWNSHIP 13 SOUTH, RANGE 24 EAST, THENCE SOUTH 2 DEGREES 16 MINUTES 53 SECONDS EAST ALONG THE EAST LINE OF SAID NORTHWEST QUARTER A DISTANCE OF 30.00 FEET TO A POINT, THENCE SOUTH 87 DEGREES 49 MINUTES 21 SECONDS WEST, A DISTANCE OF 30.00 FEET TO THE TRUE POINT OF BEGINNING, SAID POINT BEING ON THE WEST RIGHT-OF-WAY LINE OF ROSHILL ROAD, THENCE CONTINUE SOUTH 87 DEGREES 49 MINUTES 21 SECONDS WEST, A DISTANCE OF 67.69 FEET TO A POINT ON THE SOUTH AND EASTERSY RIGHT-OF-WAY LINE OF INTERSTATE HIGHWAY 35; THENCE SOUTH 24 DEGREES 15 MINUTES 44 SECONDS WEST ALONG SAID RIGHT-OF-WAY LINE A DISTANCE OF 21.72 FEET TO A POINT; THENCE ALONG SAID RIGHT-OF-WAY LINE SOUTH 39 DEGREES 48 MINUTES 59 SECONDS WEST A DISTANCE OF 1.00 FEET TO A POINT; THENCE SOUTH 87 DEGREES 49 MINUTES 21 SECONDS EAST A DISTANCE OF 379.30 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF ROSHILL ROAD, THENCE NORTH 2 DEGREES 16 MINUTES 53 SECONDS WEST ALONG SAID WEST RIGHT-OF-WAY LINE A DISTANCE OF 122.00 FEET TO THE PLACE OF BEGINNING, EXCEPT THAT PART TAKEN IN WARRANTY DEED FILED APRIL 14, 1981 IN VOLUME 1664 AT PAGE 24, FOR PUBLIC STREET RIGHT-OF-WAY.

TRACT 2:  
ALL THAT PART OF THE NORTHWEST QUARTER OF SECTION 3, TOWNSHIP 13 SOUTH, RANGE 24 EAST, LENEXA, JOHNSON COUNTY, KANSAS DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF THE SOUTH HALF OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 3, TOWNSHIP 13 SOUTH, RANGE 24 EAST, THENCE SOUTH 2 DEGREES 16 MINUTES 53 SECONDS EAST ALONG THE EAST LINE OF THE SAID NORTHWEST QUARTER AND ALONG THE CONTINUE OF ROSHILL ROAD A DISTANCE OF 538 FEET TO A POINT, THENCE SOUTH 87 DEGREES 49 MINUTES 21 SECONDS WEST A DISTANCE OF 30 FEET TO THE POINT OF BEGINNING, THENCE SOUTH 2 DEGREES 16 MINUTES 53 SECONDS EAST ALONG THE WEST RIGHT-OF-WAY LINE OF ROSHILL ROAD, A DISTANCE OF 287.79 FEET TO A POINT, THENCE NORTH 2 DEGREES 16 MINUTES 53 SECONDS WEST, A DISTANCE OF 285 FEET TO A POINT, THENCE NORTH 87 DEGREES 49 MINUTES 21 SECONDS EAST, A DISTANCE OF 287.79 FEET TO THE POINT OF BEGINNING.

**OWNER:**

KWB, LLC  
1113 S MINNESOTA AVE  
SIOUX FALLS, SD 57105  
(605) 259-6914  
EMAIL: DAWUNDS@KWBLLC.COM

**DEVELOPER:**

HUNT MIDWEST DEVELOPMENT, INC.  
1881 MAIN STREET, SUITE 200  
KANSAS CITY, MO 64108  
(816) 458-4241  
EMAIL: TBORCHERS@HUNTMIDWEST.COM

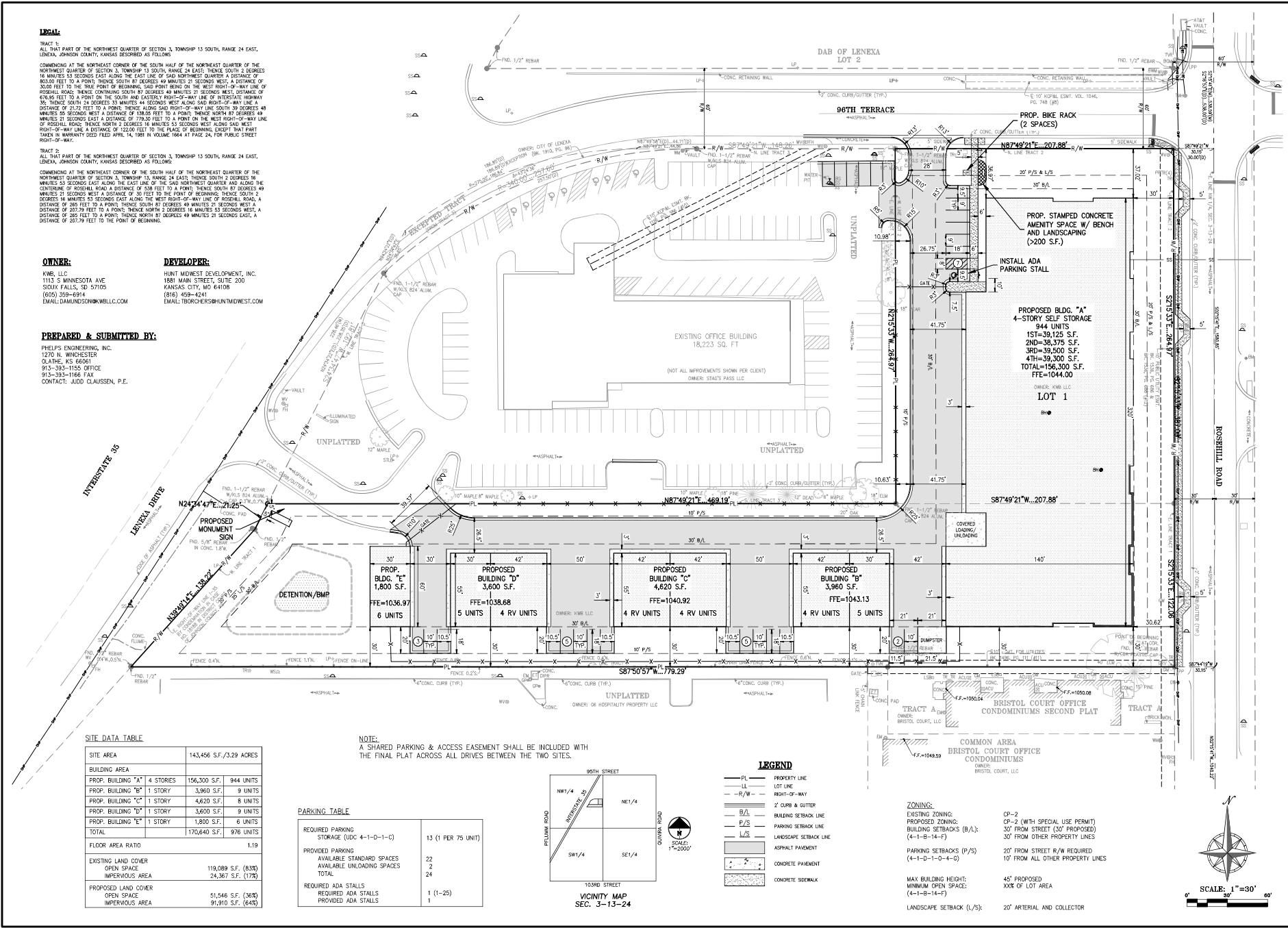
**PREPARED & SUBMITTED BY:**

PHILIPS ENGINEERING, INC.  
1270 N. WINCHESTER  
OLATHE, KS 66061  
913-393-1155 OFFICE  
913-393-1166 FAX  
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**SITE PLAN & PRELIMINARY PLAT**  
STORTROPOLIS  
LENEXA, JOHNSON COUNTY, KANSAS  
ADDRESS: 9620 ROSEHILL ROAD



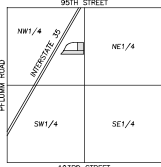
**SITE DATA TABLE**

SITE AREA	143,456 S.F./3.29 ACRES
BUILDING AREA	
PROP. BUILDING "A" 4 STORIES	156,300 S.F. 944 UNITS
PROP. BUILDING "B" 1 STORY	3,960 S.F. 9 UNITS
PROP. BUILDING "C" 1 STORY	4,620 S.F. 8 UNITS
PROP. BUILDING "D" 1 STORY	3,600 S.F. 9 UNITS
PROP. BUILDING "E" 1 STORY	1,800 S.F. 6 UNITS
TOTAL	170,640 S.F. 976 UNITS
FLOOR AREA RATIO	1.19
EXISTING LAND COVER	
OPEN SPACE	119,089 S.F. (83%)
IMPERVIOUS AREA	24,367 S.F. (17%)
PROPOSED LAND COVER	
OPEN SPACE	51,546 S.F. (36%)
IMPERVIOUS AREA	91,910 S.F. (64%)

**PARKING TABLE**

REQUIRED PARKING STORAGE (UDC 4-1-D-1-C)	13 (1 PER 75 UNIT)
PROVIDED PARKING	
AVAILABLE STANDARD SPACES	22
AVAILABLE UNLOADING SPACES	2
TOTAL	24
REQUIRED ADA STALLS	
REQUIRED ADA STALLS	1 (1-25)
PROVIDED ADA STALLS	

**NOTE:**  
A SHARED PARKING & ACCESS EASEMENT SHALL BE INCLUDED WITH THE FINAL PLAT ACROSS ALL DRIVES BETWEEN THE TWO SITES.

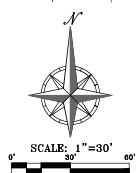


**VICINITY MAP**  
SEC. 3-13-24

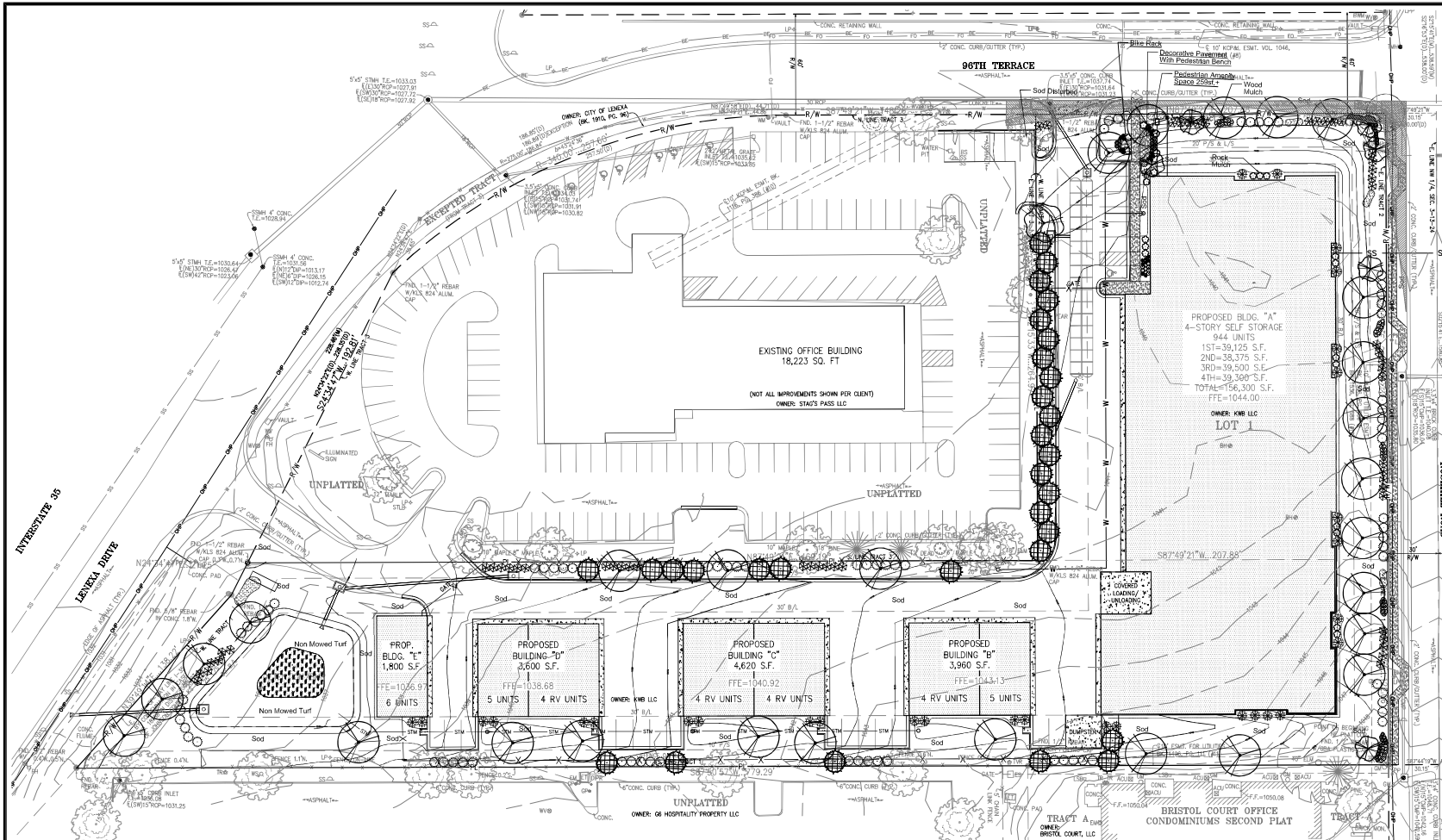
**LEGEND**

- PL — PROPERTY LINE
- LL — LOT LINE
- R/W — RIGHT-OF-WAY
- C — CURB & GUTTER
- B/S — BUILDING SETBACK LINE
- P/S — PARKING SETBACK LINE
- L/S — LANDSCAPE SETBACK LINE
- [Symbol] — ASPHALT PAVEMENT
- [Symbol] — CONCRETE PAVEMENT
- [Symbol] — CONCRETE SIDEWALK

- ZONING:**  
EXISTING ZONING:  
BUILDING SETBACKS (B/S):  
(4-1-B-14-F)
- PARKING SETBACKS (P/S)**  
(4-1-D-1-0-1-0)
- MAX BUILDING HEIGHT:**  
REQUIRED MINIMUM OPEN SPACE:  
(4-1-B-14-F)
- LANDSCAPE SETBACK (L/S):**  
20' ARTERIAL AND COLLECTOR
- CP-2**  
CP-2 (WITH SPECIAL USE PERMIT)  
30' FROM STREET (30' PROPOSED)  
30' FROM OTHER PROPERTY LINES
- 20' FROM STREET R/W REQUIRED**  
10' FROM ALL OTHER PROPERTY LINES
- 45' PROPOSED**  
XXX OF LOT AREA



SHEET  
**C1**



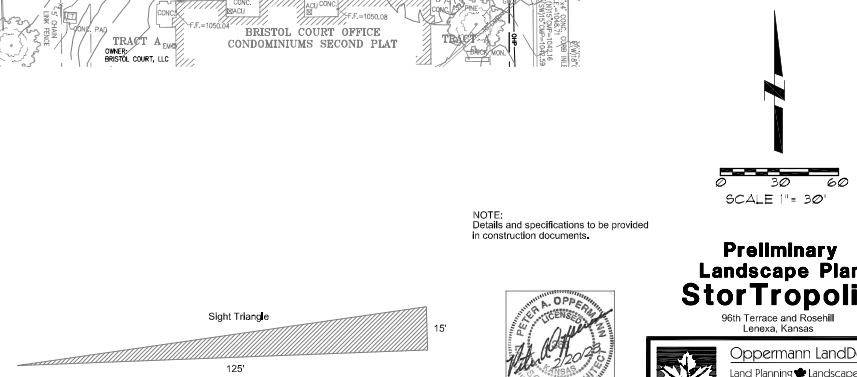
**CONCEPT PLANT SCHEDULE**

	LARGE SHADE TREES	28
	MEDIUM SHADE TREES	1
	ORNAMENTAL TREE	6
	EVERGREEN TREES	30
	SPREADER EVERGREEN SHRUB	77
	DECIDUOUS SHRUB 1	6
	LOW SPREADING EVERGREEN	31
	ORNAMENTAL GRASS TALL	59
	ORNAMENTAL GRASS MEDIUM	24
	SMALL DECIDUOUS SHRUB	22
	LARGE ORNAMENTAL SHRUB	56
	ORNAMENTAL GRASS TALL II	48
	PERENNIALS	328 sf
	NATIVE PLANTINGS	922 sf

**LANDSCAPE REQUIREMENTS:**

	REQUIRED	PROVIDED
<b>L PERIMETER LANDSCAPING</b>		
Collector (Rosehill Road) 2 Shade / 100'	7,74	8
1 Ornamental / 100 ft.	3,87	4
12 Shrubs / 100 ft.	46,44	48
75s.f. of Bed / 100ft.	290s.f.	372s.f.
Collector (96th Terrace) 2 Shade/100ft.	3,58	4
1 Ornamental/100ft.	1,79	2
12 Shrubs/100ft.	20,48	21
75s.f. of Bed/100ft.	134,25s.f.	167s.f.
Collector (Lenexa Drive) 2 Shade/100ft.	3,16	4
1 Ornamental/100ft.	1,58	2
12 Shrubs/100ft.	18,96	19
75s.f. of Bed/100ft.	118,5s.f.	129s.f.
Along Lot Lines (South PL) Building 1 shade/100ft.	3,98	4
Parking 1 shade/100ft.	1,76	2
9 shrubs/100ft.	15,3	20
Evergreen trees added	0	6
Buffers (North PL to Office) 1 Shade/100ft.	3,34	3
2 Evergreen Trees/100ft.	6,68	7
10 Shrubs/100ft.	33,4	33
(West PL to Office) Due to overhead doors dbj row of evergreen trees dictated by City	?	17

**Utility Note:**  
 Utilities shown on plan are diagrammatic and some may be missing. Before starting any construction call appropriate locating service. In Kansas call 1-800-403-SAFE (344-7233) to have utilities located.



**NOTE:**  
 Details and specifications to be provided in construction documents.



**Preliminary Landscape Plan**  
**StorTropolis**

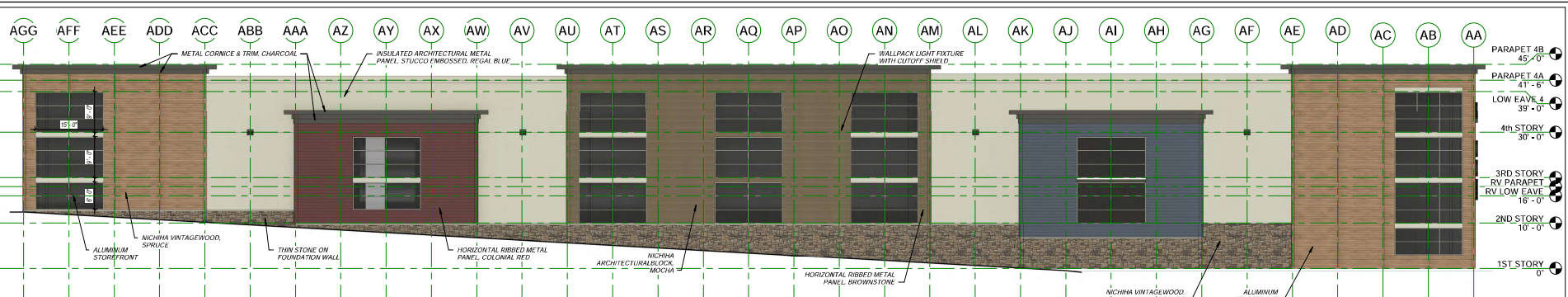
96th Terrace and Rosehill  
 Lenexa, Kansas

**Oppermann LandDesign, LLC**  
 Land Planning Landscape Architecture  
 99 Debra Lane  
 New Windsor, New York 12553

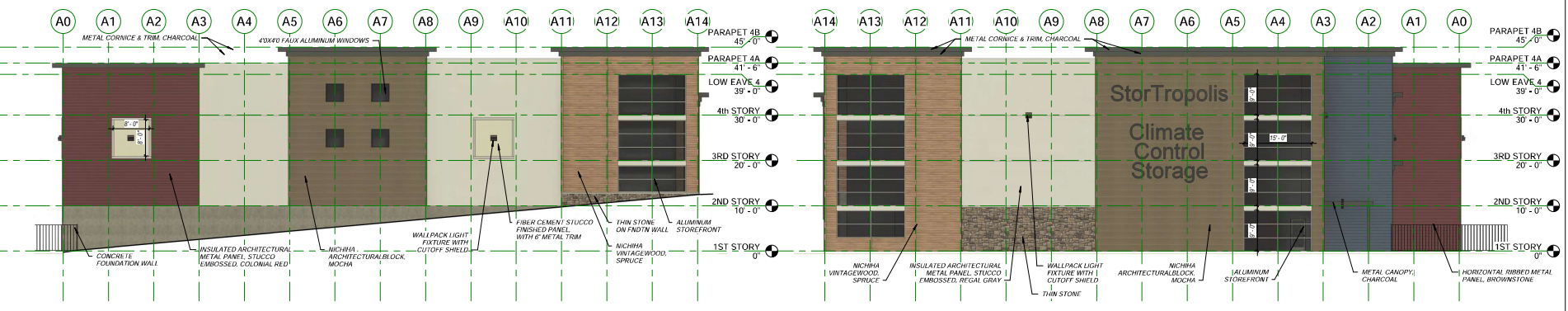
pete@oppermans.com  
 913.599.5998

02/20/2023

LS-1

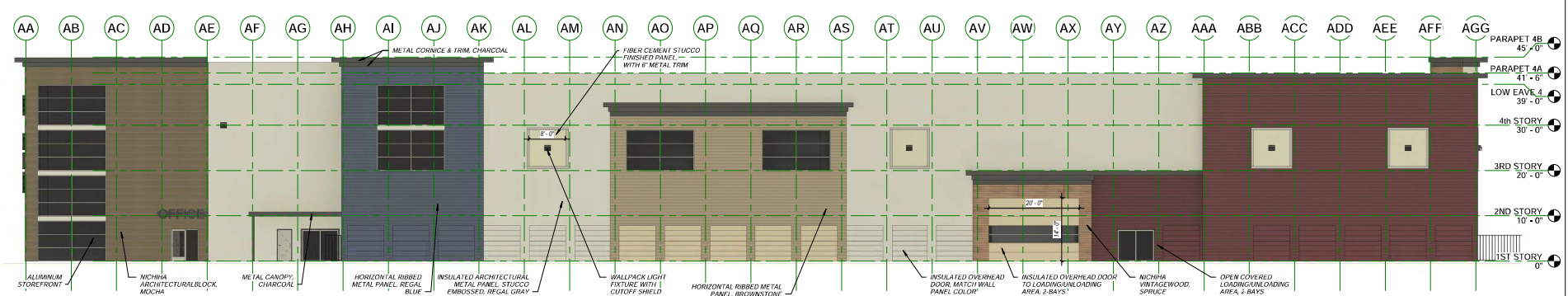


**1 BLDG A EAST ELEVATION**  
3/32" = 1'-0"



**3 BLDG A SOUTH ELEVATION**  
3/32" = 1'-0"

**2 BLDG A NORTH ELEVATION**  
3/32" = 1'-0"



**4 BLDG A WEST ELEVATION**  
3/32" = 1'-0"

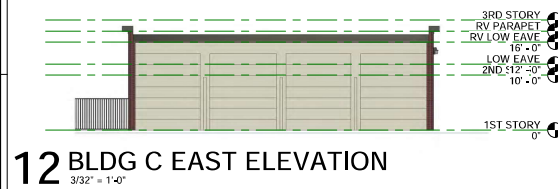
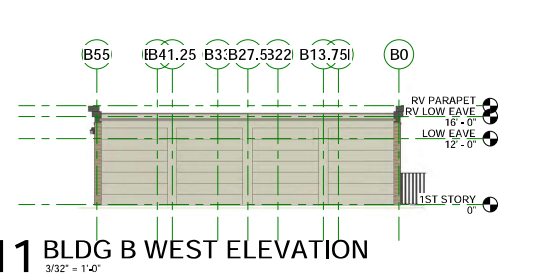
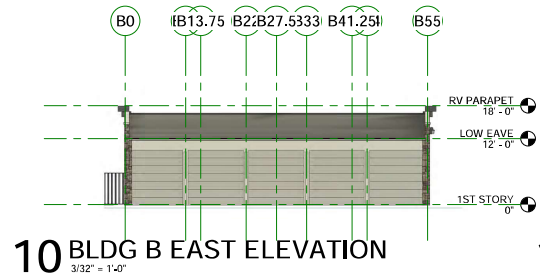
**StorTropolis LENEXA**  
96TH TER & ROSEHILL RD

**Hernly**  
ARCHITECTS  
1100 Rhoads Blvd. St. Louis, Missouri 63044  
314-755-5800

ELEVATIONS BLDG A

Date:	2023/01/19
Drawn by:	Author
Checked by:	
Revisions:	

**A2.0**



**StorTropolis LENEXA**  
96TH TER & ROSEHILL RD

**Hernly**  
ARCHITECTS  
PROFESSIONAL ENGINEERS  
ELECTRICAL ENGINEERS

1100 Rhoads Blvd. St. Louis, Missouri 63044  
314-433-5800

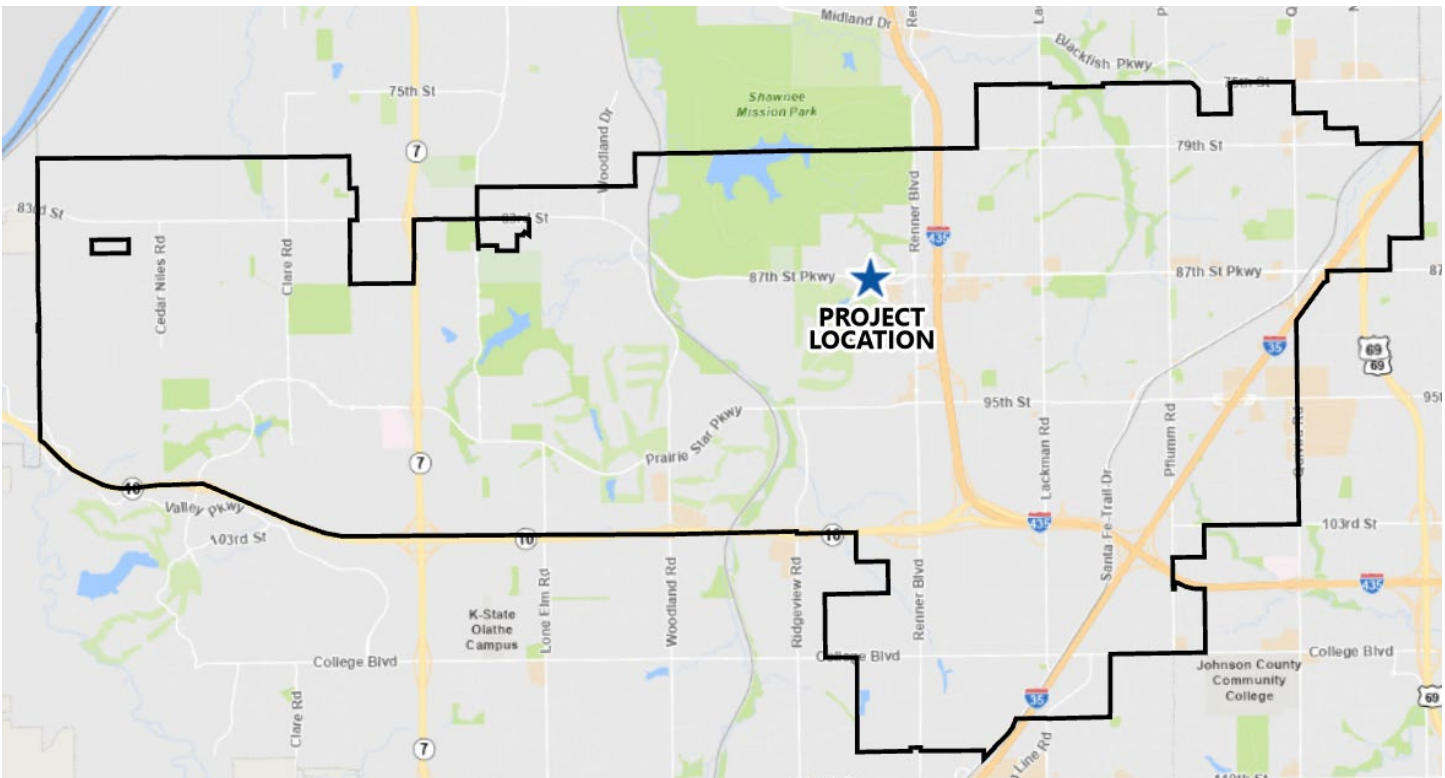
ELEVATIONS BLDGS B, C, D, & E

Date:	2023/01/19
Drawn by:	Author
Checked by:	
Revisions:	

**A2.1**

## MIDAS LENEXA CITY CENTER

<b>Project #:</b>	PL23-02P	<b>Location:</b>	Between Elmridge Street and Penrose Lane and the east and west lanes of W. 87 <sup>th</sup> Street Parkway
<b>Applicant:</b>	Haidan O’Keefe, Olsson	<b>Project Type:</b>	Preliminary Plan/Plat
<b>Staff Planner:</b>	Kimberly Portillo, AICP	<b>Proposed Use:</b>	Hotel, Retail



### PROJECT SUMMARY

The applicant proposes to construct a mixed-use development on 2.32 acres in Lenexa City Center. The development will include two hotels with a combined 260 rooms, 11,000 SF of retail space, a private hotel amenity courtyard, public amenity areas around the perimeter of the building, and a structured parking garage. Additionally, a hotel restaurant is proposed. The restaurant will be open to the public and will have direct access from the sidewalk as well as internal access from the hotel lobby area. The building, which covers the full block between Penrose Lane and Elmridge Street, will be approximately 80,000 square feet including the parking garage. The structure is five stories tall at its peak and three stories tall at the point of highest grade. The applicant requests approval of a preliminary plan for the development. The preliminary plan is intended to also serve as the preliminary plat.

**STAFF RECOMMENDATION: APPROVAL WITH CONDITIONS**

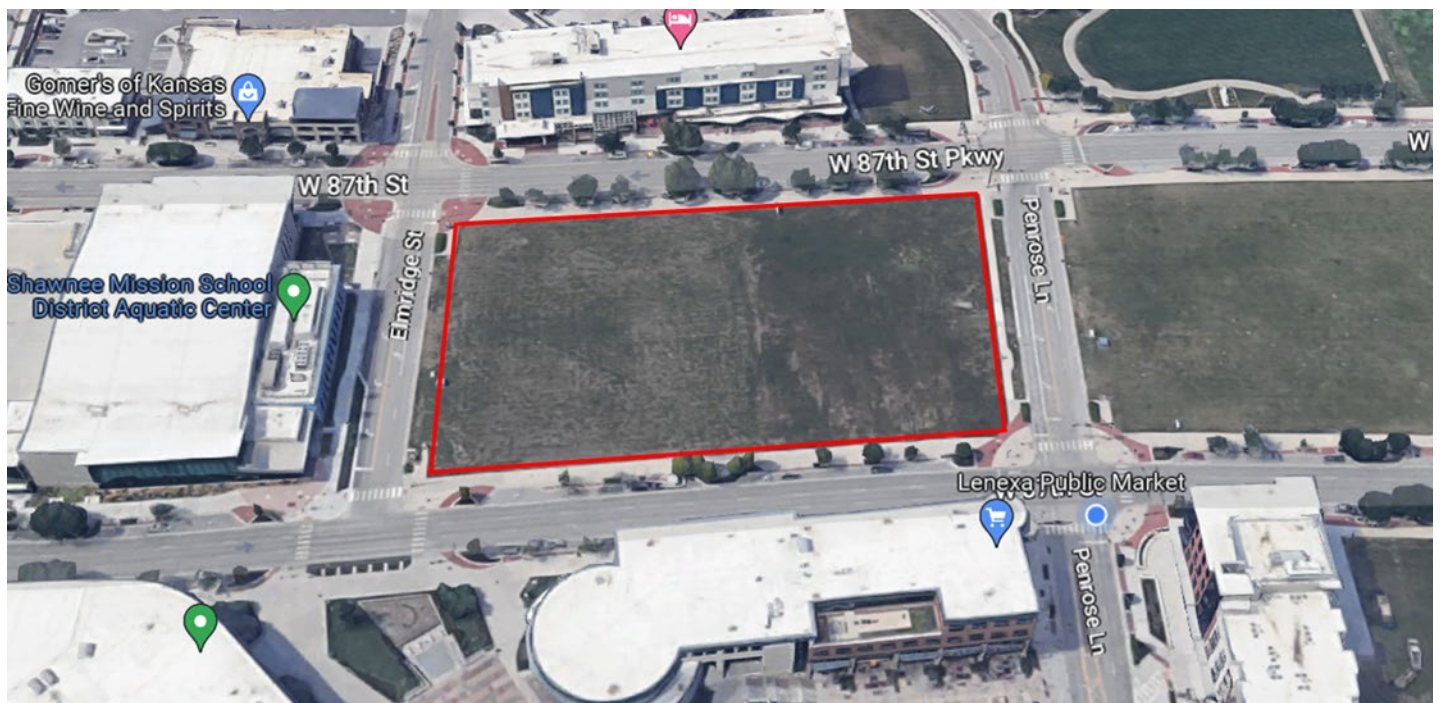
## SITE INFORMATION

This site is located within the designated City Center District. The site is subject to the City Center Neighborhood Design Standards and Guidelines, which was adopted April 17, 2007, and amended in 2015. This site is undeveloped and is not currently platted.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
2.32	79,956	CC	City Center



*Exhibit 1: Aerial image of subject site.*



*Exhibit 2: Birds eye view of subject site.*



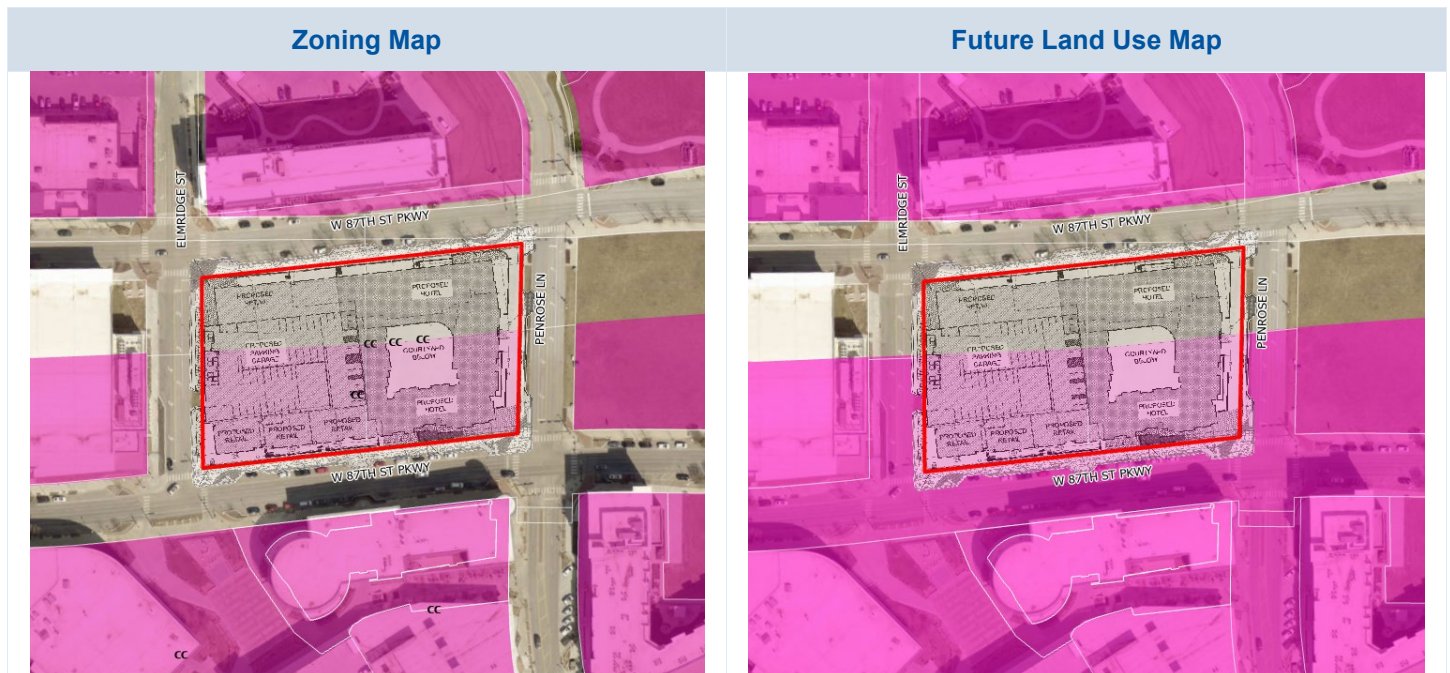
## LAND USE REVIEW

This development proposal includes the following uses:

- Two hotels featuring:
  - AC Hotel, which will have approximately 30 employees on shift at any given time and 132 rooms.
  - Residence Inn, which will have approximately 30 employees on shift at any given time and 128 rooms.
  - Guest amenities including a courtyard with landscaping and outdoor seating.
  - Restaurant
- Retail space
  - 4 tenant spaces fronting the public sidewalk totaling approximately 11,000 square feet.
- Parking Garage with a minimum of 283 spaces.

The two hotels will have a shared lobby that will be open 24 hours; however, between midnight and 5:00 a.m. only registered guests or guests checking in will be able to enter. The hotel restaurant will be available to the public and guests of the hotel. Generally, the restaurant and retail spaces will be available to the public and guests during normal business hours. Specific hours will be determined once tenants are identified.

Hotel, retail, and restaurant uses are all allowed by-right within the City Center District and will provide an appropriate and desired mix of uses along the block. As required in the City Center District, this proposal will also include a Pedestrian Amenity Zone along the exterior of the building which is characterized by pedestrian-level amenities and landscaping between the sidewalk and street.



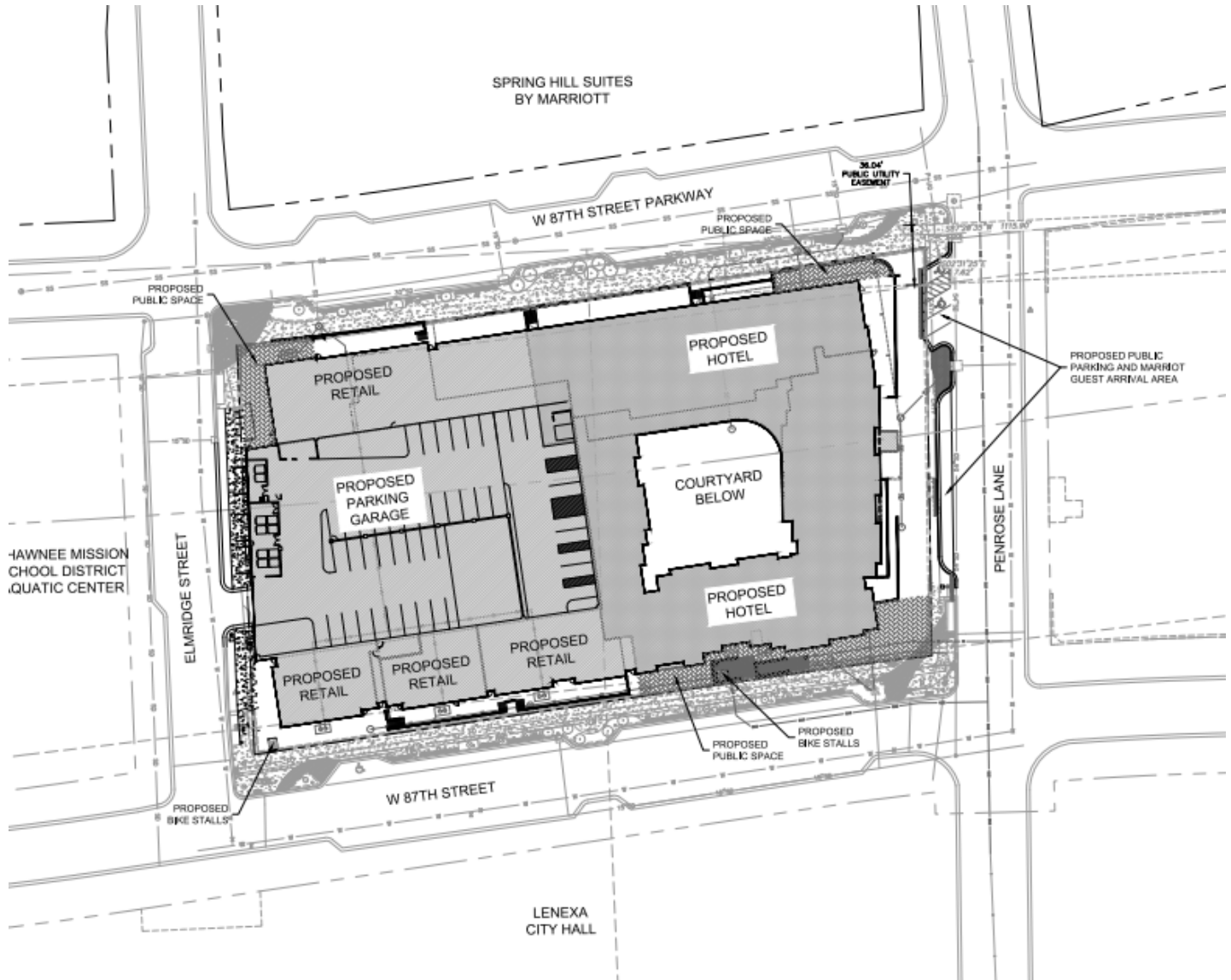
**TABLE 1: COMPARISON OF SURROUNDING PROPERTIES**

Vicinity	Land Use Designation	Zoning	Current Use
<b>Subject Property</b>	City Center	CC, Planned City Center	Undeveloped
<b>North</b>	City Center	CC, Planned City Center	Hotel and Retail
<b>South</b>	City Center	CC, Planned City Center	City Hall and Retail
<b>East</b>	City Center	CC, Planned City Center	Undeveloped
<b>West</b>	City Center	CC, Planned City Center	Aquatics Center

The future land use designation of this site is City Center, described as “unique mixed-use development in the geographic center of Lenexa. Development should be tailored to an urban environment, and thus be of a greater intensity or density, with public open spaces and pedestrian-friendly street.” The proposed use is compatible with the future land use designation. The development uses urban styles of architecture that promotes interaction with the pedestrian space. The hotel and retail uses align with the dense, mixed-use goals of City Center, and the proposed parking garage is intended to provide additional public parking for the District as well as private parking for the hotel.

## PRELIMINARY PLAN REVIEW

This preliminary plan is for a mixed-use development in the City Center District. The preliminary plan is also intended to serve as the preliminary plat. The proposal includes two hotels, AC Hotel and Residence Inn, with retail and restaurant space at the ground level. AC Hotel occupies the north half of the block while Residence Inn fronts the south and the majority of the eastern façade. The two hotels have a shared entrance and lobby space on the east with a drop-off lane to the east of the building on Penrose Lane. Parking is available in the on-site parking garage, which is accessible from Elmridge Street on the west side of the building. Limited accessible parking is available along Penrose and standard on-street parking is available on 87<sup>th</sup> Street Parkway. Site improvements include the addition of public space and pedestrian amenities. Due to site grades, low decorative walls are used throughout the site and will create defined spaces along the pedestrian amenity zone. The availability for these defined spaces to appropriately interact with the sidewalk is key to the character of City Center. Specific details of the decorative walls and spaces created are still being refined as the project progresses from preliminary to final plan.



**Exhibit 3: Site plan.**

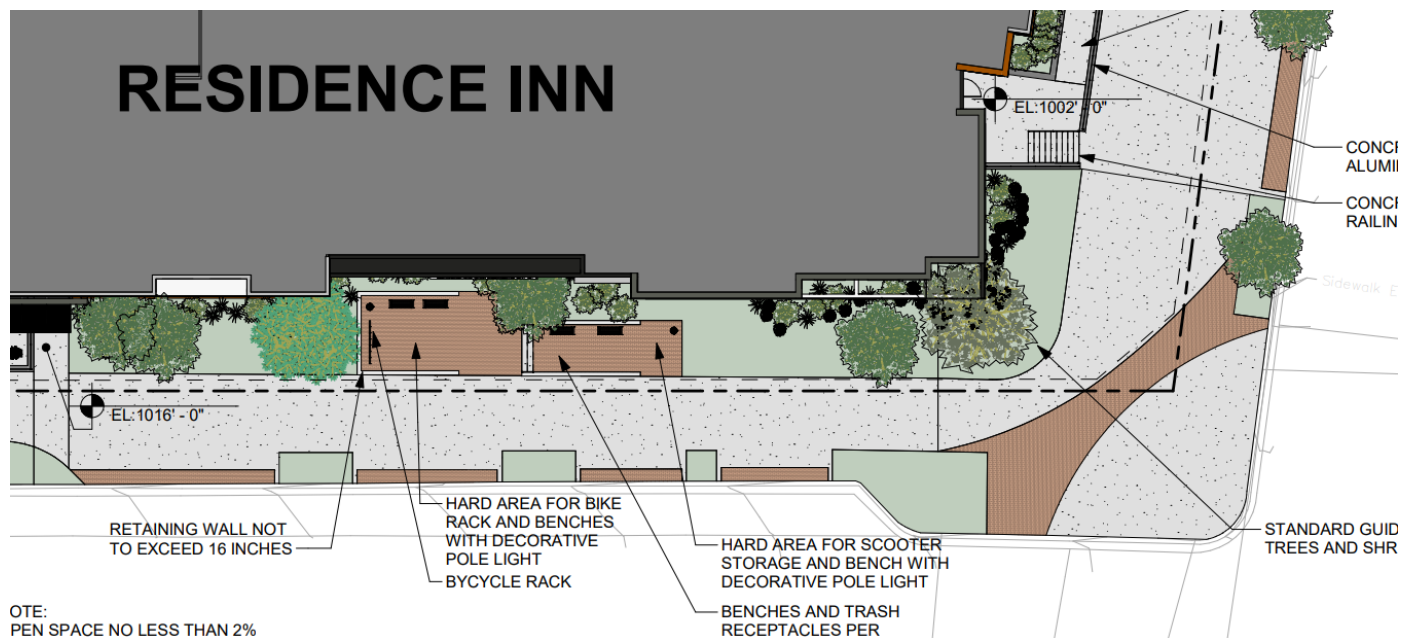
## DIMENSIONAL STANDARDS

The City Center Design Guidelines require that on mixed-use streets, buildings shall be oriented to the street with setbacks of 0'-15'. Retail tenant spaces are intended to interact directly with the public on the sidewalk, as will the restaurant and front lobby entrance of the hotel. Space is provided between the sidewalk and the storefronts, which will serve as pedestrian walking areas, planter space, outdoor patios, and amenity areas designed to promote an urban, walkable feel. Variations in the building design, such as protrusions and recessions, create a variation in setback along the facades; however, said variation will be within the acceptable range.

## PUBLIC IMPROVEMENTS

Pedestrian Amenity Zones are provided along street frontages between the sidewalk and street as required by the City Center Design Guidelines. Pedestrian amenity areas will use modular decorative pavement patterns and landscape planters consistent with other street block edges in City Center.

A separate pedestrian plaza is provided near the southeast corner of the building. Located in this area will be bike racks, benches, decorative lighting, and trash receptacles. Staff will continue to work with the applicant to further pedestrian area details during the final plan phase.



OTE:  
PEN SPACE NO LESS THAN 2%

**Exhibit 4: Location of pedestrian plaza at southeast corner of 87<sup>th</sup> Street and Penrose Lane.**

## TRAFFIC, ACCESS, AND PARKING

There is not a minimum parking requirement in the City Center District as there are in typical zoning districts. Required parking in City Center is determined on an individual basis with an emphasis on promoting structured, on-street and shared parking. The applicant is proposing a parking garage that will be accessed from Elmridge Street.

The angled on-street parking proposed for Penrose Lane must be removed, as it is not appropriate for the location shown. Penrose Lane could support two parallel accessible parking stalls if they were located as far south as possible without being contiguous with the hotel check-in pull-off and if they do not disrupt the existing storm structure. These accessible parking spaces may be revised at final plan stage. ADA accessible parking is also proposed in the parking structure.

The parking structure will be internal to the development with leasable retail spaces serving as screening for the structure on both sides of 87<sup>th</sup> Street Parkway. The exposed portion of the parking structure, along Elmridge Street, will be treated architecturally with masonry veneer and brick insets on the concrete wall. The street-facing façade of the parking garage also features windows located within a darker grey patterned concrete strip with double height soldier course above the windows.

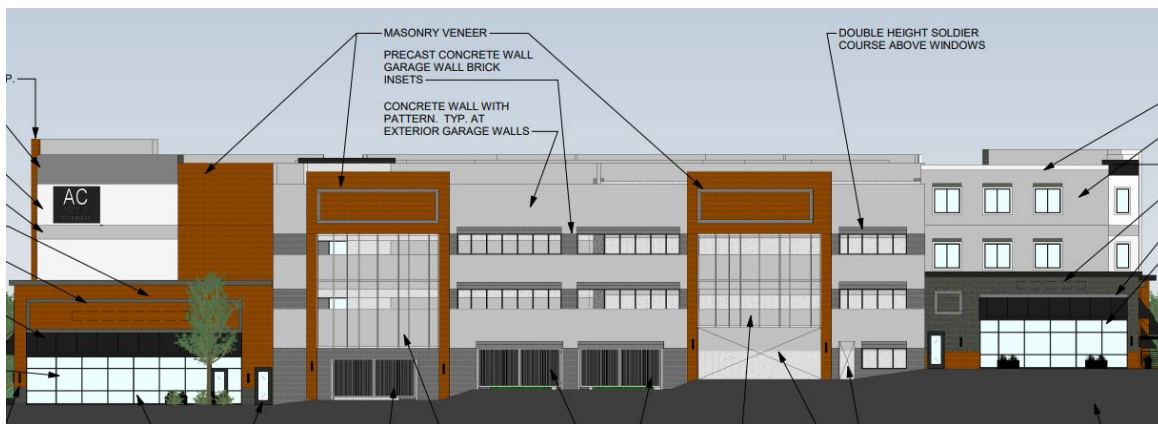
Total parking provided in the garage is proposed at 283 spaces to meet the demand as reflected in Exhibit 5.

PARKING	GARAGE	PARALLEL STREET	ANGLED STREET	15 MIN SPACES	TOTAL
5	71 (2 HC)				71
4	75 (2 HC)				75
3	75 (2 HC)				75
2	62 (8 HC)	10			73
1		12	4	6	22
<b>TOTAL PROVIDED</b>	<b>283(14 HC)</b>	<b>22</b>	<b>4</b>	<b>6</b>	<b>315</b>
	261 KEYS AT 1 SPACE PER KEY =				261
	11,079 SF RETAIL @ 1 SP. PER 250 SF=				45
<b>TOTAL REQUIRED</b>					<b>305</b>

**Exhibit 5: Parking calculation table.**

The current plan entails gating a certain number of upper-level garage parking spaces for hotel guests and keeping the other garage parking spaces available for the public. Staff approached the applicant to gauge opportunities to add a level of parking deck (approximately 75 spaces) to the project in a public/private partnership since this location is central to many high demand uses in City Center. The applicant is amenable to exploring this, but no decisions have been made at this time. Final parking details will be provided at the time of final plan.

Trash will be kept in an interior service area, accessible from Elmridge Street. The trash enclosures face the primary entrance of the Shawnee Mission School District Aquatic Center across the street. The proposed metal swing doors are not a desired manner to conceal the trash enclosures given the number of doors needed to access the trash areas. A more appropriate design for the location would be glass roll-up doors or designed to be more compatible with the building architecture and context of the aquatic center across the street.



**Exhibit 6: View of parking garage façade from Elmridge Street, showing garage entrance and proposed metal swing doors for trash enclosure.**

A single-aisle drop-off lane will be added on Penrose Lane, along with proposed head-in parking. On street parking on Penrose 87<sup>th</sup> Street east and west will remain.

Pedestrian traffic will have direct access to the hotels and storefronts from the sidewalk along east and westbound W. 87<sup>th</sup> Street and Penrose Lane. Two staircases offer pedestrian access from upper levels of the parking garage to the lower levels and sidewalk. One staircase is located at the north of the parking garage and the other at the south. Due to grade changes, stair sections are provided to connect the building entrances to the sidewalk as needed. Connections between the courtyard spaces out to the sidewalk along the street are of high importance and will continue to be refined with the final plan submittal. Eleven stalls of bicycle parking are provided in the pedestrian plaza.

### **STORMWATER**

With the addition of a hot-spot stormwater best management practice (BMP) for the parking garage and water quality BMP(s) on site for this project, stormwater management for the proposed improvements will be consistent with City Center requirements.

### **FIRE PREVENTION**

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

### **LIGHTING**

Accent lighting is proposed as part of the building architecture. Light sconces provide ground level detailing. Pedestrian-level lighting is achieved with decorative light poles. The southeast corner also features a back-lit lighted panel element. A photometric plan will be required with the final plan.

### **LANDSCAPING**

Landscaping is provided in the pedestrian amenity zones between streets and sidewalks and along the base of the building. This level of landscaping is consistent with the established landscaping in the City Center District. Street trees and seasonal plantings are installed in planter spaces within the modular paving pattern of the pedestrian amenity zone.



**Exhibit 7: Landscaping plan.**

**ARCHITECTURE**

The proposed mixed-use building will house multiple tenants and encompasses a block between Elmridge Street and Penrose Lane at 87<sup>th</sup> Street. The building is three stories on the west side and five floors on the east end of the block due to changes in grade. The ground-level floor is dedicated to retail, hotel lobby and restaurant space with the remaining floors used for the two separate hotels. The ground floor architecture is intended to appeal to the pedestrian scale and enhance the urban feel of City Center. The ground floor primarily uses a red-tone brick-colored masonry veneer material. Retail entrances provide visually distinct architectural expression using individual canopies, protrusions and recessions, masonry banding and reveals, and lighting sconces. Storefronts feature black aluminum frames with bay windows and glass doors.

The lobby entrance for both hotels is located at the center of the east elevation and features a glass entry vestibule with a raised canopy and forecourt. Individual branding for each hotel is presented at the northeast corner for AC Hotel and at the southeast corner for Residence Inn, distinguishing the two separate entities.

The brick-colored masonry veneer is continued to the top of the building at the northeast corner, creating a defined corner presence. Inset in the masonry veneer are grey colored fiber cement panels. The masonry veneer detailing appears elsewhere in the building, particularly on the east façade.

Primary façade materials for the upper floors include light gray fiber cement panels with off-white fiber cement banding. Upper floors on the north and east facades use a louver detail element while the south façade creates variation with a white trim and dark lid above the windows. Staff will continue to refine details of the façade at the final plan stage.

Due to grade, multiple low decorative walls are proposed in the pedestrian area. Retaining walls will have a masonry face and stone cap.



*Exhibit 8: View at northeast corner, AC Hotel.*



*Exhibit 9: View at southeast corner, Residence Inn.*





*Exhibit 10: View at northwest corner, Retail A.*



*Exhibit 11: View at southwest corner, Retail B.*

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## PRELIMINARY PLAT REVIEW

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The preliminary plan is intended to serve as the preliminary plat. The plat encompasses 2.32 acres in the City Center District. This plat will combine two non-platted parcels into one lot for the mixed-use development. Utilities are available to the site. An approximately 36-foot-long public utility easement is proposed along the west side of Penrose Lane. Public space is proposed at various locations throughout the plat.

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## DEVIATIONS

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The applicant is not requesting any deviations.

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## FINAL PLAN CONSIDERATIONS

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The items below are modifications to the plan set that Staff expects to see with the future submittal of the final plan. This list does not reflect all comments Staff may have at the time of final plan review.

- Pedestrian amenity zones
  - The pedestrian amenity zones along Elmridge Street and Penrose Lane should be updated with a paver and concrete pattern consistent with the existing design along W. 87<sup>th</sup> Street. Along Penrose Lane, where standard pedestrian amenity areas are not feasible due to the proposed drop-off lane, alternative amenity and design elements shall be incorporated. Examples include the use of decorative brick and planters to establish a pattern.
- Service entrances and trash enclosure gates
  - The plan set shows three separate trash enclosures proposed along Elmridge Street. Staff would like to continue working with the applicant to modify this design to reduce the visual impact along the public street. Staff recommends reviewing internal storage or circulation through the garage bay door to limit the number of gates fronting onto Elmridge Street. Additionally, it is unclear how the trash haulers would access the internal storage areas given the pedestrian amenity zone proposed between the sidewalk and street as the drives do not seem to be fully developed. Further discussion regarding layout and gate design is needed.
- Angled parking on Penrose Lane
  - As discussed in the *Traffic, Access, and Parking* section of this Staff Report, the angled on-street parking on Penrose Lane shall be removed or modified to only include necessary accessible parallel parking.
- Pedestrian lighting
  - In addition to photometric plans to be reviewed at the time of final plan, Staff also expects to see details for pedestrian-scale lighting, bollard lighting, accent lighting, or a combination thereof to aid in pedestrian wayfinding along sidewalks and amenity zones.

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## REVIEW PROCESS

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- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on March 21, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

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## RECOMMENDATION FROM PROFESSIONAL STAFF

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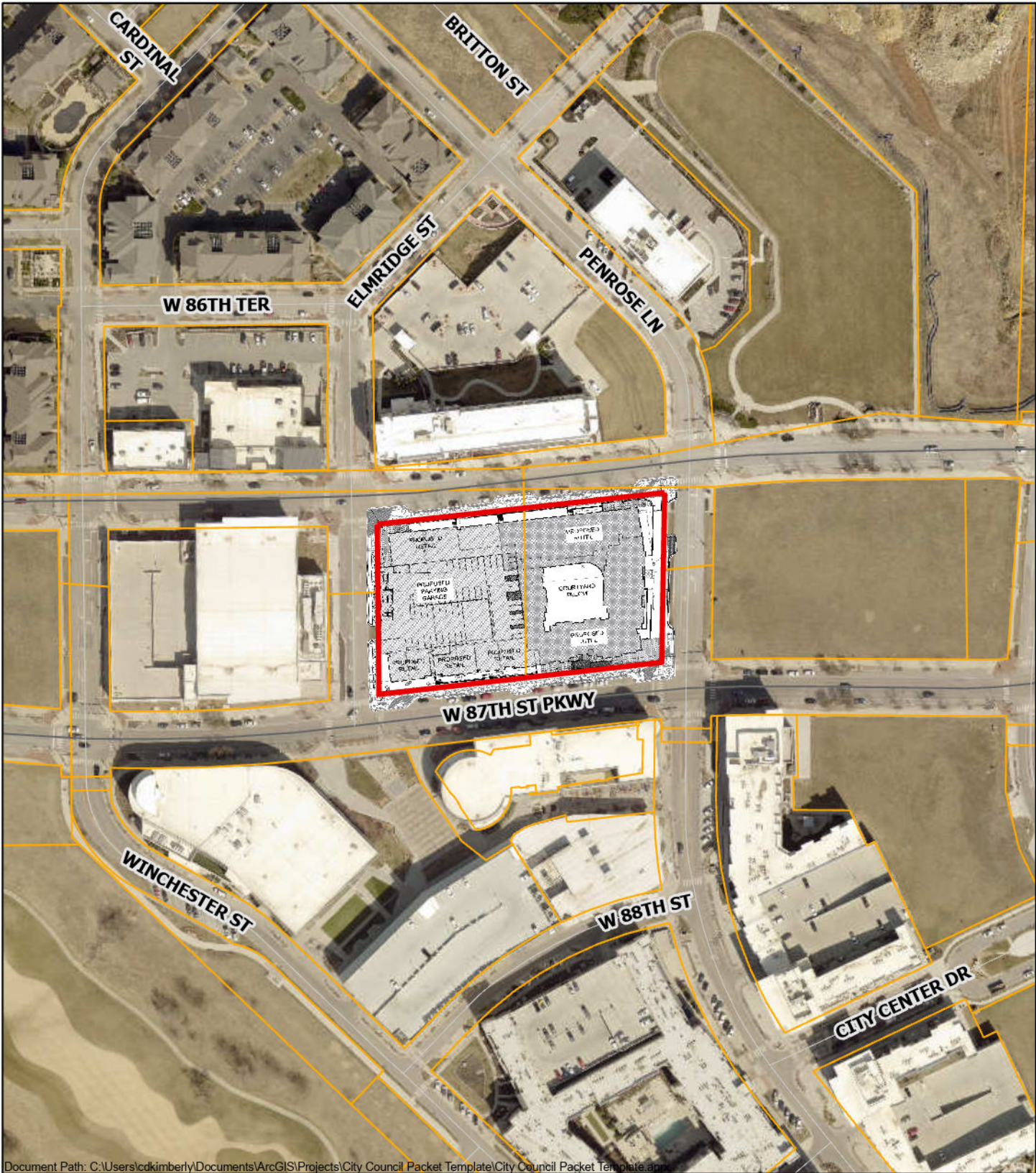
★ **Staff recommends approval of the proposed Preliminary Plan for Midas Lenexa City Center.**

- This is a proposal for a mixed-use development with hotel, retail, and public space.
- The project is consistent with Lenexa’s goals through ***Strategic Community Investment*** and ***Responsible Economic Development*** to create ***Vibrant Neighborhoods***.

### PRELIMINARY PLAN

Staff recommends **approval** of the preliminary plan for PL23-02P – **Midas Lenexa City Center**, located between Elmridge Street and Penrose Lane and the east and west lanes of W. 87<sup>th</sup> Street Parkway, for a retail and hotel mixed-use development, with the following conditions:

1. The future final plan shall address various topics outlined within the Staff Report, including pedestrian amenity zones, services entrances, trash enclosure gates, lighting, and parking.



Document Path: C:\Users\cdkimberly\Documents\ArcGIS\Projects\City Council Packet Template\City Council Packet Template.aprx

Data Source: City of Lenexa and Johnson County Kansas  
For further information, please call 913-477-7500

# Midas Lenexa City Center Preliminary Plan

## PL23-02P



DWG: F:\2023\06001-06500\02-0641\1-C-Design\AUS04A\Preliminary\Plan\Sheet\0101\02200641.dwg  
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 USER: C:\Users\jst2262341  
 PROJECT: C:\Users\jst2262341  
 C:\Users\jst2262341  
 C:\Users\jst2262341

SHAWNEE MISSION  
 SCHOOL DISTRICT  
 AQUATIC CENTER

SPRING HILL SUITES  
 BY MARRIOTT

PROPOSED  
 PUBLIC SPACE

ELMRIDGE STREET

PROPOSED  
 RETAIL

PROPOSED  
 PARKING  
 GARAGE

PROPOSED  
 RETAIL

PROPOSED  
 RETAIL

PROPOSED  
 RETAIL

PROPOSED  
 HOTEL

COURTYARD  
 BELOW

PROPOSED  
 HOTEL

PROPOSED  
 PUBLIC SPACE

PROPOSED  
 BIKE STALLS

W 87TH STREET

LENEXA CITY HALL

W 87TH STREET PARKWAY

PROPOSED  
 PUBLIC SPACE

36.04'  
 PUBLIC UTILITY  
 EASEMENT

PENROSE LANE

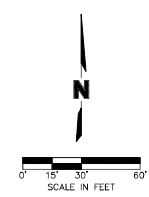
PROPOSED PUBLIC  
 PARKING AND MARRIOTT  
 GUEST ARRIVAL AREA

SITE DATA	
SITE:	2.32 ACRES
PROPOSED USE:	MIXED USE DEVELOPMENT
EXISTING ZONING:	CC
PROPOSED ZONING:	CC
PROPOSED BUILDING S.F.:	79,956 SF
UNITS PER ACRE:	111 UNITS (258/2.32)
OFFICE/RETAIL:	11,079 SF
AMENITIES:	7,211 SF
OPEN SPACE:	36%
PROPOSED REQUIRED PARKING:	261 KEYS AT 1 SPACE PER KEY = 261 11,743 SF RETAIL @ 1 SPACE PER 250 SF = 45 (306 TOTAL)
PROVIDED PARKING:	32 SURFACE STALLS 283 GARAGE PARKING (315 TOTAL)
REQUIRED BIKE STALLS:	1 SPACE PER 30 ROOMS = 9 1 SPACE PER 5000 SF RETAIL SF = 2 (11 TOTAL)
PROVIDED BIKE STALLS:	11 TOTAL BIKE STALLS

**PROPERTY DESCRIPTION:**

ALL THAT PART OF THE NORTHEAST QUARTER OF SECTION 31, TOWNSHIP 12 SOUTH, RANGE 24 EAST, IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF SAID NORTHEAST QUARTER; THENCE SOUTH 87°28'35" WEST, ALONG THE NORTH LINE OF SAID NORTHEAST QUARTER, A DISTANCE OF 1,115.80 FEET; THENCE DEPARTING SAID NORTH LINE, SOUTH 02°31'25" EAST A DISTANCE OF 7.62 FEET TO A POINT OF INTERSECTION OF THE SOUTH RIGHT-OF-WAY LINE OF 87TH STREET PARKWAY AND THE WEST RIGHT-OF-WAY LINE OF PENROSE LANE, AS THEY BOTH NOW EXIST, SAID POINT BEING THE POINT OF BEGINNING; THENCE SOUTH 00°52'05" EAST, ALONG SAID WEST RIGHT-OF-WAY LINE, A DISTANCE OF 246.27 FEET TO A POINT OF INTERSECTION WITH THE NORTH RIGHT-OF-WAY LINE OF 87TH STREET PARKWAY, AS IT NOW EXISTS; THENCE WESTERLY, ALONG SAID NORTH RIGHT-OF-WAY LINE, ALONG A NON-TANGENT CURVE TO THE LEFT HAVING A RADIUS OF 2,058 FEET, AND A CHORD WHICH BEARS SOUTH 82°38'52" WEST, A DISTANCE OF 85.19 FEET, FOR AN ARC LENGTH OF 85.19 FEET; THENCE SOUTH 81°27'38" WEST, CONTINUING ALONG SAID NORTH RIGHT-OF-WAY LINE, A DISTANCE OF 327.39 FEET TO A POINT OF INTERSECTION WITH THE EAST RIGHT-OF-WAY LINE OF ELMRIDGE STREET, AS IT NOW EXISTS; THENCE NORTH 02°28'24" WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, A DISTANCE OF 246.82 FEET TO A POINT OF INTERSECTION WITH THE SOUTH RIGHT-OF-WAY LINE OF SAID 87TH STREET PARKWAY; THENCE EASTERLY, ALONG SAID SOUTH RIGHT-OF-WAY LINE, ALONG A NON-TANGENT CURVE TO THE LEFT HAVING A RADIUS OF 2,058 FEET, AND A CHORD WHICH BEARS NORTH 83°02'28" EAST, A DISTANCE OF 113.38 FEET, FOR AN ARC LENGTH OF 113.40 FEET; THENCE NORTH 81°27'38" EAST, CONTINUING ALONG SAID SOUTH RIGHT-OF-WAY LINE, A DISTANCE OF 305.87 FEET TO THE POINT OF BEGINNING, EXCEPT ANY PART USED OR DEDICATED FOR STREETS, ROADS AND PUBLIC RIGHTS OR WAY.



Section 31-T12S-R24E  
 VICINITY MAP  
 Scale: 1" = 2000'

**olsson**

2700 West 134th Street, Suite 200  
 Overland Park, KS 66207-3475  
 TEL 813.381.1170 www.ollson.com

REV.	DATE	REVISION DESCRIPTION

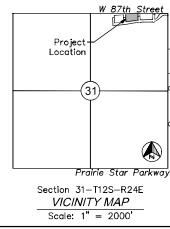
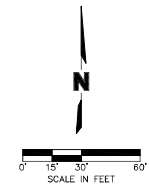
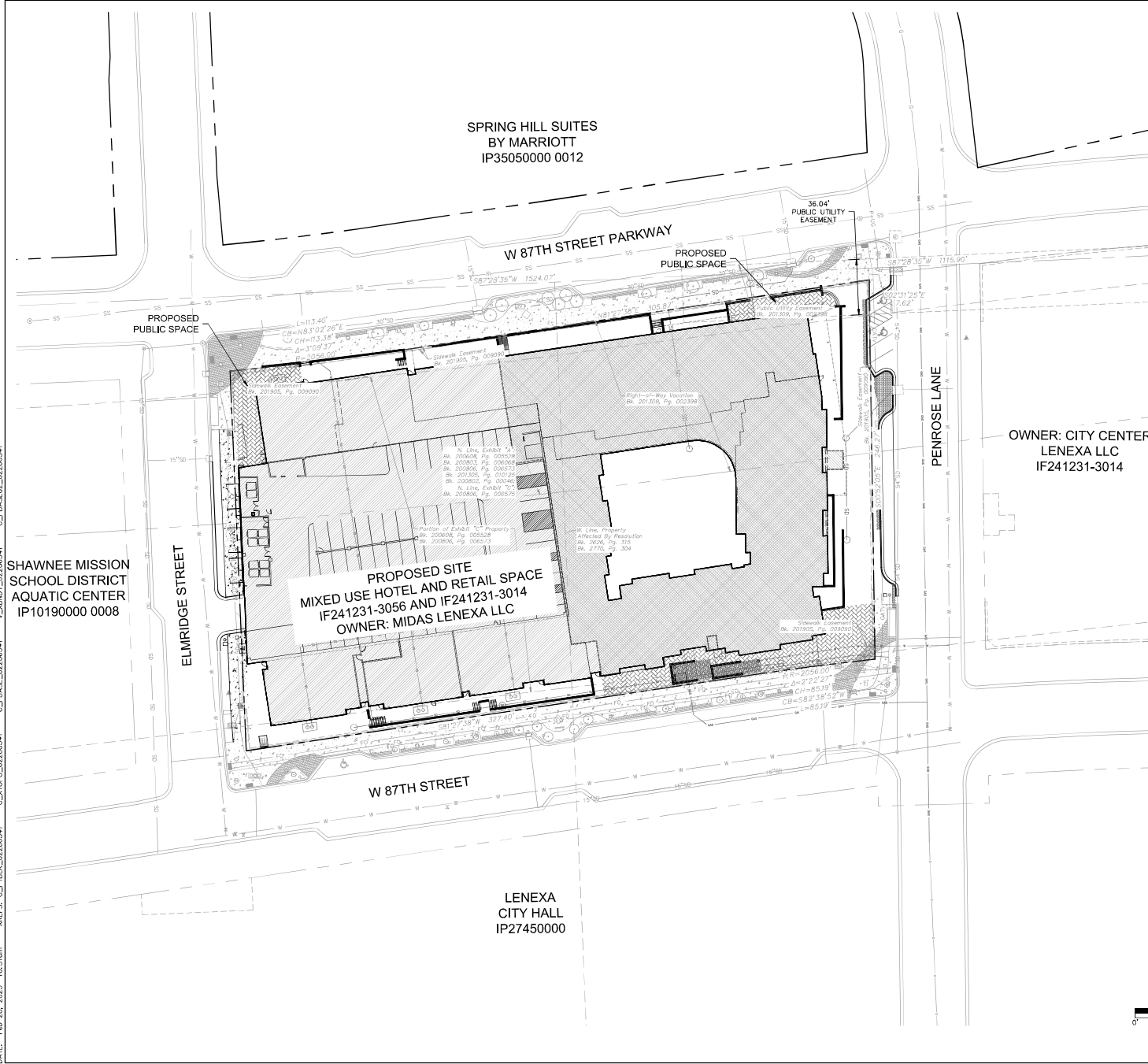
GENERAL LAYOUT PLAN  
 PRELIMINARY DEVELOPMENT PLANS  
 MIDAS LENEXA CITY CENTER  
 DUAL BRAND HOTEL & RETAIL SPACE  
 LENEXA, KS

created by: HMO  
 checked by: JL  
 approved by:  
 QA/QC by: CJB/ARJ/AT  
 project no.:  
 drawing no.:  
 date: 2023.01.30

SHEET  
 C1.0

REVISIONS

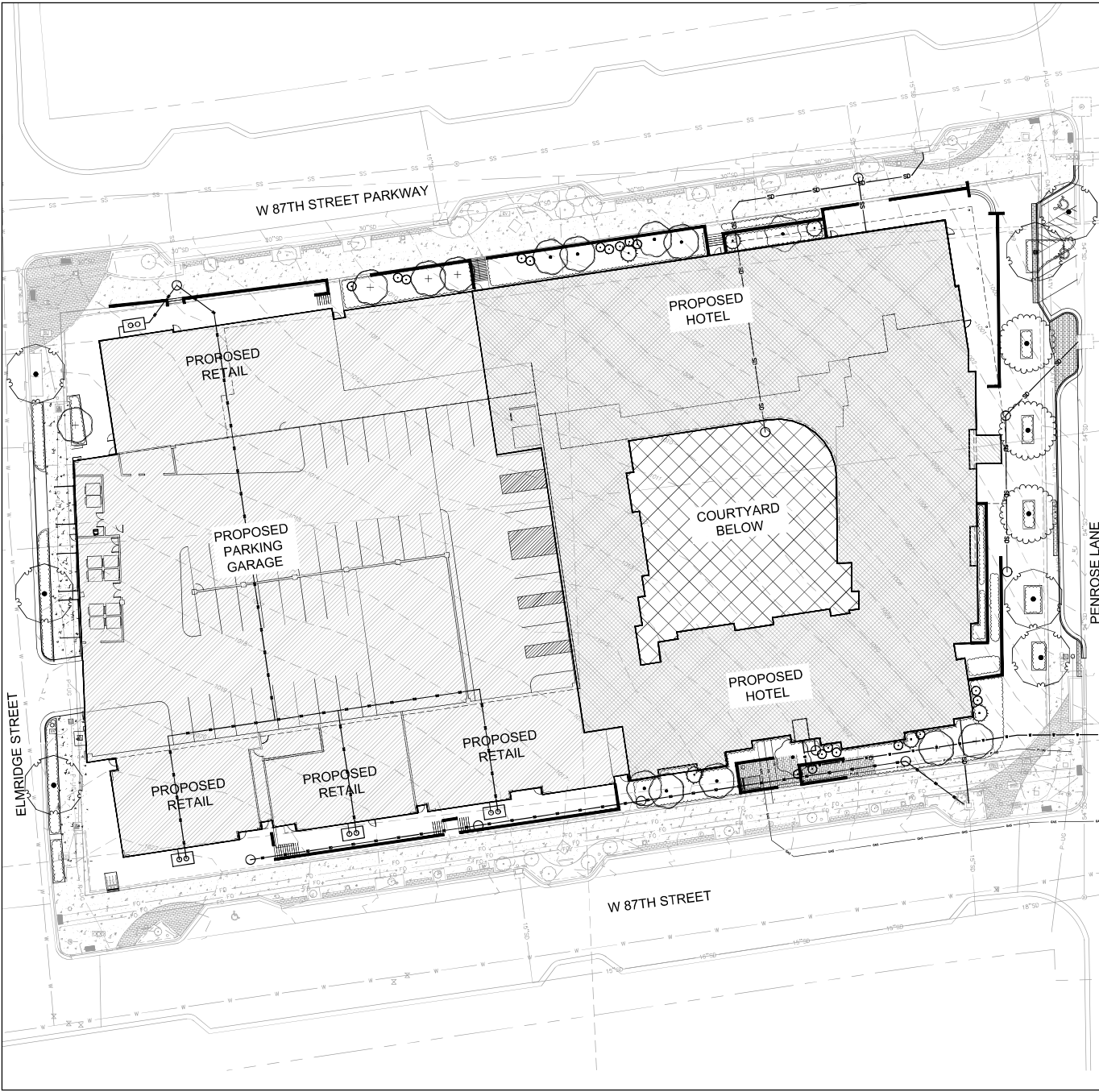
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created by: HMO  
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SHEET  
C1.1

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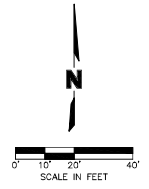
**PERIMETER PLANTING REQUIREMENT— ALONG STREET FRONTAGES**

REQUIRED	PROVIDED
<b>Elmridge Street (Local)</b> Building/Open Area (per 100 L.F.) 2 Shade Trees 12 Shrubs	<b>Parking Garage (246.6/ 100 L.F.)</b> 2 Shade Trees 30 Shrubs
<b>Penrose Lane (Collector)</b> Building/Open Area (per 100 L.F.) 2 Shade Trees 1 Ornamental Tree 12 Shrubs	<b>(246.25/ 100 L.F.)</b> 2 Shade Trees 1 Ornamental Tree 30 Shrubs
<b>W 87th Street Parkway (East Bound): (Parkway)</b> Building/Open Area (per 100 L.F.) 2 Shade Trees 3 Ornamental Tree 25 Shrubs 200 S.F. of Planting Beds	<b>(419/ 100 L.F.)</b> 8 Shade Trees (Existing) 13 Ornamental Trees (6 Existing) 105 Shrubs 838 S.F. of Planting Beds
<b>W 87th Street (West Bound): (Parkway)</b> Building/Open Area (per 100 L.F.) 2 Shade Trees 3 Ornamental Tree 25 Shrubs 200 S.F. of Planting Beds	<b>(412.56/ 100 L.F.)</b> 8 Shade Trees (Existing) 13 Ornamental Trees (5 Existing) 105 Shrubs 825 S.F. of Planting Beds

**PLANT SCHEDULE**

DECIDUOUS TREES	BOTANICAL / COMMON NAME	SIZE	CALIPER	QTY
+	ACER GRISELM PAPERBARK MAPLE	B & B	2" CAL.	6
•	GLEDITSIA TRIACANTHOS INERMIS 'SKYLINE' SKYLINE LOCUST	B & B	2" CAL.	7
•	TAXODIUM DISTICHUM 'SHAWNEE BRAVE'™ BALD CYPRESS	B & B	2" CAL.	3
EVERGREEN TREES	BOTANICAL / COMMON NAME	SIZE	CALIPER	QTY
•	JUNIPERUS CHINENSIS 'SPARTAN' SPARTAN JUNIPER	B&B, 6' HT.		19
•	JUNIPERUS VIRGINIANA 'CANERTI' CANERTI JUNIPER	B&B, 6' HT.		6
ORNAMENTAL TREES	BOTANICAL / COMMON NAME	SIZE	CALIPER	QTY
•	ACER TATARICUM 'HOT WINGS' HOT WINGS TATARIAN MAPLE	B&B, 6' HT.		7
•	MAGNOLIA GRANIFLORA 'BRACKEN'S BROWN BEAUTY' BRACKEN'S SOUTHERN MAGNOLIA	B&B 5-6' HT.		2
GROUNDCOVER	TYPICAL GROUNDCOVERS TO BE USED			
	BUFFALO JUNIPER, LITTLE BLUESTEM PENNSYLVANIA SEEDGE SWITCH GRASS, JACKSONSHE WARRIOR, RUSSIAN SAGE, ALLIUM, PENSTEMON			

NOTE:  
 INTERIOR COURTYARD AMENITIES, LANDSCAPE AND  
 HARDSCAPE TO BE DESIGNED IN ACCORDANCE WITH  
 HOTEL BRAND STANDARDS.  
 ALL EXTERIOR LANDSCAPE AND HARDSCAPE TO MEET  
 CITY CENTER DESIGN STANDARDS. SEE ACCOMPANYING  
 RENDERINGS AND ELEVATIONS FOR ADDITIONAL  
 INFORMATION.



2700 West 134th Street, Suite 200  
 Overland Park, KS 66204-3475  
 TEL 813.381.1170 www.ollson.com

REV.	DATE	DESCRIPTION

LANDSCAPE PLAN  
 PRELIMINARY DEVELOPMENT PLANS  
 MIDAS LENEXA CITY CENTER  
 DUAL BRAND HOTEL & RETAIL SPACE

2023

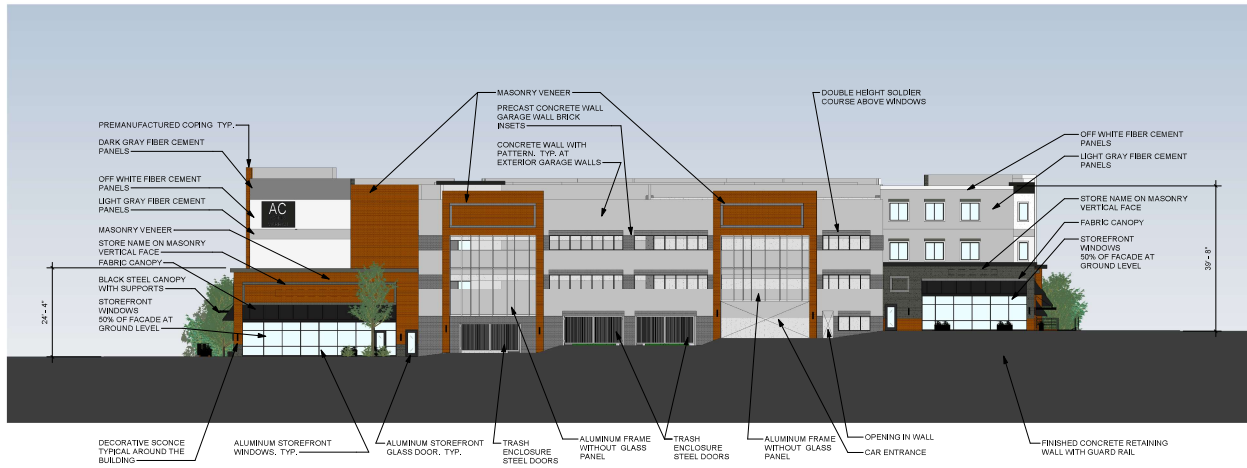
LENEXA, KS

created by:	HMO
checked by:	JR
approved by:	
QA/QC by:	02/24/2023
project no.:	
drawing no.:	2023.01.03
date:	

**SHEET L1.0**



1 NORTH ELEVATION  
PS 1/16" = 1'-0"



2 WEST ELEVATION  
PS 1/16" = 1'-0"

# Residence Inn AC Dual Brand Hotel at City Center

Lenexa, Kansas 66219

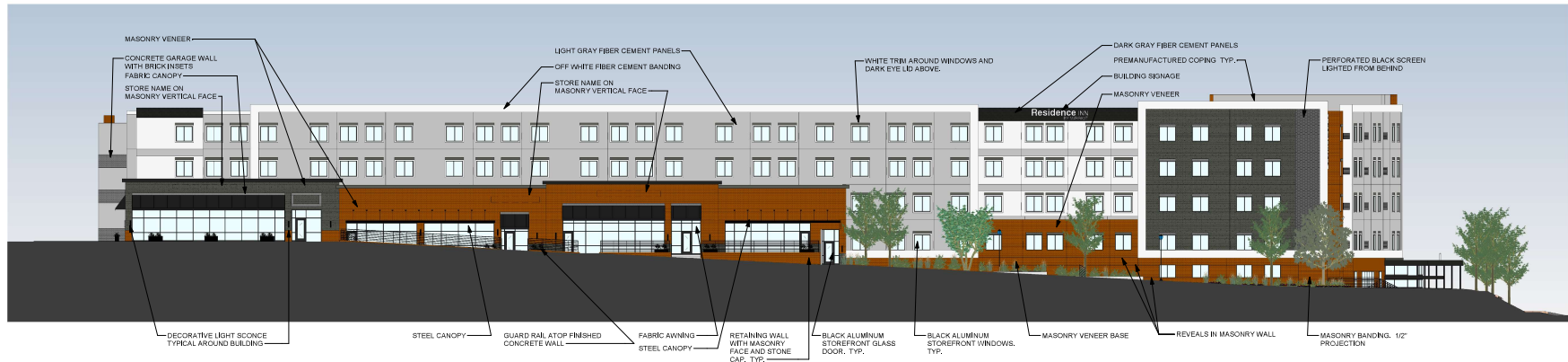
BUILDING ELEVATIONS  
1/16"=1'-0"

22446.00

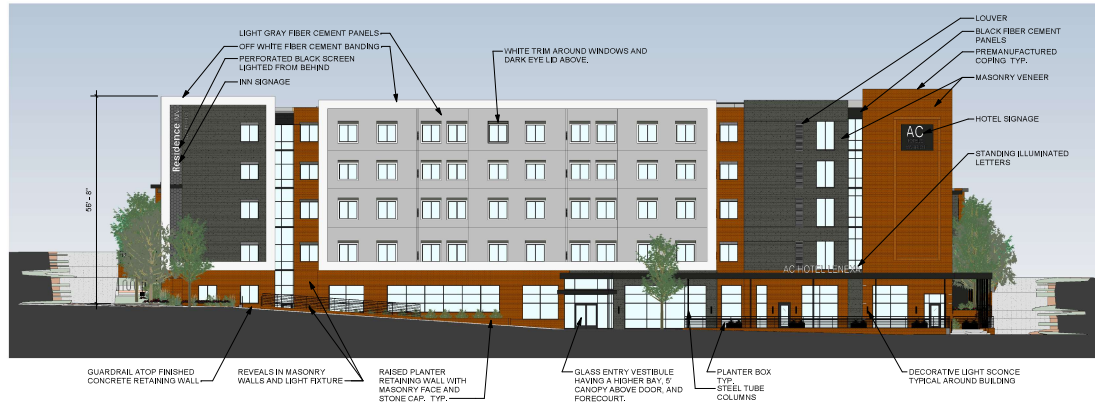
2/20/2023







1 SOUTH ELEVATION  
 P10 1/16" = 1'-0"



2 EAST ELEVATION  
 P10 1/16" = 1'-0"

## Residence Inn AC Dual Brand Hotel at City Center

Lenexa, Kansas 66219

BUILDING ELEVATIONS  
 1/16"=1'-0"

22446.00

2/20/2023



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## CALL TO ORDER

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Chairman Poss called the regular meeting of the Lenexa Planning Commission to order at 7:00 p.m. on Monday, January 30, 2023. The meeting was held in the Community Forum at Lenexa City Hall at 17101 W. 87th Street Parkway, Lenexa, Kansas.

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## ROLL CALL

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### COMMISSIONERS PRESENT

Commissioner Ben Harber  
Commissioner Don Horine  
Commissioner Curt Katterhenry  
Commissioner Jason Leib  
Commissioner Brenda Macke  
Vice-Chairman Mike Burson  
Chairman Chris Poss

### COMMISSIONERS ABSENT

Commissioner David Woolf  
Commissioner John Handley

### STAFF PRESENT

Scott McCullough, Director of Community Development  
Stephanie Kisler, Planning and Development Administrator  
Tim Collins, Engineering Construction Services Administrator  
Andrew Diekemper, Fire Division Chief  
Stephen Shrout, Assistant City Attorney  
Dave Dalecky, Planner II  
Christa McGaha, Planner II  
Kim Portillo, Planner II  
Gloria Lambert, Senior Administrative Assistant

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## APPROVAL OF MINUTES

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The minutes of the January 9, 2023 meeting were presented for approval. Hearing no changes or corrections to the minutes of the regular meeting, Chairman Poss entertained a motion to **APPROVE** the minutes as written. Moved by Commissioner Horine, seconded by Commissioner Leib, and **APPROVED** by a majority voice vote.

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## **CONSENT AGENDA**

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**1. Arbor Lake, 8th Plat - Consideration of a final plat for property located at the northeast corner of the 91st Terrace cul-de-sac, west of Clare Road, within the RP-1, Planned Residential (Low-Density) District. PT23-05F**

Consideration of a final plat to replat an existing tract by changing the dedications of the tract, updating easements, and including a portion of vacated 91<sup>st</sup> Street in the tract.

**2. Brampton West Tracts - Consideration of a final plat for property located at the intersection of West 95<sup>th</sup> Street & Aurora Street within the R-1, Single-Family Residential District. PT23-06F**

Consideration of a final plat for approximately 2.9 acres of land located between the Brampton West subdivision and right-of-way for 95th Street for the purpose of providing easements for utilities and landscaping.

Stephanie Kisler noted that the applicant for Consent Agenda Item Two, Brampton West Tracts Final Plat, requested that the item be continued to the March 6, 2023 Planning Commission meeting.

Chairman Poss entertained a motion to **APPROVE** Consent Agenda Item One. Moved by Commissioner Burson, seconded by Commissioner Macke, and carried by a unanimous voice vote.

DRAFT

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## REGULAR AGENDA

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### 3. TierPoint Sign Deviations - Consideration of sign deviations for property located at 14500 West 105<sup>th</sup> Street within the BP-2, Planned Manufacturing District. DV23-01

Consideration of sign deviations to allow two signs on facades that are not architecturally finished to the degree of the building front.

#### APPLICANT PRESENTATION:

Dustin Fankhauser of Excel Lighting and Sign presented information about the proposed signage. He noted that the request is for a sign on the north facade and the south facade.

#### STAFF PRESENTATION:

Kim Portillo presented the Staff Report. She noted that the applicant requests a deviation to allow signs on two facades that are not architecturally finished like the primary facade. She provided an overview of the sign locations on the site. She noted the criteria for evaluating sign deviation requests. She said she would discuss Sign 2 first because that request was simpler than the request for Sign 1. The front of the building (south) is brick and other facades are finished with concrete. The primary facade is brick, so the sign code requires that the signs be located on brick materials.

Staff supports the deviation for Sign 2 to be located on the north facade, which is finished with concrete.

Sign 1 is located on the south side and is proposed to be located on the concrete portion of the building that is set back behind from the main brick portion of the building. The sign is centered above the main canopy on the brick portion of the building. The brick portion of the building has multiple rooftop mechanical units that are not screened from view. She highlighted the view of the mechanical equipment. Staff would like to see the rooftop mechanical units screened. She compared a similar screening installed at 14305 W. 105<sup>th</sup> Street. She presented three options for consideration:

1. Provide a parapet wall and locate the sign on the parapet, centered above the main canopy.
2. Provide individual screening structures for the rooftop mechanical units and locate the sign in the proposed position on the concrete portion of the facade.
3. Amend the applicable sign criteria to allow the sign to be installed off-center on the brick portion of the facade.

She noted that Staff's preferred option is #1.

#### PLANNING COMMISSION DISCUSSION:

Chairman Poss asked if the proposed and option #2 were both proposed to be mounted on the concrete portion of the wall. Kim Portillo confirmed that yes, these are both on the concrete wall.

Commissioner Horine asked what the owner is willing to do. Dustin Fankhauser said the owner is obtaining bids for the screening. The owner directed him to work toward obtaining the deviations, although they may pursue the north sign first and work on the south sign later. They will decide when they find out how much the screening will cost. He went further to say, the reason they want the sign on the rear concrete portion of the building is because that is where their customer entrance exists and they want to guide customers to that rear part of the building.

Commissioner Katterhenry prefers option #1. He is supportive of the north sign. There were some questions about centering the sign.

Commissioner Burson said he supports the north sign deviation. He does not support option #2 because it does not esthetically make sense but does support option #1 and #3. Dustin noted that his client would not pursue option #2 because of issues with visibility.

Chairman Poss asked Dustin about which entrance people are supposed to be using. He asked Kim to display the photo of the parking lot view. Dustin said the main door is located where the brick meets the concrete on the west side of the building. There was discussion about placing the sign on the concrete portion but “left justified” to be located closer to the door. Chairman Poss summarized the options and said option #1 would clean up the front of the building and look esthetically pleasing, option #2 does not make a lot of sense to place it centered between the units and option #3 he is not opposed to. He is not sure if installation of a new sign triggered the requirement for a full parapet of screening.

Commissioner Leib asked if placing a sign on the concrete on the left side would require Planning Commission approval and revising the sign criteria. Kim Portillo confirmed. Scott McCullough confirmed. Commissioner Leib asked about how to most easily approve a south sign. Scott McCullough said revising the conditions to require the screen wall or brick area and not back on the concrete may be the option. Commissioner Leib asked if they can consider an option #4. Scott McCullough said that would need to be decided separately.

Commissioner Horine said to be careful that the Commissioner is not redesigning the sign. He said the Commission can consider whether or not the sign can be placed on the concrete and if the Commission agrees they can put it anywhere on the concrete. If we say no, they can't put it anywhere on the concrete.

Chairman Poss said there should be a motion for the north sign and a motion for the south sign.

Commissioner Horine asked why they could be allowed to install the sign on the parapet when it is not brick. Scott McCullough said that this is complicated. Allowing the sign on the parapet is a compromise because we do not allow signs above rooflines. It would look more like the facade than a metal screening wall.

Commissioner Burson said it seems like the applicant doesn't know what they want to do. He supports the north sign. He is not supportive of a sign on the south until the applicant knows what they want. He requested two motions.

Chairman Poss asked if it makes sense to table the south side and make a motion. Scott McCullough said that the alternatives provided were within the sign criteria and the sign code. He said the south sign could be tabled and the applicant could come back later with a decision.

Commissioner Burson asked if they denied the sign if they could come back and essentially ask for the same thing again later. Scott McCullough said that he wasn't sure if the code precluded them from coming back, but given the context, it is a possibility. Dustin Fankhauser said his clients would likely pursue options #1 or #3.

Commissioner Leib asked whether we could approve option #1. Commissioner Horine said he believes if the Commission approves alternate #1, that is the only one that's approved and if their cost comes back exorbitant, they won't get a sign at all. Commissioner Leib said how could they be flexible?

Dustin said that it was his understanding that the sign deviation would not expire, and they could come back for a different request in the future if needed.

**MOTION:**

Chairman Poss entertained a motion to recommend **APPROVAL** for sign deviations for the north sign (Sign 2) for **TierPoint** located at 14500 W. 105<sup>th</sup> Street within the BP-2 Zoning District with the following conditions:

Moved by Commissioner Leib, seconded by Commissioner Horine, and carried by a unanimous voice vote.

Chairman Poss entertained a motion from Commissioner Leib to recommend **APPROVAL** for sign deviations for the south sign (Sign 1) in the form of alternate #1 for **TierPoint** located at 14500 W. 105<sup>th</sup> Street within the BP-2 Zoning District.

Moved by Commissioner Leib, seconded by Commissioner Horine, and carried by a unanimous voice vote.

DRAFT

#### **4. Presentation from Staff: Building Architecture and Materials**

##### **STAFF PRESENTATION:**

Dave Dalecky presented information about Building Architecture and Materials.

#### **STAFF REPORT**

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Stephanie Kisler noted that registration is open for the American Planning Association's National Conference. This year the conference is in Philadelphia. The conference begins with the opening keynote the morning of Saturday, April 1<sup>st</sup> and concludes Tuesday, April 4<sup>th</sup>. The City is able to send up to two Planning Commissioners to the conference. There is also a virtual-only option, which is live from April 26<sup>th</sup> to 28<sup>th</sup>. She asked the Commissioners to consider their interest in attending and watch their email inboxes for more details from her within the next couple of days.

#### **ADJOURNMENT**

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Chairman Poss ended the regular meeting of the Lenexa Planning Commission at 8:12 p.m. on Monday, January 30, 2023.